

**CITY OF BLOOMINGTON**



**SEPTEMBER 10, 2012 @ 5:30 p.m.  
COUNCIL CHAMBERS #115  
CITY HALL**

**CITY OF BLOOMINGTON  
PLAN COMMISSION AGENDA  
Sept. 10, 2012 @ 5:30 p.m.**

**❖ City Hall Council Chambers, #115**

**ROLL CALL**

**MINUTES TO BE APPROVED: July 9, 2012**

**REPORTS, RESOLUTIONS AND COMMUNICATIONS:**

**PETITION CONTINUED TO NEXT MEETING:**

**PUD-37-12 Chick-Fil-A**  
**3020 E. 3<sup>rd</sup> St.**  
PUD final plan approval (*Case Manager: Katie Bannon*)

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**PETITIONS:**

**SP-31-12 Bloomington Public Transportation Corporation**  
**301 S. Walnut St.**  
Site plan review for an approximately 10,000 square foot building containing a downtown transit facility and dispatch area (*Case Manager: Jim Roach*)

**SP-36-12 REI Investments (Hyatt Place)**  
**217 W. Kirkwood Ave.**  
Site plan approval for a 168-room hotel. (*Case Manager: Patrick Shay*)

**ZO-38-12 City of Bloomington**  
**613 E. 12<sup>th</sup>, 403 E. 6<sup>th</sup>, 718 E. 8<sup>th</sup>, 702 E. 10<sup>th</sup>, 525 N. Park, 514 N. Fess**  
Request from City to rezone multiple properties within the Old Northeast Neighborhood to conform to zoning which occurred prior to 2007, the last comprehensive update of the City's zoning map (*Case Manager: Tom Micuda*)

End of Agenda

*\*\*Next Plan Commission hearing scheduled for October 8, 2012*

**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT  
Location: 301 S. Walnut Street**

**CASE #: SP-31-12  
DATE: September 10, 2012**

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**PETITIONER:** Bloomington Public Transportation Corp.  
130 W. Grimes Lane, Bloomington

**CONSULTANT:** Parsons Brinkerhoff  
300 N. Meridian Street, Indianapolis

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**REQUEST:** The petitioner is requesting site plan approval in order to construct a Transportation Terminal and a Police Station (Central Emergency Dispatch Center).

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<b>Area:</b>	1.28 Acres
<b>Zoning:</b>	Commercial Downtown/Downtown Core Overlay (CD/DCO)
<b>GPP Designation:</b>	Downtown
<b>Existing Land Use:</b>	Vacant
<b>Proposed Land Use:</b>	Transportation terminal and police station
<b>Surrounding Uses:</b>	North, west, south – commercial East – commercial, park, police station Northeast – Office, vacant Fleener building Southeast – Middle Way House

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**REPORT SUMMARY:** The property is located at the southeast corner of E. 3<sup>rd</sup> street and S. Walnut Street and is currently vacant and used as construction staging for the S. Walnut Street Streetscaping project and the Spanker’s Branch stormwater reconstruction project. The property is made up of four lots that are divided by a north-south alley. The property is bound by 3<sup>rd</sup> St. to the north, Walnut St. to the west, S. Washington Street to the east and an east-west alley to the south. The most recent uses on the property included a restaurant, a multi-tenant office building and a warehouse. Past uses include a gasoline service station and an auto repair business. This property and all surrounding properties are zoned Commercial Downtown (CD) and this property is within the Downtown Core Overlay (DCO).

The petitioner proposes to construct a 2-story building and bus boarding areas on the property. The proposed building would include waiting, ticketing and service areas for Bloomington Transit on the first floor and the Central Emergency Dispatch Center on the second floor. This facility would replace the existing Bloomington Transit Downtown Transfer Center at the northwest corner of S. Washington St. and E. 4<sup>th</sup> Street, one block to the north. This center has been in place since 1987 and no longer serves the needs of Bloomington Transit. The new Dispatch Center would replace the existing 500 square foot facility currently located within the Bloomington Police Headquarters, one block to the east.

The building is a modern/contemporary design utilizing a curved main corner element with recessed entrance and secondary entrances along the transit plaza to the south. The building is clad in limestone, with some areas of spandrel glass. The building mostly has an

arched metal roof, however some parts of the roof are flat. The transportation terminal itself will allow for more and longer buses and is anticipated to be able to serve the needs of Bloomington Transit for at least the next 20 years. The terminal includes three lanes for bus loading and unloading that will be accessed from both Walnut St. and Washington St. It also includes long term bike lockers and covered passenger waiting areas. The petitioner has designed the building to meet a LEED Silver rating.

**Plan Commission Site Plan Review:** One aspects of this project require that the petition be reviewed by the Plan Commission, per BMC 20.03.090. These aspects are as follows:

- The proposal includes waivers to the standards in 20.03.130

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### **Petition Overview:**

**Bus Movements:** The petitioner’s consultants have designed the layout of the terminal to maximize the efficiency of the site and to allow expansion of public transportation service in the future. The site includes three lanes for buses and passengers. Two of these lanes will have buses pulling into the site from Washington St., while the third brings buses into the site from Walnut St. The waiting areas have been designed to provide a comfortable area for passengers with benches, planters and canopies for protection from the rain. Two lanes will then leave the site onto Walnut Street and one will exit onto Washington St. To accommodate the needs of the buses turning onto Walnut St., the petitioner has worked with City Engineering and Public Works to facilitate a new traffic signal at Walnut St. and Smith Ave. This signal will be linked with the 3<sup>rd</sup> St. and Walnut St. signal to allow a gap in the traffic on Walnut St. to give the buses an opportunity to safely exit the site. Finally, a short term emergency service parking area is proposed on the south side of 3<sup>rd</sup> St. that could be converted into an additional bus loading area in the future.

**Alley Encroachment:** The petitioner will be bringing forward an alley encroachment request to the Board of Public Works following the Plan Commission meeting. The encroachment is necessary to allow for the north-south alley that bisects the property to be partially blocked. The petitioner intends to block the southern end of the alley, immediately north of the east-west alley located along on the south side of the property. They also plan to block the northern end of the alley, immediately south of the Fleener building. Within this closed alley to regular through traffic, the petitioner plans to construct a raised speed table to allow passengers to walk from the terminal to the large waiting island in the middle of the site. It will also allow for construction of three canopies over the alley right-of-way that will shelter passengers from the weather as they are walking and waiting for the buses.

The petitioner has been working with neighboring property owners to ensure occasional access to the alley. The Middle Way House has the need for delivery trucks to access the rear of their property. Two features on the site plan are proposed to accommodate this. The first is the widening of the east-west alley between the sites. This will allow for deliveries to be made in this alley and still allow other vehicles to use the alley. The second feature is a gate at the southern end of the north-south alley that will allow larger trucks traveling northbound to be able to traverse the petitioner’s site and exit onto Walnut St. Finally, the

petitioner has been working with the owners of the Fleener building to ensure access to the rear of the building, which will be used as a storage area.

**Building Entrances:** The building contains one prominent pedestrian entrance at the northwest corner of the property, facing the intersection of 3<sup>rd</sup> St. and Walnut St. This entrance will serve both the employees and riders of Bloomington Transit as well as the employees of the Dispatch Center. The entrance is recessed from the face of the building, contains a prominent building name and address and is part of a prominent corner element with a raised parapet, as required by the UDO. A pedestrian entrance is also provided on the south side of the building from the transit plaza.

**Parking:** No parking is proposed or required with this petition. Overnight bus parking will take place at the W. Grimes Lane Bloomington Transit location. Parking for the Central Emergency Dispatch Center will take place at the main Bloomington Police Department one block to the east. A short term pull-off area for emergency service parking is proposed along the south side of 3<sup>rd</sup> St.

**Bicycle Parking:** The site plan shows 18 uncovered spaces and 8 long-term, class-1 bike lockers. This exceeds minimum UDO requirements for bike parking. Two areas are shown for possible additional bike parking at the northeast corner of the site and one area is shown along Walnut St. The UDO requires some bicycle parking spaces within 50 feet of the main entrance. Staff recommends that the additional bicycle racks along Walnut St. be included.

**Materials:** The majority of the building is clad in limestone. Parts of the second floor frontage along 3<sup>rd</sup> St. include spandrel glass. This is needed to provide a dark room for the dispatch officers and their numerous computer terminals. During the design process large, transparent display windows was added to the ground floor façade. Additions, a previously proposed arcade has been removed and the building has been setback further from the street and incorporating attached metal canopies along 3<sup>rd</sup> St. The proposed materials meet the standards of the DCO.

**Height:** The building is two-stories tall with a curved metal roof and some area of flat roof with parapet. At its tallest, the building is 43 feet tall. The maximum height in the DCO is 50 feet and the minimum is 35 feet.

**Requirements adjacent to historic structure:** The proposed building is adjacent to the Fleener Building to the east, which is listed in the 2001 Survey of Historic Sites and Structures and is a locally designated to a historic structure. The DCO requires that portions of new buildings immediately adjacent historic structure not be more than one-story or 14-feet taller than the historic structure. Immediately adjacent to the Fleener Building, the proposed building is 7 feet taller.

The DCO requires that new buildings also maintain the same alignment with the street as adjacent historic structures. The proposed building's support columns align with the Fleener

Building. The main mass of the building is set back approximately three feet. Staff believes this meets the intent of the DCO standard.

Finally, the DCO requires that new buildings align their horizontal elements visually with similar elements in adjacent historic structures. The petitioner has accomplished this by aligning the canopies along 3<sup>rd</sup> St. with the 2<sup>nd</sup> floor window sills of the Fleener Building. They have also aligned the tops of the proposed second floor windows with the top of the Fleener Building parapet. Greater floor to ceiling heights in the proposed building make aligning window sills in the two buildings difficult.

**Streetscape:** Street trees along Walnut Street will be within tree grates. Along 3<sup>rd</sup> St. street trees are not feasible because of inadequate space in the right-of-way and UDO requirements to align the building with the Fleener building and build close to the right-of-way line.

Street lights are required along Walnut St. that will match the new lights being installed by the City as part of the South Walnut Streetscaping project. This is not yet reflected on all of the site plans. Due to inadequate space within the sidewalk along 3<sup>rd</sup> St., the street lighting will be attached to the side of the building.

**Utilities:** Water and sanitary sewer service is available along 3<sup>rd</sup> St. Stormwater detention is not required, but stormwater will be captured, filtered through a mechanical separator and directed to the nearby public storm sewers. The plaza also utilizes pervious pavers and under drains under a landscaped island. Stormwater and utility plans have been submitted to the City Utilities Department and are under review.

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#### **SITE PLAN REVIEW:**

**Window Design:** The DCO requires upper story windows that are at a minimum 1:1.5 ratio of width to height. Several window openings on second floor street façade do not meet the standard because the wall openings are wider than they are tall.

**Window Design Waiver-20.03.130(b)(3)(C):** A waiver from the standards of the UDO is required to allow windows that are less than 1:1.5 ratio. The Downtown Plan's recommendations for upper story windows (guidelines 3.13-3.14) call for windows with a vertical emphasis and trim that aligns with adjacent traditional buildings. While the overall window openings do not meet the required ratio, the windows allow for increased sunlight into the building and are keeping with the more contemporary aesthetic of the building. Although the windows could be modified to meet the standards, staff finds it appropriate to allow some flexibility in design to achieve more variety in building façades and allow a more modern design.

**Void-to-solid Percentage:** The DCO sets a minimum first floor void-to-solid percentage at 60%, "consisting of display windows, entries and doors." The building as proposed contains approximately 38% void along 3<sup>rd</sup> St. and 20% void along Walnut St. A waiver is required for this standard. The petition meets the minimum 20% void on the 2<sup>nd</sup> floor.

**Void-to-solid Waiver-20.03.130(b)(2)(A):** A waiver from the architectural standard of the UDO is required to allow portions of the first floor to have less than 60% void area. The need for this waiver is driven by the fact that the petitioner has placed an emphasis on a large expanse of glass on the south side of the building facing the passenger boarding area from the interior passenger waiting area. This forces mechanical, storage and bathroom areas to the north and west sides of the building. The first floor along Walnut St. is made up almost entirely of a stair tower and public bathroom space. This space will likely include public art, ticketing information, and ATM or some other feature to break up the blank wall. The far eastern end of the building includes stair tower and electronics storage space. The Downtown Plan makes no specific recommendation for a void percentage, but does encourage building with high levels of void and pedestrian interest along streets. Staff believes this goal has been met and recommend approval of this waiver.

**Façade Modulation:** The DCO requires that buildings be broken into façade modules of no more than 65 feet. These modules must be offset, either projecting or recessing from the neighboring module, by 3% of the total building width. The proposed building does not contain any modulation along 3<sup>rd</sup> St. A waiver is required for this standard.

**Modulation Waiver-20.03.130(c)(1):** A waiver from the architectural standard of the UDO is required to allow for the 3<sup>rd</sup> St. frontage to not have any modulation. The proposed building is approximately 126 feet wide. This requires at least one modulation in the façade. The modulation requirement is intended to ensure interesting buildings without long expanses of uniform or blank walls. The proposed building includes the use of a roof supported by columns to break up the long façade. The Downtown Plan states that “A larger building should be divided into “modules” that are similar in scale to buildings seen historically.” Staff notes that this property is a corner lot and many historic buildings contain a “side” along the non-primary frontage that was often less detailed and less modulated than the “primary street.” Staff recommends approval.

**Entrance and Drive Variances:** Apart from the three required waivers, two variances from the standards of Chapter 20.05 are required. The first is a variance from the entrances and drives standards. This variance is necessary to allow drives that are as wide as 50 feet, whereas the UDO only permits drives 34 feet or less. The UDO also requires a minimum of 100 feet between drives. Some drives associated with this project are as close as 15 feet to another drive. These variances will be reviewed by the Board of Zoning Appeals or the Hearing Officer. Staff recommends approval of the site plan contingent on variance approval.

**Fence/Wall height Variance:** The second variance is associated with the wall at the southeast corner of the site designed to screen the Middle Way House building, which is on the National Registry of Historic Places, from the proposed use. This wall was required by the State Historic Preservation Office. The UDO does not permit fences and walls taller than four feet between a building and the street. This wall is eight feet tall. This variance will

be reviewed by the Board of Zoning Appeals or the Hearing Officer. Staff recommends approval of the site plan contingent on variance approval.

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**ENVIRONMENTAL COMMISSION RECOMMENDATIONS:** The Bloomington Environmental Commission (EC) has made 1 recommendation concerning this development.

1. The EC recommends diversifying the plants in the Landscape Plan, especially the *Acer rubrum* 'Bowhall'. It would be desirable to use at least some native species given many native plants can survive harsh environments, require little water after establishment, don't require chemical fertilizers, and contribute to local ecological service

**Staff response:** While the species proposed are not native plants, they are also not known invasive species. These species have been chosen to fulfill certain landscaping goals, such as screening or hardiness along the passenger islands. Staff recommends that the petitioner continue to work with the Environmental Commission to investigate other species that might still meet these goals.

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**DEVELOPER TRACK RECORD:** Bloomington Transit has been in operation since 1982. Their current downtown transfer facility at 4<sup>th</sup> and Washington and their Grimes Lane operations center have been in existence for many years. This will be the first recent construction project for Bloomington Transit.

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**CONCLUSIONS:** The Planning Department staff finds that the petition satisfies most requirements of the Unified Development Ordinance. While some parts of the building design do not meet the standards of the DCO, the requested waivers are appropriate based on the merits of proposal, its compatibility with surrounding buildings and compliance with the recommendations of the Downtown Plan. This petition will allow for the expansion of two essential public services and the construction of a signature modern building at a prominent location in the community.

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**RECOMMENDATION:** Based on the written findings above, staff recommends approval of SP-31-12, and all associated waivers, with the following conditions:

1. Approval of the site plan is contingent on approval by the BZA or Hearing Officer of all necessary variances.
2. Alley encroachments and changes to public right-of-way are subject to Board of Public Works review.
3. Street lights along Walnut St. shall match the lights specified for the S. Walnut Street Streetscaping plan.
4. The petitioner should work with the Environmental Commission to incorporate more native plant species into the landscaping plan.
5. An ingress/egress and utility easement must be recorded for the widened east-west alley.

6. A photometric plan must be reviewed and approved for all site lighting, including building mounted lights meant to replace street lights on 3<sup>rd</sup> St.
7. Bicycle parking spaces shall be provided within 50 feet of the main entrance at 3<sup>rd</sup> St. and Walnut St.

# MEMORANDUM

**Date:** August 30, 2012  
**To:** Bloomington Plan Commission  
**From:** Bloomington Environmental Commission  
**Through:** Linda Thompson, Senior Environmental Planner  
**Subject:** SP-31-12, Bloomington Public Transportation Corporation  
301 S. Walnut Street

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This memorandum contains the Environmental Commission's (EC) input and recommendations regarding the request for a Site Plan approval for a transportation terminal and dispatch station. The site is within the Commercial Downtown Zoning District and the Downtown Core Overlay District.

The EC is pleased that this facility is planned to be a modern, Leadership in Energy and Environmental Design (LEED) Silver Certified showcase of green building, which is situated in a prominent in-fill location in the heart of downtown. Green building can provide substantial savings in energy costs to a building over its life cycle and is thus an especially prudent investment in this time of rising energy prices. Green building features are consistent with the spirit of the UDO and supported by Bloomington's overall commitment to sustainability and its green building initiative (<http://Bloomington.in.gov/greenbuild>). Sustainable building practices are explicitly called for by the Mayors' Climate Protection Agreement signed by Mayor Krizan, by City Council resolution 06-05 supporting the Kyoto Protocol and reduction of our community's greenhouse gas emissions, and by City Council resolution 06-07, which recognizes and calls for planning for peak oil.

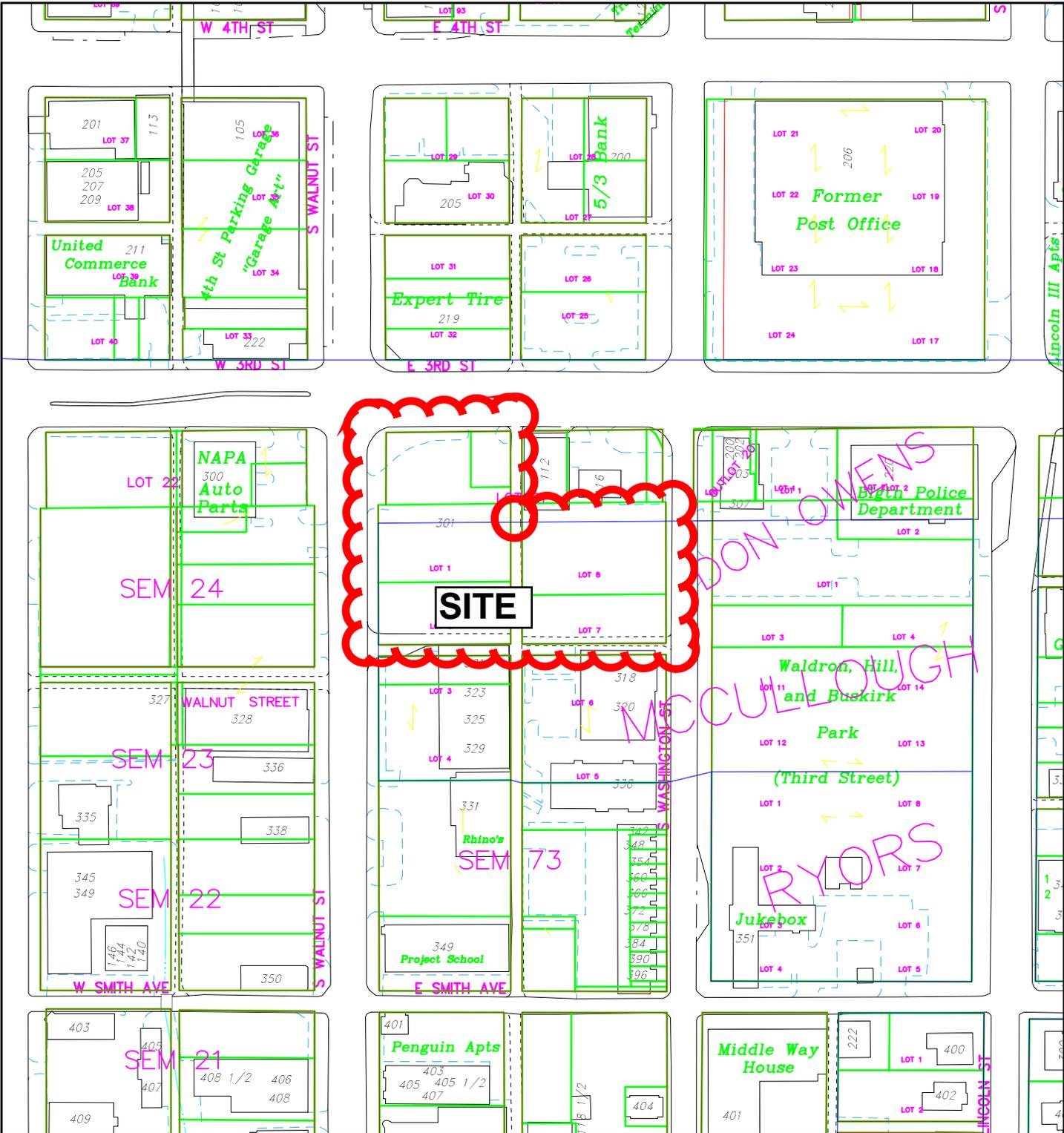
The EC understands that there are few landscape requirements in a Downtown Core Overlay, and the challenge to growing plants in such a harsh environmental location as this. However, the EC believes that a plan for almost all exotic and cultivated plants with only one native species does not fulfill the intent of green site design. Therefore, the EC recommends that the petitioner add more native species into the landscaping plan.

The landscape plan calls for only eight (8) different kinds of plants, and that is not a good policy for diversity in a landscape or community. There are two (2) different tree species, three (3) different shrub species, and three (3) different groundcover species. The EC recommends incorporating additional varieties of all three categories into the plan. The one deciduous tree choice, Bowhall Maple (*Acer rubrum* 'Bowhall') especially needs to be diversified because there is an over abundance of red maples (*Acer rubrum*) in our tree inventory. Understanding that Bowhall maples do work well as street trees, the EC advises that the nine on the south side be

traded for a different species.

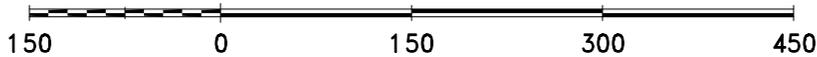
**RECOMENDTIONS:**

1. The EC recommends diversifying the plants in the Landscape Plan, especially the *Acer rubrum* 'Bowhall'. It would be desirable to use at least some native species given many native plants can survive harsh environments, require little water after establishment, don't require chemical fertilizers, and contribute to local ecological service

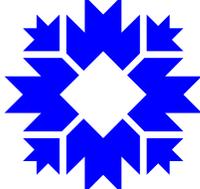


SP-31-12  
Location Map

By: roachja  
2 Jul 12



City of Bloomington  
Planning



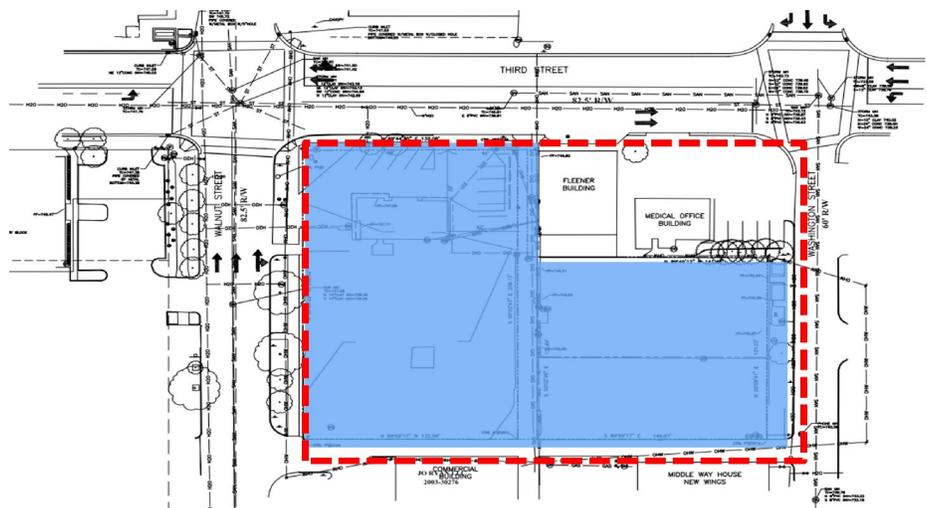
Scale: 1" = 150'

For reference only; map information NOT warranted.

**Petitioner's Statement**  
**Downtown Passenger Transfer Facility and Emergency Dispatch Center**  
**Bloomington Public Transportation Corporation (BPTC), City of Bloomington, Monroe County**  
**July 10, 2012**

**Introduction**

This petitioner's statement is being submitted for the shared use project referenced above which is to be located at the southeast corner of the intersection of 3<sup>rd</sup> Street and Walnut Street in downtown Bloomington, Indiana. The property on which the development will be situated is owned by Bloomington Public Transportation Corporation (BPTC). The proposed passenger transfer facility will replace the existing BPTC transit facility at the corner of 4<sup>th</sup> Street and Washington Street. The proposed emergency dispatch center will replace the existing Bloomington/Monroe emergency dispatch facility, which is located within the Bloomington Police Department's (PD) headquarters at 220 East 3rd Street.



**Description of existing facilities and statement of project need**

BPTC Passenger Transfer Facility

The existing BPTC Downtown Passenger Transfer Facility was opened in 1987 and consists of a small indoor passenger waiting area with ticket sales booth, passenger seating, heating (no air conditioning), lighting, soft drink vending, water fountain, two small employee restrooms, and public information boards. The interior size of the facility is approximately 1,000 square feet with about 460 square feet devoted to the passenger waiting area. Outdoor passenger amenities include benches, signage, marked bus stops, sidewalks, newspaper vending, and a small amount of landscaping. When the existing facility at 4<sup>th</sup> and Washington was opened in 1987, system ridership was about 480,000 passengers annually. In 2011, ridership was 3.39 million riders. Much of the growth in passengers in the past ten years has come from the use of the system by Indiana University students. Increased use of the system has created new challenges at the existing downtown facility. Probably the most significant challenge has been the lack of adequate bus parking space along 4<sup>th</sup> and Washington Streets. The length of buses has grown from 30-foot buses used in 1987 to 30, 35, and 40-foot buses being used since 2007. As the existing facility was designed for 30-foot buses, the longer length buses used today do not fit well in the current facility. Other important challenges are the need for improved amenities such as wider sidewalks, air conditioning within the facility, improved lighting, improved employee restrooms, the addition of public restrooms, improved passenger and employee security, and larger space for pass and



ticket sales. The existing location at 4<sup>th</sup> and Washington has a constrained footprint and expansion of the existing facility is not considered a viable option.

Given recent development patterns, it is anticipated that downtown Bloomington will continue to be one of the most important destinations for persons using BPTC transit services over the next 20 years. Downtown will become an increasingly important origin for transit trips as downtown residential development continues. With its central location relative to the rest of the city, downtown is well situated for the purposes of transit riders transferring between routes.

#### Bloomington / Monroe County Emergency Dispatch Facility

The existing emergency dispatch facility for the City of Bloomington and Monroe County is located with the City police department's downtown headquarters along 3rd Street. The current dispatch room is 500 SF and contains five operator positions. Another 500 SF accommodates the radio and computer rooms. In addition to critical space constraints, challenges at the existing facility include poor lighting, inadequate display areas, lack of storage space, inconsistent and poor temperature control, and an inefficient layout that makes cleaning difficult and noise from bathrooms, break room, and lockers disruptive.

The opportunity for a new dispatch facility to be co-located with Bloomington Transit's passenger facility provides a mutually beneficial solution for all three agencies. Bloomington Transit will benefit from the ongoing presence of uniformed police personnel coming and going from the facility. The City of Bloomington and Monroe County benefit by not having to pay land and site development costs for their new facility. All agencies benefit from increased levels of cooperation that come from the coordinated planning, design, and operation of the new facility.

### **Project Funding**

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BPTC has acquired several Federal grants to facilitate the purchase of property and the development of a new downtown transit center. American Recovery and Reinvestment Act (ARRA), Federal discretionary and formula funds, and local BPTC funds will be used in development of the transit portion of the site.

The dispatch facility will be funded through local sources using a mix of TIF district (City portion) revenues and potentially bonding (County portion).

### **Project Development**

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In 2005-2006, BPTC conducted a study entitled *Downtown Passenger Transfer Facility Location Analysis* which developed a master plan for developing a new passenger transfer facility in the downtown area. The study involved assessment of existing and future BPTC operations and analysis of multiple potential station sites in and around downtown Bloomington. In December 2008, as a result of that study's findings, BPTC purchased several parcels of property at 3<sup>rd</sup> and Walnut – the location of the former

Royal Dog. The subject property extends from Walnut Street over to Washington Street and included a 2-story office building on Washington Street and the former Royal Dog building at the northwest corner of the site. Both buildings that existed on the site at the time of purchase have since been demolished to make way for the new transit center.

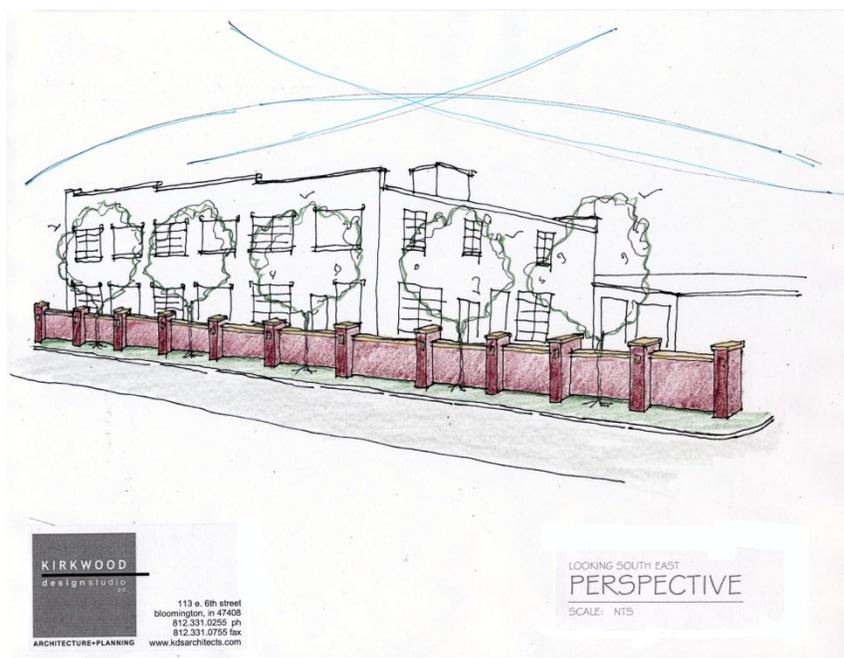
### NEPA

In accordance with the National Environmental Policy Act (NEPA) and as a requirement for receiving Federal Transit Administration (FTA) funding, BPTC completed an environmental survey of the site prior to starting the design effort. The required Categorical Exclusion (CE) document summarizing the study's findings was submitted to FTA in October 2008. The CE documented potential impacts of the project.

As part of the environmental survey, a hazardous materials assessment was conducted on the site. The results and findings from the Phase I and Phase II Environmental Site Assessment (ESA) reports were forwarded to the Indiana Department of Environmental Management (IDEM) and the Federal Transit Administration for review and comment. IDEM responded with two letters dated February 25, 2008 (Comfort Letter) and May 23, 2008 (Site Status Letter). In these letters IDEM stated that environmental restrictive covenants (ERC) would be required to be placed on the deeds for the parcels comprising the project site. These ERC's require that BPTC:

1. Neither engage in nor allow installation of any water wells on the site or allow the use of any existing water wells on the site, except for investigatory or remediation purposes,
2. Install vapor intrusion systems for all existing or planned enclosed structures on the site that will be occupied, or provide documentation to IDEM demonstrating that any contamination underlying the site does not pose a vapor intrusion threat.
3. Neither engage in nor allow the use of the site for residential or agricultural purposes.

Additionally, the Section 106 process, which included review of adjacent historic properties and coordination with consulting parties and property owners, resulted in commitments being made by BPTC for mitigation of potential impacts to historic properties on or eligible for the National Register of Historic Properties. Through this process, BPTC has committed to constructing a masonry wall and landscaping buffer along the south border of the property and north of the Coca-Cola Bottling Plant building (318 South Washington Street). The commitment is for a masonry wall designed to complement the historic, colorful, vertically scored brick of the historic Coca



Cola Bottling Company building. In accordance with this commitment, the overall size and rhythm of the wall will relate to the scale of the building as well. The wall will have a system of piers that reference the pilasters of the building, creating sections of wall with proportions similar to the large, rectangular window openings. Limestone will be incorporated as a cap and each pier will have an inset limestone medallion like those at the window heads of the Coca Cola building. The intent is for the wall and buffer to mitigate any adverse effects to the adjacent historic properties by reducing noise, bus emissions, and the visual effect of the BPTC project next door.

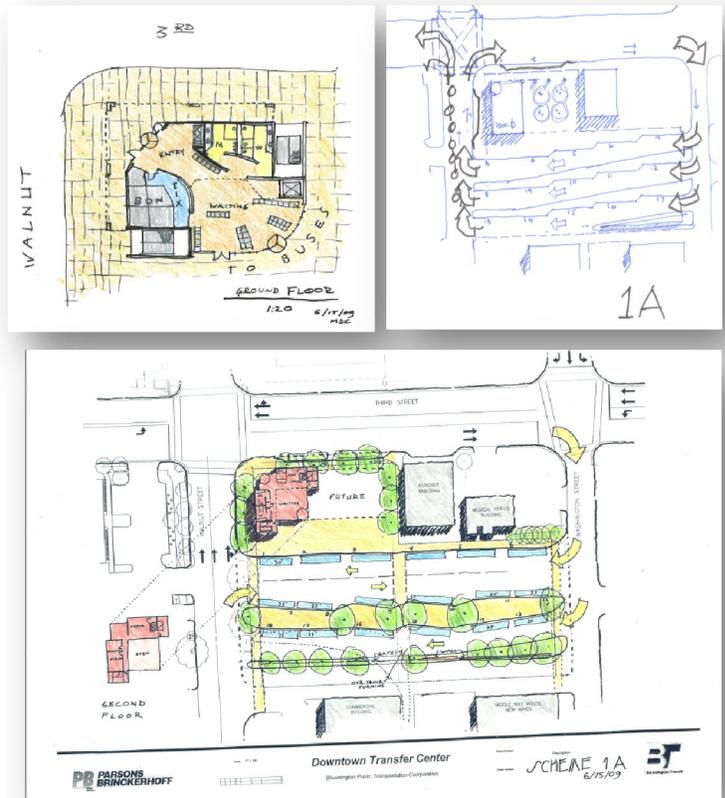
In spring 2011, after altering the project to incorporate a second-story dispatch facility, BPTC re-engaged FTA and State Historic Preservation Office (SHPO) to confirm that the newly conceived project would meet with a Finding of No Adverse Effect. On May 19, 2011 the Indiana Department of Natural Resources SHPO provided their agreement with the finding. And on July 14, 2011, FTA provided a written letter stating their agreement with the SHPO findings.

Upon receiving FTA's approval of the NEPA document, BPTC initiated an Request for Qualifications (RFQ) process to select an engineering and architectural firm to design the new facility. The Parsons Brinckerhoff (PB) / Bledsoe Riggert and Guerretaz (BRG) team was selected in early 2009 as the consultant for the design of the new facility.

### Planning Phase

The initial step in the design process was to identify the project components and overall programming for the site and the building. In order to understand the needs and desires of the community and the project stakeholders, BPTC facilitated two multi-day workshops in March and May 2009 to solicit input and ideas. Attendees of the workshops included representatives from:

- Rural Transit
- City Public Works
- Middle Way House
- Pavillion Inc.
- Chamber of Commerce
- City Council
- City Planning
- Monroe County Commissioners
- Rhinos / Harmony
- Indiana University Campus Bus
- Indiana University Sustainability
- Bloomington Transit
- WFIU Radio
- Prospect Hill Neighborhood Association
- Herald Times
- General Public





The workshops discussed the needs, goals, and objectives of the project with a focus on bus operations and site configuration. The result was the site layout that is being presented in this filing.

### **Coordination with City**

Soon after conducting the initial design planning workshops for this project, the City of Bloomington and BPTC began discussions regarding the potential of co-locating an emergency dispatch facility with the transit facility. In January 2011, the City and BPTC began negotiating in earnest to have the two projects merged onto the same site. Because of the mix of transit and non-transit uses being proposed, FTA required the BPTC to resubmit the revised project scope of work for additional SHPO review. In addition, BPTC was required to develop an Interlocal Agreement with the City of Bloomington which was reviewed by FTA in November 2011. Having met the FTA requirements for additional SHPO review of the revised project scope of work and FTA review of the Interlocal Agreement, BPTC resumed design work on the project.

As the design process proceeds, BPTC continues to work with the City of Bloomington on the following site-related items that affect the use of the property:

- Encroachment of north-south alley through site. BPTC is proposing that the north-south alley be closed to vehicular traffic through the subject site (excluding emergency and maintenance vehicles), equating to the need for an Encroachment agreement for the alleyway. Justification for this decision, for reasons which include maintaining safe pedestrian movement and efficient transit operations, has been provided to DPW. The DPW Board will be reviewing and responding to the request in July. The encroached area of the alley will be constructed out of stamped, colored concrete or another distinguishable material that will help to identify it as a pedestrian and bicycle way. It will be raised up to the curb level to act as a traffic calming device for buses travelling east-west through the site and with a profile to accommodate both emergency vehicle traffic parallel and bus traffic perpendicular to the alleyway. The final pavement design selection will take into account ease of maintenance and durability under heavy vehicle traffic. Canopies over the pathway will be constructed at a minimum of 15-feet in height to permit transit and emergency vehicle access.
- Relocation of overhead utilities. The consultant is currently working with local utility companies to plan for relocation of existing overhead lines underground. The presence of overhead lines through the site would detract from the aesthetics of the transit plaza and, more importantly, would introduce the potential for maintenance needs that impact transit operations. Existing utilities will be routed underneath the alleyway through the site. Coordination of this effort is on-going.
- Installation of new signal at Walnut /Smith. After extensive discussion, analysis, and traffic modeling of various scenarios, the City of Bloomington and BPTC have agreed to partner on the installation of a traffic signal at the intersection of Walnut Street and Smith Avenue in order to provide pedestrian access at Smith and facilitate efficient platooning of vehicles along Walnut Street. Because of the close proximity of the transit site drives to 3<sup>rd</sup> Street, BPTC and the City found that larger gaps in the northbound traffic stream would be helpful to transit vehicles



exiting the subject site. The new signal will provide the gaps necessary to improve transit egress onto Walnut Street.

## **Architectural / Site Features**

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The primary building occupant will be staff for BPTC and City of Bloomington / Monroe County Dispatch Services. This includes, pass/ticket sales attendants, supervisors and bus operators on the transit side; and staff director, manager, training supervisor, shift supervisor, dispatch operators, IT support staff and City of Bloomington Fire Department plan reviewers housed in the second floor Dispatch Center.

The primary structure on the site is a two story, steel framed building to house transit functions on the 1<sup>st</sup> floor and dispatch operations on the 2<sup>nd</sup> floor. Exterior materials include native limestone with a variation of textural finishes, colored metal panels and aluminum framed insulated glazing. Glass along the 1<sup>st</sup> floor, in public spaces will be highly transparent to encourage transparency and public safety.

Some additional features of the plan:

- The east-west alley that runs along the south edge of the BPTC property will be widened from 12-feet to approximately 20-feet to accommodate vehicles that may be turning northbound-to-westbound from the portion of the north-south alley that remains south of the site and to provide enough width for delivery vehicles to safely maneuver into the MiddleWay loading docks. Vehicles larger than single-unit trucks would be accommodated in a northbound to westbound turn between the two alleys via a gate that will be installed at the south edge of the encroached alley. The gate would allow northbound trucks to enter the southern bus lane of the transit center in order to complete their northbound-to-westbound move, then exit onto northbound Walnut Street. (See figures attached) The gate will be operated by a BPTC staff member situated inside the transit facility.
- Transit boarding platforms consisting of a primary two-sided island located in the southern portion of the site, which includes a sawtooth configuration and can host a variety of buses and shuttles including articulated buses; a boarding platform along the southern edge of the transit plaza and a location for additional bus bays along the northern edge of the property along 3<sup>rd</sup> Street. The overall capacity of the new boarding platforms can accommodate up to (15) buses at one time on the site which represents an increase of 87 % over the existing facility. All boarding areas will be provided with overhead canopy structures to protect waiting passengers from rain / sun. Additional amenities to the boarding platforms include bench seating, planters, trash receptacles, wayfinding and transit signage, lighting and provision for future dynamic message signs.
- Passenger and pedestrian movements on the site include wheelchair accessible curb cuts at each entry point to the site, a primary north-south passenger connector between the transit plaza and the primary boarding island. This connector is raised to meet the elevation of the transit plaza and boarding platforms and additionally acts as speed restrictor for bus traffic moving through the site. Vegetative strips are provided in an east-west configuration along the site between boarding locations which also include decorative fences or masonry walls to organize passenger movement to “preferred” locations on the site in an effort to better control points of interaction between buses and passengers.



- Pedestrian movement for non-transit uses is accommodated by accessible walks along the eastern and western perimeter of the site along Walnut Street and Washington Street. Along 3<sup>rd</sup> Street, the building recesses in a colonnade on the first floor to accommodate pedestrian movement without being restricted by transit functions along 3<sup>rd</sup> Street.
- Additional site amenities include a masonry wall along the southeastern portion of the site north of the historic Coca-Cola building as prescribed in SHPO documentation. This masonry wall will be built of brick and limestone to match the scale and proportion of the historic building. Along the western portion of the southern alleyway, an additional masonry enclosure will be provided for maintenance equipment storage and recycling.
- Bicycle amenities for the site include three (3) sets of bike racks and ten (10) bike lockers.

**LEED**

The site and building are being designed to achieve LEED Silver Certification per the current LEED-NC standard. An interesting element of this project is that its design will distinguish the new facility as a LEED-certified project, which will showcase the agencies' and the community's commitment to good stewardship of the environment.

The following LEED credits are being sought at this stage of development:

- Sustainable Sites – minimum 18 credits identified
- Water Efficiency – minimum 5 credits identified
- Energy and Atmosphere – minimum 7 credits identified
- Materials and Resources – minimum 7 credits identified
- Indoor Environmental Quality – minimum 11 credits identified
- Innovation in Design – minimum 2 credits identified
- Regional Priority – 1 credit identified

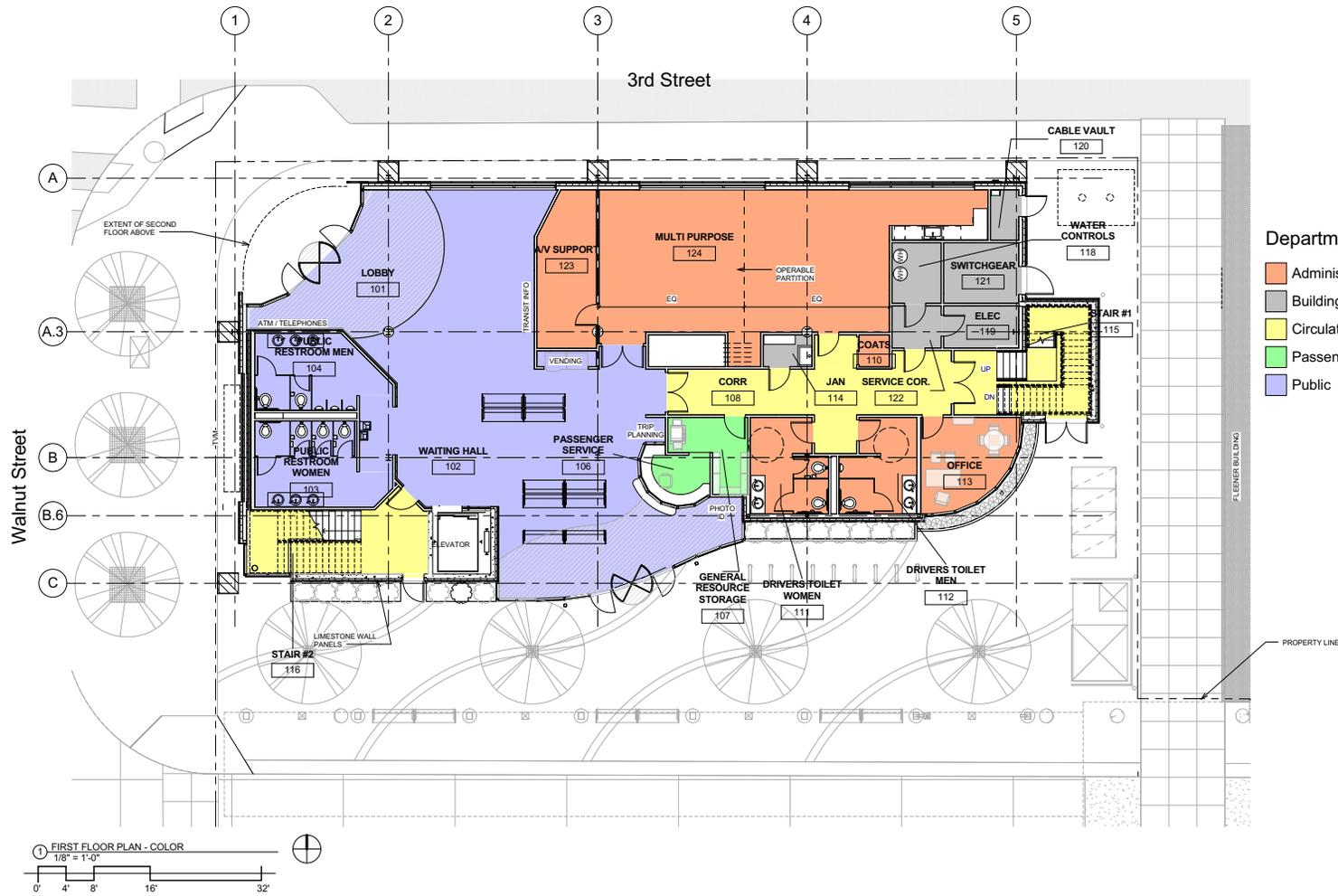
Staff for both the transit operations and dispatch services will be trained to comprehend and operate the user interface components of the facilities systems. Maintenance staff will receive training to understand efficient operational requirements for the building systems and associated amenities. This includes preventive maintenance and operating procedures (such as environmentally friendly methods of snow removal, leaf collection, etc.) as well as use of "green" products for cleaning and facility upkeep.

**Schedule**

Remaining milestones for the project are scheduled as follows:

Plan Commission Hearing	August 6, 2012
Submittal of final construction documents	October 26, 2012
Advertise for Bid	October 29, 2012
Contract Award	December 5, 2012
Construction	December 5, 2012 through December 4, 2013

SP-31-12  
First Floor Plans



- Department Legend**
- Administration
  - Building Services
  - Circulation
  - Passenger Services
  - Public

**Bloomington Transit** CITY OF BLOOMINGTON

Prime Consultant: Architecture, Structure, Systems

**PARSONS BRINCKERHOFF**

300 North Meridian Street Indianapolis, Indiana 46204  
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Civil / Survey / Landscape

**Bledsoe Riggert Quarrette**  
ARCHITECTS INC. BLOOMINGTON

1351 West Tapp Road Bloomington, Indiana 47403  
T: 812.338.6277 F: 812.338.0817 | www.brgovill.com

Mechanical / Electrical / Plumbing / Fire Prot

**L'ACQUIS** ENGINEERS  
CONSULTING ENGINEERS

9229 Delegation Row Ste250 Indianapolis, Indiana 46240  
T: 317.706.2075 F: 317.706.2078 | www.lacqus.com

REV	DESCRIPTION	DATE

New Construction of:

**Downtown Transfer Center Dispatch Center**

Walnut St. / Third St.  
Bloomington, Indiana 47401

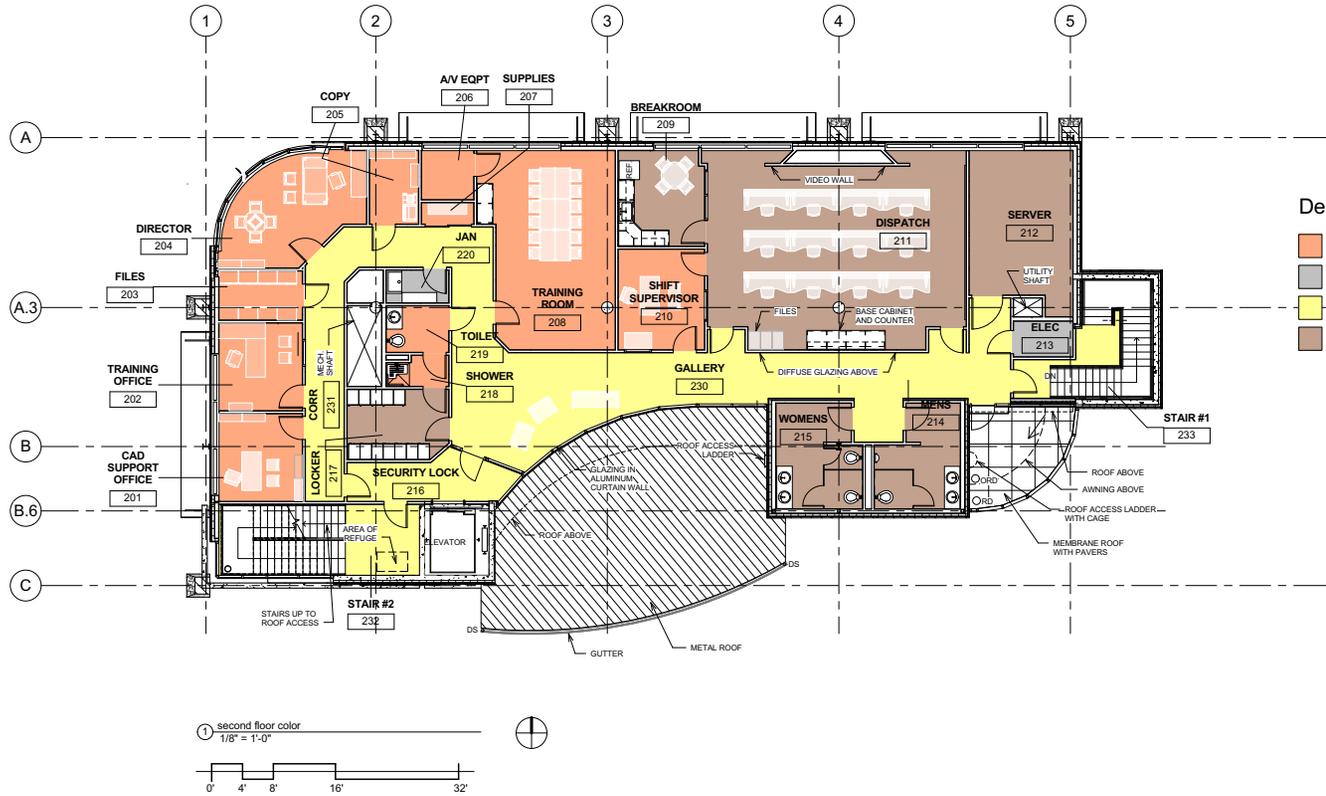
Bloomington Public Transportation Corporation  
City of Bloomington

PLAN COMMISSION  
FILING DOCUMENTS

Sheet Content	
FIRST FLOOR PLAN	
DATE	2012 SEPTEMBER 10
SCALE	1/8" = 1'-0"
DRAWN BY	AJH
CHECKED BY	MDC
APPROVED BY	TBP
PB Project Number	35654A/B

Sheet Number **1.1 c**

SP-31-12  
Second Floor Plans



- Department Legend**
- Administration
  - Building Services
  - Circulation
  - Dispatch



Prime Consultant: Architecture, Structure, Systems  
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**L'ACQUIS**  
 CONSULTING ENGINEERS  
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REV	DESCRIPTION	DATE

New Construction of:  
**Downtown Transfer Center**  
**Dispatch Center**  
 Walnut St. / Third St.  
 Bloomington, Indiana 47401  
 Bloomington Public Transportation Corporation  
 City of Bloomington

PLAN COMMISSION  
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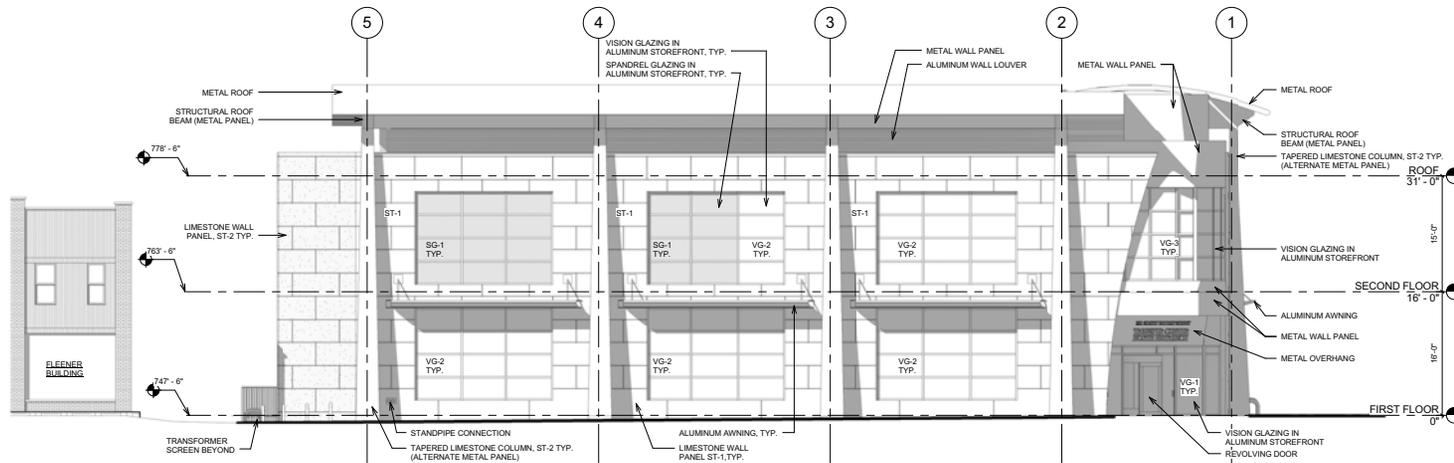
Sheet Content  
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DATE	2012 SEPTEMBER 10
SCALE	1/8" = 1'-0"
DRAWN BY	AJH
CHECKED BY	MDC
APPROVED BY	TBP

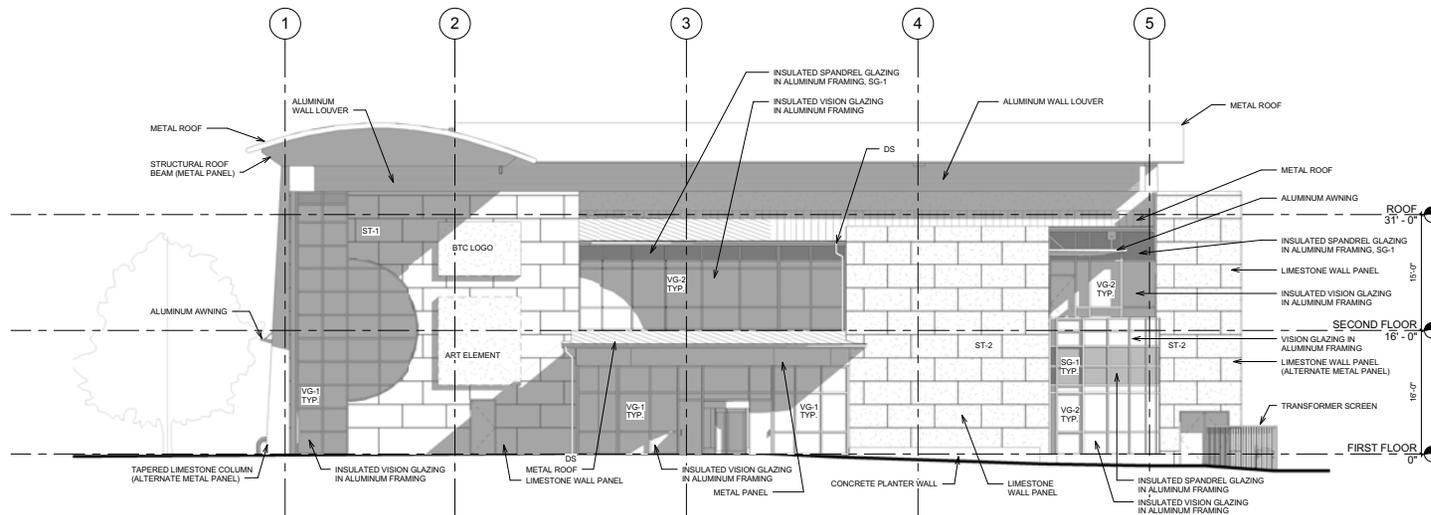
PB Project Number 35654A/B

Sheet Number **1.2 c**

SP-31-12  
North and South Elevations



① NORTH ELEVATION  
1/8" = 1'-0"



② SOUTH ELEVATION  
1/8" = 1'-0"



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REV	DESCRIPTION	DATE

New Construction of:  
**Downtown Transfer Center  
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 Walnut St. / Third St.  
 Bloomington, Indiana 47401  
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PLAN COMMISSION  
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Sheet Content  
 EXTERIOR BUILDING ELEVATIONS

DATE 2012 SEPTEMBER 10  
 SCALE 1/8" = 1'-0"  
 DRAWN BY AJH  
 CHECKED BY MDC  
 APPROVED BY TBP

PB Project Number 35654AB

Sheet Number **2.1**

REV	DESCRIPTION	DATE

New Construction of:  
**Downtown Transfer Center  
 Dispatch Center**  
 Walnut St. / Third St.  
 Bloomington, Indiana 47401  
 Bloomington Public Transportation Corporation  
 City of Bloomington

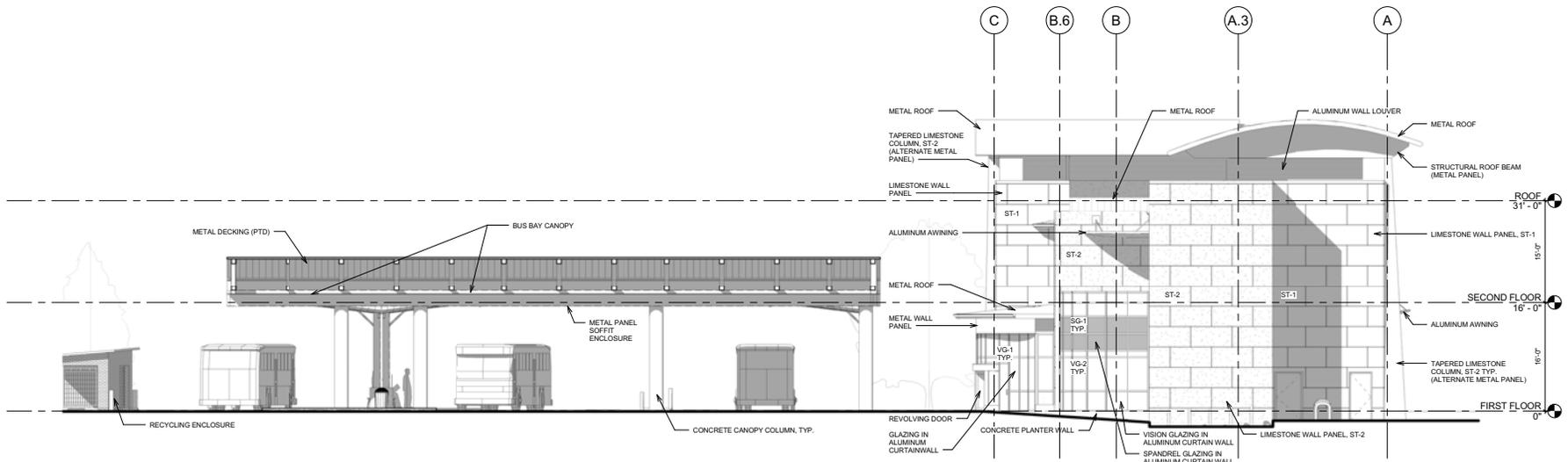
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Sheet Content  
 EXTERIOR BUILDING ELEVATIONS

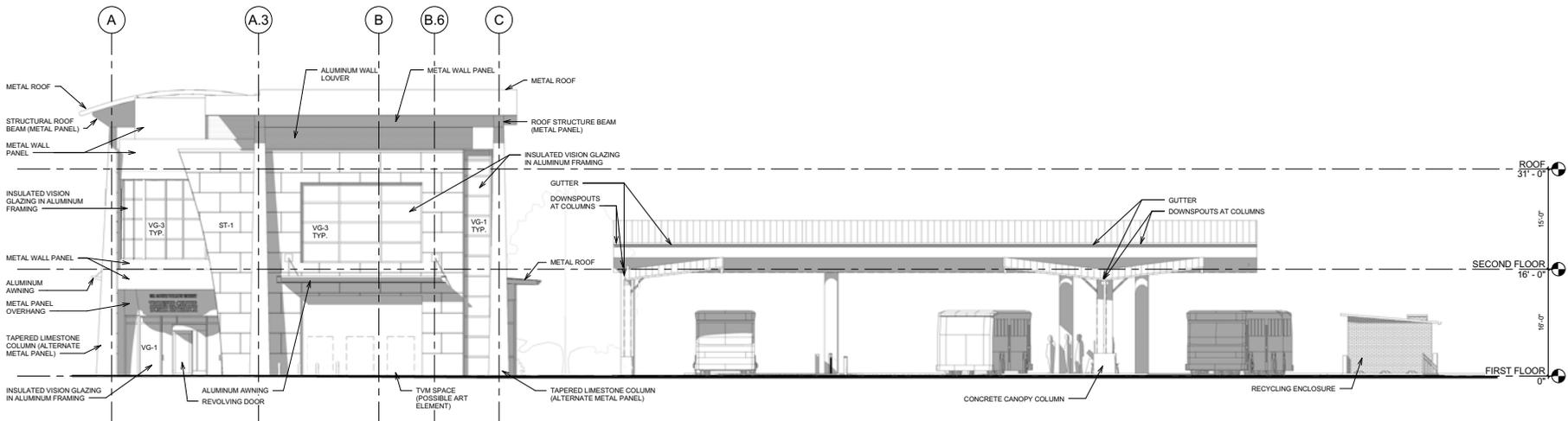
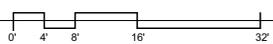
DATE	2012 SEPTEMBER 10
SCALE	1/8" = 1'-0"
DRAWN BY	AJH
CHECKED BY	MDC
APPROVED BY	TBP

PB Project Number	35654A/B
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Sheet Number **2.2**



① EAST ELEVATION  
 1/8" = 1'-0"

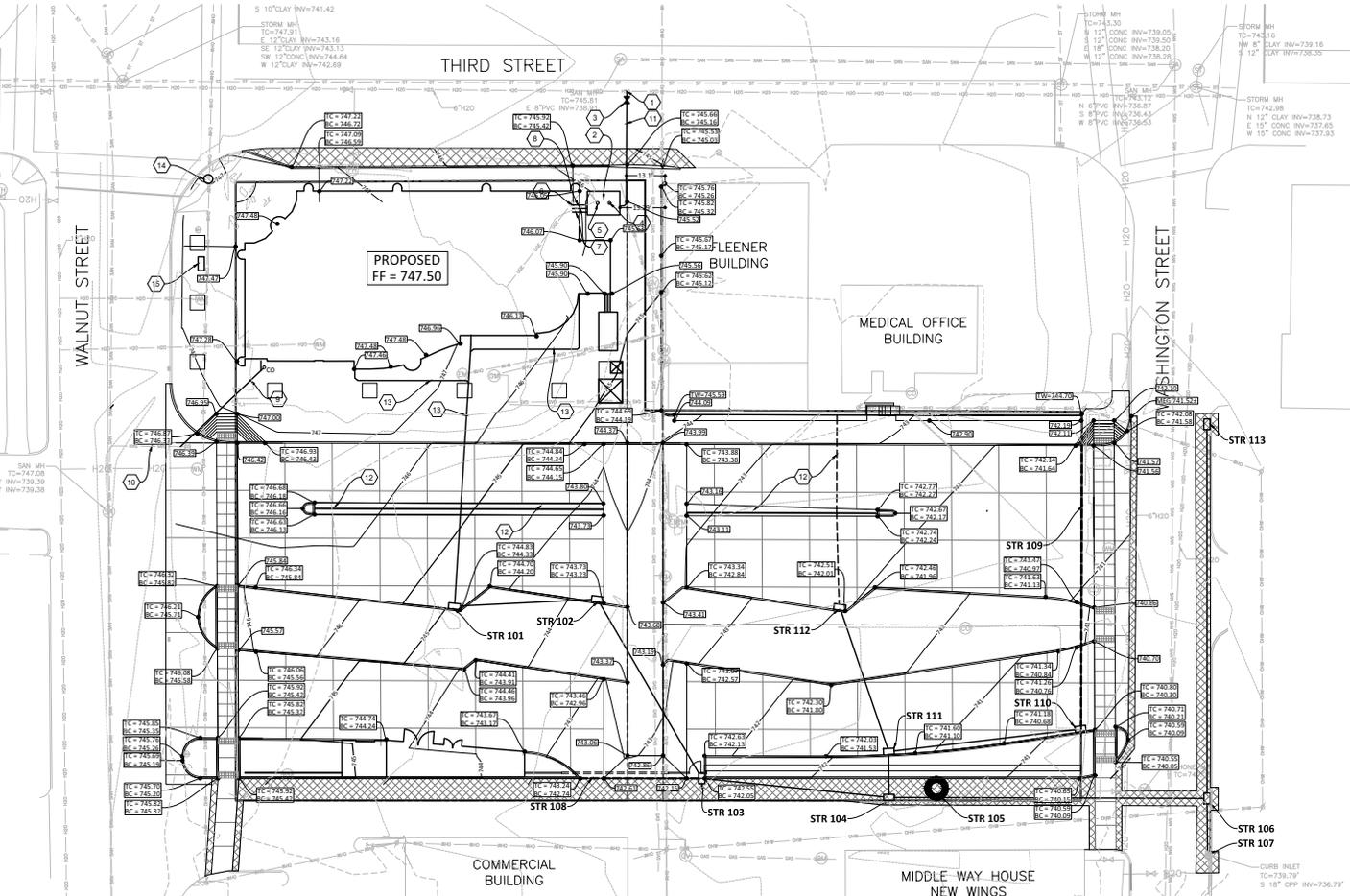


② WEST ELEVATION  
 1/8" = 1'-0"





SP-31-12  
Utility Plan

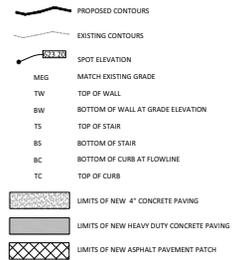


STORM STRUCTURE DATA TABLE

STR #	CASTING	STRUCTURE TYPE	UP STR #	DN STR #	PIPE LENGTH	PIPE SIZE	PIPE SLOPE	UP ELEVATION	DN ELEVATION	TOP OF CASTING	RII ELEVATION	COMMENTS	
101	ERW 7505	INLET TYPE 'V'	STR # 102	48"	12'	HDPE TYPE 5	2.29%	743.83	740.73	744.83	744.83		
102	ERW 7505	INLET TYPE 'V'	STR #103	70"	12'	HDPE TYPE 5	1.69%	740.73	739.55	743.73	743.73		
103	ERW V560.80	INLET TYPE 'V'	STR #102	64"	12'	HDPE TYPE 5	1.19%	739.55	738.79	742.05	742.05		
104	ERW V560.80	INLET TYPE 'V'	STR #105	18"	15'	HDPE TYPE 5	0.50%	737.46	737.38	741.54	741.54		
105	AQUANIRL AS-3	PROPRIETARY MANHOLE	STR #104	81"	15'	HDPE TYPE 5	0.49%	737.38	736.93	741.77	741.77		
106	ERW 7505	INLET TYPE 'V'	STR #105	STR #107	20"	15'	HDPE TYPE 5	0.50%	736.93	736.83	740.57	740.57	
107	EXISTING CASTINGS	EXISTING STR.	STR #101	107"	18'	EXIST CPP	1.21%	736.83	735.54		739.69	⊙	
108			STR #103	41"	12'		2.90%	740.74	739.55	742.74	742.74		
109		SLOTTED PIPE	STR #110	96"	12'		2.34%	740.11	737.98	742.11	741.61	⊙	
110	ERW 7505	INLET TYPE 'V'	STR #109	STR #111	60"	12'	HDPE TYPE 5	0.49%	737.96	737.54	740.86	740.86	
111	ERW 7505	INLET TYPE 'V'	STR #105	STR #104	15"	15'	HDPE TYPE 5	0.53%	737.54	737.46	741.72	741.22	
112	ERW 7505	INLET TYPE 'V'	STR #111	52"	15'	HDPE TYPE 5	3.94%	739.55	737.46	742.51	742.01		
113	ERW 7505	INLET TYPE 'V'	STR #106	154"	15'	HDPE TYPE 5	1.31%	738.84	736.83	742.09	741.59		

ERW = EAST JORDAN IRON WORKS OR APPROVED EQUAL  
 COMMENTS:  
 ⊙ CORE DRILL AND CONNECT PIPE TO EXISTING INLET STRUCTURE. SEAL ALL CONNECTIONS AND STRUCTURE WATER TIGHT.  
 ⊙ WHERE SLOTTED PIPE RUNS UNDER RAISED CONCRETE CENTER ISLAND, TRANSITION TO UN-SLOTTED CORRUGATED METAL PIPE.

LEGEND

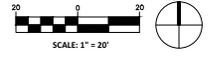


GENERAL NOTES

- GRADE ALL AREAS TO THE FINISH GRADES SHOWN.
- CONTRACTOR TO VERIFY FIELD CONDITIONS WITH RESPECT TO THE PROPOSED GRADING PLANS AND NOTIFY ENGINEER OF ANY AND ALL DISCREPANCIES PRIOR TO BEGINNING WORK.
- INSTALL AND MAINTAIN EROSION CONTROL DEVICES AS REQUIRED AND WHERE NECESSARY TO CONTROL SEDIMENT.
- CONTRACTOR SHALL PREVENT SURFACE WATER AND GROUND WATER FROM ENTERING EXCAVATIONS, FROM PONDING ON PREPARED SUBGRADES AND FROM FLOODING PROJECT SITE AND SURROUNDING AREAS. PROTECT SUBGRADES FROM SOILING, UNDERMINING, WASHOUT AND DAMAGE BY RAIN OR WATER ACCUMULATION. THIS SHALL REQUIRE SUPPLEMENTAL GRADING ABOVE AND BEYOND THE WORK AREA.
- CONTRACTOR SHALL ESTABLISH FINISH GRADES TO ENSURE POSITIVE DRAINAGE WITH NO PONDING.
- UNUSUAL SLOPES SHALL NOT EXCEED 5%, UNLESS NOTED OTHERWISE. TRANSVERSE SIDEWALK SLOPE SHALL NOT EXCEED 2%.
- SPOT GRADES GIVEN AT THE FACE OF CURB INDICATE PAVEMENT EDGE/CURB INTERFACE FLOW LINE ELEVATION, UNLESS NOTED OTHERWISE. BOTTOM OF WALL ELEVATIONS INDICATE WHERE FINISH GRADE AND WALL MEET.
- ALL SLOPES GREATER THAN 3:1 TO BE COVERED WITH NORTH AMERICAN GREEN SB50 EROSION CONTROL BLANKET OR APPROVED EQUAL. INSTALL PER MANUFACTURER'S INSTRUCTIONS.
- REFER TO DEMOLITION PLANS FOR SEQUENCE OF UTILITY REPLACEMENT TO ENSURE CONTINUOUS SERVICE OF ALL UTILITIES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR CUTTING AND PATCHING AS REQUIRED TO COMPLETELY INSTALL THE WORK INDICATED.
- CONTRACTOR SHALL COORDINATE EXACT UTILITY LOCATIONS WITH THE OWNER AND LOCALITY COMPANIES PRIOR TO COMMENCING ANY WORK. CONTACT THE INDIANA UNDERGROUND PLANS AND RECORDS DIVISION AT 1-800-382-5544 AND OTHER UTILITIES PRIOR TO ANY EXCAVATION ON THE SITE.
- ALL WORK ASSOCIATED WITH WATER AND SEWER SYSTEMS SHALL COMPLY WITH THE STANDARDS & REQUIREMENTS OF THE INDIANA DEPT. OF ENVIRONMENTAL MANAGEMENT, THE INDIANA STATE DEPARTMENT OF HEALTH, THE AMERICAN WATER WORKS ASSOCIATION (AWWA), THE GREAT LAKES UPPER MISSISSIPPI BOARD OF STATE PUBLIC HEALTH AND ENVIRONMENTAL MANAGERS (GLUMBE).
- CONTRACTOR SHALL SET ALL EXISTING AND PROPOSED CASTINGS AND CLEANOUT COVERS TO FINAL FINISHED GRADE.
- A MINIMUM OF 18 INCHES VERTICAL SEPARATION SHALL BE MAINTAINED BETWEEN WATER AND SANITARY/STORM SEWER UNLESS OTHERWISE INDICATED, OR UNLESS WRITTEN PERMISSION IS GIVEN BY THE CITY OF BLOOMINGTON UTILITIES DEPARTMENT. THE CASE WHETHER THE WATER MAIN AND OUTLET OF THE SEWER MAIN SHALL BE EQUIQUANT SHALL BE ARRANGED SO THAT THE WATER MAIN IS ABOVE THE SEWER MAIN. THE SEWER MAIN SHALL BE AT A MINIMUM ANGLE OF 45° MEASURED FROM THE CENTERLINE OF THE SEWER AND WATER MAIN. WHERE A WATER MAIN CROSSES UNDER A SEWER, ADEQUATE STRUCTURAL SUPPORT SHALL BE PROVIDED FOR THE SEWER TO MAINTAIN LINE AND GRADE.
- A MINIMUM OF 30 FEET HORIZONTAL SEPARATION SHALL BE MAINTAINED BETWEEN WATER AND SANITARY/STORM SEWER UNLESS OTHERWISE INDICATED, OR UNLESS WRITTEN PERMISSION IS GIVEN BY THE ENGINEER.
- ALL SANITARY AND STORM LATERALS SHALL HAVE A MINIMUM COVER OF 24" UNLESS NOTED OTHERWISE.
- ALL SANITARY AND STORM LATERALS SHALL HAVE A MINIMUM SLOPE OF 1/8" PER FOOT UNLESS NOTED OTHERWISE.
- ALL WATER LINES SHALL HAVE A MINIMUM COVER OF 48". INSTALL LINES WITH NO ISOLATED HIGH POINTS.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL, PLUMBING, AND LANDSCAPE PLANS FOR ADDITIONAL UTILITY MODIFICATIONS AND NOTIFY ARCHITECT/ENGINEER OF ANY DISCREPANCIES PRIOR TO ANY CONSTRUCTION.
- CONTRACTOR TO EXTEND ALL FOUNDATION, SUBSIDIARY, UNDERGROUND, INTERNAL DRAIN, ROOF DRAIN AND RETAINING WALL DRAIN PIPING TO THE NEAREST PROPOSED STORM STRUCTURE WHILE MAINTAINING POSITIVE FLOW, UNLESS NOTED OTHERWISE. ALL CONNECTIONS SHALL BE WATER TIGHT.
- PRE-CONSTRUCTION MEETING: EFFECTIVE MARCH 1, 2024, ALL PROJECTS WILL REQUIRE A PRE-CONSTRUCTION MEETING WITH THE CITY OF BLOOMINGTON UTILITIES PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR AND/OR DEVELOPER MUST CONTACT THE UTILITIES TECHNICIAN AT (317)349-3033 TO SCHEDULE THE MEETING.
- UTILITIES INSPECTOR: CONTRACTOR SHALL NOTIFY THE CITY OF BLOOMINGTON UTILITIES ENGINEERING DEPARTMENT ONE (1) WORKING DAY PRIOR TO CONSTRUCTION OF ANY WATER, STORM OR SANITARY SEWER WORK. A CITY INSPECTOR MUST BE PRESENT AT ALL WORK AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSPECTOR'S OVERTIME. FOR CBU WORK HOURS AND HOLIDAY INFORMATION, CONTACT THE CITY OF BLOOMINGTON UTILITIES DEPARTMENT AT (317)349-3660.

PLAN NOTES

- WATER TAPS - CONTACT CBU TO APPLY FOR TAP AT LEAST 48 HOURS IN ADVANCE. CONTRACTOR TO EXCAVATE AND INSTALL TAPPING SADDLE AND VALVE.
- WATER METER/VALVE VAULT - CONTRACTOR TO OBTAIN WATER METER FROM CITY OF BLOOMINGTON UTILITIES. CONTACT MARY KAY BROWN AT (317) 349-3039 FOR DETAILS. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH PROPOSED WATER SERVICE.
- WATER VALVE
- POST INDICATOR VALVE WITH TAMPER PROOF SWITCH - REFER TO DETAIL.
- FIRE DEPARTMENT CONNECTION, STORM TIE - REFER TO DETAIL.
- A PERMANENT INDICATING VALVE IS TO BE INSTALLED 12" ABOVE THE FLOOR ON THE FIRE LINE MARK AT THE TERMINATION POINT. THIS VALVE WILL BE USED TO HYDROSTATIC PRESSURE TEST AGAINST AND WILL REMAIN AS PART OF THE SYSTEM ONCE ALL TESTING IS COMPLETE, THE LINE WILL NOT BE DISMANTLED FOR CONNECTION TO THE FIRE SUPPRESSION SYSTEM.
- DOMESTIC WATER SERVICE LINE - 2" DUCTILE IRON PIPE OR COPPER PIPE.
- FIRE PROTECTION SERVICE LINE - 6", DUCTILE IRON PIPE PRESSURE CLASS 350 AND FITTINGS.
- INSTALL SANITARY LATERAL AND CLEANOUT - REFER TO DETAIL AND "B" SERIES DRAWINGS FOR EXTENSION OF LATERAL INSIDE BUILDING.
- SANITARY LATERAL AND CLEANOUT - PROVIDED BY CITY OF BLOOMINGTON PRIOR TO START OF PROJECT.
- INSTALL 6" DUCTILE IRON SERVICE LINE
- INSTALL 4" DUAL WALL HOPE TYPE 'S' PIPE @ 0.50% SLOPE TYPEICAL AND CLEANOUT
- INSTALL 3/4" PVC PIPE
- RELOCATED LIGHT POLE
- RELOCATED SIGNAL CABINET



Prime Consultant: Architecture, Structures, Systems

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Mechanical / Electrical / Plumbing / Fire Prot

9229 Delegates Row, Ste 550 Indianapolis, Indiana 46240  
 T 317.706.2075 F 317.706.2078 I www.hargiss.com

REV DESCRIPTION DATE

New Construction of  
**Downtown Transfer Center  
 Dispatch Center**  
 Walnut St / Third St  
 Bloomington, Indiana 47401  
 Bloomington Public Transportation Corporation  
 City of Bloomington

PLAN COMMISSION FILING DOCUMENTS

Sheet Content  
 Description  
**SITE GRADING AND UTILITIES PLAN**

DATE 2012-SEPTEMBER-10

SCALE 1" = 20'

DRAWN BY DLN

CHECKED BY WSR

APPROVED BY WSR

PB Project Number 35654A

Sheet Number

C3.1



SP-31-12  
Elevations







SP-31-12  
Elevations



SP-31-12  
Elevations





SP-31-12  
Massing Model - Looking southeast

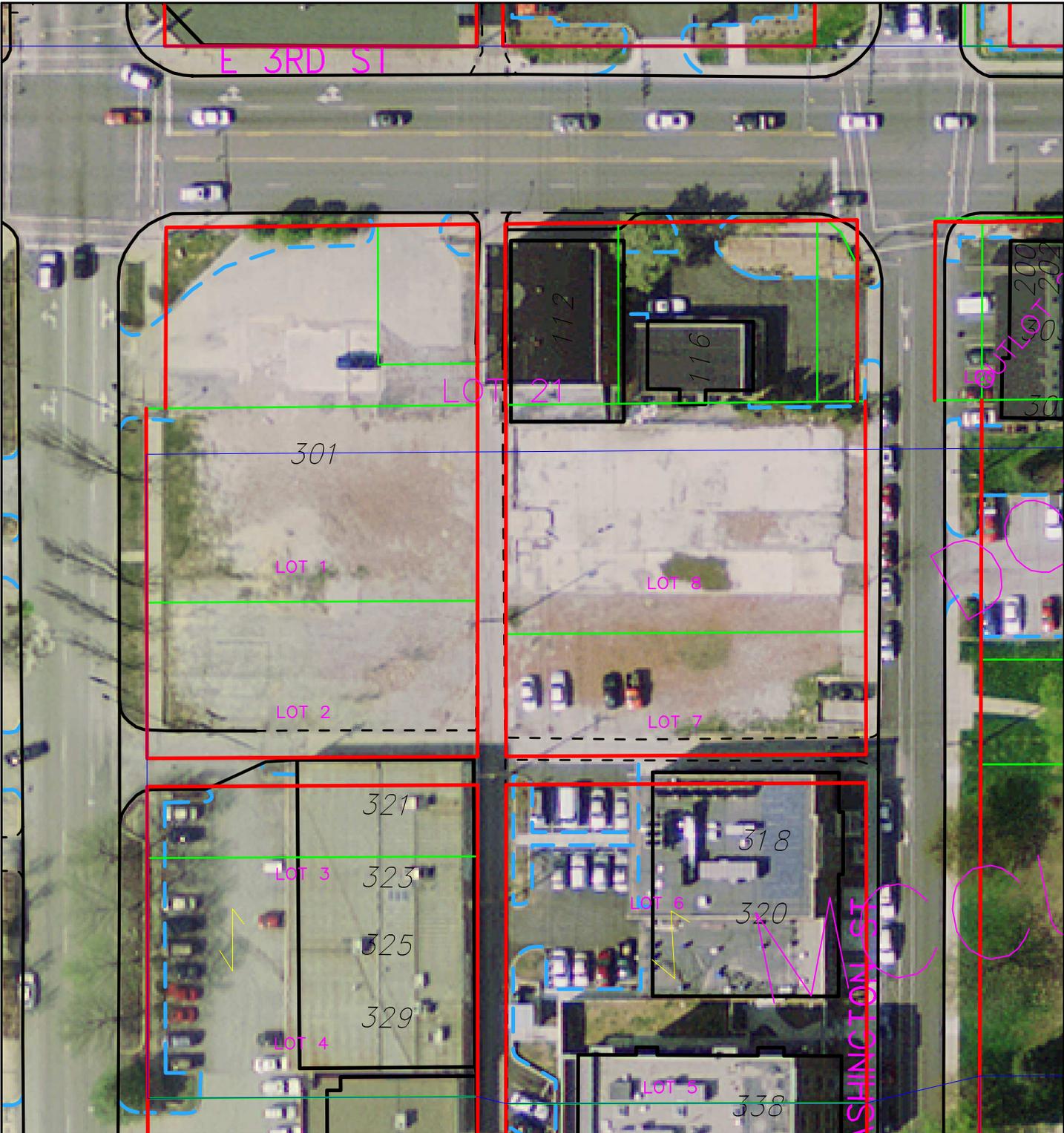


SP-31-12  
Massing Model - Looking northwest



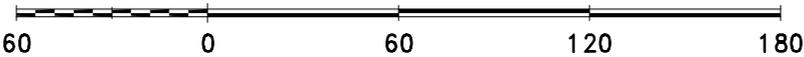
SP-31-12  
Massing Model - Looking northeast



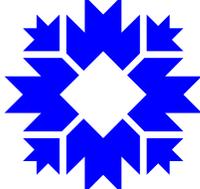


SP-31-12  
2010 Aerial Photo

By: roachja  
2 Jul 12



City of Bloomington  
Planning



Scale: 1" = 60'

For reference only; map information NOT warranted.

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**PETITIONERS: REI Investments (Hyatt Place)  
11711 N. Pennsylvania, Suite 200 Carmel 46032**

**CONSULTANT: Smith Neubecker & Associates  
453 S. Clarizz Blvd, Bloomington**

---

**REQUEST:** The petitioners are requesting site plan approval to allow construction of a 168-room hotel in the Commercial Downtown (CD) Zoning District.

---

<b>Area:</b>	<b>Approximately 0.85 Acres</b>
<b>Zoning:</b>	<b>Commercial Downtown (CD)</b>
<b>Downtown Overlay:</b>	<b>Courthouse Square Overlay (CSO)</b>
<b>GPP Designation:</b>	<b>Downtown</b>
<b>Existing Land Use:</b>	<b>Former drive-through bank</b>
<b>Proposed Land Use:</b>	<b>Hotel</b>
<b>Surrounding Uses:</b>	<b>East</b> - Bank, Commercial
	<b>South</b> - Office, Banquet Facility, Commercial, Utility
	<b>West</b> - B-Line Trail, Multi-family, Museum, Parking Lot
	<b>North</b> - Commercial

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**REPORT SUMMARY:** The petitioners are seeking an approval to redevelop a 0.85 acre parcel located on the western border of the Courthouse Square Overlay (CSO) within the Commercial Downtown (CD) zoning district. The site is bound on three sides by public streets. W. Kirkwood Avenue is located to the north of the property with W. 4<sup>th</sup> Street immediately to the south. The long narrow property also has a 20-foot right-of-way and a 12-foot alley right-of-way located immediately to the east. The alley right-of-way is currently utilized as a public sidewalk that runs in front of several mixed-use buildings located north of 4<sup>th</sup> St. and the western façade of the Chase Bank building. The 20-foot right-of-way is currently a one-way public street (S. Gentry Street) that allows only northbound traffic. The western property line is adjacent to the downtown portion of the B-Line Trail.

The site is covered with nearly 100% impervious surface excepting two small landscaped areas north and south of the existing building. The existing structure on the site is only approximately 7 feet in width and was formerly used as a drive-up bank facility. A surface parking lot and a one-way access aisle is located immediately west of the existing building. There are approximately 60 existing surface parking spaces on the site. There is also an unbuilt east-west alley right-of-way that bisects the property and runs under the building. If this request is approved, the petitioners will seek an alley vacation request to incorporate this right-of-way into the proposed site plan.

The petitioners are seeking approval to remove the existing structure and replace it with a 168-room, 130 parking space hotel (Hyatt Place). The proposed structure would range from 3 to 7 stories in height. The first floor would include the main lobby, fitness area, service areas, parking ramp, pool, as well as the on-site food service, bar, and dining area. A vehicle entrance, located at the southeast corner of the proposed building would access an internal ramp leading to the parking decks located on the second and third levels of the building. Floors 4-7 of the building would house the 168 proposed hotel rooms. To better accommodate hotel traffic, the petitioners are proposing to reverse the one way nature of Gentry Street to a southbound flow. This will allow the maximum number of cars to exit off of Kirkwood prior to entering the parking garage.

In a manner similar to the hotel recently approved between W. College Avenue and N. Morton Street along W. 9<sup>th</sup> Street (Springhill Suites by Marriott), the site has some difficulties in being redeveloped. As was the case with the other site, this site has three street frontages. This is further complicated by the presence of the B-Line Trail to the west. In designing the structure, the petitioners worked closely with staff to determine the highest priorities between the four facades in terms of height, void areas, architecture, and storefront design. The current proposal has attempted to place the greatest aesthetic emphasis and storefront design along Kirkwood Ave. and the B-Line Trail. Balancing these priorities is the necessity to create a vehicle ramp to the parking decks as well as the need to provide internal service areas and mechanical spaces.

Two of the main questions for the Plan Commission with this request are that of height and massing of the proposed hotel. Downtown hotels are a desirable land use that have traditionally received several waivers from the Plan Commission. They have unique architectural, mechanical, parking, and height challenges that many other uses do not have. In evaluating this petition, staff has found it useful to compare this proposal to two other downtown hotel projects, the recently approved Springhill Suites proposal and the constructed Hilton Garden Inn:

- Hyatt Place – 168 Rooms
- Springhill Suites – 155 Rooms
- Hilton Garden Inn – 168 Rooms
  
- Hyatt Place - ~65 feet in height
- Springhill Suites – 71 feet (57' on College)
- Hilton Garden Inn – 67 feet
  
- Hyatt Place – 130 parking spaces
- Springhill Suites – 133 parking spaces
- Hilton Garden Inn – secures spaces within Garage Market

Staff finds these numbers to be very consistent. Although the Springhill Suites site is located within a different overlay, the Hilton Garden Inn is also located within the Courthouse Square Overlay. All three have utilized structured parking in some fashion. Staff finds the petitioners' proposal to utilize upper levels of the structure to be more

desirable than utilizing street level structured parking.

**Plan Commission Site Plan Review:** Three aspects of the proposal require the Plan Commission to hear this petition for site plan approval. These aspects are:

- The project includes more than 25,000 square feet of non-residential space
- The proposal is adjacent to a residential use
- The petitioners are requesting waivers to the standards in BMC 20.03.050 & 20.03.060. The following waivers are being requested:
  - Building Height
  - B-Line Trail Setback
  - Street tree waiver on Gentry Street
  - Void-to-Solid Ratio
  - Window Design
  - Vertical/Horizontal Design (Cap)
  - Building Modulation

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## **SITE PLAN REVIEW DEVELOPMENT STANDARDS**

- *Maximum Impervious Surface:* The petitioners' project would be permitted to cover as much as 100% of the site with impervious surface. As submitted, the proposal would utilize 100% of the site for the structure.
- *Height:* As measured by the UDO, the proposed hotel stands approximately 65 feet in height. This exceeds the overlay standard of 40 feet. As proposed, this structure is slightly lower than the Hilton Garden Inn and the recently approved Springhill Suites. Most importantly, the height of the building has been significantly lowered along the B-Line Trail. **A waiver from this standard is requested.**
- *Parking:* As a non-residential use within the CSO, the petitioners are not required to install any parking spaces for this development. The UDO parking maximum for a hotel is one parking space per lodging unit. The petitioners are proposing to have 130 parking spaces for their 168 rooms. Unlike most uses in the downtown, staff finds parking for a hotel use to have a larger inherited need over other potential uses. The petitioners are proposing these 130 spaces within 2 levels of structured parking located on the second and third floors of the hotel that would be accessed from a single drive cut onto Gentry St.

With this proposal, the number of public parallel street spaces would also increase due to the removal of several drive cuts onto Kirkwood and 4<sup>th</sup> Streets.

- *Setbacks:* The building is required to be placed at the right-of-way line for all street frontages. The petitioners have met these requirements. Although the western sideyard setback is also 0 feet, the CSO calls for an additional 10-foot setback from

the B-Line Trail right-of-way. The petitioners are requesting a waiver from this standard to allow the building to remain at 0 feet. This request is due to the narrow width of the property and the desire to utilize the most efficient parking design within the building. To help minimize the impacts of this reduced setback, the petitioners have proposed that the height for the main mass of the building against the trail to be between 20 and 23 feet in height, lower than the 40 feet allowed in the district. With the larger trail right-of-way for this area, the interaction proposed between the hotel and the trail, and the reduced massing along the trail, staff is supportive of this reduction. ***A waiver from this standard is required.***

- ***Bicycle Parking:*** The petitioners are required to place a minimum of four covered bicycle parking spaces. Although the required number of bicycle parking spaces has been shown along Kirkwood Ave., staff recommends that the petitioners continue to work with staff to find additional bicycle parking opportunities prior to a second hearing. Due to the size of this structure, all of the provided spaces must be covered. This can be achieved with a canopy or by moving the required spaces to an internal room within the building. Staff recommends that additional spaces be added to the plan and will work with the petitioner to locate appropriate placements prior to the second hearing.

***ARCHITECTURAL STANDARDS:*** Architectural elevations and a color model of the proposed structure have been included in your packet.

- ***Building Frontage, Alignment, Orientation and Entrances:*** The structure is proposed to be constructed with a building forward design that fills all street frontages. The CSO requires that 90% of a building facade be placed at the build to line of 0 feet. Although there are surveyed historic structures located across Gentry St. and Kirkwood Ave., there are no adjacent surveyed structures to this site.

The petitioners have designed the hotel building with several entrances. The CSO would require a minimum of one entry on all three streets and two pedestrian entries onto the B-Line Trail. The petitioners are continuing to develop the floor plan to meet these requirements as well as all entrance and detailing requirements.

- ***Streetscape:*** The petitioners will be improving the existing sidewalk/street tree area along both Kirkwood Ave. and 4<sup>th</sup> St. in a manner consistent with the W. Kirkwood Streetscape and to comply with the UDO standards. These improvements include sidewalk reconstruction, brick pavers, parallel parking, ADA ramp upgrades, tree grates and pedestrian lighting. There are no existing street trees along any of the adjacent frontages.

The current Gentry St. right-of-way is very narrow (20 feet) and does not have adequate width to allow fire access, sidewalk, and street trees within the right-of-way. Due to the narrow nature and its proximity to College Avenue, Gentry St. functions much as an alley. The petitioners are essentially seeking an approval to treat it more in this manner with no sidewalk or street trees along the eastern

façade. This would require a waiver of street trees and a variance from sidewalk requirements by the Board of Zoning Appeals or the Hearing Officer. Staff would note that pedestrian movements along Gentry St. are adequately served by the existing sidewalk on the east side of Gentry within the alley right-of-way. **A waiver from this standard is required.**

- *Lighting:* The UDO requires that the petitioners utilize pedestrian scale lighting of a maximum 15 feet in height. The DCO allows either traditional or contemporary style lights to be used. Some of these lights have been shown on Kirkwood Ave, and must be consistent with the other existing lights along Kirkwood Ave. Staff will continue to work with the petitioners to develop a street light plan for 4<sup>th</sup> St. as well as the addition of wall mounted pedestrian scale lights on the Gentry St. façade.
- *Mechanicals:* The UDO requires all mechanicals to be screened through a combination of physical barriers and location. The petitioners will achieve these screening requirements by placing a majority of mechanicals on the roof. Staff would like to see more details regarding mechanical placement prior to second hearing.
- *Void-to-Solid Percentage* – The CSO requires all street facades to maintain a first floor void-to-solid ratio of 70% and upper floors to maintain a 20% ratio. The proposed hotel building does not meet these standards. The petitioners have found this standard to be difficult to meet as they do not have a “rear” to this building. There are three street frontages and a fourth front facing the B-Line Trail. When combined with the need for an internal ramp to accommodate the structured parking on the second and third floors and the large amount of service area associated with a hotel, staff finds this standard to be impractical. With that in mind, staff has directed the petitioners to focus the “store fronts” to the north and the west toward Kirkwood Ave. and the B-Line Trail. Although, the petitioners do not meet this standard, staff finds that they have designed the structure with an appropriate amount of void-to-solid in line with other downtown hotels and buildings. Where true storefront glass is not possible, the petitioners have incorporated spandrel glass and are working with staff to develop an art component for the blank walls facing the trail and 4<sup>th</sup> St. **A waiver from this standard is requested.**
- *Windows:* As proposed, the upper level windows do not meet the minimum 1.5 to 1 window height to width ratio. There is also spandrel glass with this structure to minimize the amount of blank wall on the more functional spaces of the building. A waiver is also required to allow a more modern structure with no sills or lintels on upper windows. **A waiver from these standards is requested.**
- *Materials:* Although the petitioners are continuing to develop the material details, they are proposing a building with a mix of limestone, brick, stucco, glass, and a newer material called Terra Neo. Staff would like the Plan Commission to discuss the proposed building materials at the first hearing.
- *Additional Design Elements:* Due to the style of building chosen, the building does

not have a traditional building cap. However, it does utilize color and material changes on the top floor to achieve many of the goals of the required cap, while maintaining a more modern appearance. ***A waiver from this standard is requested.***

**MASS, SCALE, AND FORM:**

- ***Building Façade Modulation:*** The UDO requires building offsets to be 3% of the total façade length facing a street. This would require approximately 4 feet of modulation along 4<sup>th</sup> St. and Kirkwood Ave. and slightly more than 8 feet along Gentry St., with maximum module widths of 50 feet. Due to the parking layout and the modern design of the building, the petitioners are not proposing traditional modules. However, they have incorporated significant articulation in the building with a large step down to the adjacent trail, roofline variation, material variation, and a glass corner element. ***A waiver from this standard is requested.***

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**ADDITIONAL SITE PLAN ISSUES:**

**Transit:** Transit service was analyzed with this petition. Transit has two routes that use 4<sup>th</sup> St. There are several other routes within one block of the petition site. Prior to the second hearing, staff will work with Bloomington Transit to determine if an additional shelter is necessary along 4<sup>th</sup> Street.

**Entrance Standards:** The petitioners have proposed to place the entrance to the garage off Gentry St. within the minimum setback of 100 feet from 4<sup>th</sup> St. When combining the 150-foot setback from Kirkwood Ave. and the 100-foot setback from 4<sup>th</sup> Street, there is only a 26-foot area that an entrance could meet UDO standards. With the internal parking decks, the ramp must be located near one end of the building. Both the petitioners and staff would prefer this ramp to be located closer to 4<sup>th</sup> Street than Kirkwood Ave. This location will need a variance from the Board of Zoning Appeals or the Hearing Officer. Staff is supportive of this variance request.

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**ENVIRONMENTAL COMMISSION:** The Environmental Commission reviewed this petition and offered the following recommendation:

1. The EC recommends designing a sustainable building that makes Bloomington proud by using state-of-the-art green building practices. Ideally, the Hyatt Place should be a LEED Platinum green building, certified by the Leadership in Energy and Environmental Design Green Building Rating System.

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**DEVELOPER TRACK RECORD:** REI Investments has previously worked with the City of Bloomington in the development of the Hilton Garden Inn project.

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**SUMMARY:** Staff is supportive of facilitating the design of a new hotel at this location. Staff also finds that the general massing and layout of the building is appropriate for this location. Prior to the second hearing staff would like additional guidance from the Plan Commission

regarding the proposed materials and waivers. In particular, staff would like to discuss the 4<sup>th</sup> St. façade and ensuring maximum interaction along the B-Line Trail.

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**RECOMMENDATION:** This project will be heard and receive a recommendation at the October 8, 2012 Plan Commission meeting.

# MEMORANDUM

**Date:** August 30, 2012  
**To:** Bloomington Plan Commission  
**From:** Bloomington Environmental Commission  
**Through:** Linda Thompson, Senior Environmental Planner  
**Subject:** SP-36-12: Hyatt Place  
217 W. Kirkwood Ave.

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This memorandum contains the Environmental Commission's (EC) input and recommendations regarding the request for a Site Plan approval for a hotel within the Commercial Downtown Zoning District and the Courthouse Square Overlay District. Unfortunately, given the odd shape of the lot and its location, there is no room or requirement for greenspace. The petitioner is requesting a waiver from the UDO for street trees along Gentry Street, and a waiver from the 10 foot building setback requirement from the B-Line Trail that could have accommodated landscaping. Therefore, the EC believes it is reasonable that the building itself be state-of-the-art architecturally and environmentally.

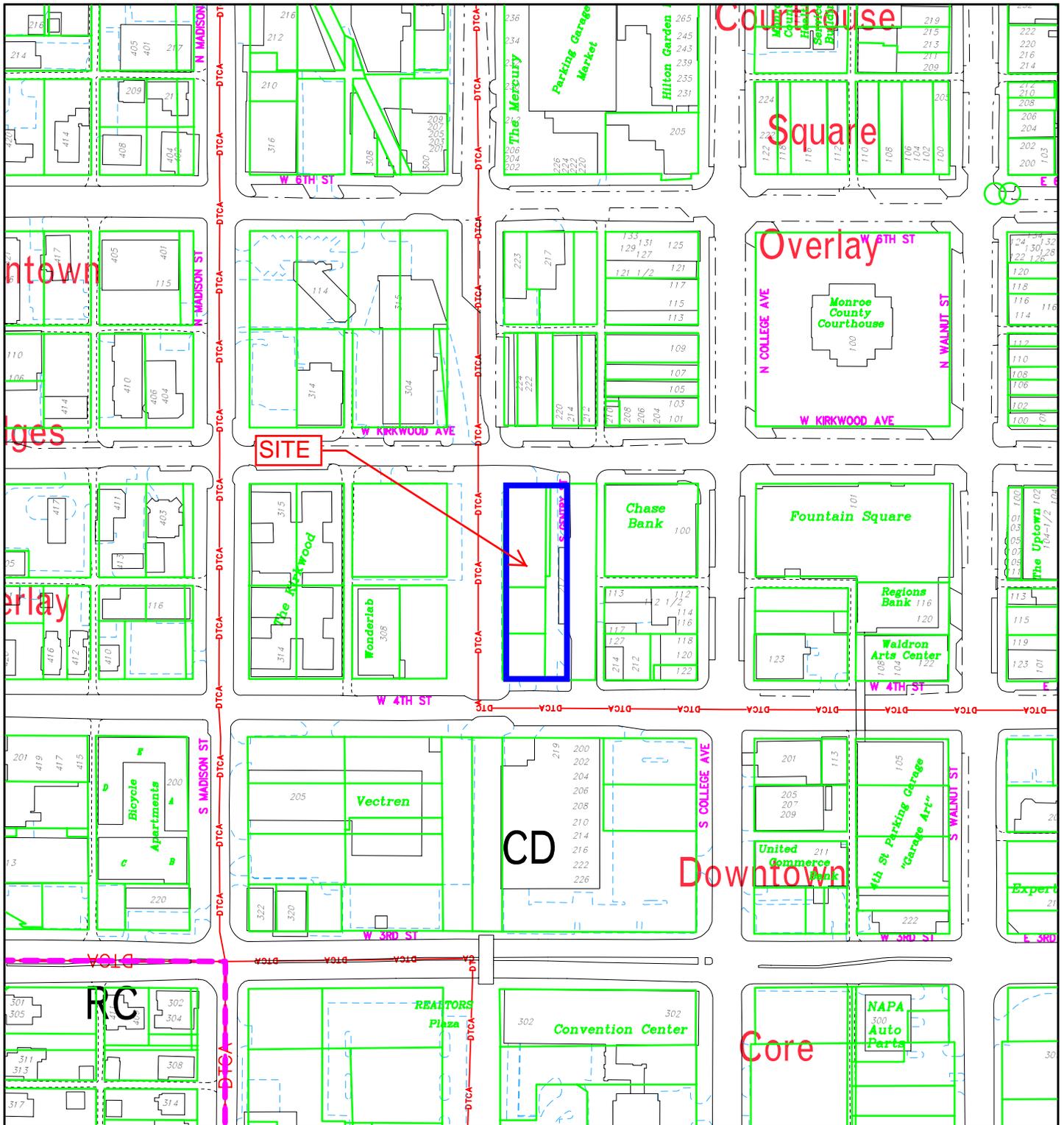
This hotel is proposed on a prominent in-fill location in the heart of downtown that is prime for both a beautiful and sustainable building. It is within the Bloomington Entertainment and Arts District (BEAD), therefore the building should be an object of art itself. Green building and environmental stewardship are of utmost importance to the people of Bloomington and sustainable features are consistent with the spirit of the Unified Development Ordinance (UDO). Additionally, they are supported by Bloomington's overall commitment to sustainability and its green building initiative (<http://Bloomington.in.gov/greenbuild>). Sustainable building practices are explicitly called for by the Mayors' Climate Protection Agreement signed by Mayor Kruzan, by City Council resolution 06-05 supporting the Kyoto Protocol and reduction of our community's greenhouse gas emissions, by City Council resolution 06-07, which recognizes and calls for planning for peak oil, and by Redefining Prosperity: Energy Decent and Community Resilience Report of the Bloomington Peak Oil Task Force.

A few sustainable features that the EC recommends using on this building specifically include light-emitting diode (LED) lighting throughout, but especially outside to match the city's efforts along the B-Line Trail to conserve energy consumption; a "cool roof" to reduce the urban heat island effect, energy use, and carbon emissions. (For additional information on cool roofs please see Adapting to Urban Heat: A Tool Kit for Local Governments published by the Georgetown Climate Center <http://www.icleiusa.org/blog/archive/2012/08/27/georgetown-climate-center-releases-new-tool-kit-to-help-local-governments-adapt-to-record-heat>); solar-powered energy; and electric car power outlets in the parking garage. For some examples of what other hotels

practice for environmental stewardship see Green Lodging News at <http://www.greenlodgingnews.com/> , Proximity Hotel, the nation's first LEED Platinum "green hotel" that follows the guidelines of the Leadership in Energy and Environmental Design (LEED) Green Building Rating System <http://www.proximityhotel.com/green.htm> , or Ecogreen Hotel at <http://www.ecogreenhotel.com/index.php> .

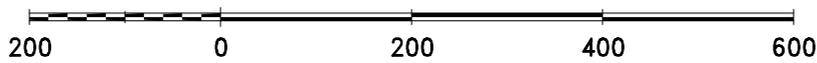
### **RECOMENDTIONS:**

1. The EC recommends designing a sustainable building that makes Bloomington proud by using state-of-the-art green building practices. Ideally, the Hyatt Place should be a LEED Platinum green building, certified by the Leadership in Energy and Environmental Design Green Building Rating System.



SP-36-12 REI Investments  
 217 W. Kirkwood Avenue  
 Location/Zoning/Land Use Map

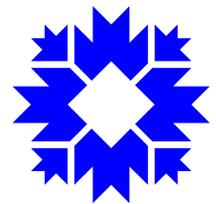
By: shapp  
 3 Aug 12



For reference only; map information NOT warranted.



City of Bloomington  
 Planning



Scale: 1" = 200'



*"Providing professional land planning, design, surveying and approval processing for a quality environment."*

Stephen L. Smith P.E., L.S.  
Daniel Neubecker L.A.  
Steven A. Brehob, B.S.Cn.T.

August 10, 2012

City of Bloomington Plan Commission  
C/o Pat Shay  
Planning Department  
Showers Building  
Bloomington, Indiana

Re; Hyatt Place Site Plan Application

Dear Pat and Commissioners,

After making early contacts with the City Administration, multiple meetings with City staff, other community leaders and tweaking/refinement of the proposal, REI Investments is pleased to submit for site plan approval for a new downtown Hyatt Place hotel. The seven story hotel will face West Kirkwood Avenue and also front on the B-line, Gentry Street and Fourth Street. The hotel is just one half block from the courthouse square and one block from the convention center.

The following items are a part of this application (printed copy or e-mail);

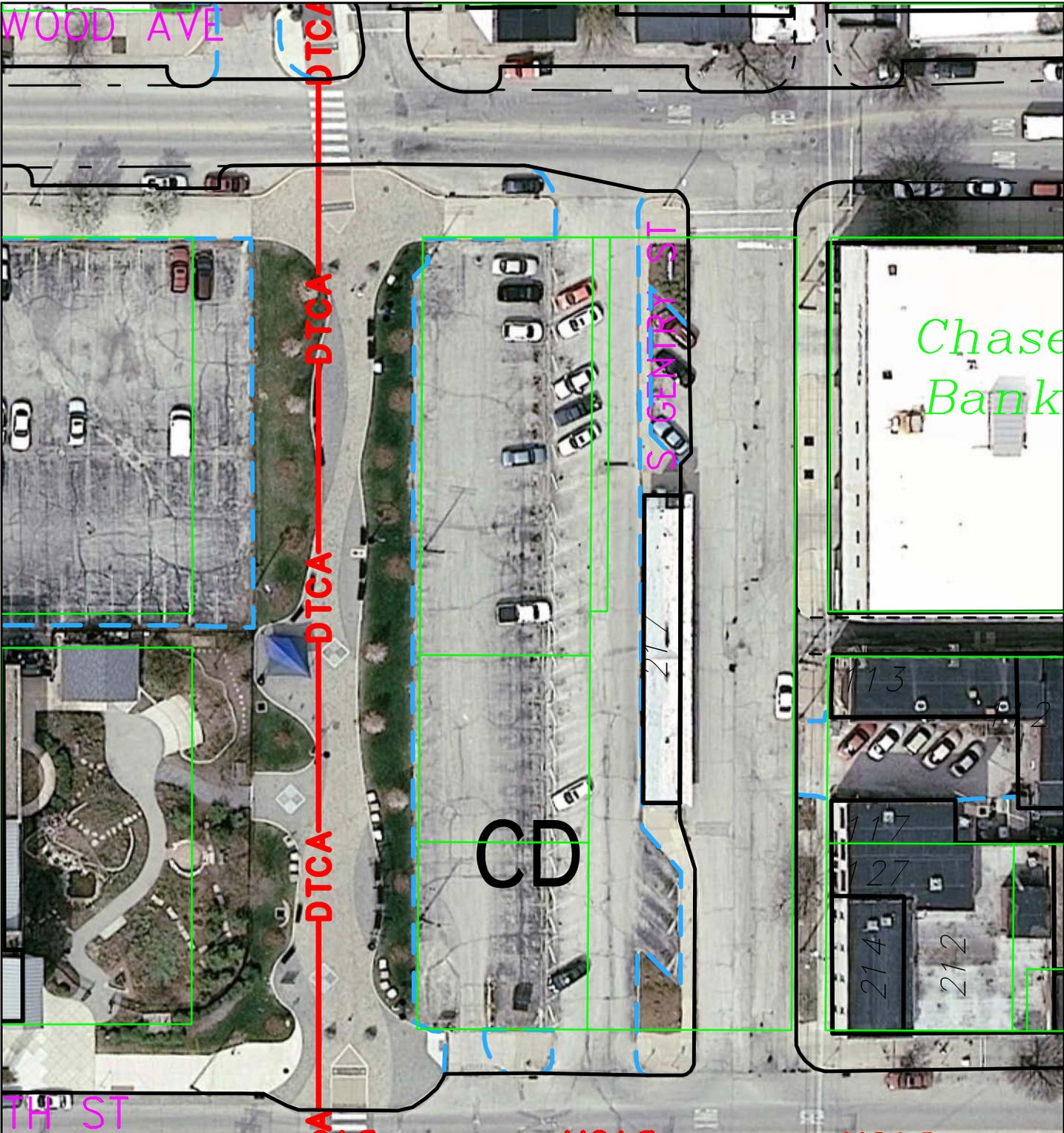
- Preliminary architectural floor plans and perspectives
- Civil site drawings
- 3D computer model of the hotel and its surroundings
- Building elevations
- Materials detail/descriptions
- Application form
- Application fee

Thank you for all of your assistance getting the project to this application stage. We look forward to working with you as we proceed through the formal hearing and approval process.

Very truly yours,

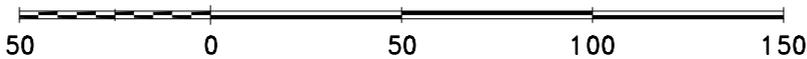
A handwritten signature in blue ink, appearing to be 'S. L. Smith', written over a faint, larger version of the same signature.

Stephen L Smith  
Engineer for;  
REI Investments; Bloomington Hyatt Place

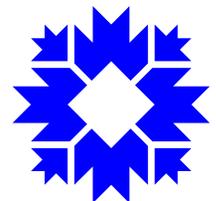


SP-36-12 Hyatt Place  
Aerial Photo

By: shayp  
3 Aug 12

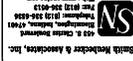


City of Bloomington  
Planning



Scale: 1" = 50'

For reference only; map information NOT warranted.



217 W. KIRKWOOD AVE.  
HYATT PLACE  
JOB TITLE

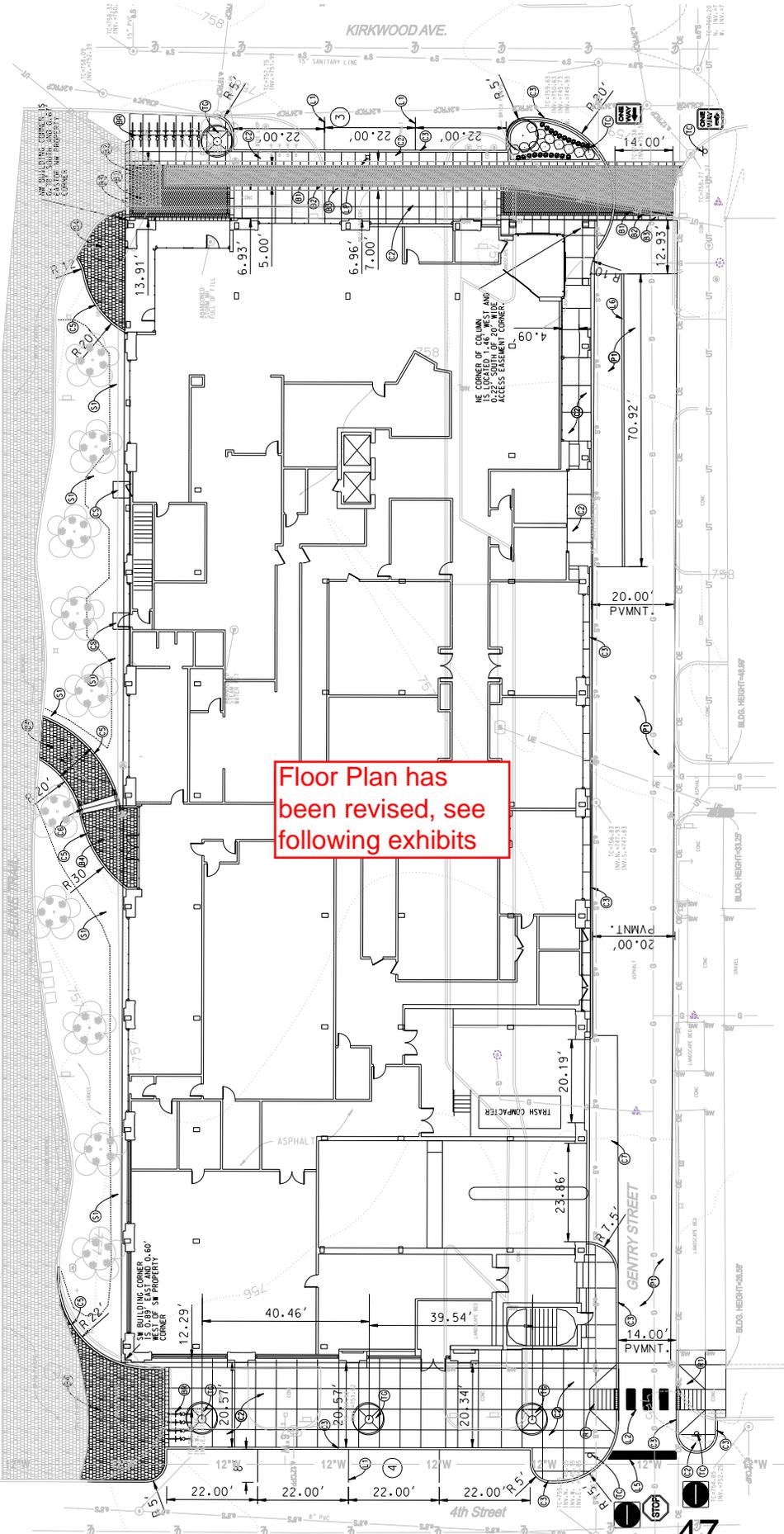
NO.	BY	DATE	REVISIONS

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DESIGNED BY: S.A.B.		
CHECKED BY: R.O.B.		
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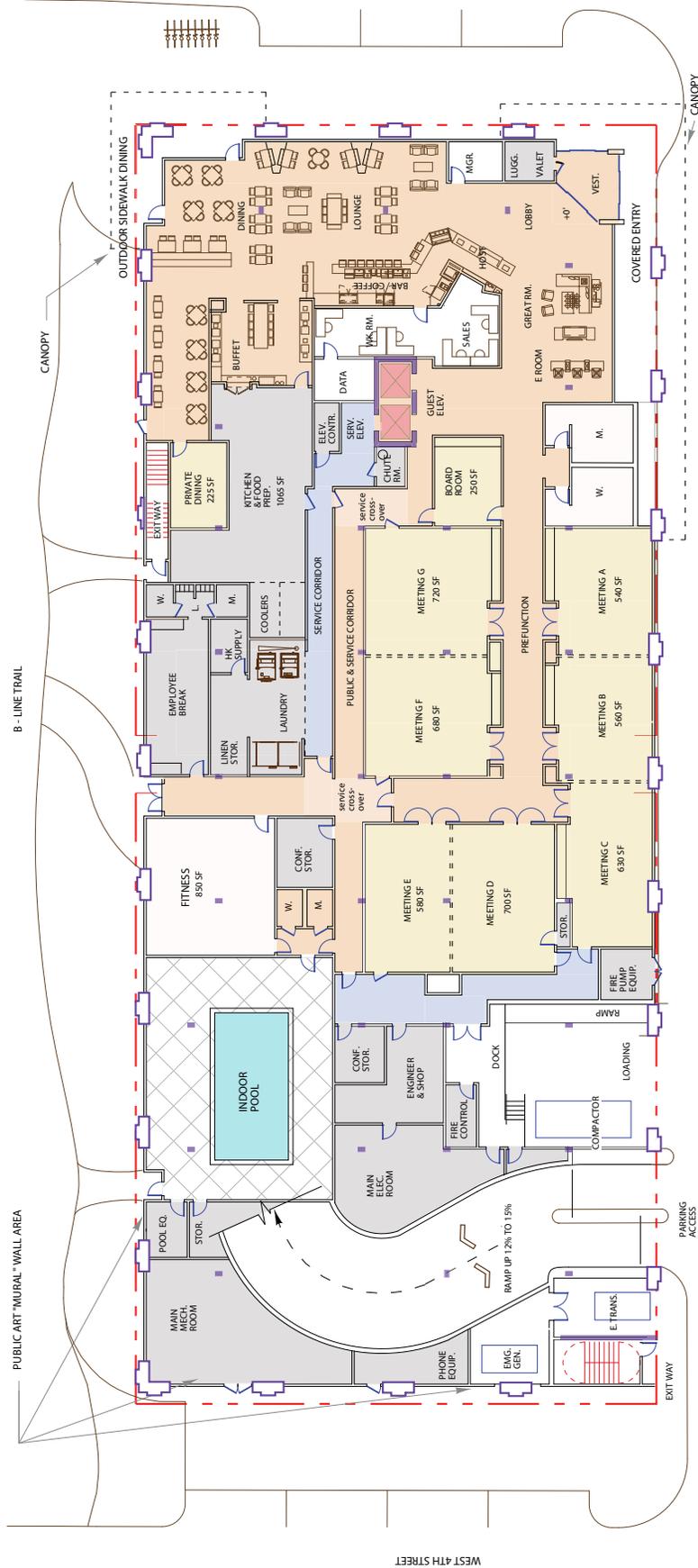
- GENERAL NOTES**
1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE IBC AND ALL APPLICABLE LOCAL ORDINANCES.
  2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF ST. LOUIS.
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- SITE PLAN NOTES**
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- SITE LEGEND**
- 1. ASPHALT
  - 2. CONCRETE
  - 3. GRAVEL
  - 4. SAND
  - 5. SOIL
  - 6. FILL
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- MATERIALS LEGEND**
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Floor Plan has been revised, see following exhibits

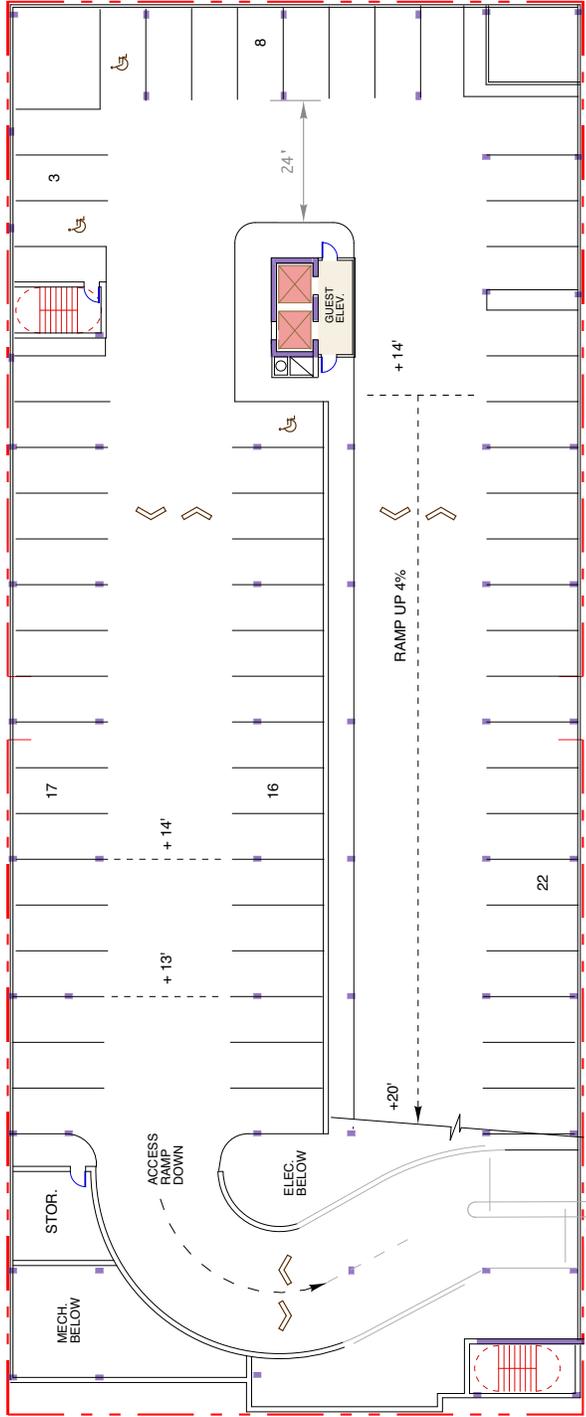


**BUILDING DATA:**

LEVEL	BUILDING SF	PARKING SF	PARKING RAMP	No. of CARS	No. of KEYS
LEVEL 1	24,800	2,200	25,800	66	0
LEVEL 2	800	22,450	-	-	0
LEVEL 3	800	-	-	-	42
LEVEL 4	21,100	-	-	-	42
LEVEL 5	21,100	-	-	-	42
LEVEL 6	21,100	-	-	-	42
LEVEL 7	21,100	-	-	-	42
<b>TOTALS:</b>	<b>110,800</b>	<b>50,250</b>	<b>130</b>	<b>168</b>	<b>659 SF / KEY</b>

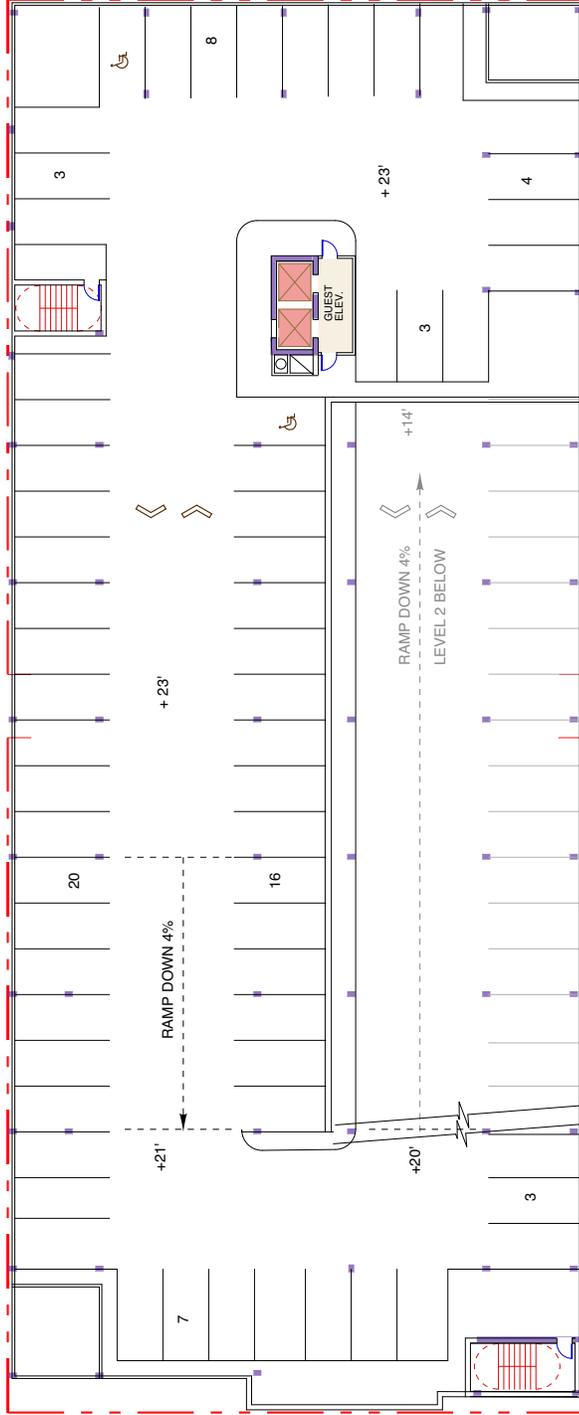


**SP-36-12 First Floor - Floor Plan**



66 CARS - LEVEL 2  
 PARKING LEVEL 2

BUILDING DATA:	BUILDING SF	PARKING SF	No. of CARS	NO. of KEYS
LEVEL 1	24,800	2,200 RAMP	0	0
LEVEL 2	800	25,800	66	0
LEVEL 3	800	22,450	64	0
LEVEL 4	21,100	-	-	42
LEVEL 5	21,100	-	-	42
LEVEL 6	21,100	-	-	42
LEVEL 7	21,100	-	-	42
TOTALS:	110,800	50,250	130	168
				659 SF / KEY



**BUILDING DATA:**

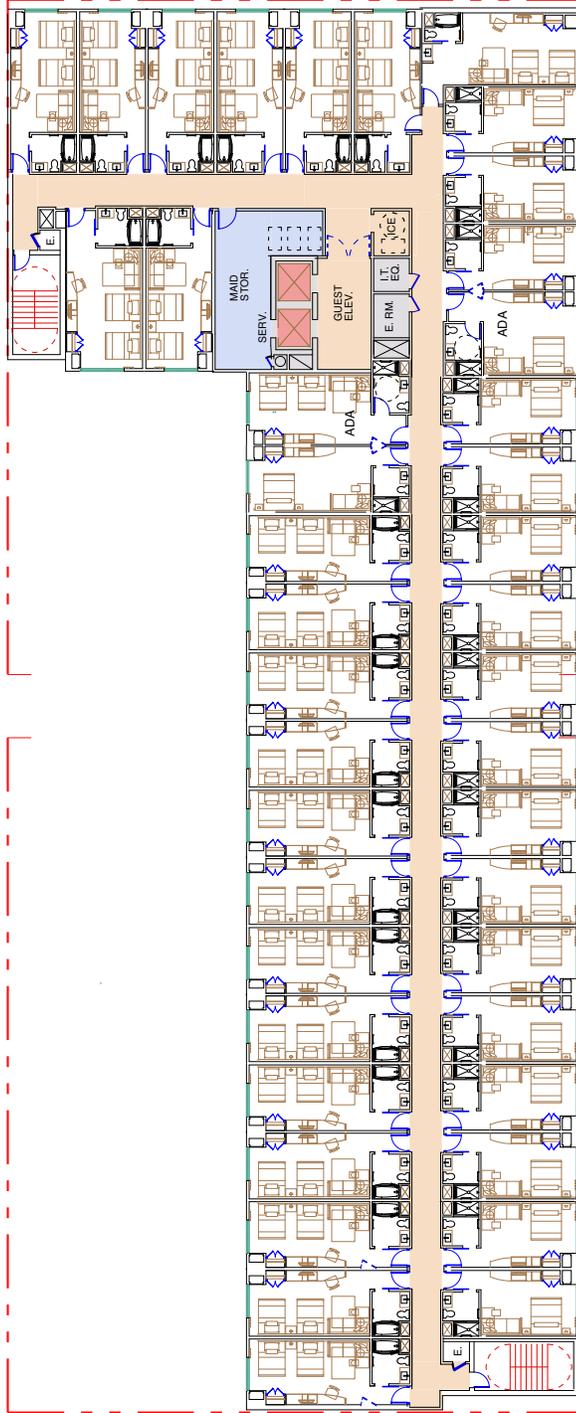
LEVEL	BUILDING SF	PARKING SF	2,200 RAMP	No. of CARS	NO. of KEYS
LEVEL 1	24,800			0	0
LEVEL 2	800	25,800		66	0
LEVEL 3	800	22,450		64	0
LEVEL 4	21,100	-	-	-	42
LEVEL 5	21,100	-	-	-	42
LEVEL 6	21,100	-	-	-	42
LEVEL 7	21,100	-	-	-	42
<b>TOTALS:</b>	<b>110,800</b>	<b>50,250</b>		<b>130</b>	<b>168</b>
					<b>659 SF / KEY</b>

PARKING LEVEL 3



WHITE LODGING

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LEVEL 4 - 7

BUILDING DATA:	BUILDING SF	PARKING SF	2,200 RAMP	No. of CARS	No. of KEYS
LEVEL 1	24,800			0	0
LEVEL 2	800	25,800		66	0
LEVEL 3	800	22,450		64	0
LEVEL 4	21,100	-		-	42
LEVEL 5	21,100	-		-	42
LEVEL 6	21,100	-		-	42
LEVEL 7	21,100	-		-	42
TOTALS:	110,800	50,250		130	168
					659 SF / KEY

MATERIAL LEGEND

- (A) SIMULATED FLAME FINISHED GRANITE (TERRA NEO)
- (B) 1" HIGH PERFORMANCE INSULATED GLASS/STOREFRONT SYSTEM
- (C) KING BRICKS (COLOR 2)
- (D) HARD COAT STUCCO - COLOR 1
- (E) HARD COAT STUCCO - COLOR 2
- (F) MASONRY - KING BRICKS WITH CAST-STONE ACCENTS (BRICK COLOR 1)
- (G) LIMESTONE - SMOOTH CUT FINISH

Garage Entrance



East Elevation

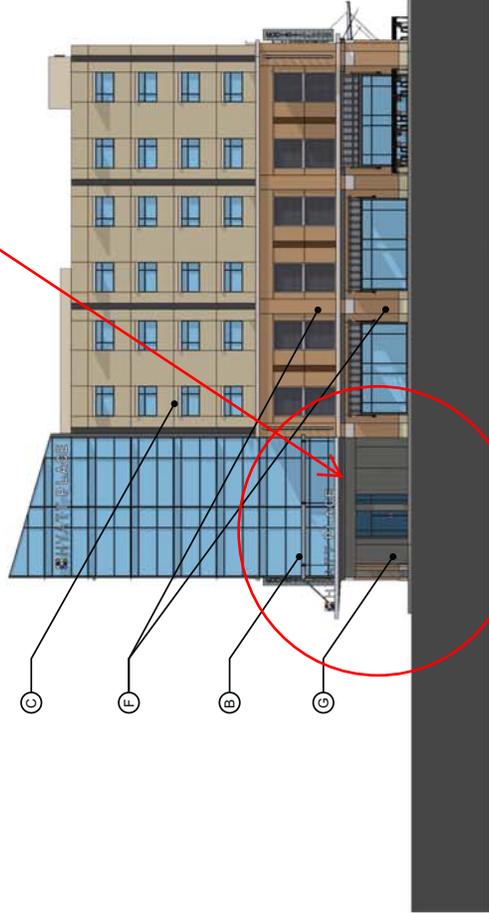
SP-36-12

MATERIAL LEGEND

- (A) SIMULATED FLAME FINISHED GRANITE (TERRA NEO)
- (B) 1" HIGH PERFORMANCE INSULATED GLASS/STOREFRONT SYSTEM
- (C) KING BRICKS (COLOR 2)
- (D) HARD COAT STUCCO - COLOR 1
- (E) HARD COAT STUCCO - COLOR 2
- (F) MASONRY - KING BRICKS WITH CAST-STONE ACCENTS (BRICK COLOR 1)
- (G) LIMESTONE - SMOOTH CUT FINISH

Main Entry

B-Line Trail



North Elevation

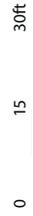
**MATERIAL LEGEND**

- (A) SIMULATED FLAME FINISHED GRANITE (TERRA NEO)
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- (C) KING BRICKS (COLOR 2)
- (D) HARD COAT STUCCO - COLOR 1
- (E) HARD COAT STUCCO - COLOR 2
- (F) MASONRY - KING BRICKS WITH CAST-STONE ACCENTS (BRICK COLOR 1)
- (G) LIMESTONE - SMOOTH CUT FINISH

**B-Line Trail Entrance**



West Elevation



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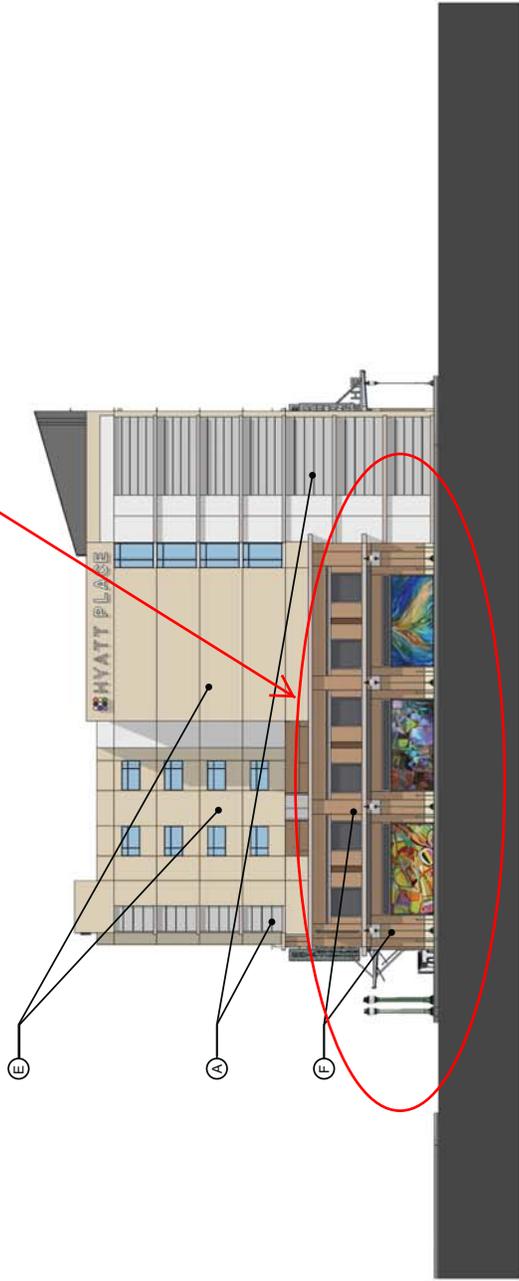


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© DESIGN COPYRIGHT 2012

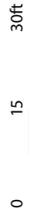
MATERIAL LEGEND

- (A) SIMULATED FLAME FINISHED GRANITE (TERRA NEO)
- (B) 1" HIGH PERFORMANCE INSULATED GLASS/STOREFRONT SYSTEM
- (C) KING BRICKS (COLOR 2)
- (D) HARD COAT STUCCO - COLOR 1
- (E) HARD COAT STUCCO - COLOR 2
- (F) MASONRY - KING BRICKS WITH CAST-STONE ACCENTS (BRICK COLOR 1)
- (G) LIMESTONE - SMOOTH CUT FINISH

Art Component



South Elevation



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#10027 - 0/5/12  
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SP-36-12  
SW from Kirkwood  
and Gentry



SE from Kirkwood



NE from 4th St

**P.F.V.S.**  
ARCHITECTURE  
PLANNING  
INTERIORS

4/10/22 9:57:12  
© DESIGN COPYRIGHT 2012

HYATT PLACE · Bloomington, Indiana

**W**  
WHITE LODGING

**REI**  
REAL ESTATE SERVICES

Perspective View Looking North 13



NW from 4th St

**P.F.V.S.**  
ARCHITECTURE  
PLANNING  
INTERIORS

4/10/12 9:57:12  
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**WILHELM LODGING**



Perspective View Looking North



Pedestrian entry from B-Line Trail





**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT**

**CASE #: ZO-38-12  
DATE: Sept. 10, 2012**

**Location: 718 E. 8<sup>th</sup> Street, 702 E. 10<sup>th</sup> Street, 525 N. Park Avenue, 514 N. Fess Avenue, 403 E. 6<sup>th</sup> Street, 613 E. 12<sup>th</sup> Street**

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**PETITIONER:** City of Bloomington  
401 N. Morton Street

---

**REQUEST:** The petitioner, the City of Bloomington, is requesting the rezone of six individual properties located within the Old Northeast Neighborhood.

---

**Specific Property Characteristics:**

**Address:** 718 E. 8<sup>th</sup> Street  
**Current Zoning:** IN  
**Proposed Zoning:** RM  
**Area:** 0.18 acres  
**GPP Designation:** Core Residential

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**Address:** 702 E. 10<sup>th</sup> Street  
**Current Zoning:** IN  
**Proposed Zoning:** RM  
**Area:** 0.092 acres  
**GPP Designation:** Core Residential

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**Address:** 525 N. Park Avenue  
**Current Zoning:** IN  
**Proposed Zoning:** RM  
**Area:** 0.150 acres  
**GPP Designation:** Core Residential

---

**Address:** 514 N. Fess Avenue  
**Current Zoning:** IN  
**Proposed Zoning:** RM  
**Area:** 0.150 acres  
**GPP Designation:** Core Residential

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**Address:** 403 E. 6<sup>th</sup> Street  
**Current Zoning:** IN  
**Proposed Zoning:** CD  
**Area:** 0.200 acres  
**GPP Designation:** Downtown

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**Address:** 613 E. 12<sup>th</sup> Street  
**Current Zoning:** IN  
**Proposed Zoning:** CG  
**Area:** 0.200 acres

## **GPP Designation: Core Residential**

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**Background:** This request stems from a previous rezoning case (ZO-27-11) in which the Plan Commission denied the case, but requested the planning staff reevaluate the Old Northeast neighborhood for possible rezonings. In that case, the property owner of 718 E. 8<sup>th</sup> Street requested that the zoning of the property be taken back to the previous Residential Multifamily (RM) zoning. Specifically, the Plan Commission directed staff to assess private versus public ownership in the neighborhood, review the GPP designations in the area, and make a more comprehensive rezoning proposal. At the conclusion of this analysis, staff identified 6 properties that it believes should be rezoned.

The Planning Staff has been in contact with each of the subject property owners to listen, answer questions, and confirm their desired zoning requests. Planning staff attended the May 2012 Old Northeast Neighborhood Association meeting. At this meeting attendees were shown zoning maps of the neighborhood both prior to 2007 and the current map. Planning Staff explained the rezoning petition and answered questions from the attending neighbors. As part of this process, Planning Staff also met with representatives from Indiana University and informed them of the rezoning request. The representatives from Indiana University were pleased to be included in the conversation and had no concerns.

**Report:** The first four properties listed, 718 E. 8<sup>th</sup> Street, 702 E. 10<sup>th</sup> Street, 525 N. Park Avenue, and 514 N. Fess Avenue are residential structures with residential uses. All are registered rentals with the exception of 514 N. Fess Avenue which is owner occupied. Staff recommends that their zoning be taken back to the previous 2007 zoning of Residential Multifamily (RM).

The fifth property listed, 403 E. 6<sup>th</sup> Street, is a professional law office use. The zoning previous to the 2007 rezone was Commercial General (CG). Staff recommends that the property be rezoned to Commercial Downtown (CD) to bring the current use into conforming standards and to allow the property to be zoned the same as the adjacent properties to the south.

The sixth property listed, 613 E. 12<sup>th</sup> Street, is a registered rental residence that was previously used as an art studio. Previous to the 2007 rezone, the property was zoned General Commercial (CG) to reflect the art studio use as well as the proximity to the neighborhood's Village Pantry convenience store. Although the GPP designates the property as Core Residential, staff recommends rezoning to commercial to be consistent with the 2007 zoning map and previous use of the property. Because the property is a small platted lot, there is no potential for negative impacts associated with future commercial development.

**Rationale for Previous Zoning:** In 2007, as part of the City's creation of the Unified Development Ordinance as well as the update of the City-wide zoning map, all six of these properties were rezoned to Institutional (IN). This zoning change was made for two principal reasons:

- 1) The properties are located in an area designated by Indiana University as part of

its campus master plan.

- 2) Between 6<sup>th</sup> Street, Indiana Avenue, 12<sup>th</sup> Street, and Woodlawn Avenue, Indiana University currently owns the bulk of the properties.

In 2007, given that all six properties are located within the University's Master Plan area, the University is the dominant lot owner in this section of the Old Northeast Neighborhood, and the University is the prevailing owner on many individual block faces, Planning Staff opted to zone these properties Institutional rather than Multifamily, Downtown, and General Commercial. Given the concern expressed by several individual owners, the neighborhood association, and Plan Commissioners, staff is essentially recommending that zoning be modified to reflect the pre-2007 map.

**GROWTH POLICIES PLAN:** The GPP designates five of these properties as Core Residential. The property located at 403 E. 6<sup>th</sup> Street is designated as Downtown. The area within the western edge of the IU Campus Master Plan and the eastern edge of the Old Northeast Neighborhood has always been a fine grained mix of public versus private ownership, private rental property and IU rentals, and a mix of institutional style buildings and older historic homes. With the University's ownership in the area gradually increasing over the years, determining Institutional versus Multifamily zoning has been a difficult zoning decision to make for the Planning staff. Ultimately, the proposed zoning back to commercial and multifamily is more consistent with the GPP's land use recommendations for this core neighborhood.

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**RECOMMENDATION:** Staff recommends approval of the proposed rezone of the six properties.

August 17, 2012

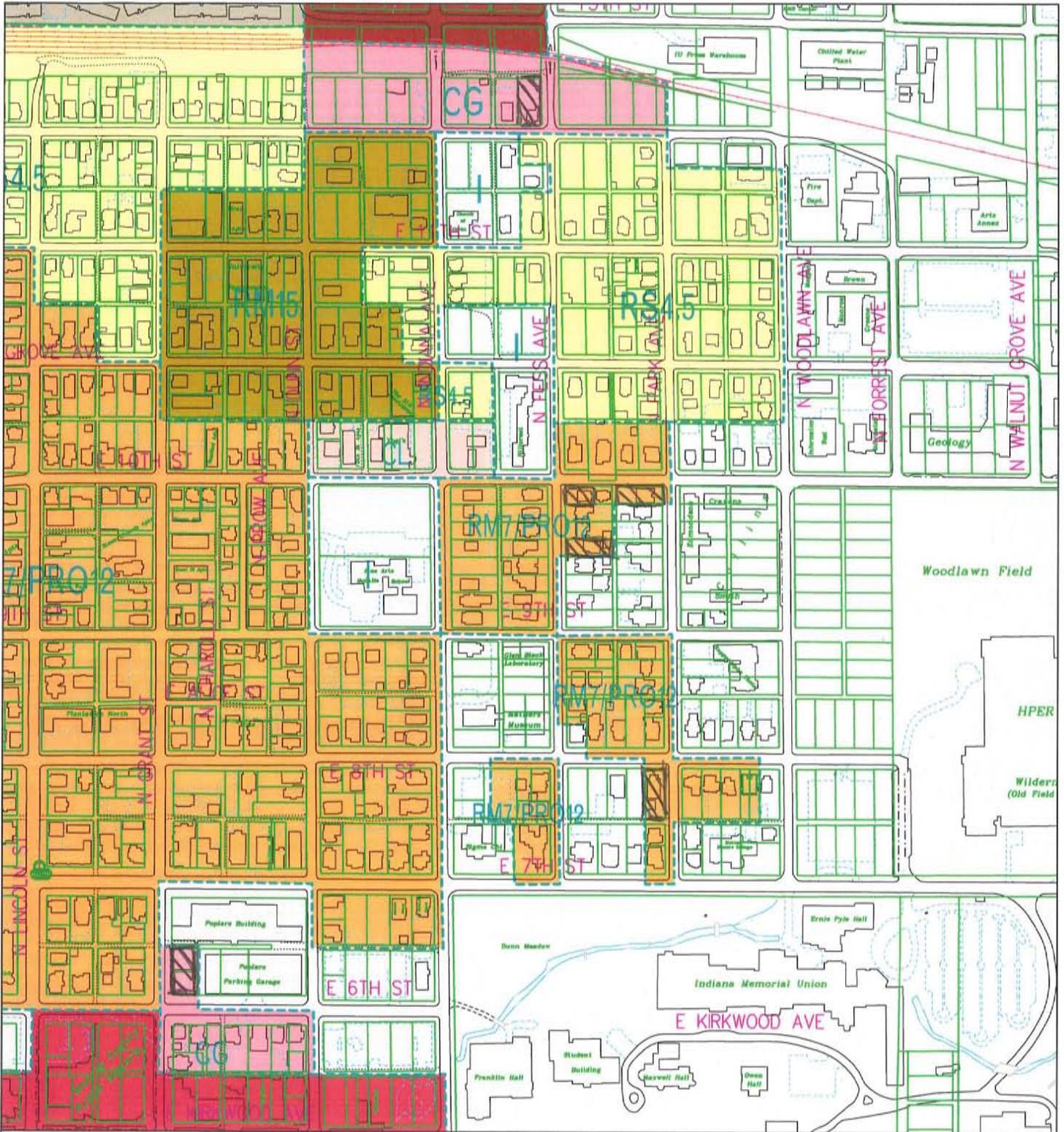
Letter of Intent / Petitioner's Statement

This is a request by the City of Bloomington to petition for the rezone of six individual properties located in the Old Northeast neighborhood. The specific properties are located at 718 E. 8<sup>th</sup> Street, 702 E. 10<sup>th</sup> Street, 525 N. Park Avenue, 514 N. Fess Avenue, 403 E. 6<sup>th</sup> Street, and 613 E. 12<sup>th</sup> Street.

The request stems from a previous rezoning case (ZO-27-11) in which the Plan Commission denied the case, but requested the planning staff reevaluate the Old Northeast neighborhood for possible rezonings. The reason for the Plan Commission's request was to assess private versus public ownership of property in order to conform to zoning which occurred prior to 2007, the last update of the City's zoning map.

The planning staff has been in contact with each of the subject property owners to answer questions and confirm their desired zoning requests. Planning staff attended the May 2012 Old Northeast Neighborhood Association meeting. At this meeting attendees were shown zoning maps of the neighborhood both prior to 2007 and the current map. Planning Staff explained the rezoning petition and answered questions from the attending neighbors. As part of this process, planning staff also met with representatives from Indiana University and informed them of the rezoning request.

As a result of many meetings and research conducted by the Planning Department the above stated properties are being petitioned for rezone. Please contact either Tom Micuda or Lynne Darland in the Planning Department with any questions or concerns you may have concerning this petition at 349-3423.



2006 Zoning  
 ▨ - properties to be rezoned

By: haley  
 29 Aug 12

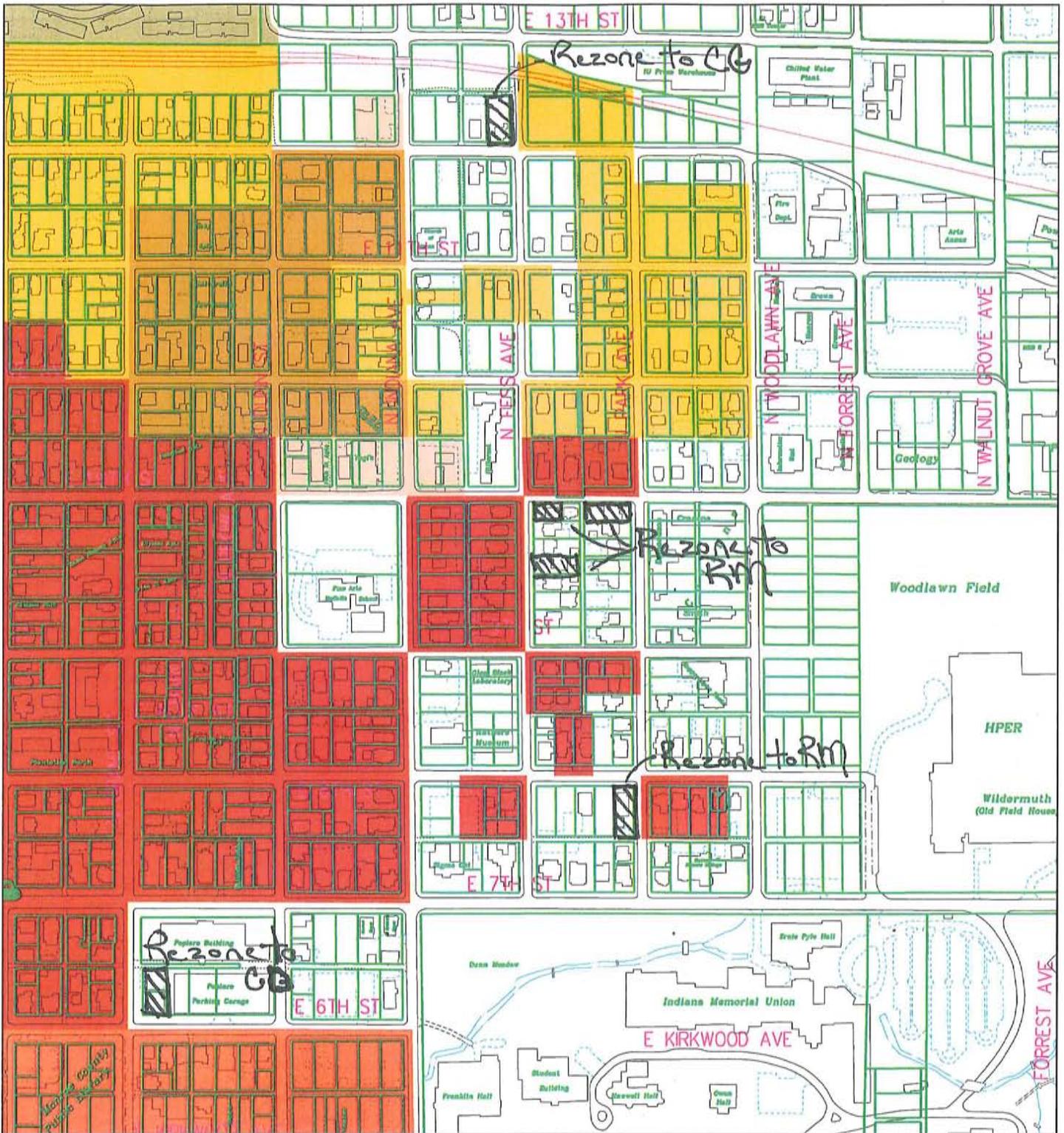


For reference only; map information NOT warranted.

City of Bloomington



Scale: 1" = 400'



Current Zoning  
 ▨ - properties to be rezoned

By: darlandl  
 16 Jul 12



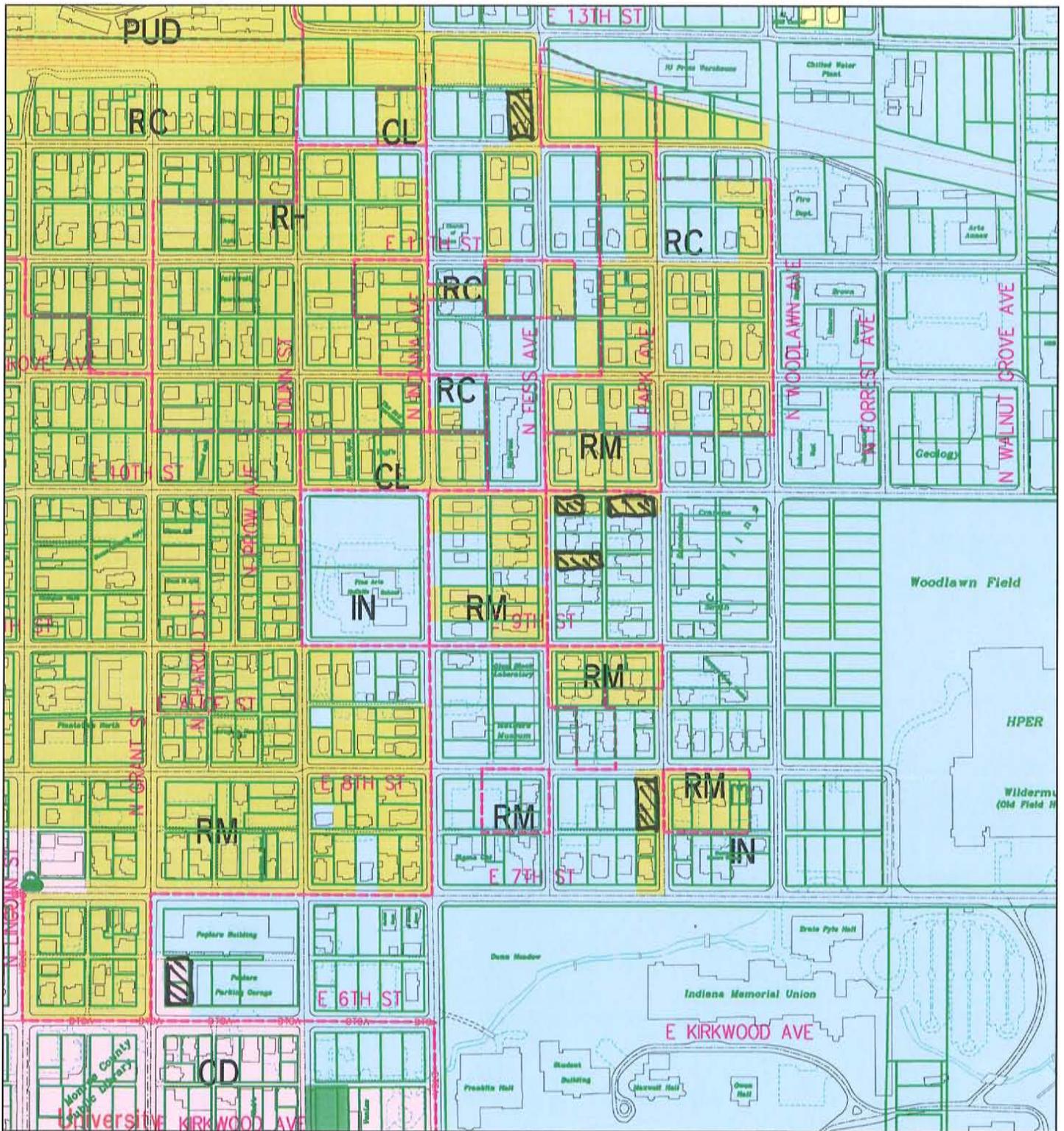
For reference only; map information NOT warranted.



City of Bloomington  
 Planning



Scale: 1" = 400'



Zoning & GPP Map  
 Institutional - blue  
 Core Residential - yellow  
 Downtown - pink

By: darlandl  
 29 Aug 12



For reference only; map information NOT warranted.



City of Bloomington  
 Planning



Scale: 1" = 400'