

# CITY OF BLOOMINGTON



DECEMBER 8, 2014 @ 5:30 p.m.  
COUNCIL CHAMBERS #115  
CITY HALL

**CITY OF BLOOMINGTON  
PLAN COMMISSION**

**December 8, 2014 @ 5:30 p.m.**

**❖ City Hall Council Chambers - Room #115**

**ROLL CALL**

**MINUTES TO BE APPROVED:** November 3, 2014

**REPORTS, RESOLUTIONS AND COMMUNICATIONS:** Approval of 2015 Hearing and Work Session Calendar

**ITEMS FOR CONSENT AGENDA:** None at this time

**PETITION WITHDRAWN:**

SP-26-14     **GP-GMS Bloomington, LLC.**  
 111 S. Lincoln St and 315 E 4<sup>th</sup> St  
 Site plan approval to allow construction of a mixed-use development on approximately 0.74 Acres within the University Village Overlay of the Commercial Downtown (CD) zoning district.  
Case Manager: Jim Roach

**PETITION:**

- SP/UV-34-13     **GMS – Pavilion Properties**  
 306 E. Kirkwood Ave.  
 Site plan approval for a 2-story mixed-use building. Also, Plan Commission review of a use variance for a bank drive-through in the Commercial Downtown (CD) zoning district.  
Case Manager: Jim Roach
- SP-14-14     **AJ Capital Partners**  
 210 E. Kirkwood Ave.  
 Site plan approval for a 146-room downtown hotel.  
Case Manager: Patrick Shay
- SP/UV-34-14     **GP-GMS Bloomington LLC.**  
 111 S Lincoln St  
 Site Plan approval to allow reconstruction of a parking lot. Also requested is use variance review to allow a surface parking lot in the Commercial Downtown (CD) zoning district.  
Case Manager: Jim Roach
- SP-32-14     **Motels of Bloomington, LLC**  
 1410 N. Walnut St.  
 Site plan approval for a new 107-room hotel.  
Case Manager: Eric Greulich

**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT**  
Location: 306 E. Kirkwood Ave.

**CASE #: SP/UV-34-13**  
**DATE: December 8, 2014**

**PETITIONER:** GMS – Pavilion Properties  
112 E. 3<sup>rd</sup> Street, Bloomington

**CONSULTANT:** Studio 3 Design  
8604 Allisonville Road, Indianapolis, IN

**REQUEST:** The petitioner is requesting site plan approval in order to build a new bank. Also requested is Plan Commission review of a Use Variance to allow a drive-through bank in the CD zoning district.

<b>Area:</b>	<b>0.40 Acres</b>								
<b>Zoning:</b>	<b>CD/University Village Overlay (UVO)</b>								
<b>GPP Designation:</b>	<b>Downtown</b>								
<b>Existing Land Use:</b>	<b>Bank drive-through, surface parking</b>								
<b>Proposed Land Use:</b>	<b>Mixed use (retail bank branch, bank drive-through, bank office space, apartments)</b>								
<b>Surrounding Uses:</b>	<table> <tr> <td><b>North</b></td> <td>– Monroe County Public Library</td> </tr> <tr> <td><b>West</b></td> <td>– Existing ONB Bank</td> </tr> <tr> <td><b>South</b></td> <td>– Vacant; former Monroe Bank Loan Center</td> </tr> <tr> <td><b>East</b></td> <td>– Commercial</td> </tr> </table>	<b>North</b>	– Monroe County Public Library	<b>West</b>	– Existing ONB Bank	<b>South</b>	– Vacant; former Monroe Bank Loan Center	<b>East</b>	– Commercial
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<b>South</b>	– Vacant; former Monroe Bank Loan Center								
<b>East</b>	– Commercial								

**PROJECT REVIEW PROCESS:** Due to the unique aggregation of the Old National Bank properties in such a prominent downtown location, staff verbally presented cases SP/UV-34-13, SP-26-14 (withdrawn, replaced by SP/UV-34-14), and SP-14-14 as an integrated report to the Plan Commission on October 6, 2014. The petitioners have followed staff and Plan Commission guidance to bring forward all of the development projects at the same time to achieve this coordinated review. The proposals have now been separated to allow for independent evaluation as site plan requests. In summary, Old National Bank owns the following properties in the heart of the downtown area:

- **Site A:** Contains the former Workingman’s Bank/ONB building, 121 E. Kirkwood Ave. - 0.45 acres – Recently approved for a CVS Pharmacy by the BZA, CU/V-32-14 **\*Please note that Site A is not part of this petition.**
- **Site B:** Contains the former Monroe Bank/Current ONB Bank building, 210 E. Kirkwood Ave. - 0.60 acres – Proposed hotel, SP-14-14
- **Site C:** Current ONB Drive-through - 0.40 acres – This petition
- **Site D:** Contains the former Monroe Bank Loan Center building, 111 E. 4<sup>th</sup> Street - 0.40 acres – Surface parking - SP/UV-34-14
- **Site E:** Surface parking lot at the northwest corner of 4<sup>th</sup> and Grant - 0.30 acres – No current proposal. Will remain surface parking

The aggregate total of these properties is 2.15 acres, with 1.45 acres being located directly on Kirkwood Ave.

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**PROPERTY SUMMARY:** Site C is located at the southeast corner of E. Kirkwood Ave. and S. Lincoln Street. This 0.40 acre property is zoned Commercial Downtown (CD) and is within the Kirkwood Corridor portion of the University Village Overlay (UVO). The property currently contains the one-story Old National Bank (ONB) drive-through and a surface parking lot. The property is surrounded to the west, south and southeast by other properties owned by the bank. To the north is the Monroe County Public Library and to the east are commercial uses.

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**PETITION HISTORY:** This petition was originally heard at the October 7, 2013 meeting. A revised version of the petition was heard at the April 7, 2014, May 5, 2014, and October 6, 2014 Plan Commission hearings. The petitioner sought Commission approval to construct a new 3-story mixed use building. Since the October meeting, the petitioner has removed all apartments and the 3<sup>rd</sup> floor from the petition. This removes the height waiver from the request.

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**Plan Commission Site Plan Review:** Two aspects of this project require that the petition be reviewed by the Plan Commission, per BMC 20.03.160. These aspects are:

- The petitioner is requesting two waivers from development standards.
- Plan Commission review of the Use Variance is required for the bank drive-through.

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#### **SITE PLAN REVIEW:**

**Access:** The proposed building contains 3 public pedestrian entries. The main entry is located along Lincoln Street, but a primary pedestrian entry is provided along Kirkwood Ave. that meets entrance detailing requirements. Vehicular entry to the drive-through is from Lincoln Street with a new curb cut. Drive-through traffic will exit into the north-south alley. At the request of the Plan Commission and an adjacent property owner, the drive-through exit into the alley has been designed to force traffic to the south, to eliminate vehicle conflicts with pedestrians on Kirkwood.

**Impervious Surface Coverage:** The Kirkwood Corridor portions of the UVO permit up to 100% impervious surface coverage. The petitioners are proposing a bank and alley loaded parking that would utilize the entire property. This complies with code requirements.

**Building Height:** The proposed building has been modified to comply with the code requirement. The maximum height for the UVO district is 40 feet and the proposed height is 38'8".

**Parking:** The UVO does not require on-site parking to be provided for non-residential space. However to provide employee parking, the petition includes 10 parking spaces with direct back-out access from the east-west alley south of the site. These spaces require a variance from the Board of Zoning Appeals to exceed the maximum 8 back-out spaces permitted.

Future retail bank customers would have adequate access to parking due to the presence of on-street spaces surrounding the property. The proposed bank office space creates a need for additional employee parking beyond Site C. The petitioner proposes to park these employees within the existing parking lot on what is known as Site E.

**Streetscape:** The Kirkwood Ave. streetscape was installed as part of a larger City streetscape capital project. Construction of the bank and the removal of the existing drive cuts will create a need for much of the streetscape to be reconstructed. The petitioners have committed to reconstructing the streetscape to match the existing Kirkwood Ave. streetscape including but not limited to materials, patterns, lighting, planters, and curbing. The proposed streetscape will also maintain the same number of parking spaces along the frontage and will maintain a bus stop at the southeast corner of Kirkwood and Lincoln.

The petitioners have also proposed to continue the Kirkwood streetscape design south on Lincoln St. In addition, they plan to preserve the existing street trees on this street frontage by keeping a large tree plot around the trees.

**First Floor Commercial Percentage:** The ordinance requires 50% of the space to be used commercially. The proposed 100% allocated to the bank use complies with this requirement.

**Materials:** The proposed building contains a combination of two colors of brick and a man made “cast stone.” Both are permitted materials. One Plan Commissioner has expressed concern that the brick color proposed is not vibrant enough. This concern has been relayed to the petitioner but is not a code requirement.

**Void-to-Solid Ratio:** The ordinance requires a 60% ratio on the first floor and 20% for upper floors. The proposal complies with the upper floor standard, but does not comply with the ground floor standard. First floor void percentage of 45.5% along Kirkwood and 53.5% along Lincoln Street are proposed. This requires waiver approval from the Plan Commission.

**Void to Solid Waiver - BMC 20.03.200(b)(2)(a)(ii):** A waiver of this standard is required to allow a first floor void of less than 60%. The higher solid levels are associated with the tall floor to ceiling heights. The first floor commercial space is 100% of the first floor and fills out the corner. The Downtown Plan does not make specific recommendations to first floor void percentage, but does include several recommendations about creating active, pedestrian friendly first floors and streetspaces. Staff has no objection to this waiver. If the Plan Commission supports the proposed Use Variance for the bank drive-through, the remaining portions of the building are suitably designed to include storefront windows and pedestrian entry points.

**Window Proportions:** While most of the windows maintain the 1:1.5 ration of width to height, the large corner window on the second floor does not meet this proportion. A Waiver is required.

**Window Waiver – BMC 20.03.200(b)(3)(c)(i):** The DCO requires that upper story windows have a 1:1.5 ratio of width to height. The Downtown Plan's recommendations of upper story windows (guidelines 3.13-3.14) call for windows with a vertical emphasis and trim that aligns with adjacent traditional buildings. While the vast majority of the windows meet this requirement, the large corner window does not. Staff has no objection to this waiver because this larger window adds architectural interest to the building.

**Use Variance Request:** The petitioner is requesting to utilize an internal drive-through use for the proposed first floor bank. Drive-through uses are not a permitted use in the CD zoning district. Staff believes that because this drive-through is internal to the building, it will mitigate the negative impacts associated with a typical drive-through. It will be hidden from view from the street and will not impact pedestrian walkability like traditional drive-through uses. The Plan Commission must forward a recommendation to the Board of Zoning Appeals as to whether this use substantially interferes with the Growth Policies Plan.

**Growth Policies Plan Guidance:** The Growth Policies Plan states that new surface parking areas and drive-through uses should be limited, if not forbidden, within the Downtown area. Staff believes that the intent of this policy is to reduce the dependency on cars in the downtown area and to create a more walkable and pedestrian-friendly downtown environment.

The proposed internal drive-through will have limited impact on the walkability of the area. All curb cuts will be removed from Kirkwood Ave, which is a heavy pedestrian street. Drive through traffic would be forced to exit to the south to further limit impacts on Kirkwood Ave. While there is a street cut introduced onto Lincoln Street, the disruption to pedestrians is minimized by the one way nature of the access and the outlet into the north-south alley. In addition, the visual impacts of the drive-through will be eliminated by the internal design. Therefore, the negative aesthetic effects of a paved drive-through use will not be an issue. Furthermore, it should be noted that usage of a bank drive-through is typically less than that of a drive-through fast food restaurant. The layout is also different, making this site difficult to be used for a use other than a bank in the future.

The Growth Policies Plan states in its Compact Urban Form Policy that commercial development should be directed to existing commercially zoned land, and incentives should be provided to encourage the re-use and improvement of vacant or under-developed commercial sites. Staff believes that this site is an underutilized property and the proposed commercial structure would be a positive addition to the downtown commercial district. The use of the drive-through is well designed to have minimal impacts on the surrounding area. Furthermore, bank uses are typically considered long-term uses.

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**ENVIRONMENTAL COMMISSION RECOMMENDATIONS:** The Environmental Commission (EC) has made 2 recommendations concerning this development.

- 1.) The Petitioner should apply state-of-the-art green building and site design practices

to create a high performance, low carbon-footprint structure.

**Staff response:** While green building design and site features are desirably, they are not required by the UDO.

2.) The Petitioner should employ the best-designed gateway-character features possible to build upon the unique Kirkwood Corridor desirability.

**Staff response:** The petitioners have designed a building which meets their needs while satisfying nearly all of the UDO design requirements. Impacts of the drive-through use to Kirkwood have been minimized by prohibiting traffic exiting toward Kirkwood. The unique Kirkwood Ave. streetscape will be extended south on Lincoln St.

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**DEVELOPER TRACK RECORD:** The developer, GMS-Pavilion Properties, has completed other recent downtown projects, including 501 N. Walnut Street, 601 N. Walnut Street and 219 E. 7<sup>th</sup> Street. They also own several other rental properties and are in the process of remodeling the historic Fleener Building at 112 E. 3<sup>rd</sup> Street. There are no outstanding zoning violations associated with these properties. GMS-Pavilion Properties is also partner in the petition to the south, SP-26-14.

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**CONCLUSIONS:** The proposed building is within the height recommendations of the Downtown Plan (2-4 stories) and fills out this important Kirkwood Ave. corridor property. While this petition is smaller in scope than previous versions, with removal of the mixed use component and third floor, the building now complies with the height maximum. Banks are often considered an ideal long term downtown tenant. Development of this property will allow ONB to relocate to this site, stay in downtown Bloomington, and free up Site B for development of the hotel. While drive-through uses are not normally permitted in the CD district, internal bank drive-through uses have been developed on two other downtown properties with little impact to the street, sidewalk or aesthetics of the building. This drive-through replaces an existing drive-through, is hidden inside of the building, and is designed to limit the impacts onto Kirkwood Ave.

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**RECOMMENDATION:** Staff recommends that the Plan Commission forward this request to the BZA with a positive recommendation. Staff also recommends approval of the proposed site plan, including all associated waivers with the following conditions:

1. The petitioners must coordinate all proposed streetscape improvements with staff as well as the City Public Works and Parks and Recreation Departments with the intent to require all improvements to be consistent with the existing streetscape in terms of materials, colors, patterns, and other items such as benches, bike racks, planter boxes and lighting. Any trees to remain must be protected with fencing adequate to prevent vehicle/equipment damage and storage to their root systems.
2. The petitioner shall work with Bloomington Transit to determine the most appropriate bus stop design at the south east corner of Kirkwood and Lincoln.
3. Approval of the site plan is contingent on use variance approval by the Board of Zoning Appeals.
4. Drive through exiting lanes must include signage prohibiting left turns.

# MEMORANDUM

**Date:** September 26, 2014

**To:** Bloomington Plan Commission

**From:** Bloomington Environmental Commission

**Through:** Linda Thompson, Senior Environmental Planner

**Subject:** SP/UV-34-13 & UV-46-13, Pavilion Properties Bank, Site C  
306 E. Kirkwood Ave.  
Site Plan & Use Variance

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This memorandum contains the Environmental Commission's (EC) recommendations regarding the request for a Site Plan and Use Variance for a 3-story, mixed-use building. The site currently contains a surface parking lot and a drive-through bank, which will both be razed and replaced. The parcel is within the Commercial Downtown Zoning District, University Village Overlay, and Kirkwood Corridor Subsection.

## **ISSUES OF SOUND ENVIRONMENTAL DESIGN:**

### 1.) GREEN BUILDING & SITE DESIGN:

The EC is very pleased that the Petitioner has included so many green building practices, which are committed to in the Petitioner's Statement. Green building and environmental stewardship are of utmost importance to the people of Bloomington and sustainable features are consistent with the spirit of the Unified Development Ordinance (UDO). Additionally, they are supported by Bloomington's overall commitment to sustainability and its green building initiative (<http://Bloomington.in.gov/greenbuild>). Sustainable building practices are explicitly called for by the Mayors' Climate Protection Agreement signed by Mayor Kruzan; by City Council Resolution 06-05 supporting the Kyoto Protocol and reduction of our community's greenhouse gas emissions; by City Council Resolution 06-07, which recognizes and calls for planning for peak oil; and by a report from the Bloomington Peak Oil Task Force, *Redefining Prosperity: Energy Descent and Community Resilience Report*.

The EC has some specific suggestions for additional practices that could reduce the carbon footprint of this site even further.

- ~ creation of "green walls" on the outside of the building to reduce the urban heat-island effects;
- ~ installation of charging stations for electric vehicles for some of the parking spaces; and
- ~ creation of some areas planted with native vegetation for ecological-services benefits such as pollinator habitat and biodiversity. With specific regard to the proposed street tree and lawn plantings, the EC recommends that the developer work with Planning and the EC to create a

diverse tree, shrub, and native perennial landscape that evokes the beauty and spirit of Indiana's natural heritage. For suggestions, please see the EC's Natural Landscaping materials at [www.bloomington.in.gov/beqi/greeninfrastructure/htm](http://www.bloomington.in.gov/beqi/greeninfrastructure/htm) under 'Resources' in the left column. For additional suggestions plus an excellent guide to Midwest sources of native plants see: <http://www.inpaws.org/landscaping.html>.

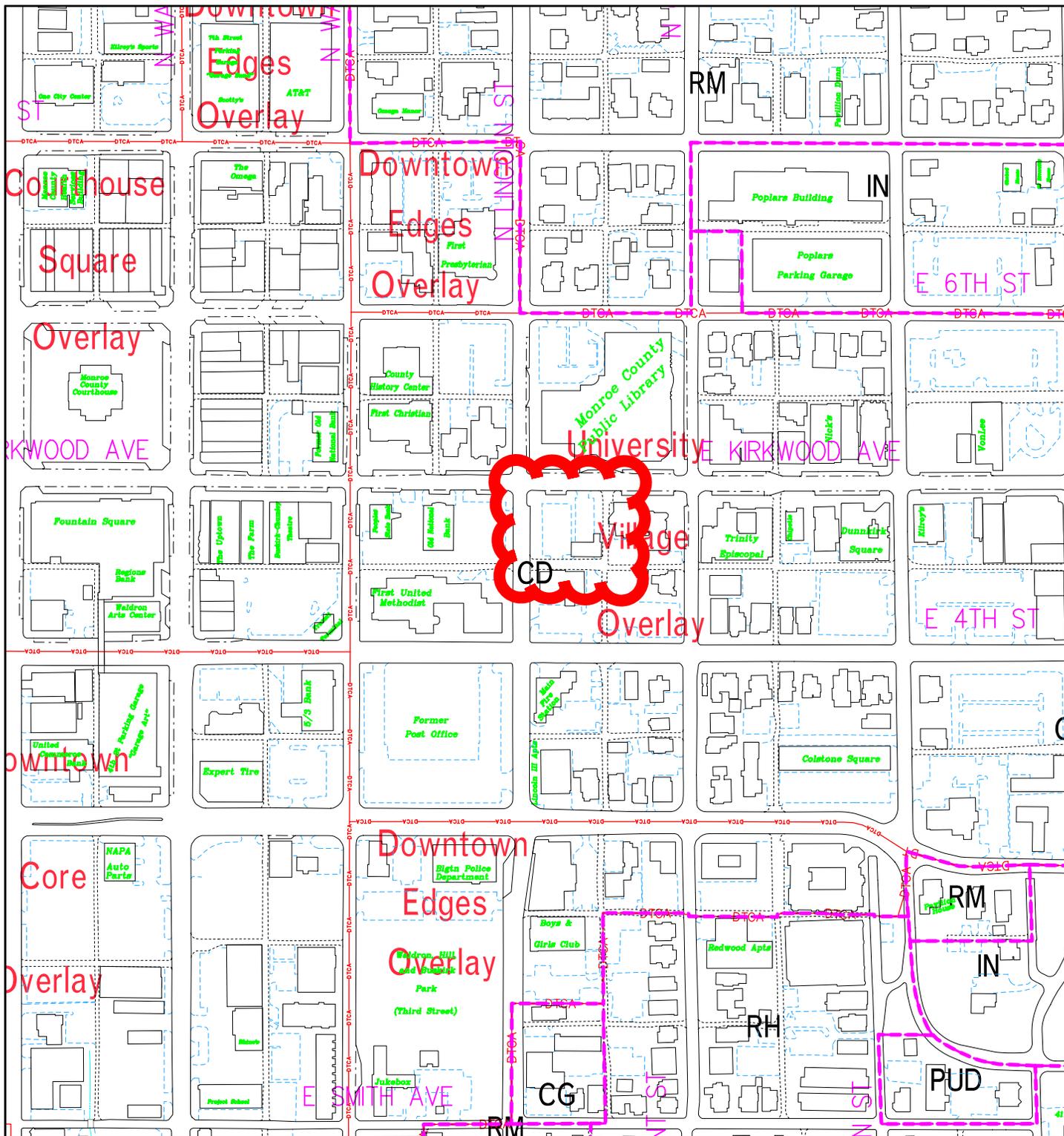
## 2.) GATEWAY CHARACTER:

This proposed development is on a major corridor of the City and Indiana University campus that hosts a special sense of place and unique character. The EC therefore encourages the developer to go above and beyond a typical building design to enhance that ambiance and celebrate our city's cultural and natural heritage. Besides benefiting both our city's overall value as a tourist destination and its native biodiversity, these efforts will attract residents, shoppers, bank customers to the proposed site, thus helping to stimulate the economic vitality of the area.

The EC also notes that this area is also an excellent candidate for a "Complete Streets" approach (<http://www.completestreets.org/>) to enhance its navigability for all users – pedestrians, bicyclists, handicapped people, and others, while simultaneously enhancing its character as both an entranceway to the campus and a city destination spot in its own right. While the EC recognizes that the developer is not responsible for the street way itself, we encourage the developer to promote a vision for the site which complements and anticipates the complete streets concept.

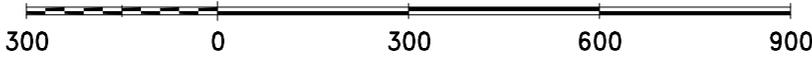
## **EC RECOMMENDATIONS:**

- 1.) The Petitioner should apply state-of-the-art green building and site design practices to create a high performance, low carbon-footprint structure.
- 2.) The Petitioner should employ the best-designed gateway-character features possible to build upon the unique Kirkwood Corridor desirability.

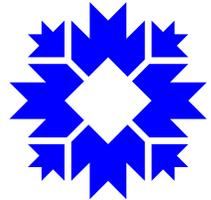


SP/UV-34-13  
Location and Zoning Map

By: roachja  
1 Jul 13

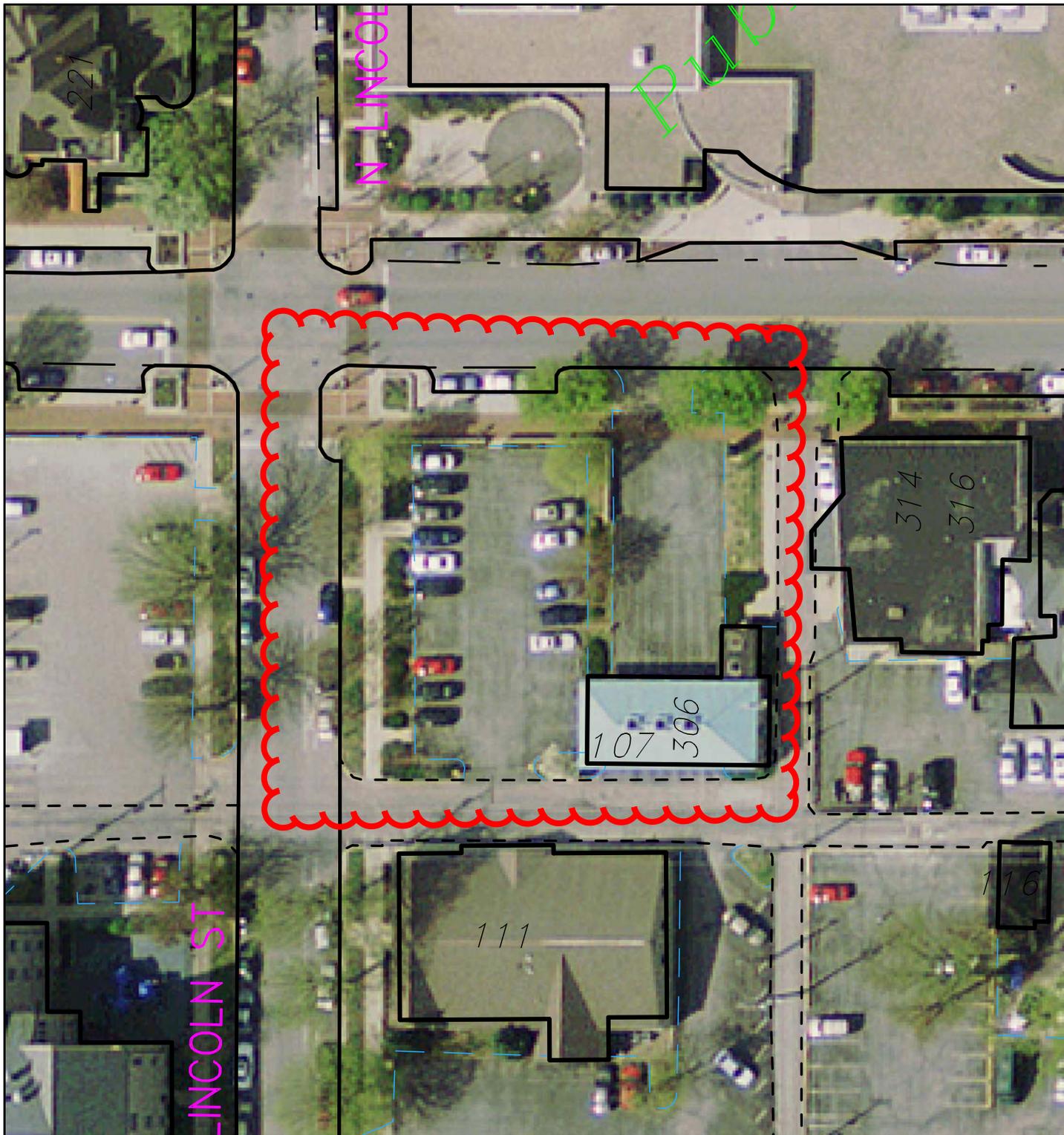


City of Bloomington  
Planning



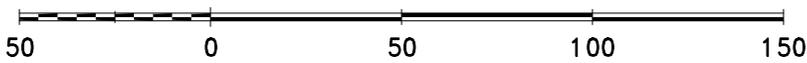
Scale: 1" = 300'

For reference only; map information NOT warranted.

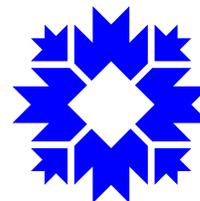


SP/UV-34-13  
2011 Aerial Photo

By: roachja  
13 Sep 13



City of Bloomington  
Planning



Scale: 1" = 50'

For reference only; map information NOT warranted.



SP/UV-34-13  
ONB Bank land holdings

By: roachja  
23 Sep 13



For reference only; map information NOT warranted.

City of Bloomington  
Planning

Scale: 1" = 200'



November 24, 2014

City of Bloomington Planning Department  
P.O. Box 100  
Bloomington, IN 47402

Attn: Mr. Tom Micuda

**RE: Bloomington Downtown Development  
ONB Development – Site C  
Bloomington, Indiana**

**PETITIONERS STATEMENT**

Dear Mr. Micuda

Studio 3 Design is pleased to submit the attached development for your consideration. The project scope covers one (1) of the 4 sites currently owned by Old National Bank within the downtown with a proposal for (3) new developments represented on these 4 sites. The project zone incorporated by these sites has been identified on the attached documentation as sites "B, C, D & E". This submittal package will focus on Site C only, but the other sites are mentioned as they are part of the holistic view of the downtown sites that we have been asked to consider.

Site "B" -bordered on the North by Kirkwood Avenue, on the East by Lincoln Street, on the South by a public alley, and on the West by an adjacent land owner.

Site "C" –bordered on the North by Kirkwood Avenue, on West by Lincoln Street, on the South by a public alley, on the east by a public alley.

Site "D" –bordered on the South by 4<sup>th</sup> street, on the West by Lincoln street, on the North by a public alley and on the east by a public alley.

Site "E" –bordered on the South by 4<sup>th</sup> street, on the East by Grant Street, On the North by an adjacent property owner and on the West by a public alley.

**Submittal Organization:**

The proposal for Site C will be for a two story brick and limestone building. The building will house an Old National Bank branch on level 1, as well as an associated drive-thru. Level 2 will be occupied by the Old National Bank Executive Offices. The area at the south side of the site bordering the existing alley will developed as 10 surface parking spaces.

**Project Overlay District:**

University Village Overlay  
Kirkwood Corridor subsection

Density: 33 units per acre

Impervious surface:

Kirkwood corridor: 100%

Restaurant row: 85%

Height Standards:

Kirkwood Corridor: min. 25' – max. 40'

Restaurant row: min. 25' – max. 35'

Parking:

None residential: No parking required for Hotel, retail, or office functions

Residential: 5 for first 20, .8 for beds 21 on.

Setbacks:

Front: 0 to 15'

Side yard: 0'

Rear yard 0'

Ground floor non-residential:

Applies to Kirkwood, 4<sup>th</sup>, Lincoln and Grant

50% min. along applicable street frontage

Building alignment:

No Outstanding, notable or contributing structures immediately adjacent to properties.

Building orientation:

Kirkwood corridor: min. of one primary entrance facing Kirkwood

Restaurant row: min. of one primary entrance facing 4<sup>th</sup> street

Street trees:

Kirkwood corridor: 5' square tree grate or large curbed planting bed

Restaurant row: 5' wide grassed tree plot area

Lighting:

Street lighting- traditional style design such as acorn or gas lamp style.

Architectural character:

Kirkwood corridor – flat roofs

Restaurant row: incorporate sloped or pitched roofs

Void to solid:

Kirkwood Corridor: lower- 60%, upper 20%

Restaurant row; lower 50%, upper 20%

Windows:

Kirkwood: large display on level 1, 1.5 to 1 ratio on upper windows with sills and lintels

Restaurant row- windows to have appearance of double hung window

Materials:

**Not permitted as Primary**

Kirkwood Corridor: Wood, EIFS, CMU, vinyl, cement siding

Restaurant Row: EIFS, CMU, natural stone or masonry, precast concrete, vinyl

**Not permitted as a secondary material:**

Kirkwood Corridor: EIFS, Vinyl, Cement siding on first level only  
 Restaurant Row: EIFS, Vinyl

**Entrance Detailing:**

General: - shall incorporate a min. of 3 of the following:

4' recessed entry, ornamental paving, Canopy/ awning, Portico, arched entry, pilaster or façade module projecting from exterior wall plane, building address with, building name and lighting, public art, raised cornice entryway parapet, rusticated masonry, landscaped patio area for outdoor seating of 8 or more.

Restaurant Row: entrance shall incorporate a front porch, canopy or awning, incorporate 2 or more of the general entrance detailing listed above.

**Mass, Scale & form:**

Façade modulation- maximum width 50' with a min. 3% offset based on total length of façade.

Height step-down: N/A – properties are not immediately adjacent to outstanding, notable, or contributing structures.

**Project Overview:**

The project is located in the heart of downtown Bloomington along Kirkwood Avenue, 4<sup>th</sup> Street and Lincoln Street and 4<sup>th</sup> street with the four sites positioned as outlined above. The projects will provide a mixed use development that will enhance the urban fabric and add life to the current spotted development along Kirkwood and Lincoln Streets. The development will provide a solid streetscape with varied heights, materials, colors and architectural styles to generate an exciting urban feel filled with hotel, retail & restaurant functions along the street levels with office space and hotel rooms occupying the upper levels.

Beyond the obvious physical changes to the area, the development offers a host of innovations and amenities including facilities that are geared toward being “green”. Items being considered to help reduce traffic emission include secured covered bike parking on site and city bus stop along Lincoln Street. Each site will provide space for recycling separation with all rooms for trash and recycling located in interior space to encourage use and accessibility along with keeping the alleys clean of debris. To reduce the urban carbon footprint of the facility, the building shells will be well insulated, windows will have low-E glazing with sun screening where determined to be appropriate at commercial locations. Rooftops will be white roofing to reflect sunlight and reduce the “heat island” effect. Internally, energy star appliances, high efficiency furnaces, insulated demise and perimeter walls, energy efficient light fixtures, motion sensor lighting in common areas, abundant natural lighting sources, will be provided. Additionally, interior finish materials will be selected based on their recycled content and or ability to be recycled. Common building materials such as lumber and masonry will come from within 500 miles to reduce transportation and associated emissions and fossil fuel consumption. While the end goal is not to go through the full certification of the buildings under Leed, it is to create a sustainable development that embraces many of the same characteristics.

The development will also improve city utilities in the area and clean up utility poles and other obstructions in the currently hard to navigate alleys to create safer, functional travel lanes. The final impacts and routing of utility lines are subject to the plans Duke Energy is developing for the overall area to address not only our sites but the impacts of the City’s expanded culvert system through the area.

### **Site Specific Information:**

#### **Site C - Kirkwood and Lincoln streets – 306 E. Kirkwood**

##### **Site C Use- Retail & Office Space. (All uses are permitted under the UDO)**

- **Two level brick and limestone building.**
- Level 1 will have new Old National Bank branch, a 4 lane drive-thru, a public meeting room, secured bike storage, trash/ recycling room, and surface parking accessed from the south alley.
- Levels 2 will contain Old National Bank Executive Offices.

### **20.03.190 Development standards:**

#### **Density and intensity standards (Project complies with UDO)**

Site: 17,560 sf/ 43,560 = .403 acre x 33 = **13.30 DUE's available**

Level 1 - ONB Bank	0.00 DUE's used
Level 2 – ONB Executive Offices	<u>0.00 DUE's used</u>

Total **0.00 DUE's used out of available 13.30**

#### **Maximum impervious service coverage: (complies with UDO)**

Site area= 0.403 acres (17,560 SF)

Impervious percentage = 100% current and proposed

Kirkwood corridor allows for 100% impervious coverage

#### **Building Height (Complies with UDO)**

Min. of 25' with a max. of 40' – The project as measure from the lowest point on the site to the top of the building is 38'-8".

#### **As a point of reference on heights, we had the neighboring building heights surveyed as well.**

Our building on Kirkwood	2 levels - 38'-8"
Library on Kirkwood	3 levels –37'
Cactus Café- Kirkwood and Grant	3 levels –36'
Fire station 4 <sup>th</sup> and Lincoln	2 levels - 41'-6"
Church on 4 <sup>th</sup> and Lincoln	3 levels - 56' roof to 80' at tower

#### **Parking Standards (Project complies with districts' guidelines)**

##### **Required:**

Retail: Bank Level 1	0 required
Offices: Bank Level 2	<u>0 required</u>

Total required:	<b>0 spaces required</b>
<b>On-site provided:</b>	
Level 1	10 spaces
Street parking	<u>07 spaces</u>
Total provided	<b>17 spaces provided</b>

**Building setback standards: (Building complies with the districts guidelines)**

Front setback- 0' to 15'  
 Side yard minimum 0'  
 Rear yard min. 0'

**Ground floor non-residential Uses: (Building complies with districts guidelines)**

Kirkwood Ave & Lincoln Street: no less than 50% of total ground floor area shall be used for non-residential uses. (the drive-through only serves the bank retail function – no parking).

Level 1: Gross building footprint SF.                      14,055 s.f.

Non-residential use %    **100% on level 1 dedicated to non-residential use.**

**Site Plan standards:**

**Site Plan: (building complies with the districts' guidelines)**

Building Frontage – NA

Building Alignment – NA (structure is not adjacent to an outstanding, notable of contributing structure).

Building Orientation and entrances:

- (1) Pedestrian entrances provided on Kirkwood
  - (2) Pedestrian entrance provided on Lincoln Street
- All entrances are within three feet of the adjacent sidewalk elevation.

**Street trees: (site plan complies with the districts' guidelines)**

Street trees in tree grates and large curbed planting beds to match up with the current Kirkwood corridor streetscape are provided.

**Lighting: (Site plan complies with the districts' guidelines)**

Pedestrian scale street lights (less than 15' in height) will be placed along Kirkwood and Lincoln streets.

Traditional style acorn light fixtures with full cut-off shall be used.

Exterior building lighting will comply with 20.05 lighting standards.

**Mechanical equipment and Service areas: (site plan complies with districts' guidelines)**

Bank mechanicals are housed internally. The transformer is located along the east alley, trash removal is located along the south alley. Condensing units on roof will be setback a min. of 10' from building edge to prevent view form adjacent streets.

**Architectural Character:**

**Roofs or building caps:**

Kirkwood corridor: flat roofs and parapets are provided.

**Void To Solid Ratio: (waiver required)**

Kirkwood Street First Level =	60% required	<b>45.5% provided</b>
Kirkwood Street Upper Levels =	20% required	<b>43.4% provided</b>
Lincoln Street First Level =	60% required	<b>53.5% provided</b>
Lincoln Street Upper Levels =	20% required	<b>40.2% provided</b>

**Windows: (waiver required)**

Windows are transparent

Level 1 windows will be large storefront display windows

Windows will incorporate sills, lintels, heads that are visually distinct from the primary exterior finish.

While most windows meet the 1:1.5 width to height ratio, there are windows in the building that do not (mostly adjacent to the allies). A waiver is being sought for this section.

**Materials: (building complies with the district guidelines)**

Primary facades on Lincoln and Kirkwood comply with allowable primary and secondary building materials. Secondary facades along east and south alleys comply with allowable primary and secondary building materials.

The building materials vary to create a traditional feel with a combination of storefront and punched window openings, limestone veneer, and brick veneer with simple limestone cornice detailing. This same feel and use of materials carries around the building on the alley facades to present a unified level of articulation on all sides – not just the street front. These various materials delineate and mitigate the scale and mass of the building vertically and horizontally with banding and color changes.

**Entrance Detailing: (Building complies with the districts' guidelines)**

A minimum of 3 architectural design features shall be incorporated: The following will be used on the building:

- Min. 4' recessed building entrance
- Ornamental paving and integral landscape planters
- Canopy/ awnings
- Prominent building address, name, and enhanced exterior lighting

**Mass, Scale and Form: (building complies with the districts' guidelines)**

Facade modulation: Maximum 50' module on street frontage  
 Module offset of a min. of 3% (4 feet)

Building height set down – NA – building is not located adjacent to a an outstanding, notable or contributing structure.

**Additional information:**

**Bank Drive-thru teller window.**

The bank will have a 4 lane drive-thru that is concealed under the building. A one way drive will access the drive-thru off of Lincoln street and the drive-thru will exit onto the north-south alley on the east side of the site with access out onto Kirkwood. Vehicular traffic will only be allowed to turn south out of the drive-thru to minimize traffic impact onto Kirkwood.

**Site Accessibility**

ADA compliant building entrances are provided on Kirkwood and Lincoln. Elevators will be access controlled for access vertically into the office space on level 2. Control gates may be incorporated at the bank drive-through to limit access to these areas at night.

**Bike Storage/ Parking**

The building provides secured bike storage on "U" racks for up to **8** bikes on Level 1. Per the UDO – (4) are required for the development. Provided:: **(8) for Retail/Office:** (2 at bank entry on Lincoln, 2 at bank entry on Kirkwood, 2 at Executive Office entry on Kirkwood, and 2 in the drive-thru space).

**Build to Line**

The project meets the requirement of the UDO to have the majority of the façade constructed to the build-to line (property line) along Kirkwood and Lincoln streets. Portions of the building step back to provide interest and natural breaking points for material changes along the façade. Additionally, step-backs in the façade create deeper zones at street level to provide relief and interest along the streetscape as well as opportunities for street furnishings and the ability for people to gather out of the main circulation path.

**Building Entrances**

The building has (3) three public pedestrian entrances. (1) entrance on Lincoln St for the bank branch and (2) entrances on Kirkwood – one for the Bank Branch an one for the Executive Offices. Access drives for the bank drive-thru are one way off of Lincoln Street and exit traveling south on the North-South alley.

### **Trash Removal**

Trash removal has been provided along the east-west alley on the south side of the building. The trash container will be located within an enclosed area furnished with a rolling door to be opened only on day(s) of trash collection. This area will also be accessed from the building for Retail and office tenants' disposal of trash. The grade will be leveled at this location to assist in the roll-out of trash container on collection day(s).

### **Recycling**

Recycling facilities for the separation of goods will be provided on site. The room will be associated with the trash room and will be an interior space with space for the separation of various recycling components. This room will be accessible for use by retail and apartment tenants alike. Provisions are in place for the time when the City provides a recycling pick-up program for the downtown – collection is not included at this time.

### **Water Service & Meter Pit**

The project will connect to an existing 12" water line on Kirkwood that is less than 5 years old. A master meter will be installed partially within the City right of way at the northeast corner - one for the bank and apartment building. It will house the necessary meters and fire apparatus. The PIV connection will be installed off of Kirkwood at the NE corner of the building for Fire department access. A separate connection will be back fed from the meter pit on Kirkwood for Fire Department access. No new mains are anticipated to be installed to provide service for the project – this has been confirmed with CBU. No new main will be installed along Lincoln Street either. There is no existing Water main on Lincoln Street, CBU did not see a need to add one.

### **Sewer Service & Grease intercept**

A connection will be made to the existing City sewer main routed down Kirkwood Avenue. All connections will be lateral connections with standard patching of the street as required. No new mains will need to be installed to provide service for the project – this has been confirmed with CBU. A grease interceptor is not provided at this time per comments from CBU. There is no immediate need for the bank function to have a grease interceptor. There is no existing Sewer on Lincoln Street- CBU did not see a need to add one.

### **Storm water**

Roof drains will be routed through a mechanical BMP or aqua-swirl as determined by CBU prior to exiting into the city storm system on Kirkwood. The storm line on Kirkwood is in good shape- no additional work is anticipated. Additional storm lines may need to be installed on Lincoln Street.

### **Site detention for water:**

The site is currently 100% impervious. No detention is required to be added as part of the new site development.

### **Private Utilities**

Duke Energy and a cable/phone/internet provider to be determined will provide for the service needs of the development. In preliminary design coordination with Duke Energy, a replacement power with pad mounted transformers will take place at the southeast corner of the adjacent site along the alley. A junction box for the phone lines to feed the development will also be in this area. The electrical meter is located near the southeast corner exterior wall of the building

### **Waivers:**

We have identified (2) waivers, and are requesting support on both.

1. Void-to-Solid %- While we have tried to achieve the 60% ground floor requirement, we fall just short of the requirement.
2. Window proportion: While many of our windows maintain the 1:1.5 ration of width to height, there are several windows around the development that do not meet this proportion.

### **Variances:**

We have identified (2) Variances that will need to be approved by the BZA, and are requesting support from both planning staff and the planning commission.

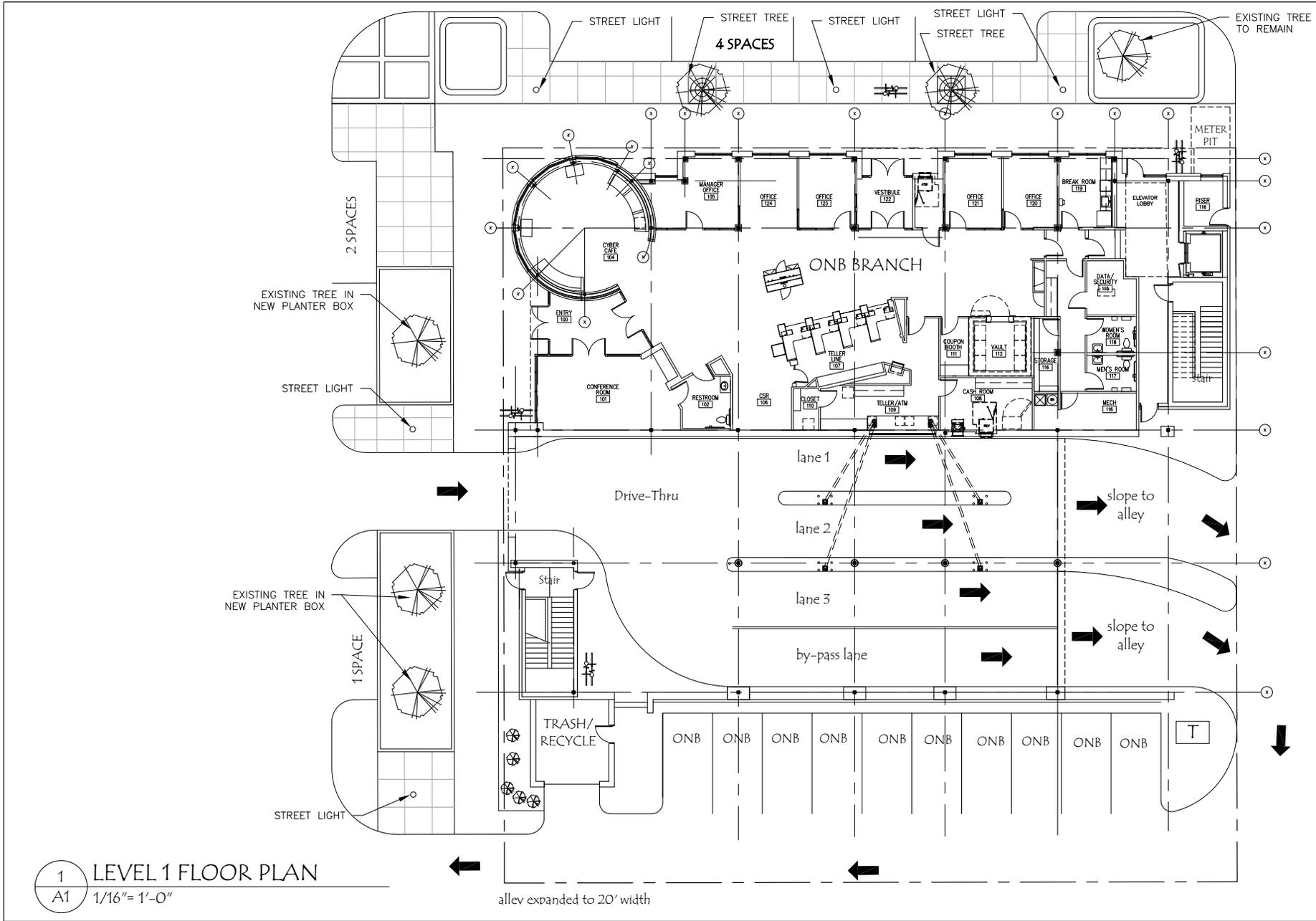
1. A variance is required to provide a drive-thru in the downtown area. We are replacing an existing drive-thru on site that is currently out in the open with one that will be fully enclosed under roof and screened from public view.
2. A variance is required to have more than (8) parking spaces backing into a single alley. The proposal has (10) parking spaces backing in to the alley at the south side of the property. We have widened this alley to a total of 24'-0", and feel that this increase should mitigate any concern over (2) additional cars backing out.

Respectfully submitted,

STUDIO 3 DESIGN, INC



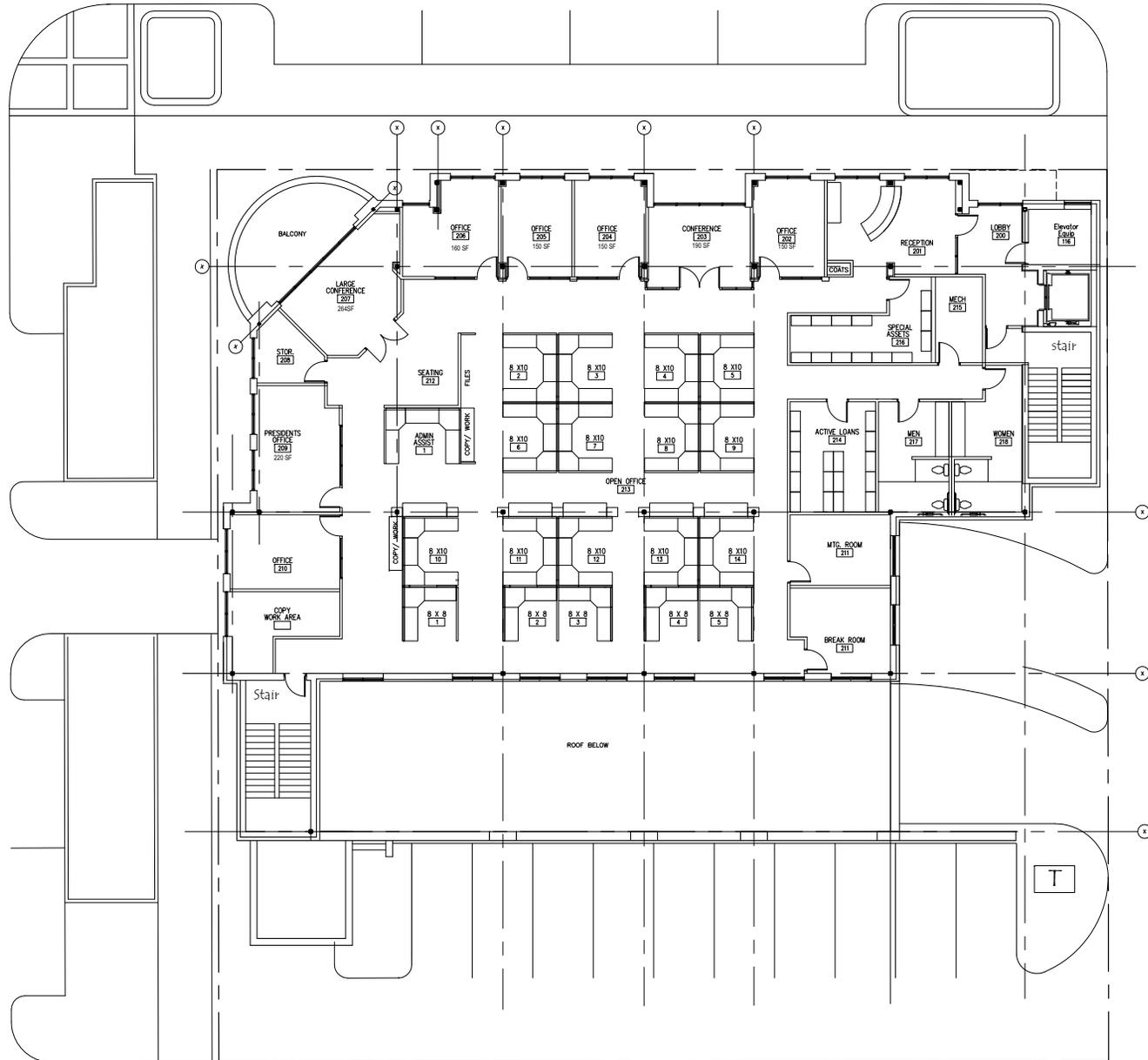
Timothy W. Cover



1 LEVEL 1 FLOOR PLAN  
A1 1/16" = 1'-0"

PROJECT NO. 13018.04		DATE 11-24-14	
SHEET NO. PROJECT NO. 13018.04		SHEET DESCRIPTION SITE C LEVEL 1 FLOOR PLAN	
GMS-PAVILION PROPERTIES, LLC GERSHMAN PARTNERS		SHEET NUMBER A1	
ONB "SITE C" DEVELOPMENT		Bloomington, Indiana	
STUDIO THREE DESIGN		www.studiothreedesign.com	
91 905 0000 main 800 446 0000 fax 800 446 0000 www.studiothreedesign.com		117 872 1238 fax 117 872 1238 www.studiothreedesign.com	

SP/UV-34-13  
Site Plan/First Floor



1 LEVEL 2 FLOOR PLAN  
 A2 1/16" = 1'-0"

SHEET NUMBER

A2

SHEET DESCRIPTION

SITE C  
 LEVEL 2  
 FLOOR PLAN

PROJECT NO.

13018.04

DATE

11-24-14

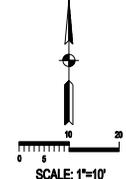
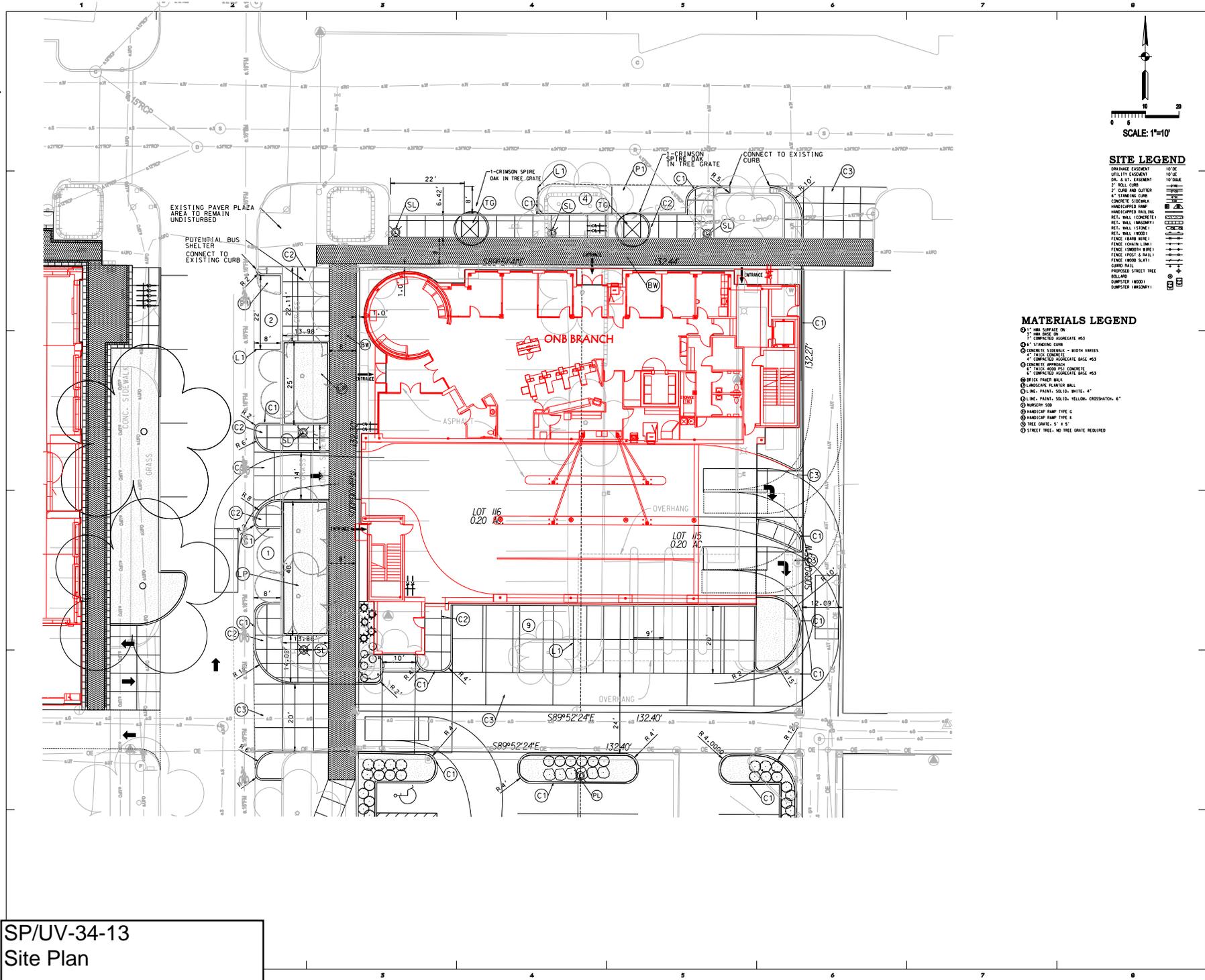
GMS-PAVILION PROPERTIES, LLC  
 GERSHMAN PARTNERS  
 ONB "SITE C" DEVELOPMENT

Bloomington, Indiana

STUDIO  
 THREE  
 DESIGN

www.studiothree.com  
 317.872.1238  
 8804 Millersville Road, Suite 200 Indianapolis, IN 46250

SP/UV-34-13  
 Second Floor



**SITE LEGEND**

UTILITY EASEMENT	10' TO 15' TO
UTILITY EASEMENT	15' TO 20' TO
2" CURB AND GUTTER	10' TO 15' TO
6" STANDING CURB	10' TO 15' TO
CONCRETE SIDEWALK	10' TO 15' TO
HANDICAPPED RAMP	10' TO 15' TO
CONCRETE SIDEWALK	10' TO 15' TO
RECY. WALL (CONCRETE)	10' TO 15' TO
RECY. WALL (WOOD)	10' TO 15' TO
RECY. WALL (STONE)	10' TO 15' TO
RECY. WALL (BRICK)	10' TO 15' TO
FENCE (CHAIN LINK)	10' TO 15' TO
FENCE (WOOD SLAT)	10' TO 15' TO
GLASS WALL	10' TO 15' TO
PROPOSED STREET TREE	10' TO 15' TO
SOLID	10' TO 15' TO
DUMPSTER (WOOD)	10' TO 15' TO
DUMPSTER (METAL)	10' TO 15' TO

**MATERIALS LEGEND**

- ① 1" HW SURFACE ON 2" HW BASE ON COMPLETED AGGREGATE BASE #5
- ② 6" STANDING CURB
- ③ CONCRETE SIDEWALK - WIDTH VARIES
- ④ 2" THICK CONCRETE
- ⑤ CONCRETE APPROACH
- ⑥ COMPLETED AGGREGATE BASE #5
- ⑦ BRICK PAVEMENT
- ⑧ LANDSCAPE PLANTER WALL
- ⑨ LINE, PAINT: SOLID, WHITE, 4"
- ⑩ LINE, PAINT: SOLID, YELLOW, CROSSHATCH, 4"
- ⑪ NUMBER SIGN
- ⑫ HANDICAP RAMP TYPE G
- ⑬ HANDICAP RAMP TYPE X
- ⑭ TREE GRATE, 3' X 3'
- ⑮ STREET TREE - NO TREE GRATE REQUIRED

**STUDIO THREE DESIGN**  
 CIVIL CONSULTING ENGINEER  
 8700 Allisonville Road, Suite 330 Indianapolis, IN 46220  
 317.556.8818  
 www.studiothreedesign.com

**OLD NATIONAL BANK SITES**  
 BLOOMINGTON, IN

PROJECT NO.  
**4933 / 5012**

DATE  
**11/24/14**

REVISIONS

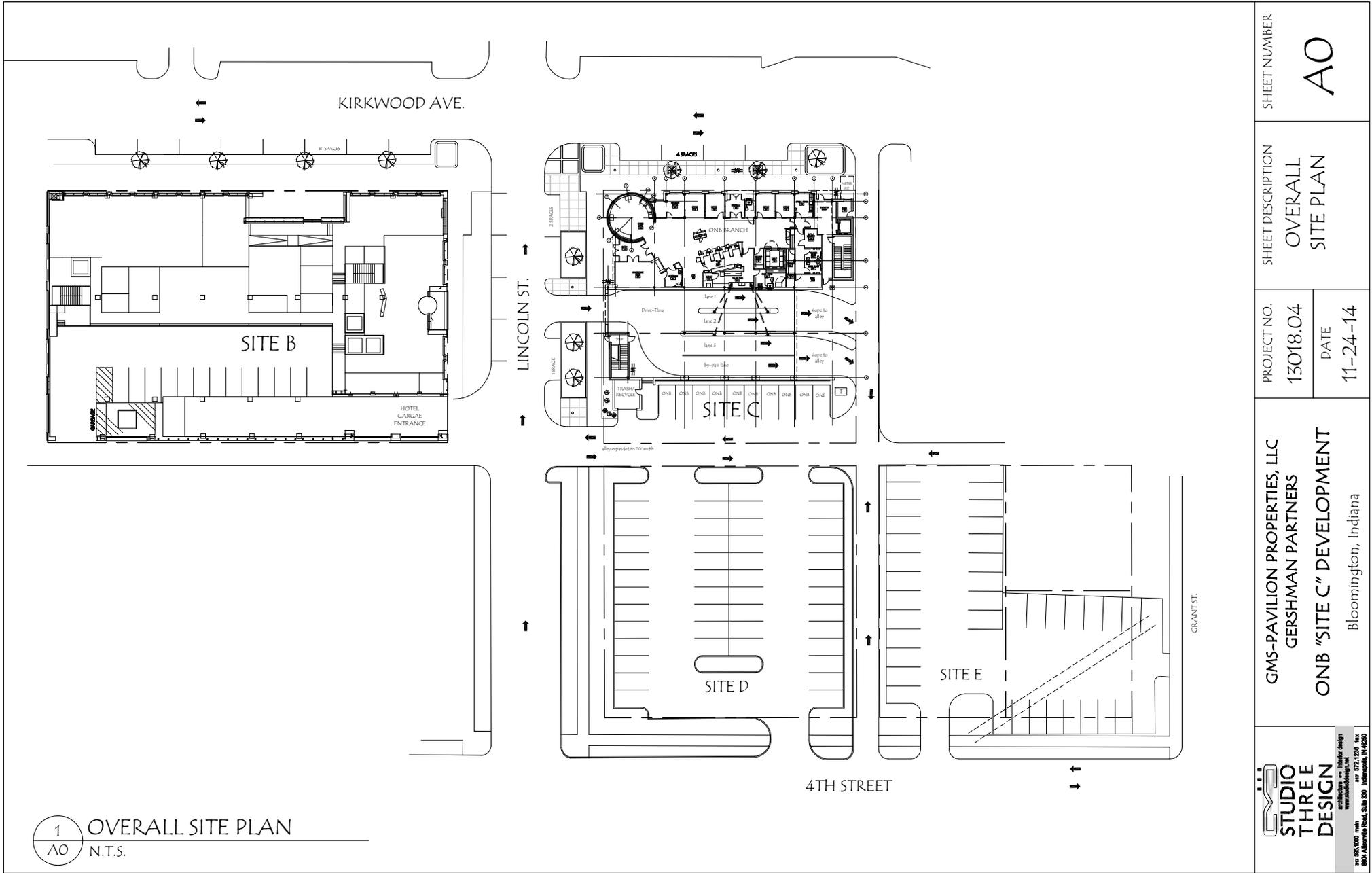
CERTIFICATION

SHEET DESCRIPTION  
**SITE C  
 SITE PLAN**

SHEET NUMBER  
**C202**

THIS DRAWING IS THE PROPERTY OF STUDIO 3 DESIGN, INC. ALL RIGHTS RESERVED

**SP/UV-34-13**  
**Site Plan**



1 OVERALL SITE PLAN  
 AO N.T.S.

SHEET NUMBER  
**AO**

SHEET DESCRIPTION  
**OVERALL  
 SITE PLAN**

PROJECT NO.  
**13018.04**

DATE  
**11-24-14**

**GMS-PAVILION PROPERTIES, LLC  
 GERSHMAN PARTNERS  
 ONB "SITE C" DEVELOPMENT**  
 Bloomington, Indiana

**STUDIO  
 THREE  
 DESIGN**  
 www.studiothreedesign.com  
 317.865.0000 main 317.872.1258 fax  
 8004 Millersville Road, Suite 200 Indianapolis, IN 46250

**SP/UV-34-13  
 Overall Site Plan**



1  
A7

RENDERED SITE PLAN



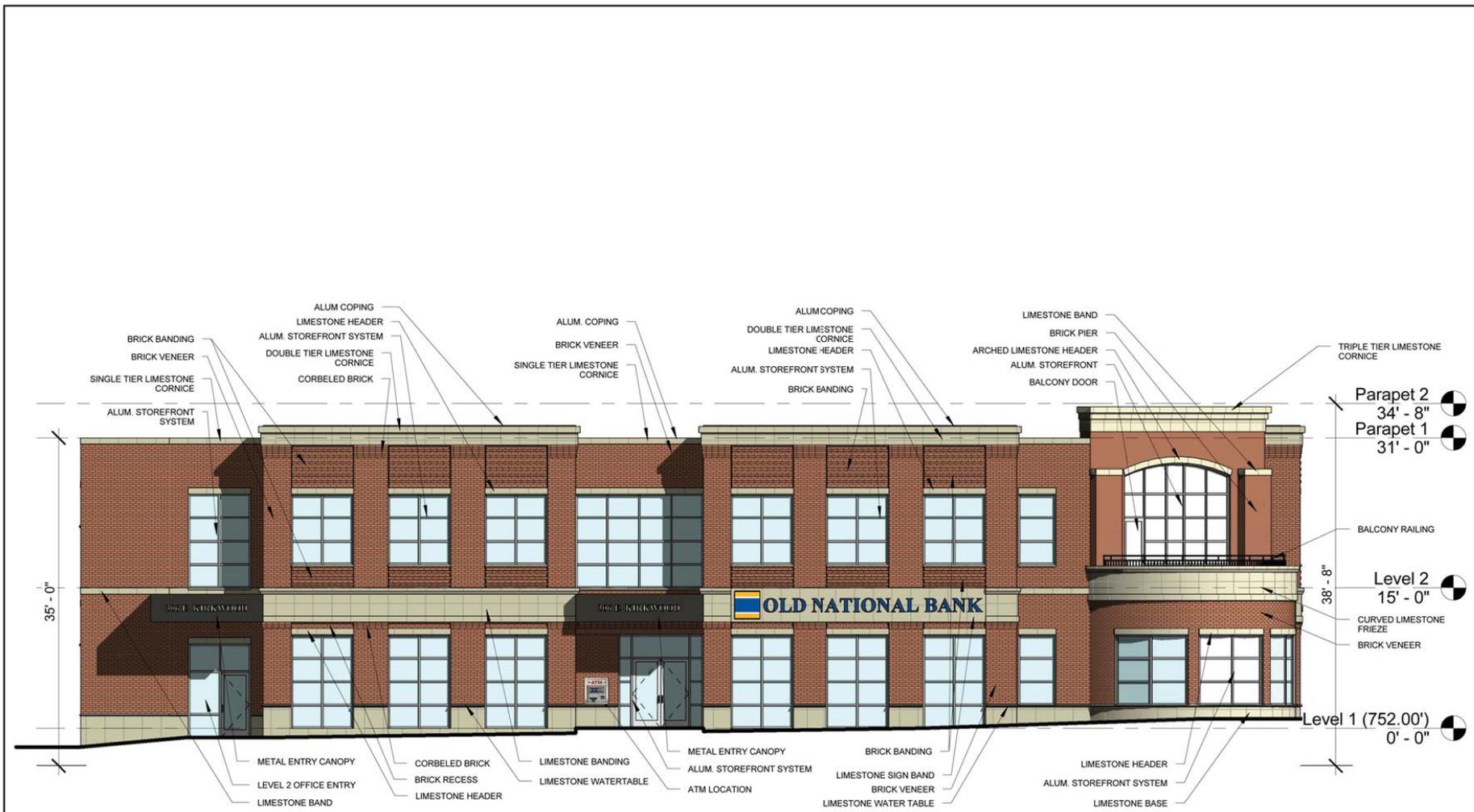
GMS-PAVILION PROPERTIES, LLC.  
 GERSHMAN PARTNERS  
 ONB "SITE C" DEVELOPMENT  
 BLOOMINGTON, INDIANA

PROJECT NO.  
 13018.04  
 DATE  
 11/24/2014

SHEET DESCRIPTION  
 Rendered Site  
 Plan

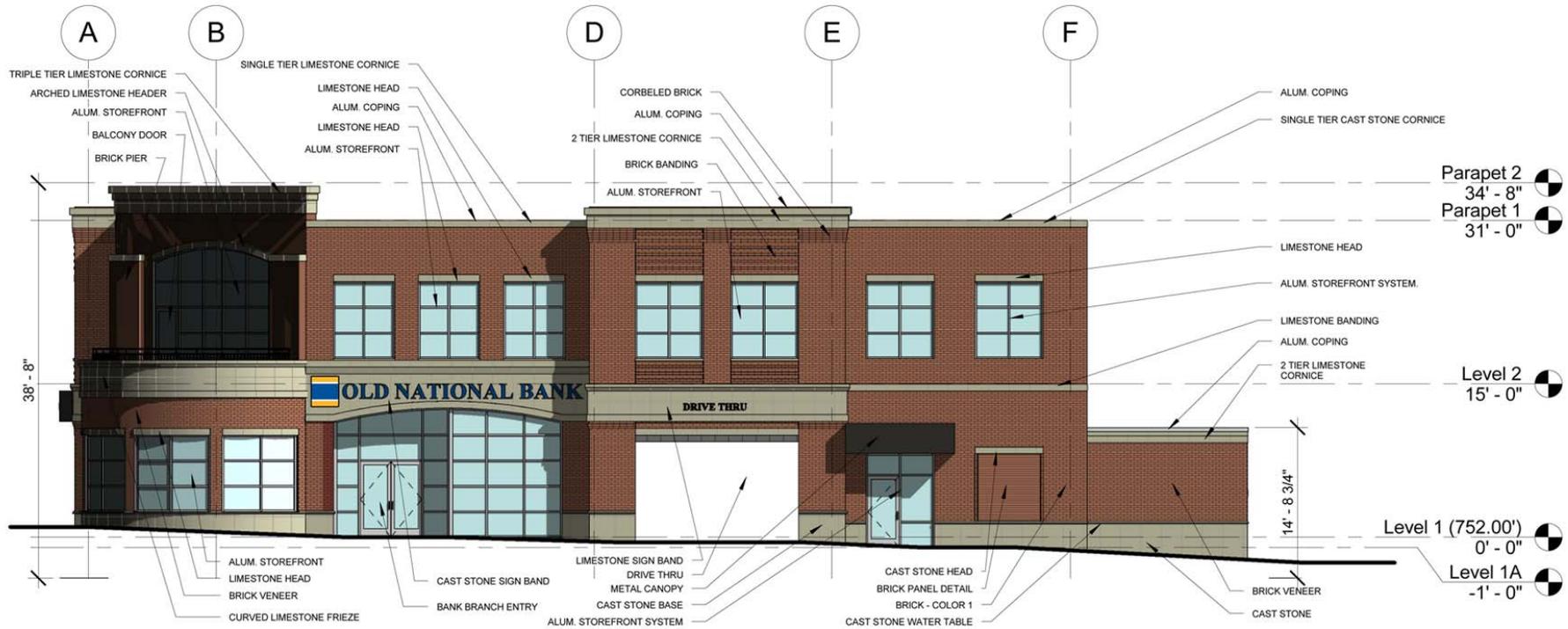
SHEET NUMBER  
 A7

SP/UV-34-13  
 Site plan for C and D



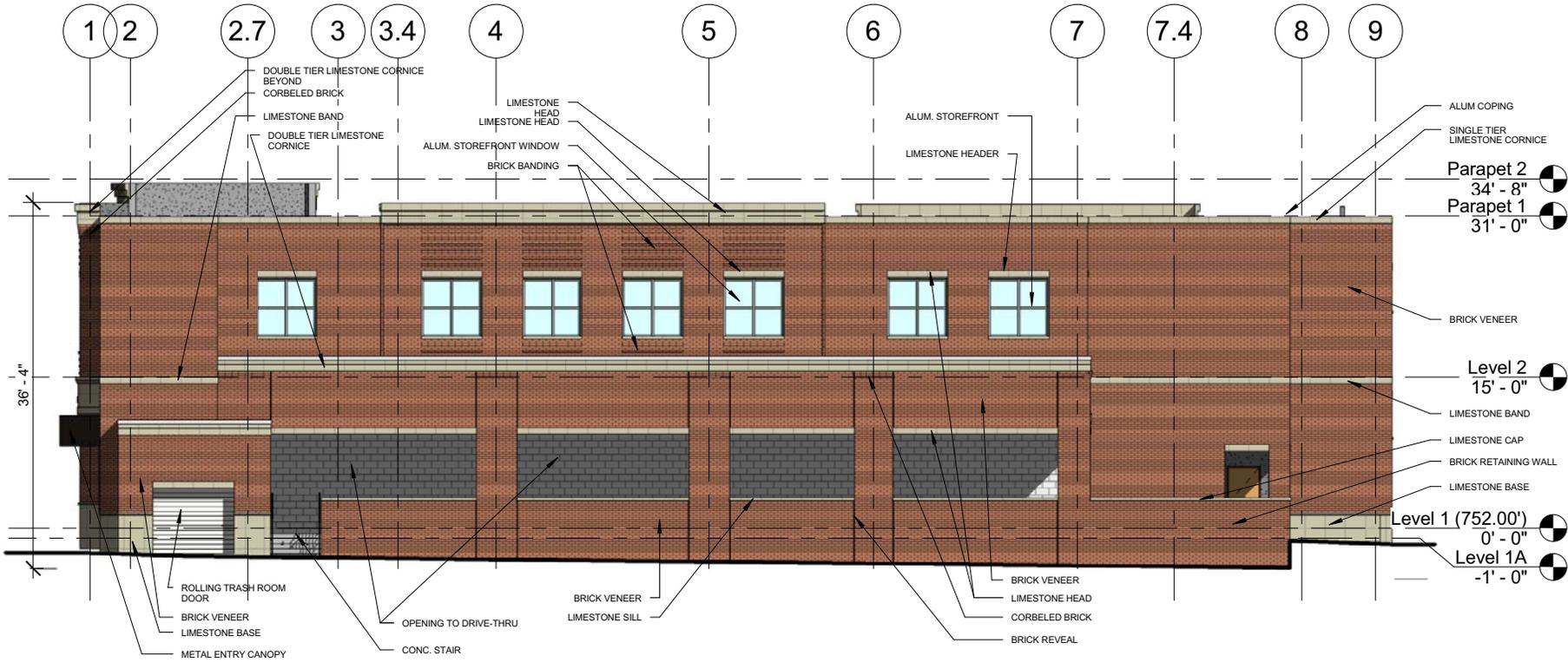
1 NORTH ELEVATION  
3/32" = 1'-0"

SHEET NUMBER <b>A3</b>	
SHEET DESCRIPTION <b>NORTH ELEVATION</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	
	



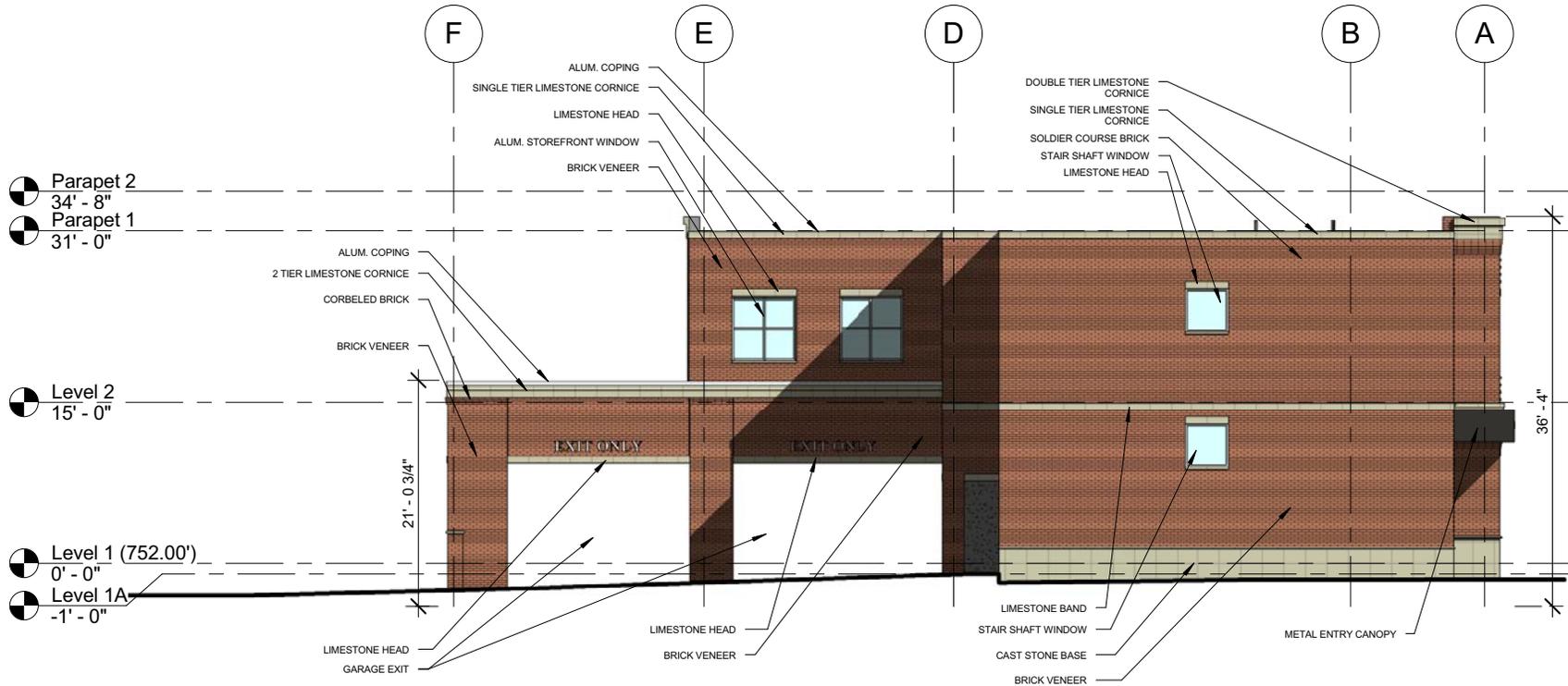
1 WEST ELEVATION  
3/52" = 1'-0"

SHEET NUMBER <b>A4</b>	
SHEET DESCRIPTION <b>WEST ELEVATION</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	



1 SOUTH ELEVATION  
5/32" = 1'-0"

SHEET NUMBER <b>A5</b>	
SHEET DESCRIPTION <b>SOUTH ELEVATION</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	
<b>STUDIO THREE DESIGN</b>	



1 EAST ELEVATION  
3/32" = 1'-0"

SHEET NUMBER <b>A6</b>	
SHEET DESCRIPTION <b>EAST ELEVATION</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	



1  
A8 VIEW FROM NORTH WEST 1

SHEET NUMBER <b>A8</b>	
SHEET DESCRIPTION <b>VIEW FROM NORTH WEST 1</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	
	

SP/UV-34-13  
Model images



1 KIRKWOOD LOOKING EAST  
A9

PROJECT NO. 13018.04		DATE 11/24/2014	
SHEET DESCRIPTION KIRKWOOD LOOKING EAST			
SHEET NUMBER A9			
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA			

SP/UV-34-13  
Model images



1  
A10

VIEW FROM NORTH EAST

SHEET NUMBER

A10

SHEET DESCRIPTION

VIEW FROM  
NORTH EAST

PROJECT NO.  
13018.04

DATE  
11/24/2014

GMS-PAVILION PROPERTIES, LLC.

GERSHMAN PARTNERS

ONB "SITE C" DEVELOPMENT

BLOOMINGTON, INDIANA

STUDIO  
THREE  
DESIGN



1 VIEW FROM NORTH WEST 2  
A11

SHEET NUMBER

A11

SHEET DESCRIPTION

VIEW FROM  
NORTH WEST  
2

PROJECT NO.  
13018.04

DATE  
11/24/2014

GMS-PAVILION PROPERTIES, LLC.

GERSHMAN PARTNERS

ONB "SITE C" DEVELOPMENT

BLOOMINGTON, INDIANA





1 LINCOLN LOOKING SOUTH  
A12

SHEET NUMBER

A12

SHEET DESCRIPTION

LINCOLN  
LOOKING  
SOUTH

PROJECT NO.  
13018.04

DATE  
11/24/2014

GMS-PAVILION PROPERTIES, LLC.

GERSHMAN PARTNERS

ONB "SITE C" DEVELOPMENT

BLOOMINGTON, INDIANA





1 VIEW FROM SOUTH WEST  
A13

SHEET NUMBER <b>A13</b>	
SHEET DESCRIPTION VIEW FROM SOUTH WEST	
PROJECT NO. 13018.04	DATE 11/24/2014
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	
	



1 VIEW FROM SOUTH EAST  
A14

SHEET NUMBER <b>A14</b>	
SHEET DESCRIPTION <b>VIEW FROM SOUTH EAST</b>	
PROJECT NO. <b>13018.04</b>	DATE <b>11/24/2014</b>
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA	
	



1 OVERALL AERIAL OF SITE  
A15

SHEET NUMBER

A15

SHEET DESCRIPTION

OVERALL  
AERIAL OF  
SITE

PROJECT NO.  
13018.04

DATE  
11/24/2014

GMS-PAVILION PROPERTIES, LLC.

GERSHMAN PARTNERS

ONB "SITE C" DEVELOPMENT

BLOOMINGTON, INDIANA





1 AERIAL  
A16

PROJECT NO. <b>13018.04</b>		SHEET DESCRIPTION <b>AERIAL</b>	SHEET NUMBER <b>A16</b>
DATE <b>11/24/2014</b>			
GMS-PAVILION PROPERTIES, LLC. GERSHMAN PARTNERS ONB "SITE C" DEVELOPMENT BLOOMINGTON, INDIANA			
STUDIO THREE DESIGN			

**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT  
Location: 210 E. Kirkwood Avenue**

**CASE #: SP-14-14  
DATE: December 8, 2014**

---

**PETITIONERS: AJ Capital Partners  
621 W/ Randolph, Suite 4, Chicago**

**CONSULTANT: Ratio Architects  
455 N. Cityfront Plaza Drive, #1800, Chicago**

**Smith Brehob & Associates  
453 S. Clarizz Blvd, Bloomington**

---

**REQUEST:** The petitioners are requesting site plan approval in order to construct a 146-room hotel.

---

<b>Area:</b>	<b>0.64 Acres</b>	
<b>Zoning:</b>	<b>CD/University Village Overlay (UVO)</b>	
<b>GPP Designation:</b>	<b>Downtown</b>	
<b>Existing Land Use:</b>	<b>2-story Bank</b>	
<b>Proposed Land Use:</b>	<b>Hotel</b>	
<b>Surrounding Uses:</b>	<b>North</b>	– First Christian Church and Commercial
	<b>West</b>	– Peoples State Bank
	<b>South</b>	– First United Methodist Church
	<b>East</b>	– Drive-through bank (SP/UV-34-13)

---

**PROJECT REVIEW PROCESS:** Due to the unique aggregation of the Old National Bank properties in such a prominent downtown location, staff verbally presented Plan Commission Case #'s SP/UV-34-13, SP-26-14 (withdrawn), and SP-14-14 as an integrated report to the Plan Commission on October 6, 2014. The petitioners have followed staff and Plan Commission guidance to bring forward all of the development projects at the same time to achieve this coordinated review. The proposals now need to be evaluated and acted upon as separate site plan requests.

In summary, Old National Bank owns the following properties in the heart of the downtown area:

- **Site A:** Contains the Workingman's Bank/ONB building, 121 E. Kirkwood Ave. - 0.45 acres – Recently approved for a CVS Pharmacy by the BZA, CU/V-32-14  
**\*Please note that Site A is not part of this petition.**
- **Site B:** Contains the Monroe Bank/Current ONB Bank building, 210 E. Kirkwood Ave. - 0.60 acres – Proposed hotel, SP-14-14

- **Site C:** Contains the ONB Drive-through facility - 0.40 acres – proposed new ONB Bank, SP/UV-34-13
- **Site D:** Contains the Former Monroe Bank Loan Center building, 111 E. 4<sup>th</sup> Street - 0.40 acres – proposed surface parking lot, SP/UV-34-14
- **Site E:** Surface parking lot at the northwest corner of 4<sup>th</sup> and Grant - 0.30 acres – No current proposal, will remain surface parking

The aggregate total of these properties is 2.15 acres, with 1.45 acres being located directly on Kirkwood Ave.

---

**PROPERTY SUMMARY:** Site B is located at the southwest corner of E. Kirkwood Avenue and S. Lincoln Street. The property is zoned Commercial Downtown (CD) and is within the Kirkwood Corridor portion of the University Village Overlay (UVO). The property currently contains a two-story bank building and surface parking.

---

**PETITION DETAILS:** The petitioners are proposing to demolish the existing building and construct a 6-story hotel with approximately 146 guest rooms. The hotel would include additional restaurant and/or retail space along the Kirkwood frontage as well as meeting space on several levels. There are also 75 structured parking spaces located on the second floor and a portion of the first floor. The garage area will also provide for the service and loading function of the hotel.

---

**PLAN COMMISSION SITE REVIEW:** Two aspects of this project require that the petition be reviewed by the Plan Commission. These aspects are:

- Per BMC 20.03.160 - The petitioners are requesting four waivers from development standards. These waivers include:
  1. Maximum Height
  2. Parapet Requirement
  3. Window Design Standards
  4. Façade Modulation
- Per BMC 20.09.120 – The petitioners are proposing a nonresidential development of more than 25,000 square feet.

---

**SUMMARY OF FIRST HEARING:** The Plan Commission discussed this petition at its October 6, 2014 hearing. Since the first hearing, the petition has not undergone significant revisions. The main issues discussed at the first hearing were height, building material mix, and massing. Parking for the petition was also discussed as part of a larger dialogue regarding the loss of a number of surface parking spaces over all of the four parcels that were previously being considered for development. The petitioners have provided a physical model to assist in the discussion of height and massing. The model was made available at a workshop and will also be made available at the Plan Commission meeting.

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## SITE PLAN REVIEW

**Impervious Surface Coverage:** The Kirkwood Corridor portions of the UVO permit up to 100% impervious surface coverage. The petitioners are proposing a hotel structure that would utilize the entire property and comply with code.

**Building Height:** The height of the building is approximately 70 feet at its highest point at the southeast corner of the building. As a result, a height waiver is clearly required. The maximum height allowed in this overlay is 40 feet. The purpose of the 40 foot standard is to protect the scale of development along the Kirkwood Corridor, which is characterized by relatively low building heights. Staff has received comments of concern from representatives of the surrounding churches regarding the proposed height of the structure. Additionally, one Plan Commissioner has indicated concern about the proposed height of the building in reference to the People's State Bank building and the United Methodist Church structure.

**Height Waiver - 20.03.190(b)(1):** The Downtown Vision and Infill Strategy Plan (Downtown Plan) states that "*the dominant scale of two-four stories should be maintained*" for new construction. The Plan also discusses the importance of upper floor recession to mitigate against the impacts of taller structures with greater mass.

As evidenced by the recent construction of the Hyatt Place Hotel and Springhill Suites project, hotels have been allowed to be constructed at higher heights than other downtown structures. Specifically, the Hilton Garden Inn has a maximum height of 67 feet, while the Hyatt is at 74 feet and Springhill Suites is at 71 feet. This creates the policy decision for the Plan Commission. In the interest of providing a viable hotel project that could bring additional energy to Kirkwood Avenue as well as a desired alternative to student housing, it is necessary to deviate from the height standard of the code and the historic building heights along the corridor. Staff recommends that the Plan Commission consider the proposed taller building height in order to accommodate this specific use. To this end, staff has worked to make sure the proposed hotel is appropriately recessed so that the pedestrian experience along Kirkwood Ave. is still suitably scaled.

To achieve a lower height along Kirkwood Ave. and to create additional articulation and visual interest, the petitioners have designed the proposed structure to recess the 6<sup>th</sup> floor and have created significant recessions above three stories. They have also included several outdoor spaces in the design along Kirkwood Ave. and Lincoln St. The main masses of the building along Kirkwood have relative heights of 30, 42 and 62 feet. The petitioners have also included a significant amount of glass to the third level meeting space to soften the massing along Kirkwood Ave.

While staff is supportive of increasing heights for hotel structures and believes the height has been suitably mitigated along the Kirkwood Corridor, staff also finds that

the increased height is also supported by the surrounding building context. The fifth floor of the hotel is approximately 57-60 feet in height with the recessed 6<sup>th</sup> floor being approximately 67-70 feet depending on the adjacent grade. For comparison, the First United Methodist Church to the south ranges from approximately 56 feet to 68 feet for the main portions of the building with the tower element reaching approximately 84 feet. The taller portion of the First Christian Church to the north is also approximately 68 feet in height.

Staff does note that the proposed structure does have a significant height increase over the People's State Bank structure to the west that is two stories in height. This façade will extend above this structure and will be visible to those approaching the site from the west. There is a drive that separates these structures and staff anticipates that in the long-term, this site has a high potential for redevelopment with a taller building. Because the existing bank structure has no historic significance and will be replaced with a taller structure, this relative height difference can be supported.

**Parking:** The UVO does not require on-site parking to be provided for non-residential space. The petitioners had proposed to construct 74 on-site parking spaces for the 146 guest rooms, or just over a 50% ratio. The hotel petitioners had also worked with the petitioners for Parcels D & E to provide additional shared parking spaces within a proposed garage structure. The garage structure is no longer being proposed and has been replaced with a petition to remove the existing one-story office building and reconfigure the existing parking lot to provide additional parking spaces. Although not required, the petitioners are proposing to use a portion of the proposed parking area to supplement the on-site parking for this site.

They have also revised their parking proposal to include the use of valet parking both within their structure and on the surface parking lot. This revision would include 75 regular parking spaces and up to 40 valet spaces for a total of 115 on-site spaces (nearly a 79% ratio). The off-site parking could include up to 46 standard spaces and 15 valet parking spaces. This total potential parking count would be able to accommodate heavy weekend usage and mitigate off-site parking impacts to surrounding land uses. The proposed hotel project also includes the removal of three existing drive cuts and the installation of new on-street parking spaces.

**Setbacks:** This site has no setback requirements for the side and rear property lines and has maximum setbacks from the front property lines. The petitioners have placed the proposed building at the right-of-way lines and meet this requirement as well as meeting the requirement to align the structure with the church structure to the south.

**Ground Floor Commercial Percentage:** The ordinance requires 50% of the first floor space to be used commercially. The proposed hotel/restaurant/retail use clearly complies with this requirement.

**Historic Compatibility:** This building is immediately adjacent to the First United Methodist Church, a surveyed historic structure. It is listed as a Notable Structure in the 2001 Survey of Historic Sites and Structures. The church structure ranges in height from approximately 56 feet to nearly 85 feet at its tower.

The UDO has two requirements regarding compatibility of new buildings adjacent to historic structures for this property.

1. Match street setbacks. The church structure is located at the right-of-way line, as is the proposed hotel.
2. The new building to the side of a historic building must be stepped down to be no taller than 1-story, or 14 feet, taller than the historic building. The proposed structure is at its highest approximately 14 feet taller than the main portions of the church and lower than the church's tower structure. Furthermore, portions of the 6<sup>th</sup> floor have been recessed.
3. The new building should align similar architectural features with the historic building. Staff finds that this guideline may be less necessary to meet due to the location of the alley between the structures, the distance between the structures, and the distinctly different architectural styles of the two structures.

In addition to the surveyed church structure, there is also one locally designated structure (Victoria Towers) located on the north side of Kirkwood Ave. as well as two notable surveyed structures also located north of Kirkwood Avenue. Because of the great distance between the hotel site and these structures, it is not necessary that they be constructed with the same architectural style or massing.

**Architecture:** The petitioners have stated that the design of the building is intended to create a balance of traditional and modern forms. The building includes both an industrial feel with more modern glass and floating roof elements. The materials are predominantly brick, glass, limestone, and metal. The design also incorporates a highlighted entry along Kirkwood Ave. that has a recessed patio and a steel and glass architectural element. The building is proposed with a high level of articulation along the Kirkwood façade with a large amount of storefront glass at the street level. All of the petitioners' materials are permitted within this overlay.

The design team has included several outdoor spaces that will be accessible to hotel patrons. These spaces and the proposed building recesses create a high level of building articulation. A large glass curtain wall has also been proposed between the large brick portions of the building along the Kirkwood frontage to reduce the overall massing along this corridor.

Staff has received a Plan Commissioner comment that more masonry should be used against the south and west facades as opposed to the proposed metal panels. Staff notes

that such panels are permitted in the overlay. Additionally, staff also notes that the usage of a different building material should accentuate the proposed recessing of the taller parts of the structure.

**Access:** Access to the first level parking garage would be from the east-west alley to the south, while access to the second level garage spaces would be from a drive cut onto Lincoln Street. This would be a nearly identical location to an existing drive cut. With this proposal, three additional existing drive cuts would be removed including two along Kirkwood Ave.

Primary pedestrian access to the hotel will be from Lincoln Street, with two additional pedestrian entries located along Kirkwood Ave. The petitioners have incorporated architectural design features into the Lincoln Street entry and the eastern entry along Kirkwood Ave. to meet the primary pedestrian entry detailing requirements for both street frontages.

**Streetscape:** The property has two adjacent street frontages. The Kirkwood Ave. streetscape was installed as part of a larger City streetscape capital project. Construction of the proposed hotel and the removal of the existing drive cuts will create a need for much of the streetscape to be reconstructed. The petitioners have committed to reconstructing the streetscape to match the existing Kirkwood Ave. streetscape including but not limited to materials, patterns, lighting, planters, and curbing.

The petitioners have also proposed to continue the Kirkwood streetscape design south on Lincoln St. In addition, they are proposing to utilize the existing street trees on this street frontage keeping a large tree plot around the trees. Staff recommends that a pedestrian connection be added between these trees to connect the adjacent street parking to the hotel entry.

The street lights along Kirkwood Ave. would be removed, stored, and reused upon completion of the hotel construction. The petitioners are proposing to replace the street tree planters along Kirkwood Ave, with 5 x 5 tree grates. With the removal of the street cuts, staff finds this to be a appropriate proposal. However, staff finds that the easternmost planter has a desirable tree that could be retained with its planter with minimal impact to the plan. One on-street parking space that would have been created would no longer be installed. The westernmost curb could be shifted slightly to the west so that the other proposed on-street spaces could be installed along Kirkwood Ave.

**Roof Design:** The proposed structure has been designed with an appropriate base, middle, and cap. The petitioners' design incorporates a recessed 6<sup>th</sup> Story that includes floor to ceiling glass with a projected flat roof that creates a cap to a portion of the building.

**Parapet Waiver – 20.03.200(b)(1)(C):** Although a large portion of the building has a traditional parapet, the flat portions of the roof about the glass “cap” do not have a

parapet. Staff finds that the intent stated in Design Guideline 3.4 of the Downtown Plan is still met with this petition. Adding a parapet to these architectural portions of the roof structure would negatively affect the aesthetic of more modern portion of the building.

**Void-to-Solid Ratio:** The petitioners are required to provide a minimum void-to-solid ratio of 60% on the first floors facing a public street. The proposed hotel has exceeded this standard, proposing nearly 75% void-to-solid along the street frontages.

**Windows:** Due to the blend of modern and industrial architecture, many of the windows do not meet the height-to-width ratio and design requirements for every window to have sills and lintels. This is due to the architectural design chosen by the petitioners. These requirements are in place to ensure a quality architectural treatment. Staff is supportive of this waiver because the proposed architectural style is appropriate and will add to the architectural variety expected along Kirkwood Ave.

**Window Design Waiver-20.03.200(b)(3)(C):** A waiver from the standard of the UDO is required to allow windows that are less than 1.5:1 in ratio of height to width and for some windows not to have sills and lintels. The Downtown Plan's recommendations for upper story windows (guidelines 3.13-3.14) call for windows with a vertical emphasis and trim that aligns with adjacent traditional buildings. While not all of the windows meet the required ratio, the intent of this standard has been met. Although the windows could be modified to meet the standards, staff finds it appropriate to allow some flexibility in design to achieve more variety in building façades and allow a more modern design.

**Façade Modulation:** The building has utilized materials and design elements to create multiple visual modules. The UDO requires modules of no more than 50 feet in width and recessions of at least 3% that extend the length of its module. The petitioners do not meet this guideline as the majority of the building is located at the right-of-way line. However there are recessed entries, roof-top decks, recessed upper floors and material changes that create a high level of articulation and visual interest.

**Façade Modulation Waiver-20.03.200(c)(1):** The Downtown Plan states that "A larger building should be divided into 'modules' that are similar in scale to buildings seen historically." It also states that the mass of a tall building should be stepped down to a lower height as it approaches traditional buildings nearby. The proposed structure is appropriately scaled with adjacent structures with the exception of the bank building to the west. However, this is not a traditional building and has a high possibility of future redevelopment. Furthermore, staff finds that the proposed architecture meets the intent of the façade modulation requirement and provides ample articulation on both building facades similar to the institutional buildings in the immediately surrounding area. The multiple columns, the corner focal element, as

well as the tiered third and fourth floor create significant articulation and architectural interest as envisioned by the Downtown Plan.

**Bike Parking:** The petitioners will exceed the minimum bicycle parking standards through the inclusion of bike parking along Lincoln St. as well as bicycle parking located within the first floor parking area.

**Signage:** It is also anticipated that if this proposal is approved, that a signage variance package for the hotel might be appropriate. However, the proposed signage on the submitted renderings should only be seen as conceptual.

**Timing:** The petitioners are seeking an extended site plan approval due to the complex nature of this project. The construction of the hotel is contingent upon the phasing of construction of the adjacent bank building on Site C. The current phasing anticipates that Site C would be constructed first. Upon completion, the bank service and offices would be relocated from the existing structure on this site to the newly constructed building on Site C. At that time, the petitioners would be able to initiate construction of the hotel facility. Therefore, the petitioners are requesting to have an extended site plan approval of 3 years rather than the usual 1 year. Staff is supportive of this request due to the complexity and phasing of this development.

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**ENVIRONMENTAL COMMISSION RECOMMENDATIONS:** The Environmental Commission (EC) has made 3 recommendations concerning this development.

1.) The Petitioner should apply state-of-the-art green building and site design practices to create a high performance, low carbon-footprint structure, and grounds that exhibit our City's commitment to environmental sustainability.

2.) The Petitioner should employ modern practices and features that result in a unique, sustainable ambiance that builds upon the famous Kirkwood Corridor desirability and Bloomington's environmental integrity.

**Staff response:** Although the petitioners are not proposing to seek LEED certification, they have included several green development elements into their proposal. These include items such as recycled materials, building materials produced in proximity to the site, energy reduction practices, and reflective and green roof elements.

3.) The Petitioner should provide bins and space for recyclable materials to be stored for collection, and a recycling contractor to pick it up.

**Staff's Response:** The petitioners have proposed to include accommodations for a full range of recycling.

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**CONCLUSIONS:** Staff finds that the petitioners have worked closely with staff to develop an appropriately-scaled structure that brings a desirable hotel use to the Kirkwood Corridor. This use will generate a significant number of new customers to the surrounding businesses and is a welcome alternative to conventional multifamily housing. Although the height of the building exceeds the standard for this area, staff has recognized in past approvals that hotels can appropriately and traditionally exceed surrounding heights. Due to the taller structures in the immediate area and the reduced heights along the Kirkwood Ave. façade, staff finds the height and mass of the structure to be appropriate.

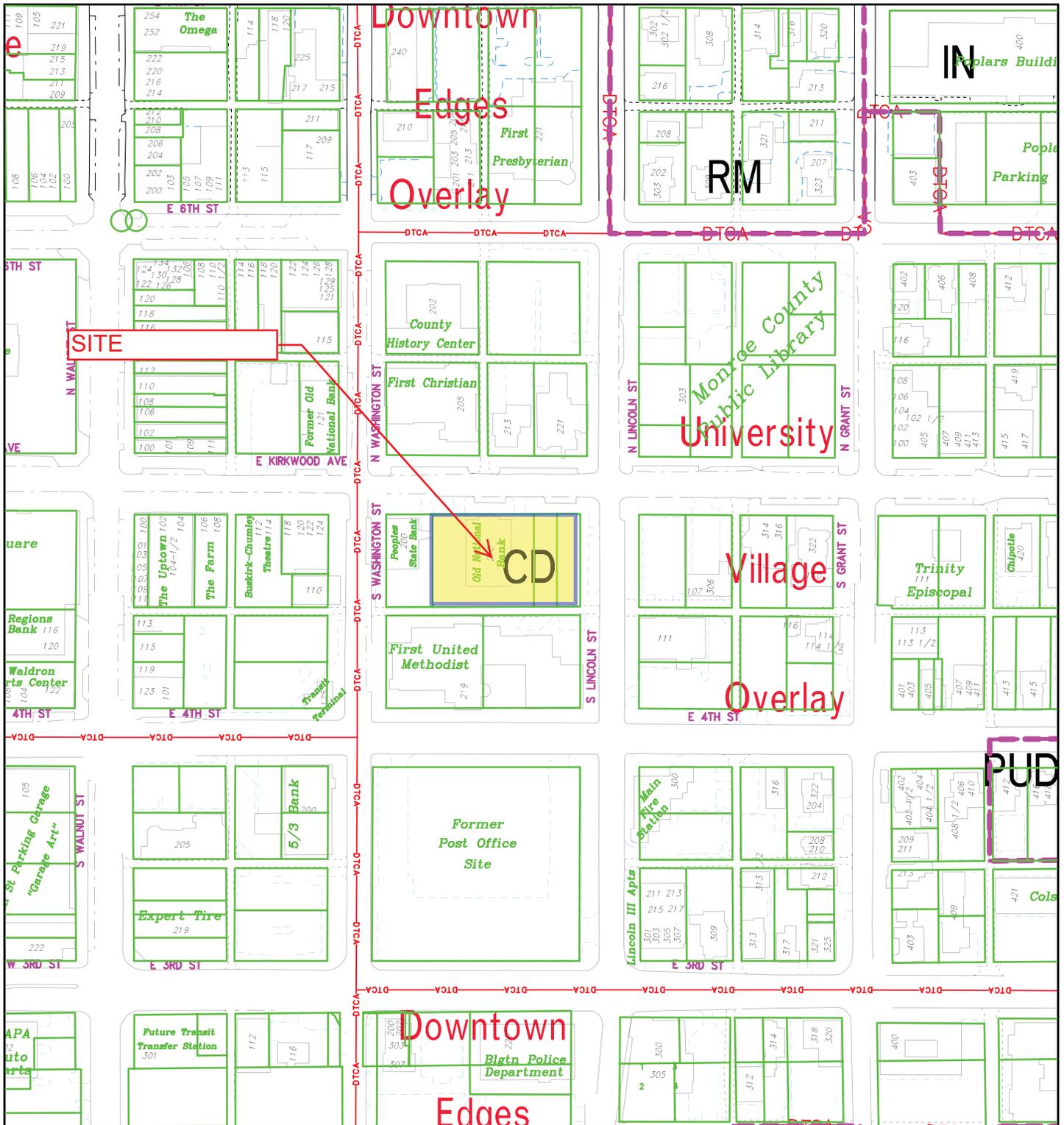
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**DEVELOPER TRACK RECORD:** AJ Capital has no past development history within the City of Bloomington.

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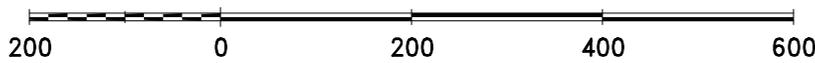
**RECOMMENDATION:** Staff recommends approval of SP-14-14 and all associated waivers with the following conditions:

1. One additional pedestrian-scaled street light must be added to the Lincoln St. streetscape.
2. A pedestrian sidewalk connection is required to be installed between the two street trees along Lincoln St.
3. The petitioners must coordinate all proposed streetscape improvements with staff as well as the City Public Works and Parks and Recreation Departments with the intent to require all improvements to be consistent with the existing streetscape in terms of materials, colors, patterns, and other items such as benches, bike racks, and lighting. The easternmost tree planter and street tree along Kirkwood Ave. and the two street trees along Lincoln St. must be retained and protected with fencing adequate to prevent vehicle/equipment damage and storage to their root systems.
4. This site plan approval shall be valid for a period of three years from the date of this approval.
5. The architecture of the proposed building shall be consistent with the submitted elevations and renderings.
6. No signage approval is granted with this petition.
7. The proposed valet parking proposal submitted with this petition shall be implemented as documented in this staff report.

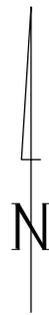


SP-14-14 AJ Capital  
 210 E. Kirkwood Ave.  
 Location/Zoning/Land Use Map

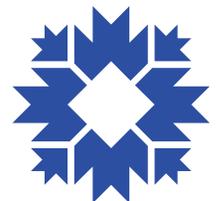
By: shapp  
 25 Apr 14



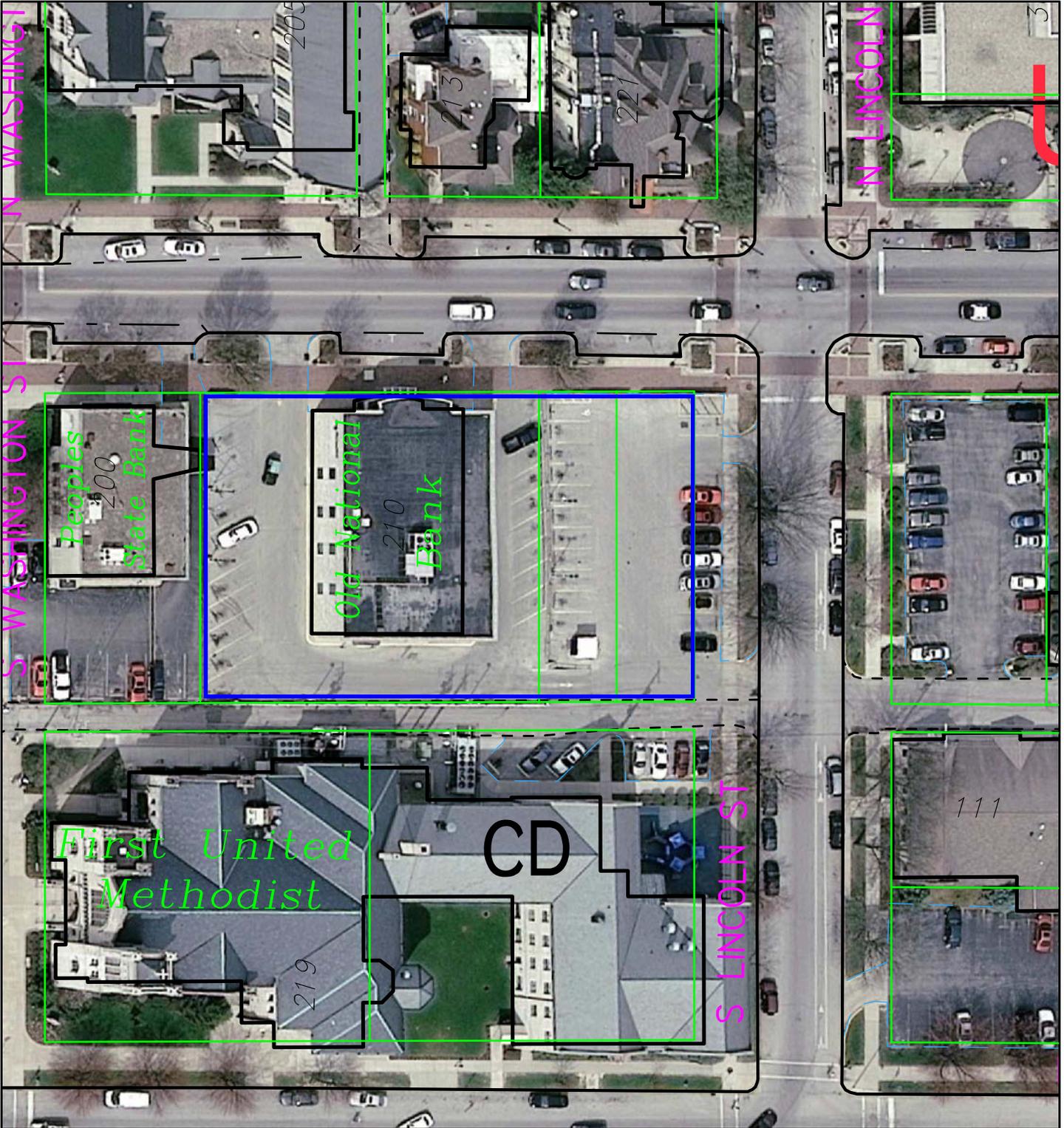
For reference only; map information NOT warranted.



City of Bloomington  
 Planning

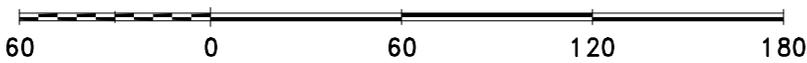


Scale: 1" = 200'



Aerial Photo  
SP-14-14

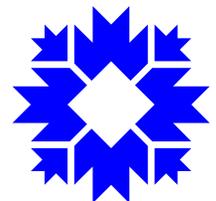
By: shayp  
25 Apr 14



For reference only; map information NOT warranted.



City of Bloomington  
Planning



Scale: 1" = 60'



## RATIO

Architecture Preservation Interior Design Landscape Architecture Urban Design & Planning Graphic Design

May 7, 2014

City of Bloomington Planning Department  
P.O. Box 100  
Bloomington, IN 47402

Attn: Mr. Tom Micuda

### **Petitioner's Statement Kirkwood ONB Site**

#### **Location**

The project is located 210 E. Kirkwood Ave, at the NE corner of Kirkwood and S. Lincoln St. The site is currently being used by Old National Bank for operations, and is generally known as "Site B" with relation to parcels being sold by Old National Bank. A large majority of the site is an existing surface parking lot for bank customers and employees.

#### **Existing Site**

The existing site is .64 acres in size and is located within the University Overlay district. The existing building is oriented with the front facing E. Kirkwood Ave. The proposed structure will be orientated with fronts facing E. Kirkwood Ave. and S. Lincoln Street. All existing improvements on site will be demolished.

#### **Project Overview**

The project is located in central Bloomington, along Kirkwood Avenue and Lincoln Street. The project will provide a hotel development that will enhance the urban character and add to the eclectic nature of the uses and design of the neighborhood. The hotel is of a boutique character that will have a unique design on the exterior and interior, modeled off other AJ Capital hotel developments in similar markets with a large University presence. The street façade will present a hotel and hospitality character, with lobby, food and beverage operations of a retail nature. The interiors will be designed in a unique, locally referenced idiom designed specifically by AJ Capital for this Bloomington location and context. The Second Floor will have parking spaces within the building to accommodate approximately 50% of the guest room key count, and the 3<sup>rd</sup>-5<sup>th</sup> Floors will be dedicated to hotel rooms. The top, 6<sup>th</sup> Floor will be set back from the façade line on both Kirkwood and Lincoln, and will provide a combination of guest rooms and a rooftop bar at the street corner facing northeast. This indoor/outdoor roof top venue is a signature element of AJ Capital's hotel developments. There will be modestly sized meeting room facility and exterior landscaped courtyard on the 3<sup>rd</sup> Floor, thus covering all parking areas and creating another asset to the community.

Presently, the site is used by Old National Bank for their Bank facility and surface parking, both of which will need to be relocated to other sites in the Kirkwood neighborhood, allowing the development of this site as a hotel, with food and beverage operations that will add to the existing vitality of the street. The Kirkwood façade along the streetscape will

**Petitioner's Statement – Kirkwood ONB Site B**

Page 2

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step back at the western end of the site to provide an urban oasis along Kirkwood that will provide outdoor seating areas for the retail uses in the hotel development.

AJ Capital does not intend to formally submit the project for LEED Certification; however, the project will be designed to be sustainable and to respect the City's desire to incorporate "green" environmental practices into project development. RATIO has designed dozens of LEED projects, including a number of LEED Gold projects and so is well acquainted with sustainable design principals. The development is located on Public Transit, and we will provide secured bicycle parking. Accommodations for a full range of recycling will be made in the design, as well as the use of recycled materials in the construction, and the utilization of building materials produced within 500 miles of the site. The building envelope will be well designed to exceed current energy demand standards in the industry, to include high performance glazing and reflective roof surfaces. The design intent is also to provide a partial green roof and landscape roof decks at selected locations. Water usage will be minimized thru the use of lower flow fixtures. Electricity usage will be minimized thru the use of efficient light fixtures, and energy management controls in the guest rooms.

**Development Information****Use-Hotel and accessory support facilities including parking (use permitted under the UDO)**

1. 6 Level Building on Kirkwood and Lincoln, 3 Levels along the southern alley.
2. Level 1-Hotel Lobby, Food and Beverage Services, Hotel Offices and Support Spaces, Loading, Trash/Recycling Room and 9 grade-level guest parking spaces for check in and short term guest use.
3. Level 2-61 enclosed and secured parking spaces.
4. Levels 3-5-Guest Keys (Rooms), Meeting Rooms, Fitness Center (Rooms)
5. Level 6-Guest Rooms and Rooftop Bar
6. Total number of Guest Keys-141

**Development Standards-University Village Overlay (UVO) District****20.03-150 University Village Overlay District Intent**

The proposed Kirkwood Hotel will meet the intent of the District by supplementing the diversity of land uses and mix of developments along Kirkwood, and serving as a key focal point and transition between the Downtown Core/Courthouse Square and the Indiana University Campus. The hotel will serve both aspects of the community, Downtown and University. The traditional main street character of the Kirkwood Corridor as a strong, pedestrian friendly route will be enhanced with a lot line façade, retail storefront design of the facades and pedestrian features in the streetscape design.

**20.03-190 University Village Overlay District Development Standards****Density and Intensity Standards-Project Complies with UDO**

*Maximum Residential Density: Not Applicable*

*Maximum Impervious Surface Coverage: Kirkwood Corridor, 100%*

**Height Standards-Project Requires Waiver from UDO**

*Maximum Structure Height: 40 Feet*

**Petitioner's Statement – Kirkwood ONB Site B**

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Actual Project Height: 67 feet (Note; top or 6<sup>th</sup> Floor is setback, so that the 5<sup>th</sup> Floor Roof line is appx. 55 Feet)

The noted heights are at the highest instance at the corner of Kirkwood and Lincoln; the building will be set into grade as Kirkwood slopes up to the west. The hotel's need for at least some level of parking on site necessitated the addition of one parking floor, which has raised the building's height by 11 feet. Without the Parking the roof line would be approximately 55 feet. The design of the building is such that the top floor will be significantly recessed back from the façade by approximately 8 feet, thus helping to relieve the overall height's visual impact. In addition, the building is backed by the Church to the south, which is one of the taller structures in the UVO District, with heights ranging from 56 feet at the roof to 80 feet at the tower.

**Parking Standards- Project Complies with UDO**

*Minimum Surface Parking Setback-Kirkwood Corridor: 20 Feet behind Primary Structure's front building wall*

Actual Project Setback: Parking is enclosed within building footprint > 70 feet

*Residential Parking Standard: Not Applicable*

*Nonresidential Parking Standard: No parking required*

Actual Project Parking: 70 Parking Spaces on Levels 1 and 2

**Building Setback Standards-Project Complies with UDO**

*Maximum Front Setback: 15 Feet*

Actual Project Front Setback: 0-15 feet

*Minimum Side and Rear Setback: 0 Feet*

Actual Project Side and Rear: 0 feet

**Ground Floor Nonresidential Uses-Project Complies with UDO**

*Minimum Ground Floor Nonresidential Uses: 50% of Ground Floor Area*

Actual Project Ground Floor Nonresidential Use: 56%-27,729sf Site Area-15,500sf Ground Floor Area (non-parking use area)

**20.03-200 University Village Overlay Architectural Standards****Site Plan-Project Complies with UDO**

*Building Orientation: Minimum of 1 Pedestrian Entrance Located on Kirkwood*

Actual Site Plan Design has two entrances on Kirkwood, one of which is within 3 feet of adjacent sidewalk.

*Street Trees*

Landscape Design will comply with Chapter 20.05, Landscaping Standards

**Petitioner's Statement – Kirkwood ONB Site B**

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*Lighting*

Lighting Design will comply with UDO and as approved by the Board of Public Works

*Mechanical Equipment and Service Areas*

All mechanical equipment will be in the rear of the building, off the alley, or in fully screened rooftop locations setback from the facades to screen the equipment from adjacent streets

**Architectural Character-Project Complies with UDO***Roofs or Building Cap: Kirkwood Corridor*

The building design will have a flat roof with parapet < 15% of supporting wall height.

*Void to Solid Ratio: First Floor transparent areas to exceed 50% of total First Floor wall area.*

The hotel will have 75% transparent glass area on the First Floor.

*Upper Story Windows*

The hotel window frame heights will be a minimum of 1.5 times the window frame width, and be visually distinct from the primary exterior finish materials (masonry).

*Materials*

The design will utilize masonry as the primary façade material facing Kirkwood and Lincoln, and no prohibited materials will be utilized as primary or secondary materials. The design intent of the Petitioner is to create an eclectic urban, contextual image thru the juxtaposition of historic and classical forms and used in a contemporary manner. References are made to historical, industrial influences of Bloomington's manufacturing and Indiana limestone mining heritage.

*Entrance Detailing: The primary pedestrian entrance shall incorporate three (3) or more of the following design features;*

The three design features to be used will be finalized as the building design evolves, but will include at a minimum; Recessed Entry (4 feet), Ornamental Paving and integrated landscape planters, Canopy, Pilasters of façade module projecting from the building plan, or Prominent Building Address, Building Name and enhances entryway lighting.

**Mass, Scale and Form-Project Complies with UDO***Building Façade Modulation*

The building design as developed will have a maximum 50' module on the street frontage, and the module offset is a minimum of 3% of the module length to conform with the UDO.

*Building Height Step Down: Applicable as the First United Methodist Church is considered a Contributing Historic Structure in the 1986 City of Bloomington Interim Report.*

The hotel adjacent to a Contributing Historic Structure, however, the project conforms to the UDO as the project as designed is approximately 11 feet higher than the church roofline, and 13 feet lower than the Church spire.

**Petitioner's Statement – Kirkwood ONB Site B**

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**Additional Information****Site Streetscape Improvements**

Construction of the project will require removal of existing sidewalks along Kirkwood and Lincoln. Completion of the project will result in a new streetscape along both roadways. The Kirkwood streetscape will match the existing concrete and brick paver pattern utilized along the Kirkwood corridor including new street trees in tree grates. The Lincoln streetscape will consist of a brick and concrete sidewalk as utilized on Kirkwood, yet will have a wide tree plot as opposed to hardscape and tree grates. It is the intent to attempt to preserve the large street trees in existing along Lincoln if they are sufficiently healthy and work can be accomplished around them.

**Bike Storage and Parking**

Bike storage and parking will be provided in the ground level parking area, in compliance with the UDO.

**Building Entrances**

The hotel will have 3 public pedestrian entrances; one at the corner, one at the western end in the site setback on Kirkwood, and one from the rear on grade parking area. Controlled access will be provided from the 2<sup>nd</sup> Floor Parking area directly into the hotel.

**Trash Removal and Recycling**

Trash removal will be accomplished from the alley, via a screened/enclosed trash area off of the grade level parking area. This area will serve the hotel and all food and beverage operations, and will be a level grade location to accommodate rolling trash containers on collection days.

Area will be provided for the separation of goods for collection, in the same area as the trash removal area noted above.

**Building Deliveries**

The 2<sup>nd</sup> Floor of the building is elevated to the extent that vans, and non-semi trucks will be able to access the loading area under the 2<sup>nd</sup> Floor Parking deck. Semi-truck deliveries will be accomplished from the alley through the grade level parking area.

**Curb Cut/Parking Driveway**

The hotel parking driveway and vehicular entrance is location adjacent to the existing alley on Lincoln, and will be provided with the following protective features, to protect pedestrians on the Lincoln sidewalk.

1. Accessible type warning paving for visually impaired pedestrians.
2. Wall Openings between the Alley and Drive way to allow visual observations of exiting vehicles.
3. Audible and visual (strobe) warning of exiting vehicles.

**Sanitary Sewer**

Sanitary sewer service for the building will be provided by a connection to the existing City main running west to east in the alley. Sewer facilities will also include a grease interceptor for the hotel kitchen and restaurant user. The grease interceptor will be located with the first level service area of the parking garage. No improvements to the existing City sanitary sewer main are required.

**Petitioner's Statement – Kirkwood ONB Site B**

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**Water Service**

Domestic water service and fire protection will be provided by a new connection from the site to the existing City water main along Kirkwood. A master meter pit with a monitored post-indicator valve and fire department connection will be located within the outdoor seating area located at the northwest corner of the site. Encroachment within the public right of way should not be required. No improvements to the existing City water main are required.

**Storm Sewer**

A new storm sewer system will be installed along Lincoln and within the alley to convey runoff from the site to the City storm sewer network. Building roof drains and downspouts will be connected to a below grade system to alleviate runoff flow onto the public sidewalks.

**Storm Water Detention**

The existing site is currently 100% impervious area. Site development will not result in an increase in runoff and therefore storm water detention is not required or proposed.

Respectfully Submitted,

RATIO Architects



Craig M. Smith, AIA

Associate Principal and Studio Leader

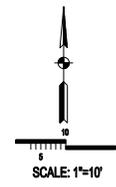
Cc: Tim Franzen – AJ Capital Partners  
Chris Boardman-RATIO  
Steve Brehob – Smith Brehob  
RATIO Personnel – RATIO Architects, Inc. CF W:\2013\13138 - AJCP Bloomington ONB Sites B & D\Management\Code and Agency Approvals\Plan Commission\Plan Commission Submittal Letter 05-07-14.docx

**Graduate Bloomington Hotel  
Parking Inventory**

**Hotel Owned and Protected Parking**

On-Site	Off-Site	On-Site and Off-Site
<b>Ground Level</b>		
Standard      13	Standard      46	On-Site        115
Valet            9	Valet           15	Off-Site        61
22	61	176
<b>Second Level</b>		
Standard      62		
Valet           31		
93		
<b>Area Municipal Parking Options</b>		
<b>Off-Site Public Parking*</b>		
Lot 1            60		* Parking in these municipal lots is free after 5 p.m. on Monday thru Friday, and all day on Saturdays and Sundays.
Lot 3            70		
Lot 5            50		
Lot 6            30		
4th St. Garage 240		
	450	
<b>Hotel On-Site Parking - Total</b>		
Standard      75		
Valet           40		
115		

Hotel Parking  
Breakdown



**SITE LEGEND**

- 1" SWIRL FACED 10" UC
- UTILITY EASEMENT 10" UC
- 3" CURB AND GUTTER 10" UC
- 2" CURB AND GUTTER 10" UC
- 6" STANDING CURB
- CONCRETE SIDEWALK
- HANDICAPPED RAMP
- HANDICAPPED RAMP
- RET. WALL (CONCRETE)
- RET. WALL (MASONRY)
- RET. WALL (STONE)
- RET. WALL (BRICK)
- FENCE (BARB WIRE)
- FENCE (CHAIN LINK)
- FENCE (SMOOTH WIRE)
- FENCE (POST & RAIL)
- FENCE (WOOD SLAT)
- GRASS
- PROPOSED STREET TREE
- RELOC.
- DUMPSTER (WOOD)
- DUMPSTER (MASONRY)

**MATERIALS LEGEND**

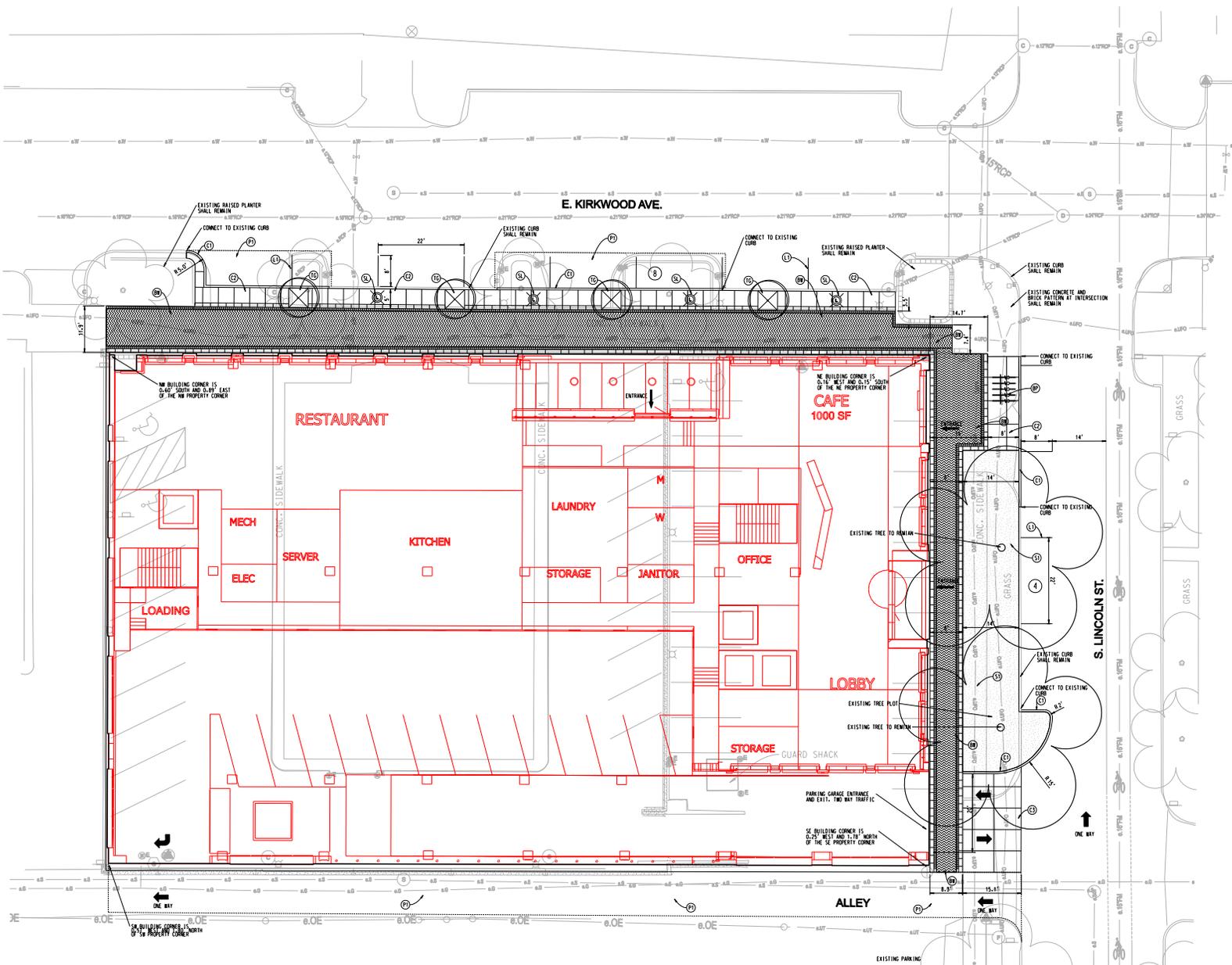
- 1" SWIRL FACED ON 10" CURB
- 7" COMPACTED AGGREGATE #5
- 6" STANDING CURB
- CONCRETE SIDEWALK - WIDTH VARIES
- 4" ASPH. CONCRETE
- 4" COMPACTED AGGREGATE #5
- CONCRETE APPROACH
- 8" COMPACTED AGGREGATE #5
- BRICK PAVED WALK
- LANDSCAPE PLANTER WALL
- LINE PAINT - SOLID WHITE, 4"
- LINE PAINT - SOLID YELLOW, CROSSHATCH, 6"
- MURSERY SID
- HANDICAP RAMP TYPE C
- HANDICAP RAMP TYPE A
- TREE GRADE, 5' X 5'
- STREET TREE, NO TREE GRADE REQUIRED
- STREET LIGHT

**GENERAL NOTES**

1. SEE SOUTH BEND STANDARD SPECIFICATIONS FOR CONSTRUCTION AND ELEVATIONS TO BE GIVEN IN THE FOLLOWING LOCATIONS AT UNLESS SHOWN OTHERWISE:
  - 1. CURB
  - 2. SIDEWALK
  - 3. DRIVE
  - 4. DRIVE
  - 5. DRIVE
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  - 99. DRIVE
  - 100. DRIVE
2. LOCATION OF EXISTING UTILITIES ARE TO BE SHOWN ON THE SITE PLAN BY THE CONTRACTOR. PLEASE NOTIFY ENGINEER IF ANY CHANGES ARE REQUIRED.
3. CONTRACTOR IS RESPONSIBLE FOR OBTAINING A CITY OF SOUTH BEND PERMIT TO BE OPEN UNDER THE PUBLIC R/W TO ACCORDANCE WITH THE PROJECT COLLECTION.
4. REFER TO ARCHITECTURAL PLAN FOR BUILDING DIMENSIONS.

**SITE PLAN NOTES**

1. SIDEWALK RAMP SHALL BE IN ACCORDANCE WITH THE LATEST ADA REQUIREMENTS.
2. PAVEMENT MARKINGS, IF SHOWN, SHALL BE APPLIED IN ACCORDANCE WITH SECTION 808 & 912.14 OF THE 1980 INDOT STANDARD SPECIFICATIONS AND IS SHOWN ON THE PLANS.
3. CONTRACTOR IS RESPONSIBLE FOR OBTAINING A CITY OF SOUTH BEND PERMIT TO BE OPEN UNDER THE PUBLIC R/W TO ACCORDANCE WITH THE PROJECT COLLECTION.
4. REFER TO ARCHITECTURAL PLAN FOR BUILDING DIMENSIONS.



JOB TITLE: **ONE SITE B REDEVELOPMENT**  
**NEW HOTEL**  
**KIRKWOOD AVE, Bloomington IN**

REVISIONS	BY	DATE

D.K. ROSS  
 PROJECT MANAGER  
 S.B.A.

SHEET NO. **5012**  
**3** OF **8**  
 DATE: **9/09/2014**

SITE PLAN

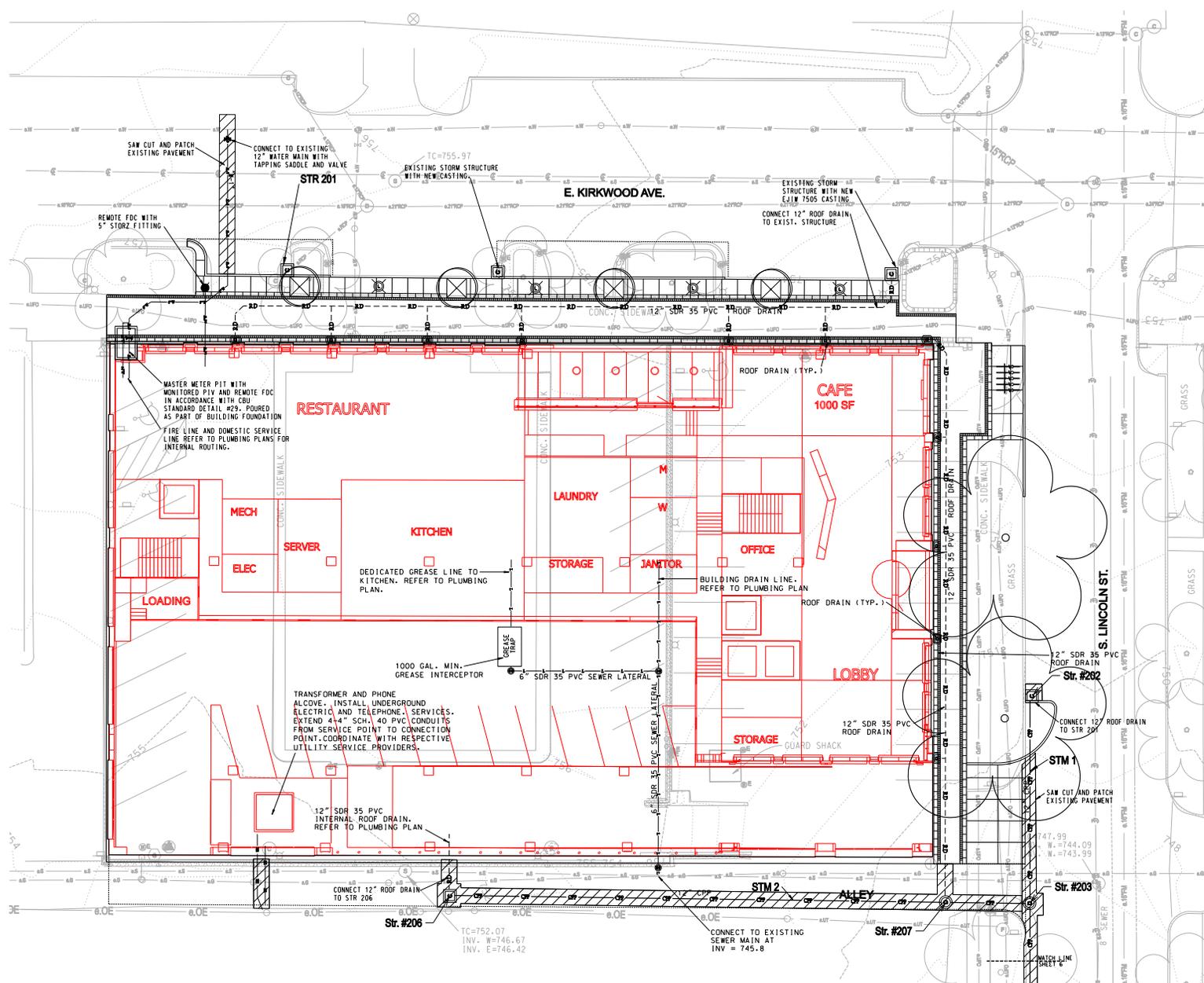
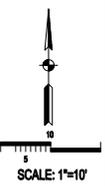
◀ BENCHMARK ▶

**GENERAL NOTES**

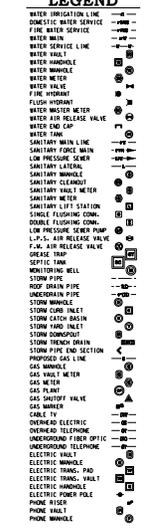
- 1) THE NEW WORK SHOWN ON THIS DRAWING FOR THE CONSTRUCTION OF THE NEW HOTEL IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF BLOOMINGTON STANDARD SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC WORKS, LATEST EDITION, AND THE CITY OF BLOOMINGTON STANDARD SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC WORKS, LATEST EDITION, AND THE CITY OF BLOOMINGTON STANDARD SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC WORKS, LATEST EDITION, AND THE CITY OF BLOOMINGTON STANDARD SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC WORKS, LATEST EDITION.
- 2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF BLOOMINGTON AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO THE START OF CONSTRUCTION.
- 3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF BLOOMINGTON AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO THE START OF CONSTRUCTION.

**UTILITY NOTES**

- 1) THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF BLOOMINGTON AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO THE START OF CONSTRUCTION.
- 2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF BLOOMINGTON AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO THE START OF CONSTRUCTION.
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**UTILITY LEGEND**



**UTILITIES**

- AT&T (Phone)**  
 Brent McCool - (812) 334-4521  
 4517 E. Indiana Blvd. Ct.  
 P.O. Box 56  
 Bloomington, Indiana 47408  
 (812) 334-4594 Fax
- Duke Energy (Electric)**  
 Kerry Ducker (812) 337-3035  
 1100 E. 2nd St.  
 Bloomington, IN 47403  
 (812) 337-3000 Fax
- CITY OF BLOOMINGTON UTILITIES (Water/Sewer)**  
 Nancy Anderson (812) 349-3689  
 600 E. Miller Dr.  
 Bloomington, IN 47402  
 (812) 331-5961 Fax
- VECTREN (Gas)**  
 Doug Anderson (812) 330-4031  
 205 S. Madison  
 Bloomington, IN 47404  
 (812) 330-4057 Fax
- COMCAST (Cable)**  
 Scott Robinson (812) 355-7822  
 2450 S. Anderson St.  
 Bloomington, IN 47401  
 (812) 332-0129 Fax

**STRUCTURE DATA TABLE**

NO.	CATEGORY	DESCRIPTION	DUTY/PIPE SPEC.				REMARKS
			SP	WT	MA	IN	
201	7505	SOB INLET	18	12	NA	NA	SEE EYEL 12" ROF RVP
202	7505	LOW INLET	18	12	NA	NA	CONNECT TO 203
203	1022-1	MANHOLE	18	12	NA	NA	CONNECT TO 204
204	1022-1	SOB INLET	18	12	NA	NA	CONNECT TO 205
205	EXISTING	EXISTING MANHOLE	18	12	NA	NA	CONNECT TO 207
206	6 5940	LOT INLET	6	12	NA	NA	CONNECT TO 207
207	1022-1	MANHOLE	18	12	NA	NA	CONNECT TO 203



**ONB SITE B REDEVELOPMENT**  
**NEW HOTEL**  
**KIRKWOOD AVE, Bloomington IN**

BY	DATE	REVISIONS

**5012**  
**SHEET**  
**5 OF 8**  
 9/09/2014  
**UTILITY PLAN**



LINCOLN (EAST) ELEVATION



KIRKWOOD (NORTH) ELEVATION



WEST ELEVATION



ALLEY (SOUTH) ELEVATION



LINCOLN STREET

KIRKWOOD AVENUE

### VIEW LOOKING WEST ALONG KIRKWOOD

REFER TO ELEVATIONS FOR ADDITIONAL MATERIAL AND DIMENSIONAL INFORMATION



KIRKWOOD AVENUE

### ELEVATION ALONG KIRKWOOD

REFER TO ELEVATIONS FOR ADDITIONAL MATERIAL AND DIMENSIONAL INFORMATION



LINCOLN STREET

KIRKWOOD AVENUE

### CORNER OF LINCOLN AND KIRKWOOD

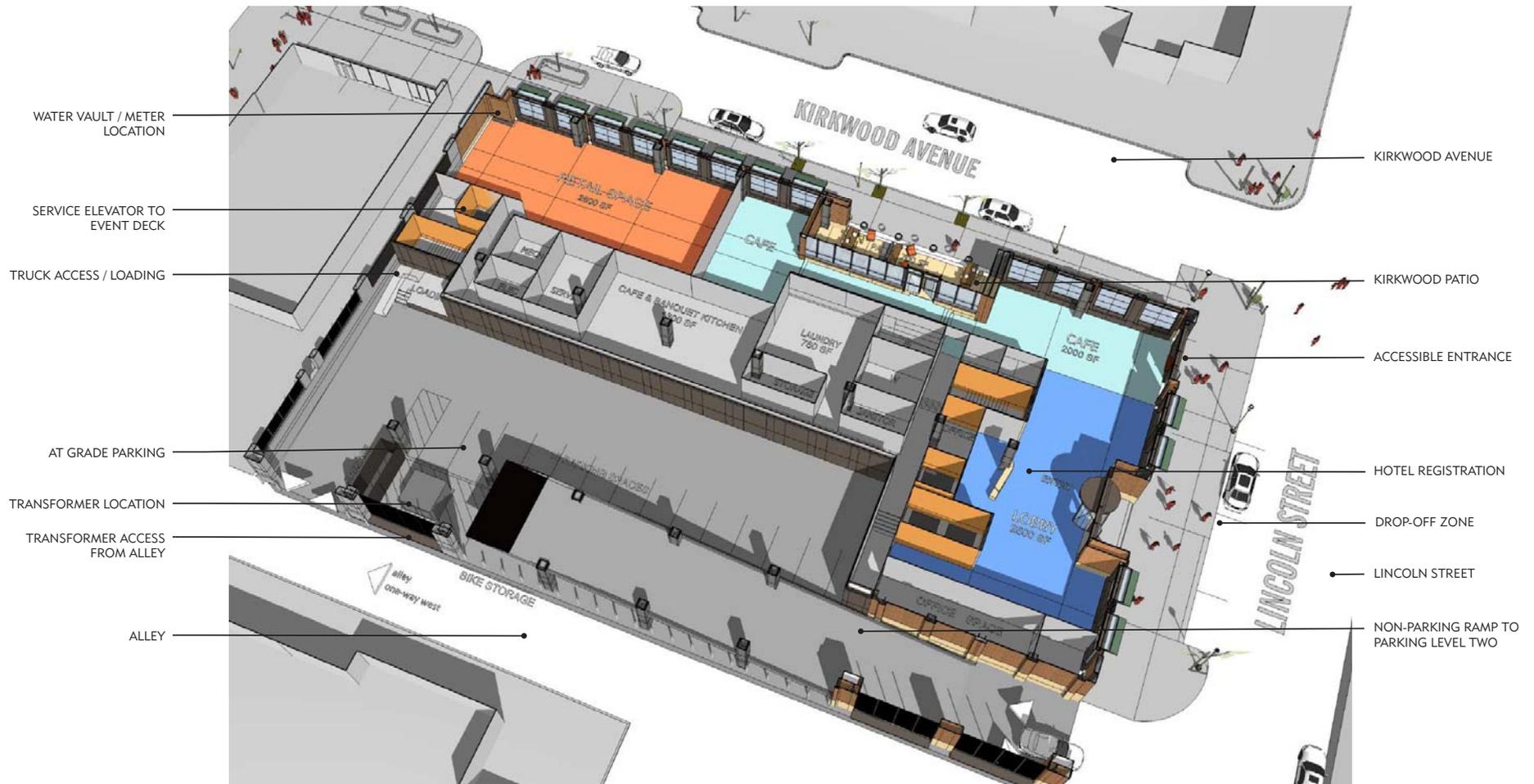
REFER TO ELEVATIONS FOR ADDITIONAL MATERIAL AND DIMENSIONAL INFORMATION



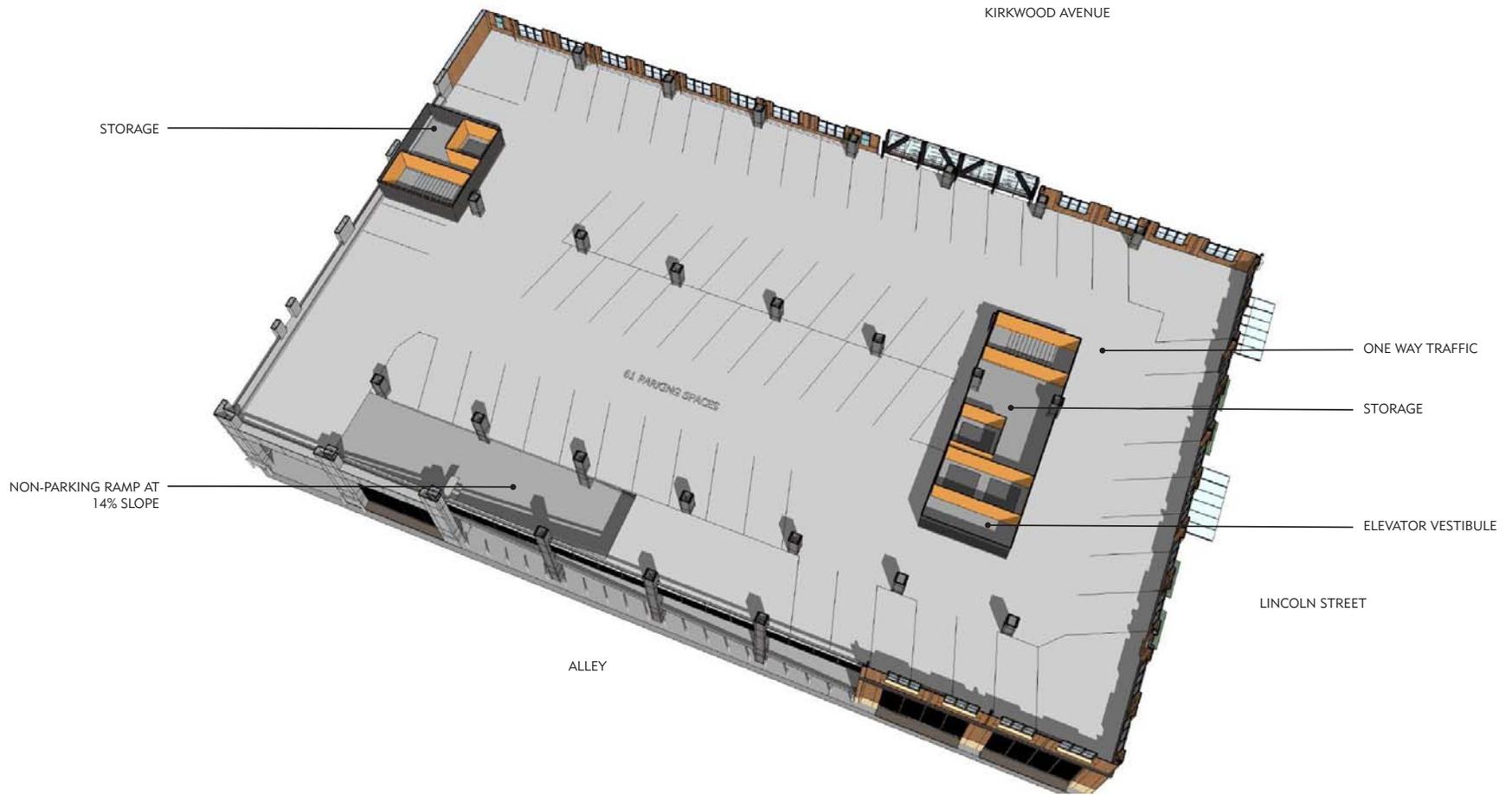
KIRKWOOD AVENUE

## LOOKING EAST ALONG KIRKWOOD

REFER TO ELEVATIONS FOR ADDITIONAL MATERIAL AND DIMENSIONAL INFORMATION

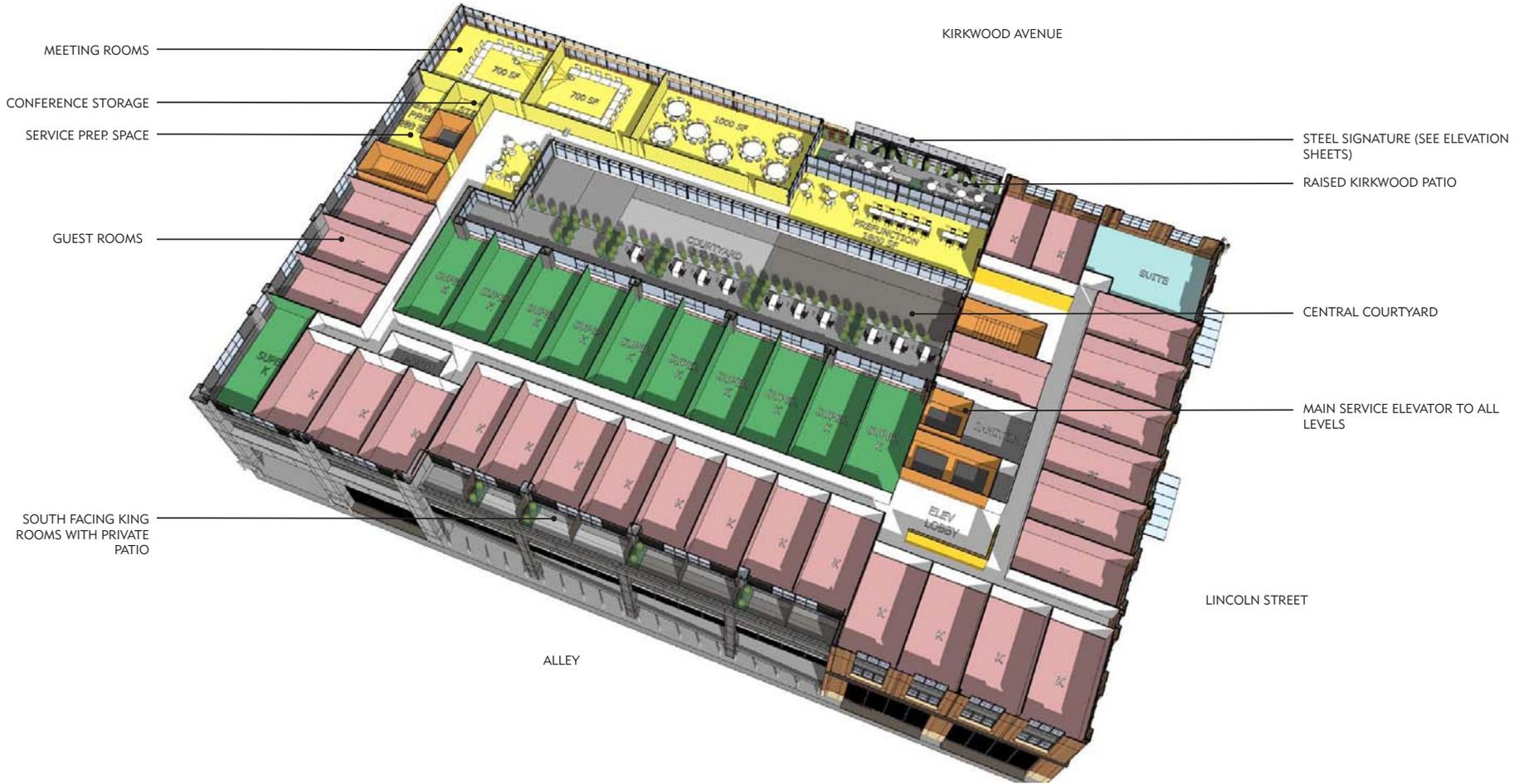


LEVEL ONE PLAN  
 NORTH



LEVEL TWO PLAN

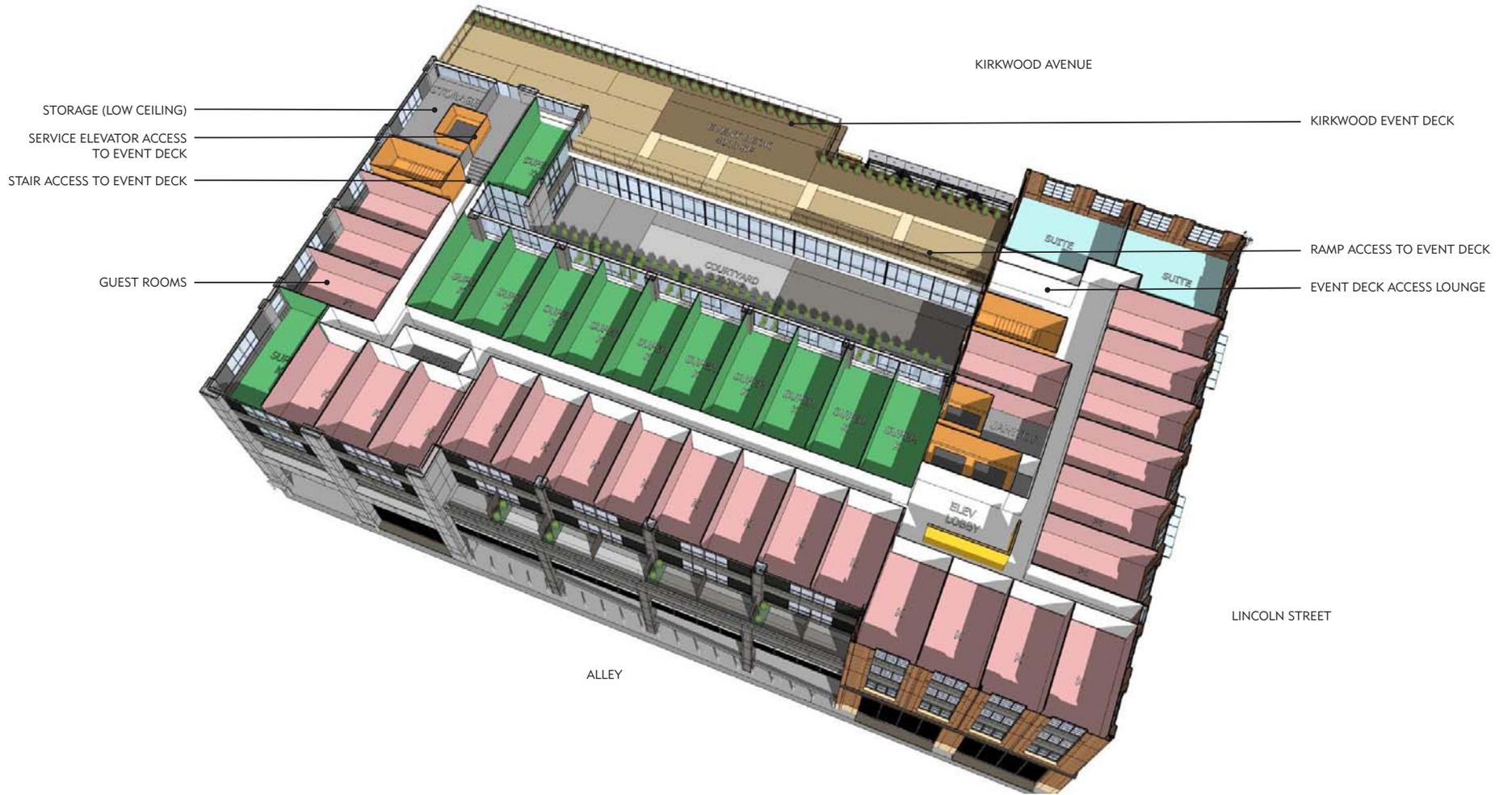


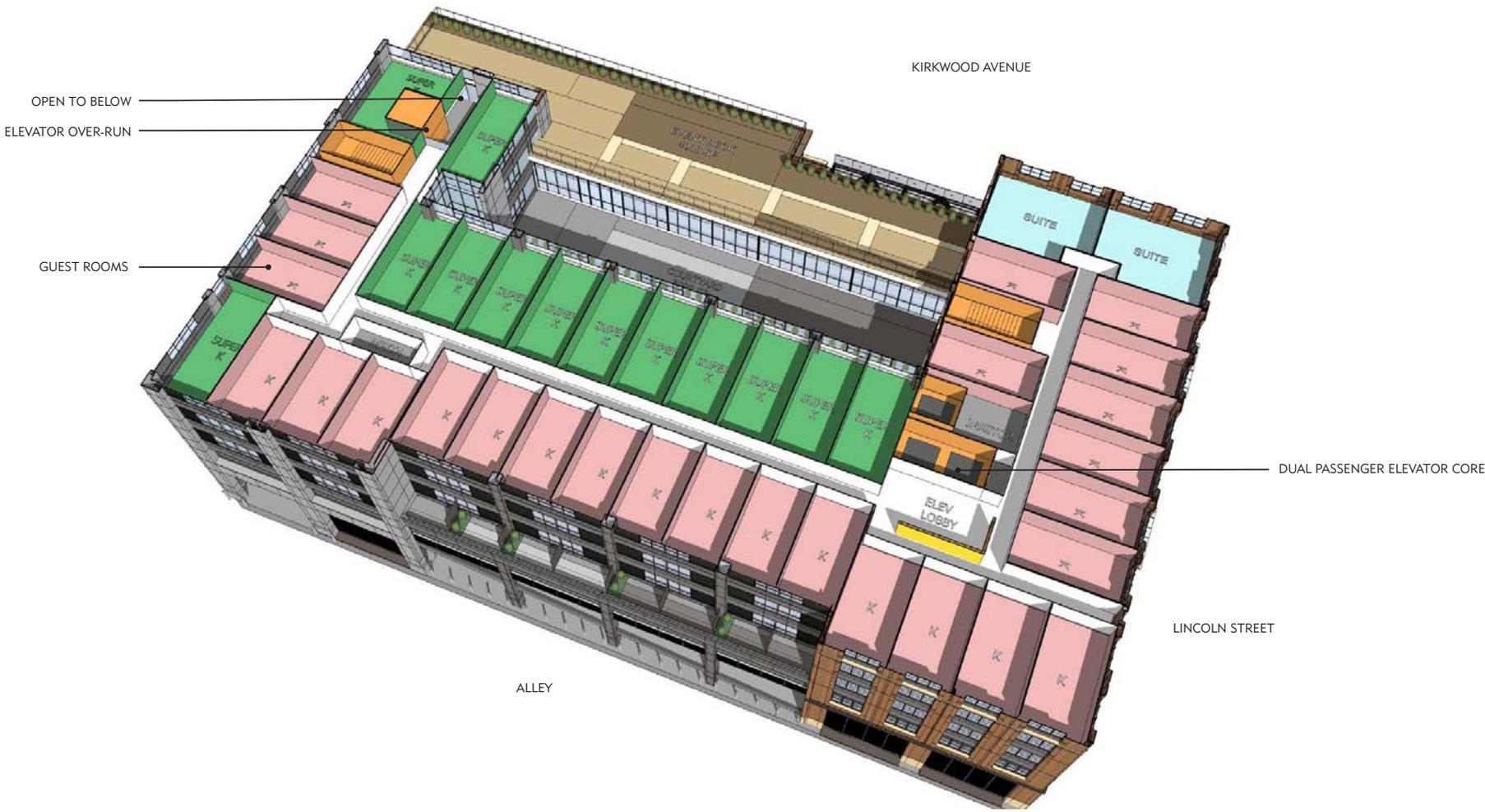


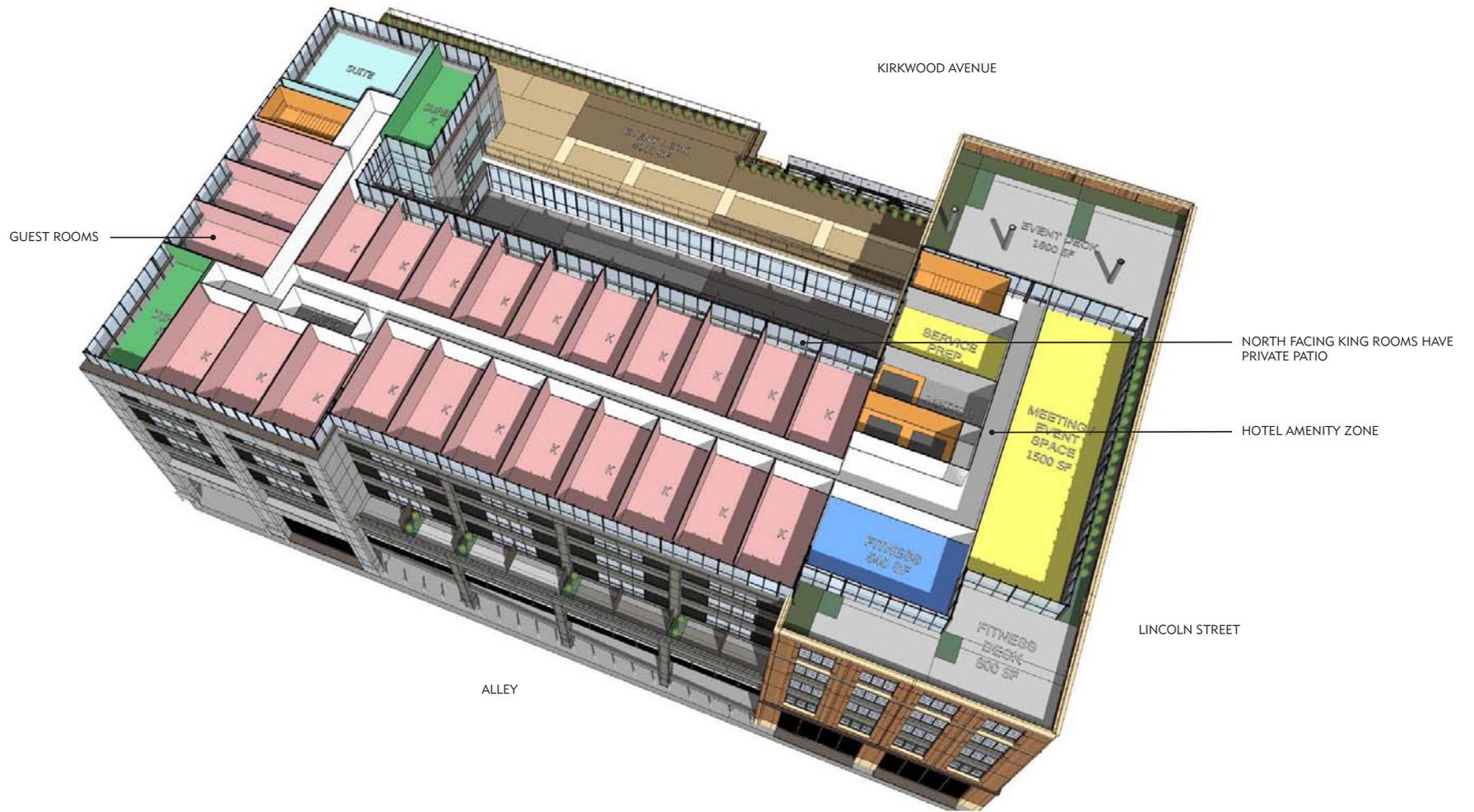
### LEVEL THREE PLAN

39 KEYS









LEVEL SIX PLAN

27 KEYS





KIRKWOOD AVENUE

VIEW WEST ALONG KIRKWOOD FROM GRANT ST.



VIEW FROM THE SAMPLE GATES



VIEW NORTH FROM 3RD STREET



VIEW NORTH FROM 3RD STREET WITH NEW DEVELOPMENT

**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT  
Location: 111 S. Lincoln St.**

**CASE #: SP/UV-34-14  
DATE: December 8, 2014**

**PETITIONERS:** GP – GMS Bloomington, LLC  
(GMS) 112 E. 3<sup>rd</sup> Street, Bloomington  
(GP) – 600 E. 96<sup>th</sup> Street, Suite 150, Indianapolis

**CONSULTANT:** Studio 3 Design  
8604 Allisonville Road, Indianapolis, IN

**REQUEST:** The petitioners are requesting site plan approval and a use variance recommendation to allow a surface parking lot within the Commercial Downtown (CD) zoning district.

<b>Area:</b>	<b>0.4 Acres</b>
<b>Zoning:</b>	<b>CD, Downtown Core Overlay</b>
<b>GPP Designation:</b>	<b>Downtown</b>
<b>Existing Land Use:</b>	<b>Vacant building and parking lot</b>
<b>Proposed Land Use:</b>	<b>Multi-Family Residential</b>
<b>Surrounding Uses:</b>	<b>East – Post Office</b>
	<b>South – Future Bus Terminal, Police Station</b>
	<b>West – Auto Repair, Office</b>
	<b>North – Bank</b>

**PROJECT REVIEW PROCESS:** Due to the unique aggregation of the Old National Bank properties in such a prominent downtown location, staff verbally presented cases SP/UV-34-13, SP-26-14 (withdrawn), and SP-14-14 as an integrated report to the Plan Commission on October 6, 2014. The petitioners have followed staff and Plan Commission guidance to bring forward all of the development projects at the same time to achieve this coordinated review. The proposals will now be voted on separately. In summary, Old National Bank owns the following properties in the heart of the downtown area:

- **Site A:** Former Workingman's Bank/ONB building, 121 E. Kirkwood Ave. - 0.45 acres – Recently approved for a CVS Pharmacy by the BZA, CU/V-32-14  
**\*Please note that Site A is not part of this petition.**
- **Site B:** Former Monroe Bank/Current ONB Bank, 210 E. Kirkwood Ave. - 0.60 acres – Proposed hotel, SP-14-14
- **Site C:** Current ONB Drive-through - 0.40 acres – proposed new ONB Bank, SP/UV-34-13
- **Site D:** Former Monroe Bank Loan Center, 111 E. 4<sup>th</sup> Street - 0.40 acres – This petition for surface parking
- **Site E:** Surface parking lot at the northwest corner of 4<sup>th</sup> and Grant - 0.30 acres – No current proposal. Will remain surface parking

The aggregate total of these properties is 2.15 acres, with 1.45 acres being located directly on Kirkwood Ave.

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**PETITION HISTORY:** The Plan Commission last discussed this property in October as part of SP-26-14, where the petitioners proposed to construct a 4-story mixed use building on this site. After discussion and recommendation from staff, the petitioners have withdrawn SP-26-14 and have filed the current petition to utilize the site for surface parking for the hotel proposed on Site B, SP-14-14.

---

**PROPERTY SUMMARY:** The subject property, known as Site D, is located at the northeast corner of E. 4<sup>th</sup> Street and S. Lincoln Street. The property is zoned Commercial Downtown (CD) and is within the Restaurant Row portion of the University Village Overlay (UVO). The property currently contains a one-story building along Lincoln St. that was previously used as the Monroe Bank loan center and is still used as office space.

Since the October Plan Commission meeting, staff and the petitioners have determined that Sites D and E would best be used as surface parking lots to service the proposed hotel on Site B and the bank on Site C, at least in the short term. Based on the feedback received during the process, staff and the petitioners believed that even a modified version of the mixed use building was not likely to be approved through the Plan Commission and City Council process and would negatively impact other portions of the development proposal.

The proposed surface parking on Site E will serve the bank and leave the site in its current state. This does not require Plan Commission approval. The surface parking lot for Site D will entail removal of the existing building and increase the number of spaces from 28 to 45 spaces. These spaces will serve the parking needs of the hotel on Site B. This petition will not require alley vacation approval by the City Council. However, it will require a use variance approval from the Board of Zoning Appeals to allow a stand-alone parking lot in the CD zoning district.

With this petition, the applicant has proposed several improvements to the parking lot to offset any negative impacts of the building removal. These improvements include:

- Removal of a drive cut onto S. Lincoln Street
- Reduction in the amount of impervious surface (from 95% to 82%)
- Removal of private parking from the right-of-way
- Creation of new landscaped setbacks and islands
- Creation of 1 new on-street parking space on Lincoln Street

The parking lot will also include improvements to make a more efficient and organized parking layout with reduced street access. Staff looks at this parking lot as necessary to facilitate the construction of the hotel which is a desirable downtown land use. Staff also looks at this parking lot as a kind of “land bank,” waiting until a more appropriate time for development. At some time in the future, once the bank and hotel are built and after the Jordan River culvert is reconstructed, this property will likely be developed into a more

intense land use.

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**GROWTH POLICIES PLAN:** The Growth Policies Plan gives guidance to avoid or prohibit new surface parking areas. Due to this policy guidance, the UDO prohibits this use within the CD zoning district.

The most relevant GPP references have been provided below with staff comments.

- *New surface parking areas and drive-through uses should be limited, if not forbidden, within the Downtown area.*

**Staff** - This petition will allow continuation of parking on the property, just in absence of a structure. This petition will facilitate the construction of Parcels B and C, which will remove three surface parking lots along Kirkwood Ave.

- *Parking must be dealt with in a manner to not discourage or harm the pedestrian nature of the downtown while at the same time providing sufficient parking to support the diverse land use mix of the downtown.*

**Staff** – The proposal will make the site and adjacent streetscapes more pedestrian friendly on Kirkwood Ave. while still providing adequate parking for the hotel.

- *Downtown must continue to be developed at a human scale, with pedestrian amenities such as street trees, sidewalks, and lighting. Existing amenities should be targeted for improvement where necessary.*

**Staff** – While this site will not contain pedestrian interest due to the lack of building activity, all public right-of-way improvements such as street trees, sidewalks, and lighting will be present. Again, the utilization of this lot for parking will facilitate great improvements to the pedestrian atmosphere on Kirkwood Ave.

- *Curb cuts along downtown streets are strongly discouraged. Rather, site access should be primarily from sidewalks for pedestrians or alleys for vehicles.*

**Staff** – A curb cut will be removed with this petition.

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**Plan Commission Site Plan Review:** One aspect of this project requires that the petition be reviewed by the Plan Commission, per BMC 20.03.160. The project proposes two waivers to the standards in BMC 20.03.190

**Parking:** These parking spaces will be used by the hotel on Parcel B. The UDO allows the hotel to have parking ranging from 0 parking spaces to a maximum of 146 parking spaces. If this petition is approved, the number of parking spaces for the hotel, both on-

site and on this site, will be 119 spaces. This does not include the additional valet parking that would be used for busy hotel weekends.

**Parking Setback:** The UDO requires that parking be located 20 feet further from the street than the building. In this situation there is no building. A waiver is required.

**Parking Setback Waiver 20.03.190 (c)(1)(a)(i):** The proposed parking lot layout removes parking from the public right-of-way and provides a setback of approximately 5 feet on Lincoln Street and 4 feet on 4<sup>th</sup> Street. Because of a wide right-of-way, the parking is approximately 13 feet from the sidewalk on both streets. The provided setbacks provide adequate planting area to meet landscaping requirements and provide the required parking space and aisle depths. Staff recommends approval of the waiver.

**Landscaping:** With the use variance request, staff has worked closely with the petitioners to develop a site plan that increases the amount of landscaping on all sides of the parking lot and removes a large amount of asphalt. This proposal includes 138 new shrubs and 12 new trees.

**Landscaping Waiver:** The petitioners are requesting a waiver from the Landscaping Standards that require a decorative wall to be placed along parking areas adjacent to a public right-of-way. The petitioner's proposal does not include any new construction. Furthermore, staff anticipates that this parking lot is not a long term use for the property. A wall would eventually have to be removed to accommodate future development of the lot. Staff supports this waiver request.

**Streetscape:** The petitioner has agreed to improve the streetscape through removal of asphalt and parking spaces along both street frontages. Street trees are already in place along both streets and will be maintained. Street lights are required on both street frontages.

**Site Plan Expiration:** Typical site plan approval is valid for a period of 1 year. The petitioners have requested that this site approval be extended to a period of three years. This is due to the fact that this property will be used for staging for the construction of the bank and hotel, which will be built first and take approximately 3 years to build. Staff has no objection to this request.

---

**DEVELOPER TRACK RECORD:** GP – GMS Bloomington LLC is a partnership between Eric Gershman and GMS Pavilion Properties. GMS Pavilion Properties is also the petitioner for SP/UV-34-13 on Parcel C. They completed other recent downtown projects, including 501 N. Walnut Street, 601 N. Walnut Street and 219 E. 7<sup>th</sup> Street. They also own several other rental properties and have recently remodeled the historic Fleener Building at 112 E. 3<sup>rd</sup> Street. There are no outstanding zoning violations associated with these properties. This is Eric Gershman's first development petition in Bloomington.

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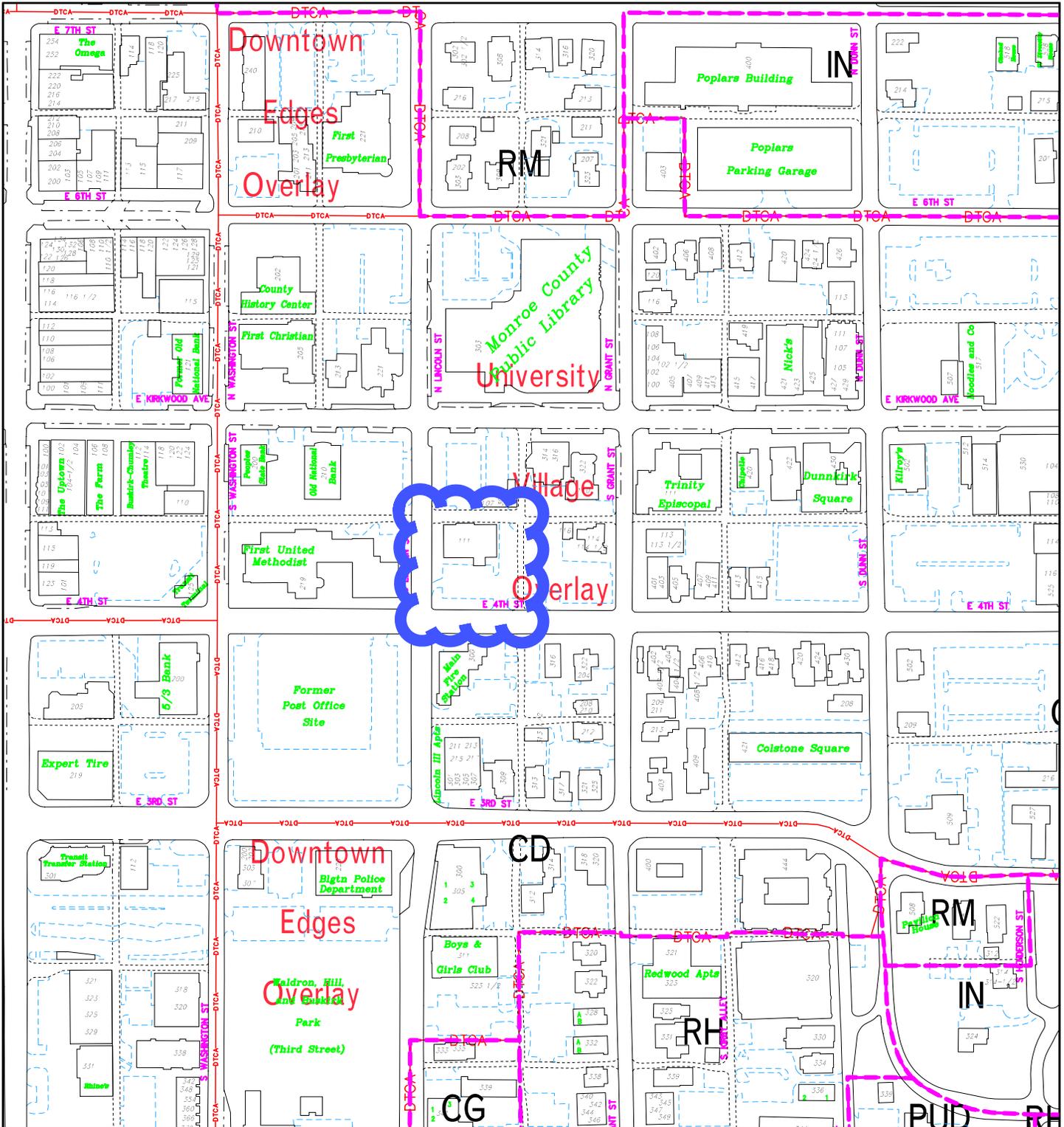
**CONCLUSION:** Staff finds that the proposed surface parking lot is beneficial to

development of the hotel on Site B. This parking lot may act as a “land bank” until such time as it is more appropriate to be developed. The improvements will enhance the aesthetics of the site and warrant the surface parking at this site. The improvements include removal of a drive cut and increased landscaping. This petition will also facilitate the redevelopment of two other ONB sites and the removal of nearly 500 lineal feet of surface parking along Kirkwood Ave. and Lincoln St. with development of the hotel and the bank. Staff finds that this approval would not substantially interfere with the Growth Policies Plan.

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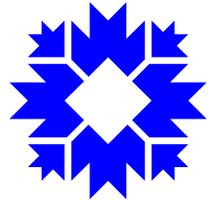
**RECOMMENDATION:** Staff recommends that the Plan Commission forward this request to the BZA with a positive recommendation. Staff also recommends approval of the proposed site plan with the following conditions:

1. Site Plan approval is contingent upon the approval of the requested use variance from the Board of Zoning Appeals.
2. Three decorative pedestrian scale lights are required to be placed; one in close proximity to the intersection, one at the northern portion of the Lincoln Street frontage and one at the eastern portion of the 4<sup>th</sup> Street frontage.
3. This site plan will be valid for a period of 3 years.



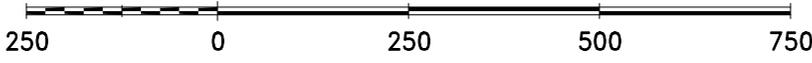
SP/UV-34-14  
Location Map

City of Bloomington  
Planning & Transportation

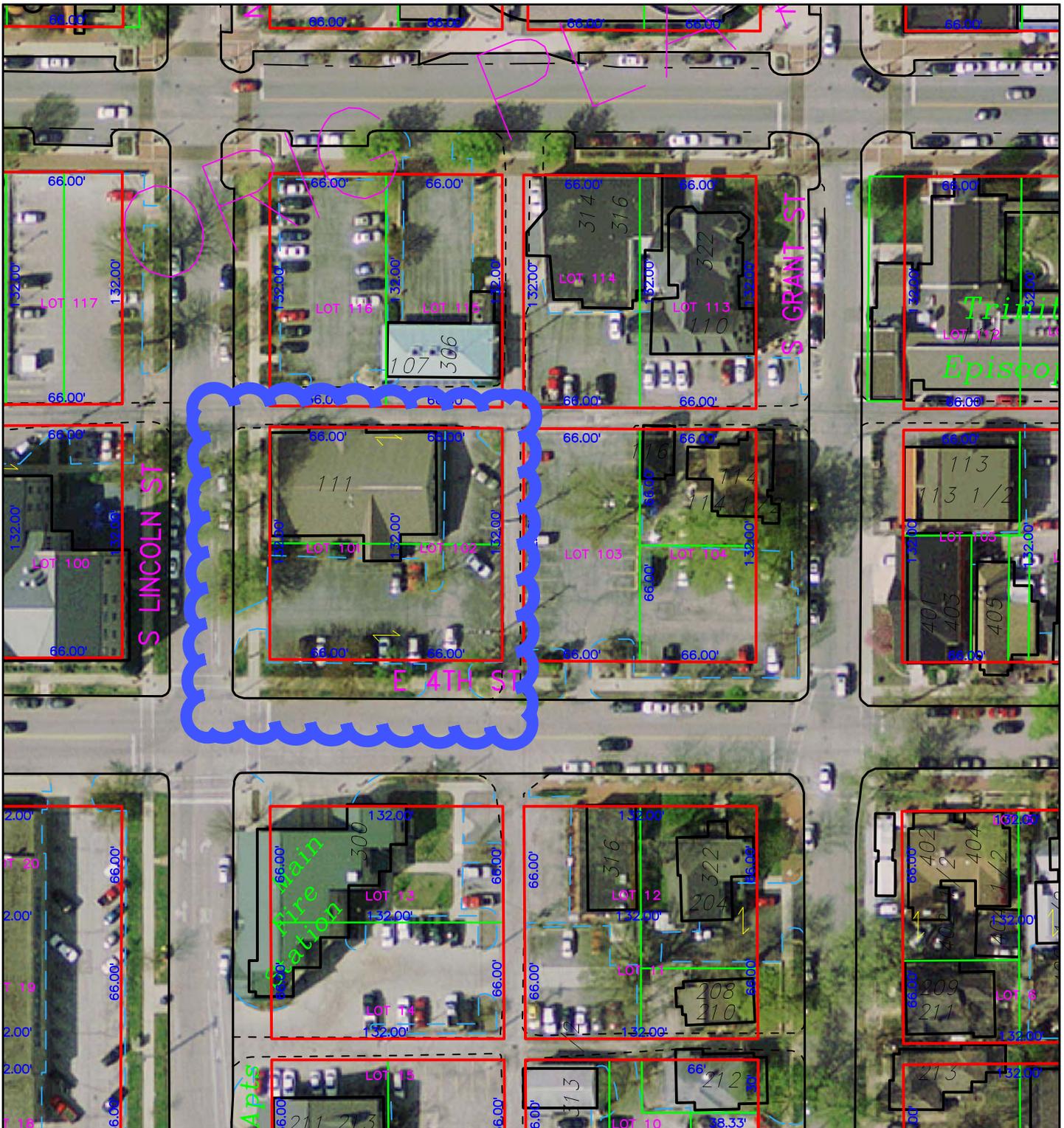


Scale: 1" = 250'

By: roachja  
1 Oct 14

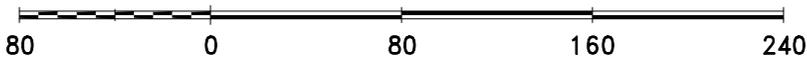


For reference only; map information NOT warranted.



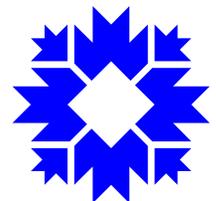
SP/UV-34-14  
2011 Aerial photo

By: roachja  
1 Oct 14



For reference only; map information NOT warranted.

City of Bloomington  
Planning & Transportation



Scale: 1" = 80'





SP/UV-34-14  
ONB Bank Holdings

By: roachja  
23 Sep 13



For reference only; map information NOT warranted.

City of Bloomington  
Planning

Scale: 1" = 200'



November 12<sup>th</sup>, 2014

City of Bloomington Planning Department  
P.O. Box 100  
Bloomington, IN 47402

Attn: Mr. Tom Micuda

**RE: Bloomington Downtown Development  
Lincoln and 4<sup>th</sup> Street – Site “D”  
Bloomington, Indiana**

### **PETITIONERS STATEMENT**

Dear Mr. Micuda

Studio 3 Design is pleased to submit the attached development for your consideration. The project scope covers one of the 4 sites (currently owned by Old National Bank) within the downtown. The project zone incorporated by these sites has been identified on the attached documentation as sites “B, C, D & E”. This submittal package will focus on Site “D” but the other sites are mentioned as they are part of the holistic view of the downtown sites that we have been asked to consider.

Site “B” -bordered on the North by Kirkwood Avenue, on the East by Lincoln Street, on the South by a public alley, and on the West by an adjacent land owner.

Site “C” –bordered on the North by Kirkwood Avenue, on West by Lincoln Street, on the South by a public alley, on the east by a public alley.

Site “D” –bordered on the South by 4<sup>th</sup> street, on the West by Lincoln street, on the North by a public alley and on the east by a public alley.

Site “E” –bordered on the South by 4<sup>th</sup> street, on the East by Grant Street, On the North by an adjacent property owner and on the West by a public alley.

### **Project Scope:**

The project request for site “D” is for a surface parking lot to be utilized by ONB Bank – Site “C”, the Graduate Hotel – Site “B”. The project includes the demolition of an existing single story structure with a basement and the creation of a new surface lot for 45 surface parking spaces.

**Project request for plan approval duration extension.**

Typical site plan approval is valid for a period of 1 year. We would request that this site approval be extended to a period of three years prior to Construction having to be started. This will allow us to use the site as a staging area for the Bank building on site "C" and then as a staging area for the Hotel on Site "B". Once the hotel is constructed, the parking lot will be finished and available for use.

**Project Overlay District:**

University Village Overlay  
**Restaurant Row subsection- sites D & E**

Density: 33 units per acre

Impervious surface:

Restaurant row: 85%

Height Standards:

Restaurant row: min. 25' – max. 35'

Parking:

None residential: No parking required for Hotel, retail, or office functions

Residential: 5 for first 20, .8 for beds 21 on.

Setbacks:

Front: 0 to 15'

Side yard: 0'

Rear yard 0'

Ground floor non-residential:

Applies to Kirkwood, 4<sup>th</sup>, Lincoln and Grant

50% min. along applicable street frontage

Building alignment:

No Outstanding, notable or contributing structures immediately adjacent to properties.

Building orientation:

Restaurant row: min. of one primary entrance facing 4<sup>th</sup> street

Street trees:

Restaurant row: 5' wide grassed tree plot area

Lighting:

Street lighting- traditional style design such as acorn or gas lamp style.

Architectural character

Restaurant row: incorporate sloped or pitched roofs

Void to solid:

Restaurant row; lower 50%, upper 20%

**Windows:**

Restaurant row- windows to have appearance of double hung window

**Materials:**

**Not permitted as Primary**

Restaurant Row: EIFS, CMU, natural stone or masonry, precast concrete, vinyl

**Not permitted as a secondary material:**

Restaurant Row: EIFS, Vinyl

**Entrance Detailing:**

General: - shall incorporate a min. of 3 of the following:

4' recessed entry, ornamental paving, Canopy/ awning, Portico, arched entry, pilaster or façade module projecting from exterior wall plane, building address with, building name and lighting, public art, raised cornice entryway parapet, rusticated masonry, landscaped patio area for outdoor seating of 8 or more.

Restaurant Row: entrance shall incorporate a front porch, canopy or awning, incorporate 2 or more of the general entrance detailing listed above.

**Mass, Scale & form;**

Façade modulation- maximum width 50' with a min. 3% offset based on total length of façade.

Height step-down: N/A – properties are not immediately adjacent to outstanding, notable, or contributing structures.

**Project Overview:**

The project is located at the corner of Lincoln Street and 4<sup>th</sup> street.

The projects will provide parking for the hotel, hotel conferences, and events and Old National Bank employees and patrons.

The project will include closing the access drive off of Lincoln Street and infilling the area with a grass zone and continuation of the existing sidewalk area.

**Waivers:**

In working with the planning department, the goal of the project is to maximize the amount of parking we can fit onto Site "D" and still maintain as many of the site development standards as possible. We have initially identified two waivers that we will be requesting support on.

1. Site setback for parking.
2. Site low wall construction on Lincoln / 4<sup>th</sup> streets.

**Parking Standards (Project complies with districts' guidelines)**

**Required:**

Retail: 00 required

Provided: 45 spaces

**Site Plan:**

Building Frontage – NA

**Street trees and perimeter trees:**

Existing street trees and grass plots will be maintained along Lincoln Street and 4<sup>th</sup> Street as they are today. Additional trees and landscaping will be incorporated around the parking lot with large growth trees along Lincoln and 4<sup>th</sup> streets and species that will not grow as tall and are approved for use around site power line locations along the alleys.

**Site landscaping:**

Perimeter landscaping will be in compliance with the UDO. Approved plantings will be incorporated around the perimeter of the parking lot and islands.

**Lighting: (Site plan complies with the districts' guidelines)**

Pedestrian scale lights (less than 15' in height) will be placed in the parking lot central islands to provide down lighting for safety and security in the lot.

**Site Accessibility**

The parking lot will be accessible from 4<sup>th</sup> street via an existing curb cut and from the Alley to the north of the site.

**Alley modification:**

The alley bordering the north side of Site "D" will be increased to 24' in width as part of the site "C" ONB bank branch improvements. This will allow for easy in-out access to the parking lot. The alley will choke down to 20' at the interface with Lincoln Street. The east west alley will be repaved. The North south alley will remain as is with patching as required along the new curb line.

**Storm water**

The project site will continue to sheet drain toward 4<sup>th</sup> street where it will be collected by two new inlet structures at the South end of the property. The current property sheet drains to 4<sup>th</sup>.

**Site detention for water:**

The site is currently 100% impervious. No detention is required to be added as part of the new site development.

**Site D -Private Utilities**

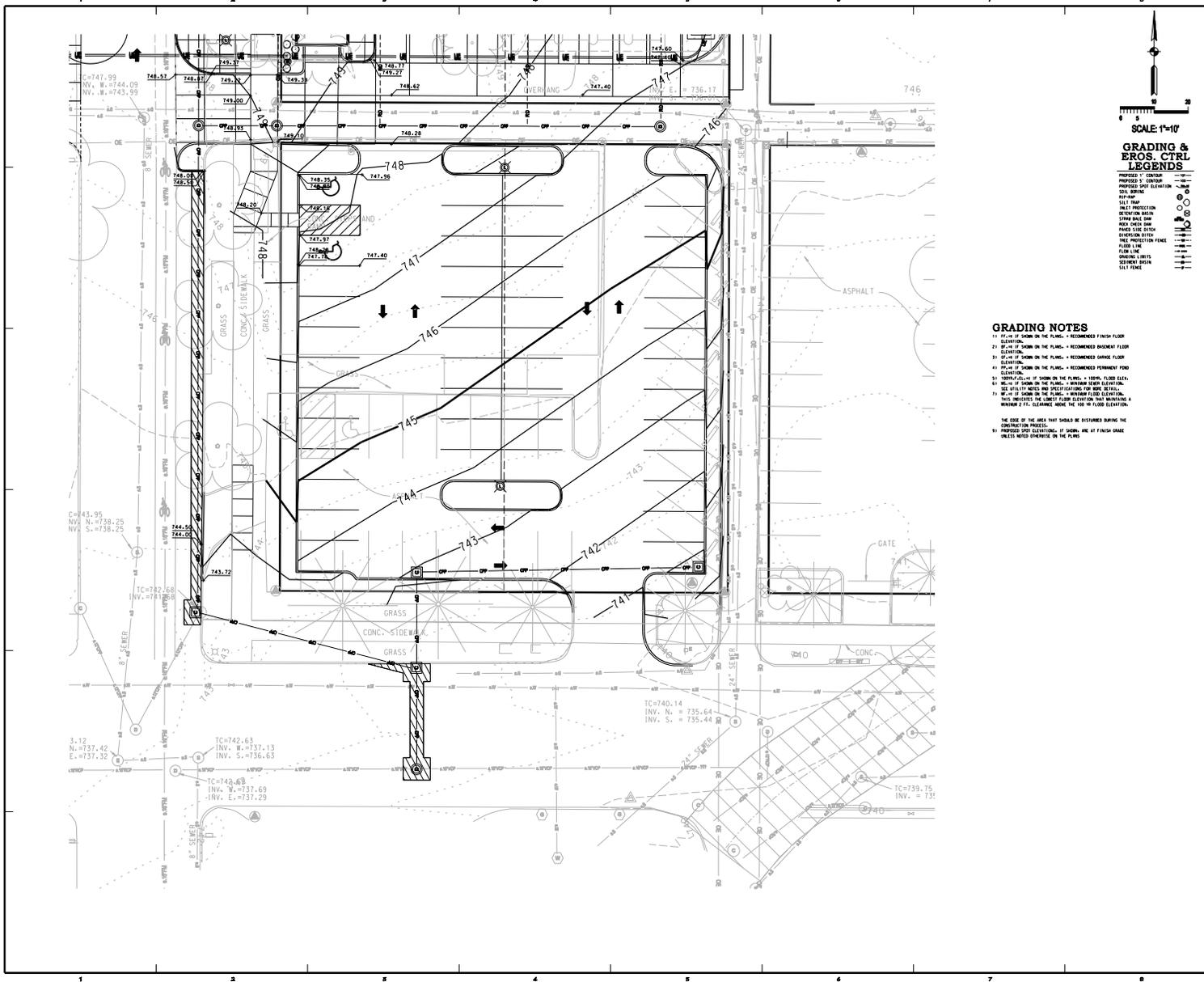
Duke Energy and a cable/phone/internet lines currently run along the east west alley and the north south alley that border the site. These lines will remain where they are.

Respectfully submitted,

STUDIO 3 DESIGN, INC



Timothy W. Cover



**GRADING & EROS. CTRL. LEGENDS**

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- GRADING NOTES**
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303 S. State Street  
Bloomington, Indiana 47401  
Phone: 317.346.8833  
Fax: 317.346.8833  
www.studiothreedesign.com

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**Old National Bank Sites**  
BLOOMINGTON, IN

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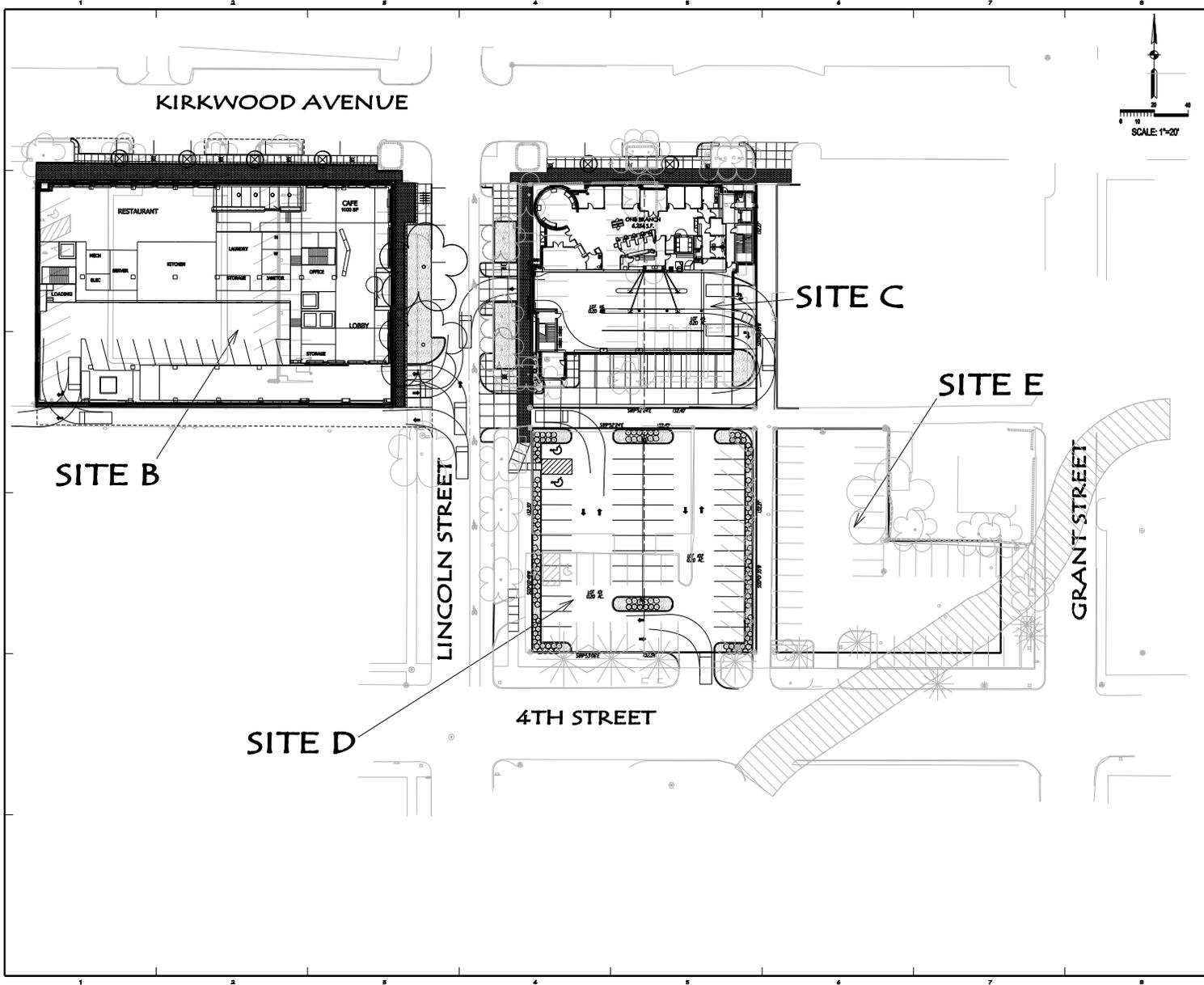
PROJECT NO. 4933 / 5012
DATE 11/10/14
REVISIONS
CERTIFICATION
SHEET DESCRIPTION SITE D GRADING PLAN
SHEET NUMBER <b>C209</b>

THIS DRAWING IS THE PROPERTY OF STUDIO THREE DESIGN, INC. ALL RIGHTS RESERVED.

SP/UV-34-14  
Grading Plan







4933\_Combined Site Plan.sht 11/12/2014 7:45:06 AM



**STUDIO THREE DESIGN**

107 N. Main Street  
4011 Altonville Road, Suite 200 Indianapolis, IN 46205

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CONSULTANTS

Smith Probst & Associates, Inc.  
401 S. State Street  
Indianapolis, IN 46201  
Tel: 317.334.4433  
Fax: 317.334.4433  
http://www.spa-inc.com

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**OLD NATIONAL BANK SITES**  
BLOOMINGTON, IN

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PROJECT NO.  
4933 / 5012

DATE  
11/10/14

REVISIONS

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CERTIFICATION

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SHEET DESCRIPTION  
COMBINED  
SITE PLAN

SHEET NUMBER  
**C200**

THIS DRAWING IS THE PROPERTY  
OF STUDIO THREE DESIGN, INC.  
ALL RIGHTS RESERVED

SP/UV-34-14  
Combined Site Plan



NORTH

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A7

RENDERED SITE PLAN



GMS-PAVILION PROPERTIES, LLC.  
 GERSHMAN PARTNERS  
 ONB "SITE C" DEVELOPMENT  
 BLOOMINGTON, INDIANA

PROJECT NO.  
 13018.04

DATE  
 11/24/2014

SHEET DESCRIPTION  
 Rendered Site  
 Plan

SHEET NUMBER

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SP/UV-34-14  
 Combined Site Plan

**BLOOMINGTON PLAN COMMISSION  
STAFF REPORT  
Location: 1410 N. Walnut St.**

**CASE #: SP-32-14  
DATE: December 8, 2014**

**PETITIONER:** Motels of Bloomington, LLC (Home 2 Suites by Hilton)  
1220 Brookville Way

**CONSULTANT:** Angela Parker  
116 W 6<sup>th</sup> Street, Bloomington, IN

**REQUEST:** The petitioner is requesting site plan approval for a new 66,000 square foot extended stay hotel with 107 rooms.

**BACKGROUND:**

**Area:** 2.2 acres  
**Current Zoning:** CA  
**GPP Designation:** Community Activity Center  
**Existing Land Use:** Vacant  
**Proposed Land Use:** Hotel  
**Surrounding Uses:** North – Single and Multifamily residences  
 West – Miller Showers Park  
 East – Single and Multifamily residences  
 South – Restaurant

**SUMMARY:** The property is located at 1410 N. Walnut St. and is zoned Commercial Arterial (CA). There is a surface parking lot on the property that was associated with the former restaurant use. This parking is accessed from drivecuts on both 17<sup>th</sup> Street and Walnut Street. The property slopes from east to west with approximately 22' of elevation change from the southeast corner to the northwest corner. This significant elevation change presents a significant challenge for site redevelopment. An additional challenge for development occurs because the corner property at 17th and Walnut Streets is in separate ownership and has not been aggregated for the petition. There is a low quality vegetated fence row along the perimeter of the property with one higher quality mature tree. There are no known environmental features on the site.

The petitioner is proposing to construct a new 66,000 sq. ft. hotel on the property with 107 rooms and 109 parking spaces. The location of the existing drivecut on 17<sup>th</sup> Street will remain in its current location. However, the curb ramps adjacent to the drivecut will be brought into compliance with ADA standards. The existing drivecuts on Walnut St. will be removed and one new drivecut installed further north. A new 5' wide concrete sidewalk and tree plot is required along the property frontage on Walnut St. and has been shown on the site plan. There is an existing monolithic sidewalk along the north side of 17<sup>th</sup> St. that extends across this property. The petitioner is proposing to utilize permeable pavers for the parking spaces in order to meet maximum impervious surface requirements. Stormwater will be directed to Miller Showers Park on the west side of Walnut St.

This property's elevated location along Walnut St. makes the proposed building highly visible from Walnut St, as well as across Miller Showers Park from College Ave. The hotel will have a brick and limestone finish along all four sides, with sections of EIFS along the horizontal and vertical accent bands. The petitioner has placed the main entrance to the hotel on the east side of the building adjacent to the parking area that is located on the east side of the building. Staff has emphasized the importance of providing a street level, ADA accessible, pedestrian entrance along Walnut St. However, due to the change in grade across the site, it is difficult to provide an ADA accessible pedestrian entrance on Walnut Street that is at the same level as the proposed main entrance on the east side of the building. In order to try and accomplish both tasks, the petitioner is proposing to create a ground floor lobby entrance along Walnut St. that will allow a street level entrance that is also ADA accessible. To avoid creating a large retaining wall along the rest of the frontage, the petitioner has extended the façade of the building all the way to the ground level and created a faux wall to use the building as a retaining wall. Staff is seeking guidance from the Plan Commission on whether or not more architectural improvements or other building modifications are necessary to make sure the proposed building complies with the pedestrian entry requirement of the UDO.

The petitioner has applied to the Board of Zoning Appeals for the following variances:

- 1) required 30' parking setback standard along the east property line
- 2) entrance and drive standards to allow the drivecut on 17<sup>th</sup> St. to be within 100' of another entrance; entrance and drive standards to allow the driveway on 17<sup>th</sup> St. to be less than 45 degrees from parallel
- 3) architectural standards regarding the exterior façade design, and
- 4) height standards to allow the lighted tower element to be 54' tall as well as to allow the overall façade on Walnut St. to be 58'.

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#### **SITE PLAN ISSUES:**

**Architecture/Design:** The petitioner has submitted elevations for all four sides of the building. These elevations have been included in the packet. The building will be finished with brick and limestone with sections of EIFS for accent. The roof will be flat with a raised parapet to hide mechanicals. Some portions of the proposed architecture do not meet the architectural requirements of the UDO. Specifically there are several modules that exceed the 40' width that lack the design elements outlined in the UDO. While there are recessed walls along the building front, a regular pattern of glass, and changes in building height, there is not a combination of the 3 required elements present in every module.

The inclusion of an awning along the entire ground floor would allow the building to come closer to compliance with that section. In addition, the building façade elevation changes should be 5', rather than 2.5' as shown in order to meet the UDO modulation requirement. Staff believes that the pattern of windows shown on the upper floors should be carried down to the ground level as well. Although there is not proposed to be useable space behind the ground floor faux wall, extending the window pattern of the upper floors through the use of faux windows would accomplish the continuity desired. Staff also feels that the proposed pedestrian entrance could be moved north to center it

and provide a more defined entry. Staff seeks guidance from the Plan Commission regarding whether or not the previously mentioned changes or other additional architectural improvements are needed.

**Access:** As previously mentioned, the property would be accessed by the current drivecut on 17<sup>th</sup> St and one new drivecut on Walnut St. There are 2 existing drivecuts on Walnut that will be removed and replaced with a new drivecut located further north. The existing drivecut on 17<sup>th</sup> St. does not meet the required 100' separation from the existing drivecuts for either the restaurant to the west or the apartments to the east. However, it has been located equidistant from each driveway and is the furthest it can be from the top of the hill on 17<sup>th</sup> street while still meeting the 150' separation from the Walnut St. intersection. Staff is supportive of the proposed driveway location.

**Landscaping:** With the new construction of the building and parking area, the property would be required to meet all landscaping requirements. The petitioner has submitted a landscape plan that meets UDO requirements. The petitioner is proposing to utilize permeable pavers for the parking spaces to improve stormwater quality and meet impervious surface coverage requirements. Street trees are required not more than 40' from center along both street frontages and have been shown on the proposed landscape plan.

**Parking:** The UDO allows for a maximum of one parking space per lodging room for a maximum of 107 parking spaces on this site. The proposed parking plan shows 109 parking spaces. Staff recommends that the 2 additional parking spaces be removed from the site plan.

**Pedestrian Facilities:** A new 5' wide concrete sidewalk and tree plot is required along the property frontage on Walnut St. and has been shown on the site plan. There is an existing monolithic sidewalk along the north side of 17<sup>th</sup> St. that extends across this property and stretches from the intersection of Walnut St. to Dunn St. to the east. Although the sidewalk along 17<sup>th</sup> Street could be replaced with separation, given the adjacent topography and existing street trees, it is more reasonable to maintain the current configuration. There will be improvements to the sidewalk ramps at the drivecut to bring them into compliance with ADA standards. New street trees not more than 40' from center will be installed behind the sidewalk along 17<sup>th</sup> St.

**Signage:** No sign package has been reviewed with this request. Any signage must meet all requirements of the Unified Development Ordinance.

**Utilities:** There is adequate water and sewer service along Walnut and 17<sup>th</sup> St. Stormwater drainage will be directed to the Miller Showers Park immediately adjacent to the site. A utility plan has been submitted to the Utilities Department and is under review. No problems have been identified with the proposed utility lines and connections. Final approval from CBU is required prior to issuance of a grading permit.

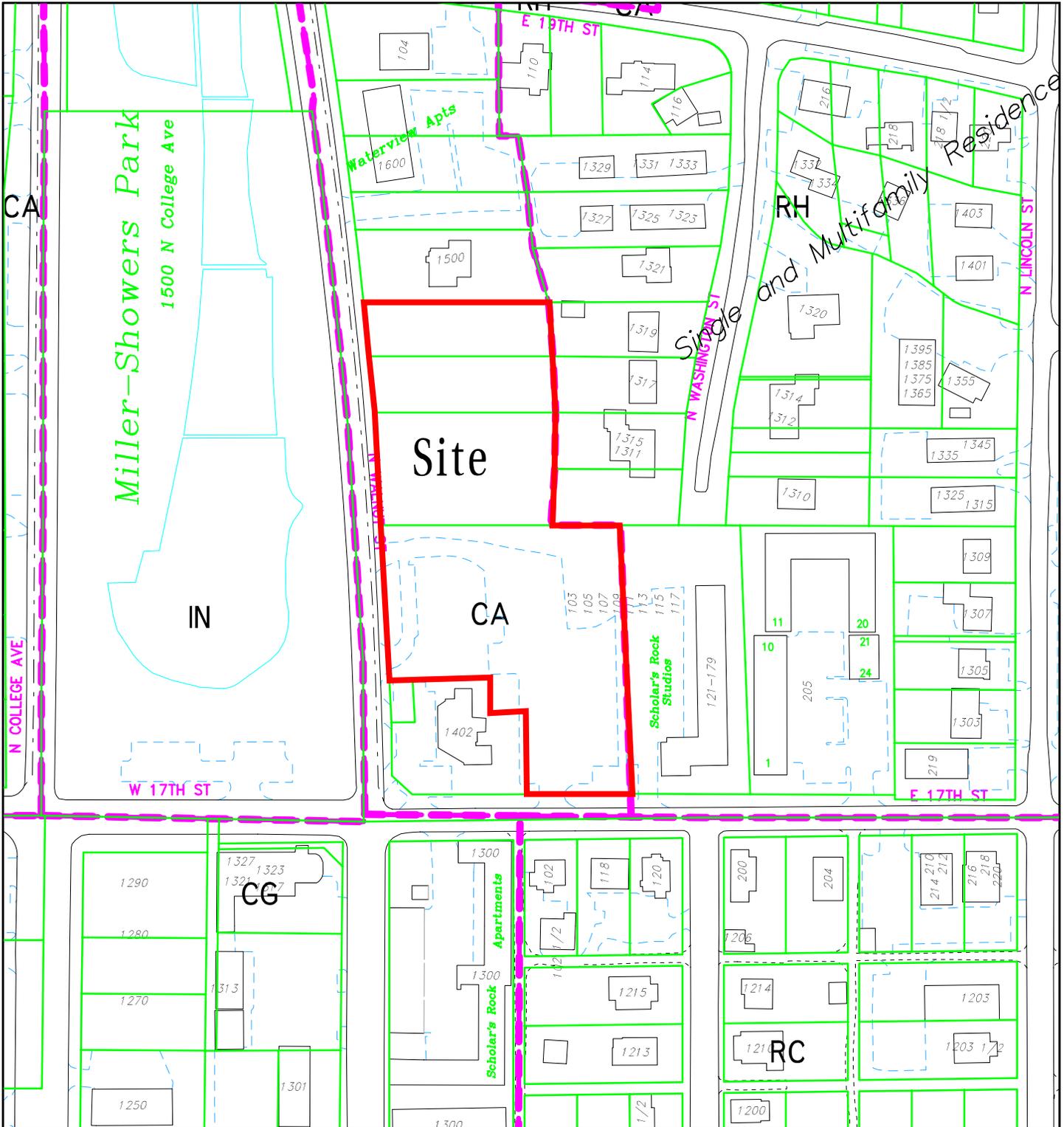
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**CONCLUSION:** The development of this vacant parcel will provide substantial improvements to this visible property and provide benefits to the community. While staff and the petitioner have worked to constantly improve the aesthetics and pedestrian accessibility to the building, staff seeks input from the Plan Commission on the plans

and elevations to further guide modifications. Specifically staff seeks input on the design of the proposed building facade along Walnut Street in terms of its interaction with the adjacent street frontage.

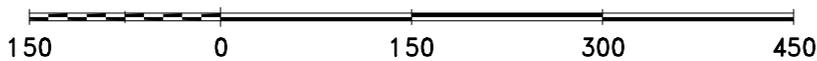
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**RECOMMENDATION:** Staff recommends continuance of this petition to the February 9, 2015 hearing.



SP-32-14 Home 2 Suites by Hilton  
 1410 N Walnut St  
 Plan Commission  
 Site Location, Zoning, Parcels, Land Use

By: greulice  
 4 Dec 14



City of Bloomington  
 Planning & Transportation

N

Scale: 1" = 150'



## Petitioner's Statement

Motels of Bloomington, LLC is an Indiana Limited liability Company that was formed on October 7, 2011. The two members of the LLC include Sanjay Patel, President and principal of Midwest Hospitality Group, Inc ("MHG"), and the title owner of the project site, Walnut Street Lodging, LLC. The entity was formed as a single-purpose company and plans to develop a Home2 Suites by Hilton at property with a common address of 1410, 1416 and 1420 North Walnut Street in Bloomington, Indiana ("Project Site"). Prior to completion of the approval process and certainly before construction, it is anticipated that the property will be transferred by Deed to Motels of Bloomington, LLC.

MHG and Mr. Patel, in particular, have been in the hotel development and management business since 1991. Under Mr. Patel's leadership, his companies have developed 17 hotels in four states with brand names that include Comfort Suites, Comfort Inn, Holiday Inn Express, Super 8 Motels, Fairfield Inn and Suites by Marriott. MHG is a privately owned company specializing in development, construction and management services in the hotel industry.

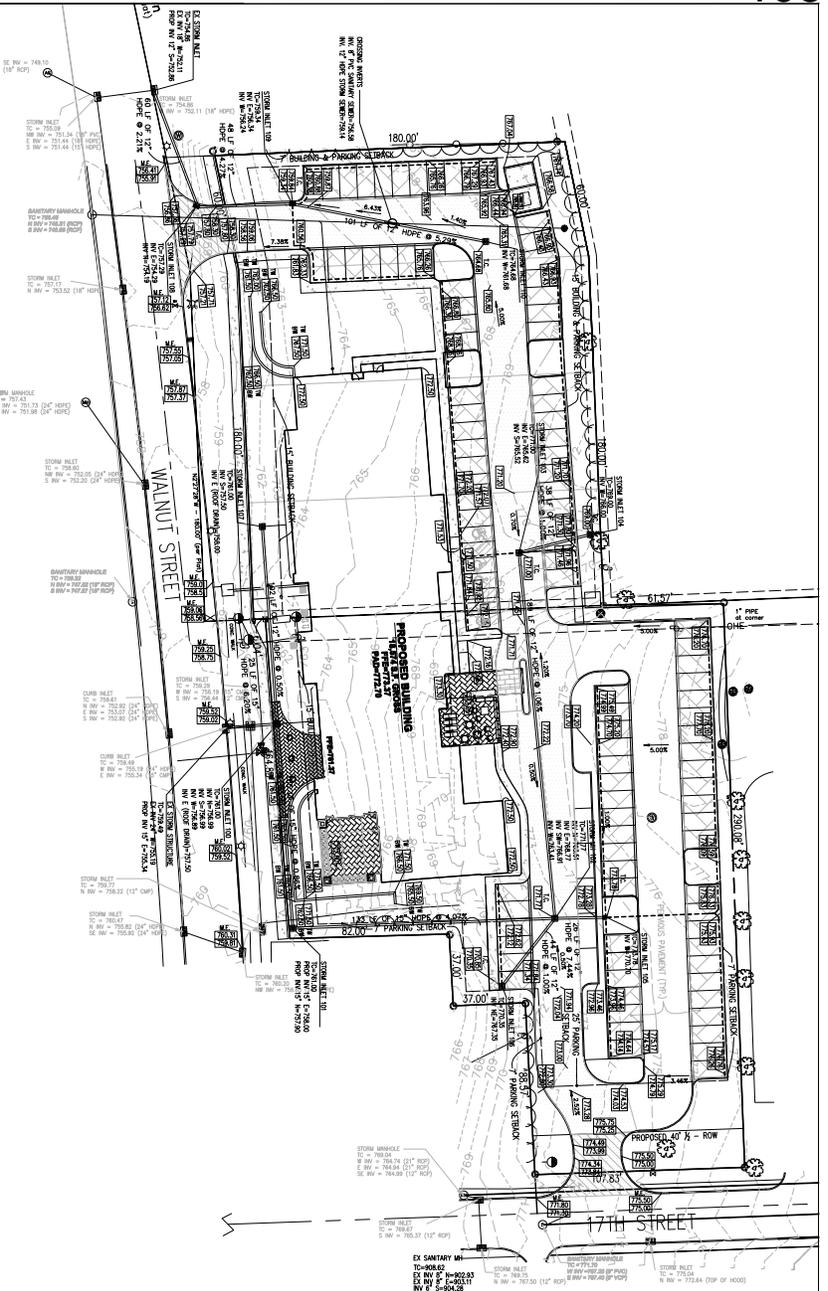
The Project will consist of a four-story, 107-room, select service, upper-tier extended-stay hotel containing approximately 66,573 sq. ft. and situated on a high-profile 2.2 acre *shovel-ready* site with on-site parking. The current zoning classification for the property allows for the development of a hotel and MHG obtained a Home 2 Suites franchise from InterContinental Hotels Group "IHG". The Hotel will feature many distinctive design features and will offer a combination of studio and one bedroom suites.

The Project Site is intended to be constructed and operated as a Hilton brand hotel – Home2 Suites. An approved Home2 Suite hotel is premised on eco-conscious products and design and also on developing connections to the local community. The Home2 principles are based on sustainability, community and culture and Motels of Bloomington, LLC is actively engaged in developing this new innovative concept in Bloomington, Indiana through this development. The Bloomington project will include complimentary continental breakfast, a market pantry, a fitness and activity room, swimming pool, business center, and gathering spaces.

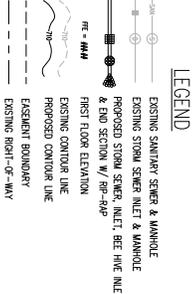
Home 2 Suite's guestrooms create a unique guest experience that couple familiarity with flexibility and ease of use. Designed with a streamlined approach to storage and function, it takes advantage of 323 square feet studios and 509 square feet one-bedroom suites with hip and stylish innovations. Each guest room features a "working wall" incorporating the kitchenette, flexible working space with moveable desk and adaptable storage options. Each room is appointed with a fully-accessorized kitchenette completed with: refrigerator/freezer with icemaker, dishwasher, microwave/convection oven and coffee maker.

Home 2 Suites are environmentally conscious. Low flow showers and faucets, dual flush toilets and sustainable product selections are part of the brand's positioning strategy to enhance guest experience.





STN #	CASTING	INVERTS	COMMITTEED PIPE DIA	STRUCTURE TYPE
101	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
102	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
103	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
104	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
105	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
106	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
107	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
108	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
109	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE
110	REINFORC. CONCRETE	36" DIA. (12' x 12' x 12')	36" DIA. (12' x 12' x 12')	24" x 24" MANHOLE



APPROVAL PENDING NOT FOR CONSTRUCTION

**FLOODPLAIN NOTE**  
THIS SITE DOES NOT LIE WITHIN A FEDERALLY DESIGNATED FLOODPLAIN - FROM MAP PANEL NUMBER: 810189 (DAED CITY OF BLOOMINGTON) - DECEMBER 17, 2010

**SITE NOTE**  
NO EARTH DISTURBING ACTIVITY MAY COMMENCE WITHOUT AN APPROVED STORMWATER MANAGEMENT PLAN.

**DRAINAGE NOTE**  
REFER TO ARCHITECTURAL & MECH PLANS FOR LOCATIONS, DETAILS AND SPECIFICATIONS RELATED TO FOUNDATION, ROOF & SUB-SURFACE DRAINAGE. ROOF DRAINAGE ARE TO BE KEPT SEPARATE FROM FOUNDATION DRAINAGE.

ASSUMED NORTH  
SCALE: 1" = 30'

- GRADING & DRAINAGE NOTES**
1. ALL GRADES AT BOUNDARY SHALL MEET EXISTING GRADES
  2. ALL SLOPES SHALL HAVE A MINIMUM SLOPE OF 1:0.05
  3. CONDUITS SHALL BE INSTALLED TO EXISTING UTILITIES
  4. SLOPES SHALL NOT BE GREATER THAN 3:1 UNLESS OTHERWISE SPECIFIED
  5. EXISTING AND PROPOSED GRADES SHALL BE SHOWN ON THE GRADING PLAN. THE SUBCONTRACTOR SHALL VERIFY IN WRITING THE OWNER AND THE ENGINEER IN ANY DISCREPANCIES WHERE THIS CANNOT BE DONE.
  6. FILL UNDER DRAIN AREAS SHALL BE COMPACTED TO AT LEAST 90 PERCENT STANDARD PROCTOR DENSITY. ALL EXCESS OR SURPLUS OF EARTH IS TO BE REMOVED FROM THE SITE OR TO BE COMPACTED TO AT LEAST 90 PERCENT STANDARD PROCTOR DENSITY (ASTM D-1557).
  7. THE CONTRIBUTOR SHALL COVER ALL EXCESS QUANTITIES FROM THE STREET OR SIDEWALK. THE CONTRIBUTOR SHALL COVER THE EXCESS QUANTITIES FOR STORMWATER, REMOVAL, OR WEARING OF EXCESS.
  8. ANY PART OF STORM SEWER, MANHOLE, INLET OR MANHOLE SHALL BE LOCATED WITHIN A MINIMUM 5' OF PAVED AREAS IS TO BE BUILT WITH GRANULAR MATERIAL.
  9. STANDARD SPECIFICATIONS FOR THE LOCAL GOVERNING AUTHORITY SHALL APPLY FOR ALL STORM SEWER CONSTRUCTION.

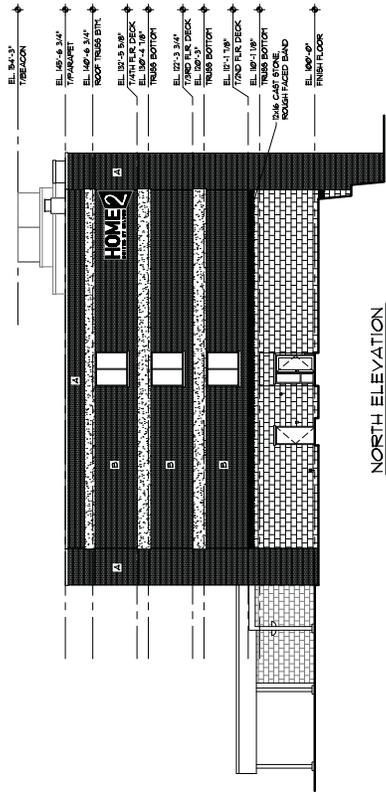
	<p><b>MHG HOTELS, LLC</b> <b>HOME2 SUITES BY HILTON</b> 17TH ST. &amp; WALNUT ST. BLOOMINGTON, INDIANA</p>	<p>DATE: 09-29-14</p>	<p>ROGER WARD ENGINEERING</p>
		<p>BY: <i>R. Ward</i></p>	<p>REVISIONS:</p>

**SP-32-14**  
**Grading Plan**

SHEET  
C103  
OF  
13

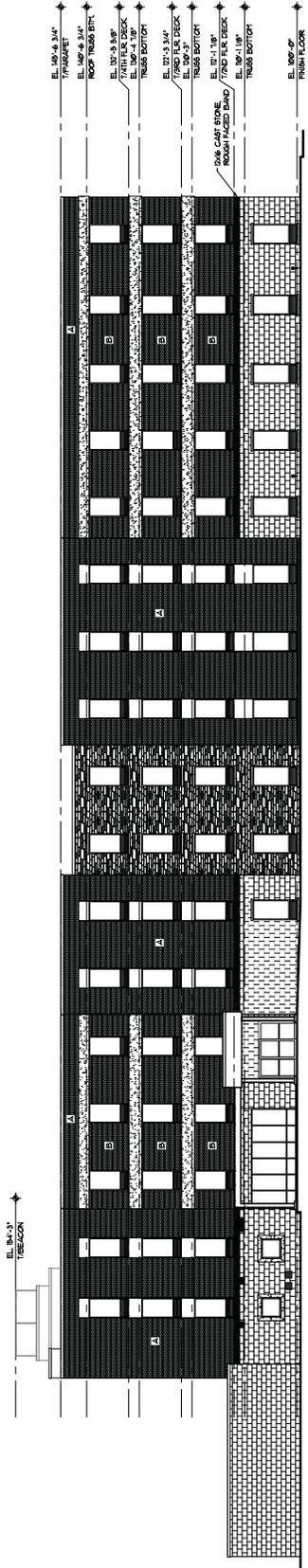
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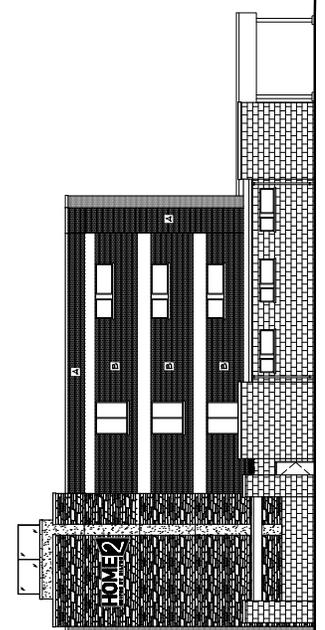
NORTH ELEVATION  
 SCALE: 3/32" = 1'-0"

- BRICK COLOR 'A'
- BRICK COLOR 'B'
- 2x6 GARY STONE SMOOTH FACED
- STACKED STONE
- E1FA FINISH REFLECTIVE
- E1FA COLOR (REQ. COMPLEMENTARY TO ADJACENT BRICK)



EAST ELEVATION  
 SCALE: 3/32" = 1'-0"

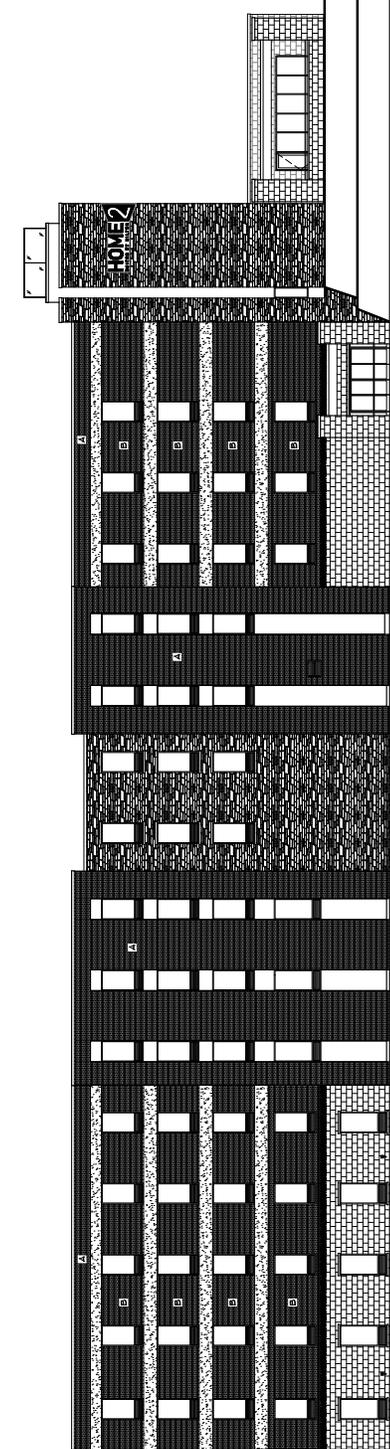
- EL. 8'-0" ± FINISH FLOOR
- EL. 10'-0" ± ROOF TOP
- EL. 11'-0" ± PARAPET
- EL. 12'-0" ± PARAPET
- EL. 13'-0" ± ROOF TOP
- EL. 14'-0" ± 7TH FLOOR DECK
- EL. 15'-0" ± 7TH FLOOR DECK
- EL. 16'-0" ± 7TH FLOOR DECK
- EL. 17'-0" ± 7TH FLOOR DECK
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SOUTH ELEVATION  
 SCALE: 3/32"=1'-0"

- BRICK COLOR 'A'
- BRICK COLOR 'B'
- DIM. CARP. STONE SMOOTH FACED
- STACKED STONE
- EIFFEL FINISH REFLECTIVE
- EIFFEL COLOR (REQ. COMPLEMENTARY TO ADJACENT BRICK)

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WEST ELEVATION  
 SCALE: 3/32"=1'-0"

DIM. CARP. STONE  
 ROUGH FACED BAND

SP-32-14  
 Elevations



