

Bike and Pedestrian Safety Commission

Agenda

Monday, September 16, 2013

5:30 p.m. – Hooker Conference Room

- I. INTRODUCTIONS
- II. APPROVAL OF MINUTES – August 19, 2013
- III. PUBLIC COMMENT
- IV. COMMISSION MEMBER’S COMMENTS
- V. DEPARTMENTAL UPDATES
  - A. City Planning
    - 1) Civil Streets/Open Streets
    - 2) 3rd Annual Bike Summit - October 17th
    - 3) Open Streets Bloomington
    - 4) 306 E. Kirkwood - ONB Site Plan Review
  - B. City Engineering
    - 1) Cascades Trail
  - C. Bloomington Police Department
- VI. NEW BUSINESS
- VII. OLD BUSINESS

NEXT WORK SESSION: Monday, October 7th, 2013 in the Hooker Conference Room

NEXT MEETING: Monday, October 21, 2013 in the Hooker Conference Room at the Showers Building

MINUTES  
BIKE AND PEDESTRIAN SAFETY COMMISSION  
8-19-2013

**INTRODUCTIONS**

Members: Present: Carl Zager, Paul Ash, Mitch Rice, Jim Rosenbarger, Betty Rose Nagle

Absent: Jacob Sinex. Anne Phillips Holahan

Ex-Officio: Scott Robinson - Planning  
Anna Dragovich - Planning  
Laurel Archer - Public Works

Citizen: Jim Williams – Park Ridge East Neighborhood Association  
Becky Appleman

**APPROVAL OF MINUTES**

Jim Rosenbarger moved to approve the minutes from July 15, 2013. Motion was seconded. The motion passed. Minutes approved.

**PUBLIC COMMENT**

Jim Williams stated he just came down Morningside and Longview and no work has started on the bike path. It was his understanding that the City wanted to be done by the time school started.

Mitch Rice stated for the record, that he believed Mr. Williams was talking about bike route, markings on the street, and not a bike path.

Jim Williams thought there are to be signs and crosswalks, and paintings of the shared bike route. Williams stated he believed the project is called Longview Greenway Project.

Scott Robinson explained this is called the Neighborhood Greenway. Robinson added the City bid the project out in two pieces. The first broke out the lane markings and that work has commenced. The bikeways project just went out and bids are due this week. Robinson suggested contacting Public Works after Wednesday for more information on when this will happen.

**COMMISSION MEMBER COMMENTS**

Paul Ash stated he noticed on Allen Street the median strip had been done and thought that was a prelude to the rest of it but nothing else has happened.

Mitch Rice wasn't sure what Paul was expecting as he hasn't seen the plans. Paul stated there will be the island at Allen and Rogers and then on pavement markings. This is the westward bound section that goes to Patterson or Allen.

Robinson stated it was bid out to go to Patterson.

Jim Rosenbarger asked about the directional signs for the bypass underpass. Students are back in town and riding all around and wondered about the status.

Betty Rose Nagle stated at the last meeting Mitch commissioned her to address the bike and pedestrian relationship. She stated there was a letter to the editor about a child being run over by a bike. She stated she will try and write a guest column the next time this occurs, or maybe sooner. She felt a little defensive walking, driving, and biking can go along way. The B-line has become too small. On nice days there is two way traffic. There are bikes, pedestrians with dogs, pedestrians with children, etc.

Mitch stated they had brought up the issue of signage at the last meeting for Market Day asking bikers to dismount. Maybe signage all up and down the trail would be a good idea.

## **DEPARTMENT UPDATES**

### **PLANNING**

**1) 304 West Kirkwood** - Scott Robinson stated this is a large project and will probably require two hearings. They would like to tear the building down and rebuild. There is frontage on Kirkwood, 6th and Madison, as well as the B-Line. The new building will be mixed used building 17,500 square feet. 1st floor office and retail. There will be 28 units on floor 2 - 4, and the 4th floor will have penthouse units for the property owners to live. It will look like a three story from the B-line, and a 4 story from the alley. This will be mostly limestone and some glass.

Mitch stated typical questions would be about bicycle parking and pedestrian amenities.

Robinson stated it looks like there is a plaza area that will probably stay.

The project was discussed. The Commission was concerned that there be adequate bicycle parking as this is right next to the B-line and that there be elevators for residents who would like to take their bicycles up to their apartments.

**2) Civil Street Update** - Anna Dagovich stated Planning is working with Jacqui Bauer, Sustainability Coordinator, to organize a Civil Streets Campaign. The City wants to target all modes of transportations to discourage negative behaviors out on the road. Information has been gathered by public comments, field observations, and people who work downtown. Some of the behaviors are texting and driving, texting and walking,

bicycles not stopping at stop signs, etc. A logo has been made and posters will be put up around the city. There will be a sticker campaign where the police, when they pull someone over, will give out a sticker and some literature on how to behave. This is a local campaign with Little Bub, and other local celebrities. A press release will be going out soon to kick off the campaign. Also, enforcement will be elevated in October and April which is the time when more bikes are on the roads and also about the time students come to town. The City has also applied for a grant from the Indiana Criminal Justice Institute to help with this campaign.

The Commission felt increased enforcement would help, as well as education. Jim also mentioned that at one time the Commission advocated for an unmarked enforcement officer walking around the square, but that did not happen.

**3) Open Streets Bloomington** - Anna Dagovich explained this is a City/County initiative for Open Streets. This event is coming up and sometimes called "Sunday Streets." The event will be on West Kirkwood between College and Rogers. The event will be on September 22nd from 10 - 3p.m. Open streets is an event where the street is closed to cars and opened for playing in the street. There will be music, yoga in the street, a climbing wall, etc. There is a website "openstreetsbloomington.com" Also a kick starter campaign was started today to help raise money for the event. This is a public private partnership and businesses are starting to approach asking how they can get involved.

Paul Ash wanted to know if there would be business booths. Anna stated yes, but they must be engaging, like hosting a bike rodeo or something.

Jim stated he is excited about the concept but thought it was a boring location. He stated he would like it to be something a bit more spectacular and gave the example of closing a bridge over the Ohio River. That said, he said he would be there and support the event.

Mitch agreed with Jim and felt East Kirkwood might be a better location. Jim thought the square would be a good location. Paul Ash thought 4th Street was a little more attractive now that the gallery and bakery is there.

**4) 25 Year Retrospection** - Scott Robinson stated the BPSC 25th anniversary is coming up. There have been a lot of initiatives put forward by this Commission. It is a good time to look back at the milestones over the years which are:

- BPSC created in 1989 (Ord89-22)
- Neighborhood Traffic Safety Program (NTSP)
- Bikes Week
- Bike Lane on Walnut and College
- College Mall Pedestrian Accessibility Study
- Bloomington Bicycle Map
- Ordinance change in 2001

Looking toward 2014 there are many opportunities, such as the Civil Streets Campaign.

Scott announced that he and Vince sit on the Platinum Task Force which is coming to fruition. Scott passed around a budget proposal to help fund some of the BPSC initiatives, such as the bike maps. If the budget proposal is approved it will be a good opportunity for this group to look at some of its priorities and directions they would like to take in education and enforcement.

Paul Ash asked about the radio spot with B97 and asked if that would be the only radio station used. Scott explained that the Parks Department already buys an advertising package for the year, and during their slow time they try to fill those paid for spots. It would be good for the BPSC to pay them to use some of their spots to get their message out. This takes advantage of something the City already has in place.

Carl Zager agreed there is a whole audience that is not getting the BPSC message.

Scott Robinson stated in order to become a Platinum Bike Community more has to be done in the area of education and enforcement. The City is very good in rolling out infrastructure. Scott Robinson stated the barrier to enforcement is staff time.

Carl Zager stated when he was on vacation recently he had an opportunity to talk to bike enforcement officers in two beach towns police departments. One of the Corporals funded an education program with bells and lights through federal grants. They partnered with bike shops in the area and bought the bells and lights from those businesses. The officer's mantra was "education rather than enforce." He said 95% of the people he deals with that do something wrong think the bike is the same class as pedestrians, yet every state views bike as a type of vehicle. Getting grants to help out and bring in bike shops form the beginning helps.

Mitch felt the Commission should be giving grants rather than receiving. Scott agreed and stated that is what this budget money would help with. This might be an opportunity to work with law enforcement to develop education programs.

Paul felt the bike maps were also a way of education.

Jim Rosenbarger stated this was a great and a way to ramp up the Commission a bit.

#### Engineering Department

No business

#### Bloomington Police Department

No business

#### OLD BUSINESS

None

NEW BUSINESS

None

Mitch Rice adjourned the meeting at 6:20 p.m.



August 13, 2013

City of Bloomington Planning Department  
P.O. Box 100  
Bloomington, IN 47402

Attn: Mr. Tom Micuda

**RE: Pavilion Centre on Kirkwood  
306 E. Kirkwood  
Bloomington, Indiana**

**PETITIONERS STATEMENT**

Dear Mr. Micuda

Studio 3 Design is pleased to submit the attached mix-use development for your consideration. The project scope includes 1 site for development at the SW corner of Kirkwood and Lincoln streets at 306 E. Kirkwood. For the purpose of this submittal, you will see reference to this site as Site "C" which coordinates to parcels sold by Old National Bank.

Site "C" is bordered on the North by Kirkwood Avenue, on West by Lincoln Street, on the South by a public alley, on the east by a Public alley. The site is currently covered 100% by an open parking lot and bank drive thru for Old National Bank.

**Submittal Organization:**

In order to organize this project for the submittal we will start with looking at the project as a whole, the requirements with-in the districts and overall highlights to consider in the review of the project.

**Project Overlay District:**

University Village Overlay  
Kirkwood Corridor subsection – site C

Density: 33 units per acre

Impervious surface:  
Kirkwood corridor: 100%

Height Standards:  
Kirkwood Corridor: min. 25' – max. 40'

**Parking:**

None residential: No parking required for bank, retail, or office functions  
Residential: 5 for first 20, .8 for beds 21 on.

**Setbacks:**

Front: 0 to 15'  
Side yard: 0'  
Rear yard 0'

**Ground floor non-residential:**

Applies to Kirkwood, 4<sup>th</sup>, Lincoln and Grant  
50% min. along applicable street frontage

**Building alignment:**

No Outstanding, notable or contributing structures immediately adjacent to properties.

**Building orientation:**

Kirkwood corridor: min. of one primary entrance facing Kirkwood

**Street trees:**

Kirkwood corridor: 5' square tree grate or large curbed planting bed

**Lighting:**

Street lighting- traditional style design such as acorn or gas lamp style.

**Architectural character:**

Kirkwood corridor – flat roofs

**Void to solid:**

Kirkwood Corridor: lower- 60%, upper 20%

**Windows:**

Kirkwood: large display on level 1, 1.5 to 1 ratio on upper windows with sills and lintels

**Materials:**

**Not permitted as Primary**

Kirkwood Corridor: Wood, EIFS, CMU, vinyl, cement siding

**Not permitted as a secondary material:**

Kirkwood Corridor: EIFS, Vinyl, Cement siding on first level only

**Entrance Detailing:**

General: - shall incorporate a min. of 3 of the following:

4' recessed entry, ornamental paving, Canopy/ awning, Portico, arched entry, pilaster or façade module projecting from exterior wall plane, building address with, building name and lighting, public art, raised cornice entryway parapet, rusticated masonry, landscaped patio area for outdoor seating of 8 or more.

**Mass, Scale & form;**

Façade modulation- maximum width 50' with a min. 3% offset based on total length of façade.

Height step-down: **N/A** – properties are not immediately adjacent to outstanding, notable, or contributing structures.

**Project Overview:**

The project is located in the heart of downtown Bloomington along Kirkwood Avenue and Lincoln Street and for the purpose of this submittal it is identified as Site "C".

The projects will provide a mixed use development that will enhance the urban fabric and add life to the current spotted development along Kirkwood and Lincoln. The development will provide a solid streetscape with varied heights, materials, colors and architectural detailing to generate an exciting urban feel filled with Bank retail-ONB on the main level along Kirkwood and market rate apartments on levels 2 and 3. The apartments are clustered around an open air internal plaza and common fitness center that serves the apartments as well as other properties owned by GMS properties. These functions combined with naturally lite stairwells encourage fitness, activity and social interaction with-in the development. The retail space stretches down Kirkwood for the length of the property and returns down the length of Lincoln. The lower level opens up to the streetscape with large expansive windows and provides a pedestrian friendly atmosphere and streetscape.

Currently, the site is covered with an open air parking lot and drive through bank lanes - providing little if any value to the city's downtown urban fabric. The new development removes this sea of parking and creates a below grade parking garage and screens the remaining grade level bank drive through with new development along Kirkwood in place of the current exposed drive lanes. The sidewalks along this new development is wide and provides opportunities for street trees, street lighting and furniture along with the potential for outdoor seating. The building steps in and out along the street front to provide relief against the 3 level volume and to designate major points of entry for the building.

Beyond the obvious physical changes to the area, the development offers a host of innovations and amenities including facilities that are going "green".

The site will provide space for covered and secured bicycle parking and is located near to public bus stops to encourage the use of alternative means of transportation. A large trash room with space for recycling is provided. The room for trash and recycling is located in interior space to encourage use and accessibility along with keeping the alleys clean of debris. To reduce the urban carbon footprint of the facility, the building shell will be well insulated with a min. of 6" of thermal insulation and insulated window units with Low-E film. On the uppermost roof plains, white TPO roofing over rigid insulation will be used to reflect sunlight and reduce heat gain. Internally, energy star appliances, high efficiency furnaces, insulated demise and perimeter walls, energy efficient light fixtures, motion sensor lighting in common areas, abundant natural lighting sources, low flow toilets and aerated shower heads and faucets designed to reduce water usage will be provided. Additionally, interior finish materials will be selected based on their recycled content and or ability to be recycled. Common building materials such as lumber and masonry will come from within 500 miles to reduce transportation and the associated emissions and fossil fuel consumption.

While the end goal is not to go through the full certification of the buildings under Leed, it is to create a sustainable community that embraces many of the same characteristics and to create a facility that adds life and vitality to the area.

The development will also improve city utilities in the area and clear out utility poles and other obstructions in the currently hard to navigate alley to create safer, functional travel lanes. As well as the rerouting / replacement of utilities as determined by CBU.

**Site Specific Information:**

**Site C - Kirkwood and Lincoln streets – 306 E. Kirkwood**

**Site C Use- Retail, & market rate apartment building.  
 (All uses are permitted under the UDO)**

- **Three level building above grade with a full parking garage below grade.**
- Lower level will have a full underground parking garage with **21** parking spaces
- Level 1 will have new Old National Bank branch, a 4 lane drive-thru, an apartment entrance lobby, a public meeting room, secured bike storage, trash/ recycling room, storage and mechanical room for the bank.
- Levels 2 and 3 will have market rate apartments surrounding a central courtyard with potted landscaping and outdoor seating and access to a large fitness center.

**20.03.190 Development standards:**

**Density and intensity standards (Project complies with UDO)**

Site: 132'x 132' = 17,424 sf/ 43,560 = .4 acre x 33 = **13.20 DUE's available**

Level 1 - ONB Bank	0.00 DUE's used
Level 2 & 3– Apartments	<u>12.58 DUE's used</u>
<b>Total</b>	<b>12.58 DUE's used out of available 13.20</b>

<u>Apartment Types</u>	<u>Count</u>	<u>Beds</u>
Two bedroom flat	13 Units	26 Beds
Three bedroom flat	04 Units	12 beds
	<u>17 Units</u>	<u>38 Beds</u>

**Property density: (13.20 DUEs available – complies with UDO)**

1 bed	.25 DUE x 0 =	0.00 DUE's
2 bed	.66 DUE x 13 =	8.58 DUE's
3 bed	1.00 Due x 4=	<u>4.00 DUE's</u>
<b>Total DUE's used</b>		<b>12.58 DUE's</b>

**Maximum impervious service coverage: (complies with UDO)**

Site area= 0.4 acres (17,424 SF)  
 Impervious percentage = 100% current and proposed  
 Kirkwood corridor allows for 100% impervious coverage

**Building Height (Project is in line with UDO –requires waiver due to grade slope)**

Min. of 25' with a max. of 40' – Project from grade is under the 40' maximum building height allowed along Kirkwood. Grade falls off to the south and east causing the buildings parapets to be as much as 44' above the lowest point of grade at the SE corner of the building along the alley.

While a waiver is required based on the technical language of the UDO, the height issue is **not the result of extra floors or excess density** - it is the result of changes in grade – a common problem in Bloomington which the UDO does not account for. This waiver has routinely been granted at sites all around the city's downtown based on a recognition that Bloomington is not "flat".

**Parking Standards (Project complies with districts' guidelines)**

**Required:**

Retail: bank level 1	00 required
Apartments:	<u>20 spaces required</u>
Total required:	<b>20 required</b>

**On-site provided:**

Parking garage	21 spaces
Level 1	00 spaces
Street parking	<u>07 spaces</u>
Total provided	<b>28 spaces</b>

**Building setback standards: (Building complies with the districts' guidelines)**

Front setback- 0' to 15'  
Side yard minimum 0'  
Rear yard min. 0'

**Ground floor non-residential Uses: (Building complies with districts' guidelines)**

Kirkwood Ave & Lincoln Street: no less than 50% of total ground floor area shall be used for non-residential uses. (the drive-through only serves the bank retail function – no parking).

Level 1: Gross SF.	15,336 sf
Bank and bank support functions	6,845 sf
Bank drive thru	5,204 sf
Apartment building lobby, bike room	
Garage ramp, and stairs/ elevators	3,287 sf

Non-residential use % **78% on level 1 dedicated to non-residential use.**

**Site Plan standards:**

**Site Plan: (building complies with the districts' guidelines)**

Building Frontage – NA

Building Alignment – NA (structure is not adjacent to an outstanding, notable of contributing structure.

Building Orientation and entrances:

- (2) Pedestrian entrances provided on Kirkwood
  - (1) Pedestrian entrance provided on Lincoln Street
- All entrances are within three feet of the adjacent sidewalk elevation.

**Street trees: (site plan complies with the districts' guidelines)**

Street trees in tree grates and or large curbed planting beds to match up with the current Kirkwood corridor streetscape are provided.

Lincoln streets streetscape will provide street trees in min. 5'x 5' tree grates along the sidewalk and maintain existing street trees where possible.

**Lighting: (Site plan complies with the districts' guidelines)**

Pedestrian scale street lights (less that 15' in height) will be placed along Kirkwood and Lincoln streets.

Traditional style acorn light fixtures with full cut-off shall be used.

Exterior building lighting will comply with 20.05 lighting standards.

**Mechanical equipment and Service areas: (site plan complies with districts' guidelines)**

Bank and garage mechanicals are housed internally with air intakes/ fans located along alley's. Transformer is located along the alley, trash removal is located along the alley. Apartment condensing units are located on the roof.

Condensing units on roof will be setback a min. of 10' from building edge to prevent view form adjacent streets.

**Architectural Character: (complies with the districts' guidelines)**

**Roofs or building caps:**

Kirkwood corridor: flat roofs and parapets are provided.

**Void To Solid Ratio:**

Kirkwood Street First Level =	60% required	<b>64% provided</b>
Kirkwood Street Upper Levels =	20% required	<b>33.2% provided</b>
Lincoln Street First Level =	60% required	<b>63.2% provided</b>
Lincoln Street Upper Levels =	20% required	<b>36.6% provided</b>

**Windows: (building complies with the districts' guidelines)**

Windows are transparent

Level 1 windows will be large storefront display windows

Level 2/3 window frame heights shall be a min. of 1.5 x the width

Windows will incorporate sills, lintels, heads that are visually distinct from the primary exterior finish.

**Materials: (building complies with the district guidelines)**

Primary facades on Lincoln and Kirkwood comply with allowable primary and secondary building materials

Secondary facades along east and south alleys comply with allowable primary and secondary building materials.

The building materials vary to create a traditional feel with a combination of storefront and punched window openings, architectural cast stone, and multiple colors of face brick and simple cornice detailing. This same feel and use of materials carries around the building on the alley facades to present a unified level of articulation on all sides – not just the street front. These various materials delineate and mitigate the scale and mass of the building vertically and horizontally with banding and color changes.

**Entrance Detailing: (Building complies with the districts' guidelines)**

A minimum of 3 architectural design features shall be incorporated: The following will be used on the building:

- Min. 4' recessed building entrance

- Ornamental paving and integral landscape planters

- Canopy/ awnings

- Prominent building address, name, and enhanced exterior lighting

**Mass, Scale and Form: (building complies with the districts' guidelines)**

Facade modulation: Maximum 50' module on street frontage

Module offset of a min. of 3% (4 feet)

Building height set down – NA – building is not located adjacent to a an outstanding, notable or contributing structure.

**Additional information:**

**Parking Garage**

Parking is located under the building on the lower level and is accessed from an internal ramp system. The garage is feed from a two way ramp entering off of the alley in the SE corner of the site.

### **Bank Drive thru teller window.**

The bank will have a 4 lane drive through that is concealed under the building. A one way drive will access the drive-thru off of Lincoln street and the drive thru will exit onto the north- south alley on the east side of the site with access out onto Kirkwood.

### **Site Accessibility**

ADA compliant building entrances are provided on Kirkwood and Lincoln. Elevators from the garage will be access controlled for access vertically into the apartments. Control gates may be incorporated at the parking garage and for after hours at the bank drive-through to limit access to these areas at night.

### **Bike Storage/ Parking**

The building provides secured covered bike storage on "U" racks for up to **12** bikes on Level 1. Per the UDO – 7 are required for residential (4 covered and 3 class II).  
4 are required for the non-residential.

Provided on site: **8 Residential:** (6 in secured bike room, 2 under building canopy at pedestrian entrance. **4 for retail component:** (2 at bank entry and 2 and community room entry).

### **Build to Line**

The project meets the requirement of the UDO to have the majority of the façade constructed to the build-to line (property line) along Kirkwood and Lincoln streets. Portions of the building step back to provide interest and natural breaking points for material changes along the façade. Additionally, step-backs in the façade create deeper zones at street level to provide relief and interest along the streetscape as well as opportunities for street furnishings and the ability for people to gather out of the main circulation path.

### **Building Entrances**

The building has (3) three public pedestrian entrances.

(2) two entrances on Kirkwood – one for the bank and one for ADA access to the bank and access to the apartments. On Lincoln Street, there is an entrance serving the apartments on level 2 and 3 of the building as well as access to the public community room. Access drives for the bank drive-thru are one way off of Lincoln Street and exit onto the alley then out to Kirkwood avenue.

Parking Garage entrance is off of the alley in the SE corner of the site. Ramp is covered by the building to protect it from bad weather.

### **Trash Removal**

Trash removal has been provided along the north-south alley on the east side of the building. The trash container will be located within an enclosed area furnished with a rolling door to be opened only on day(s) of trash collection. This area will also be accessed from the building for Retail and apartment tenants' disposal of trash. The grade will be leveled at this location to assist in the roll-out of trash container on collection day(s).

### **Recycling**

Recycling facilities for the separation of goods will be provided on site. The room will be associated with the trash room and will be an interior space with space for the separation of

various recycling components. This room will be accessible for use by retail and apartment tenants alike. Provisions are in place for the time when the City provides a recycling pick-up program for the downtown – collection is not included at this time.

### **Building deliveries / move-in**

A delivery pull-off zone in front of the apartment pedestrian entrance is provide on Lincoln street to help maintain an open flow of traffic in the area.

### **Water Service & Meter Pit**

The project will connect to an existing 12" water line on Kirkwood that is within 5 years old. A master meter will be installed outside of the City right of way along the North-South alley on the east side of the Building- one for the bank and apartment building. It will house the necessary meters and fire apparatus. The PIV connection will be installed off of Kirkwood at the NE corner of the building for Fire department access. A separate Siamese connection will be back fed from the meter pit on Kirkwood for Fire Department access. No new mains are anticipated to be installed to provide service for the project – this has been confirmed with CBU. No new main will be installed along Lincoln Street either. There is no existing Water main on Lincoln Street, CBU did not see a need to add one.

### **Sewer Service & Grease intercept**

The project will connect to the city sewer mains on Kirkwood. Main is relatively new. A connection will be made to the existing City sewer main routed down Kirkwood Avenue. All connections will be lateral connections with standard patching of the street as required. No new mains will need to be installed to provide service for the project – this has been confirmed with CBU. Grease intercepts will be located along Kirkwood if required by CBU. There is no immediate need for the bank function to have a grease interceptor. There is no existing Sewer on Lincoln Street- CBU did not see a need to add one.

### **Storm water**

Roof and plaza drains will be routed through a mechanical BMP or aqua-swirl as determined by CBU prior to exiting into the city storm system on Kirkwood. The storm line on Kirkwood is in good shape- no additional work is anticipated. Additional storm lines may need to be installed on Lincoln Street.

### **Site detention for water:**

The site is currently 100% impervious. No detention is required to be added as part of the new site development.

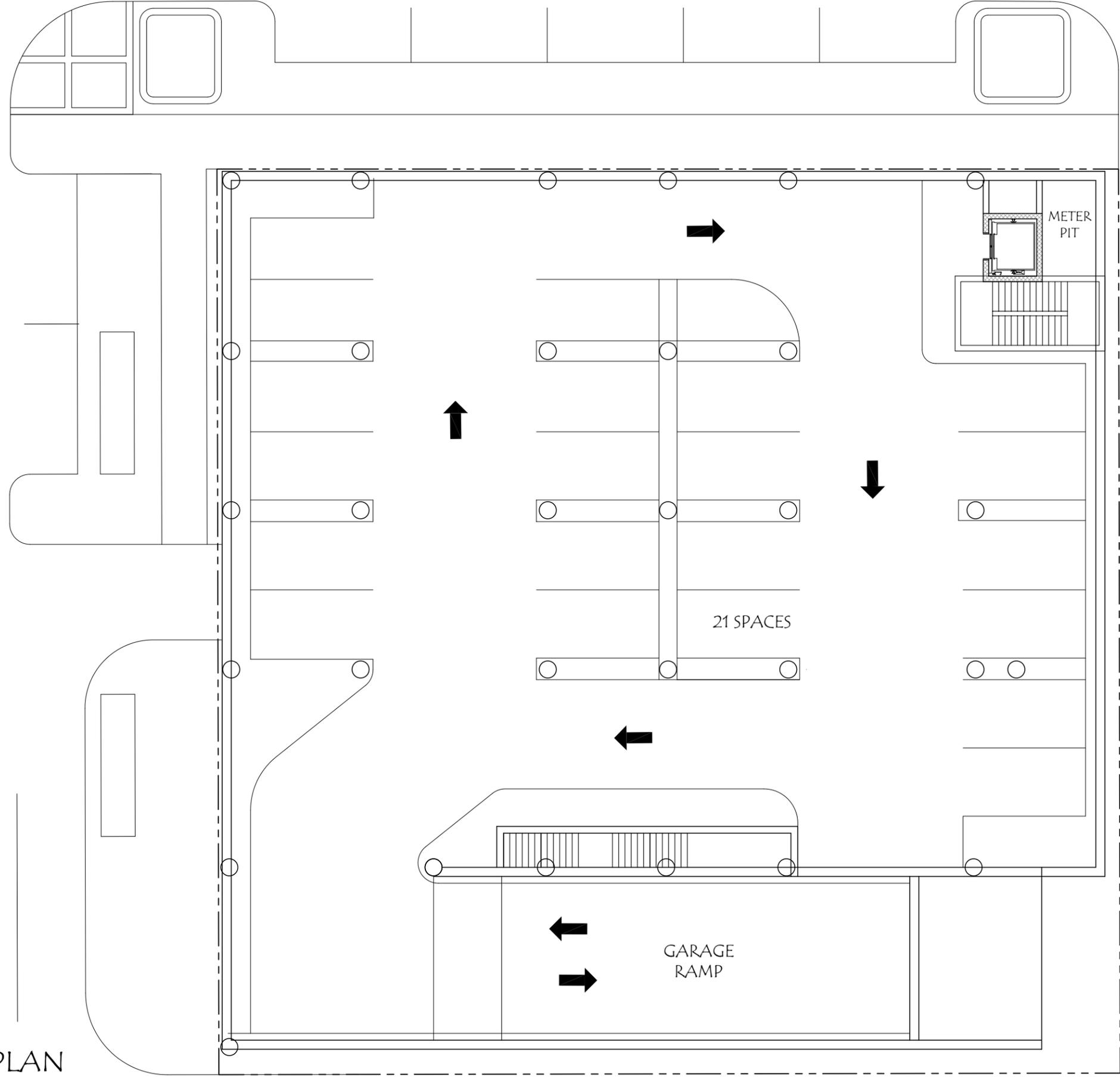
### **Private Utilities**

Duke Energy and a cable/phone/internet provider to be determined will provide for the service needs of the development. In preliminary design coordination with Duke Energy, a replacement power with pad mounted transformers will take place at the northeast corner of the adjacent site along the alley. A junction box for the phone lines to feed the development will also be in this area. The electrical meter is located near the southeast corner exterior wall of the building

Respectfully submitted,

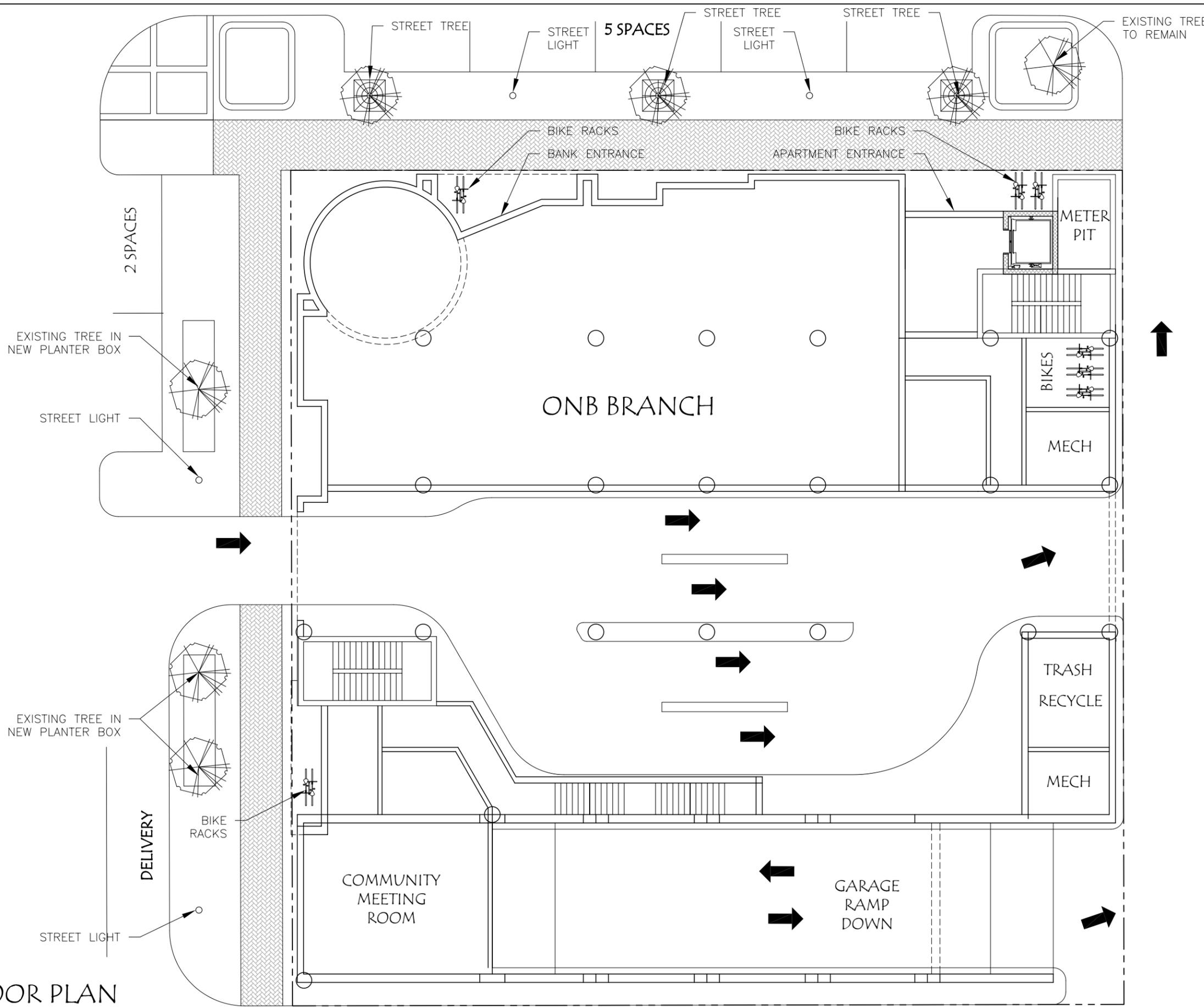
STUDIO 3 DESIGN, INC

1  
A1 GARAGE FLOOR PLAN  
1/16" = 1'-0"



1  
A2

**LEVEL 1 FLOOR PLAN**  
1/16" = 1'-0"



SHEET NUMBER  
**A2**

SHEET DESCRIPTION  
**LEVEL 1  
FLOOR PLAN**

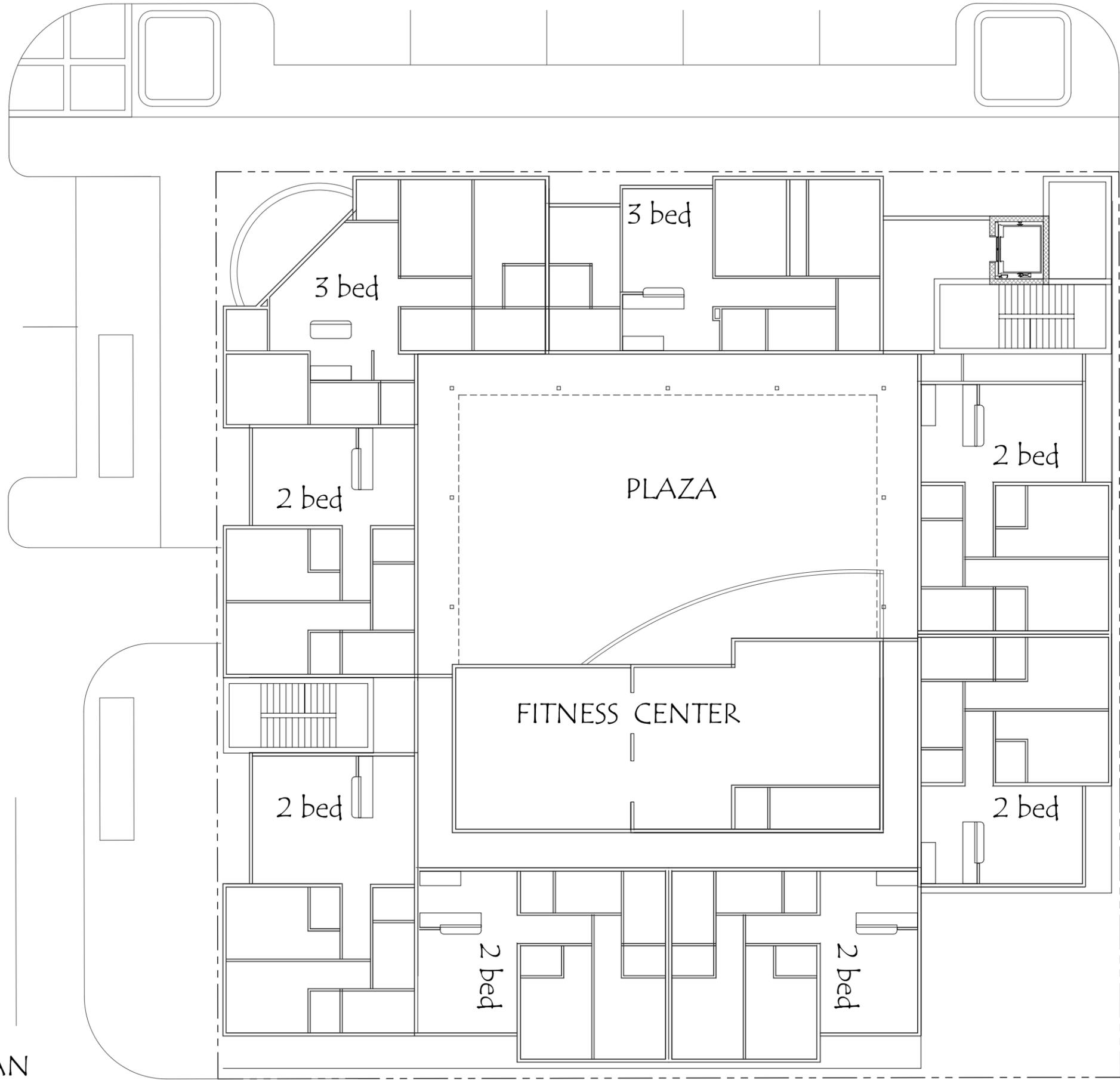
PROJECT NO.  
**13018.02**

DATE  
**9-10-13**

**GMS-PAVILION PROPERTIES, LLC**  
**PAVILION CENTER ON KIRKWOOD**  
Bloomington, Indiana

**STUDIO THREE DESIGN**  
interior design  
architecture  
www.studiodesign.net  
317.572.1236 fax  
8804 Allisonville Road, Suite 330, Indianapolis, IN 46250

1  
A3 LEVEL 2 FLOOR PLAN  
1/16" = 1'-0"



SHEET NUMBER

A3

SHEET DESCRIPTION

LEVEL 2  
FLOOR PLAN

PROJECT NO.

13018.02

DATE

9-10-13

GMS-PAVILION PROPERTIES, LLC

PAVILION CENTER ON KIRKWOOD

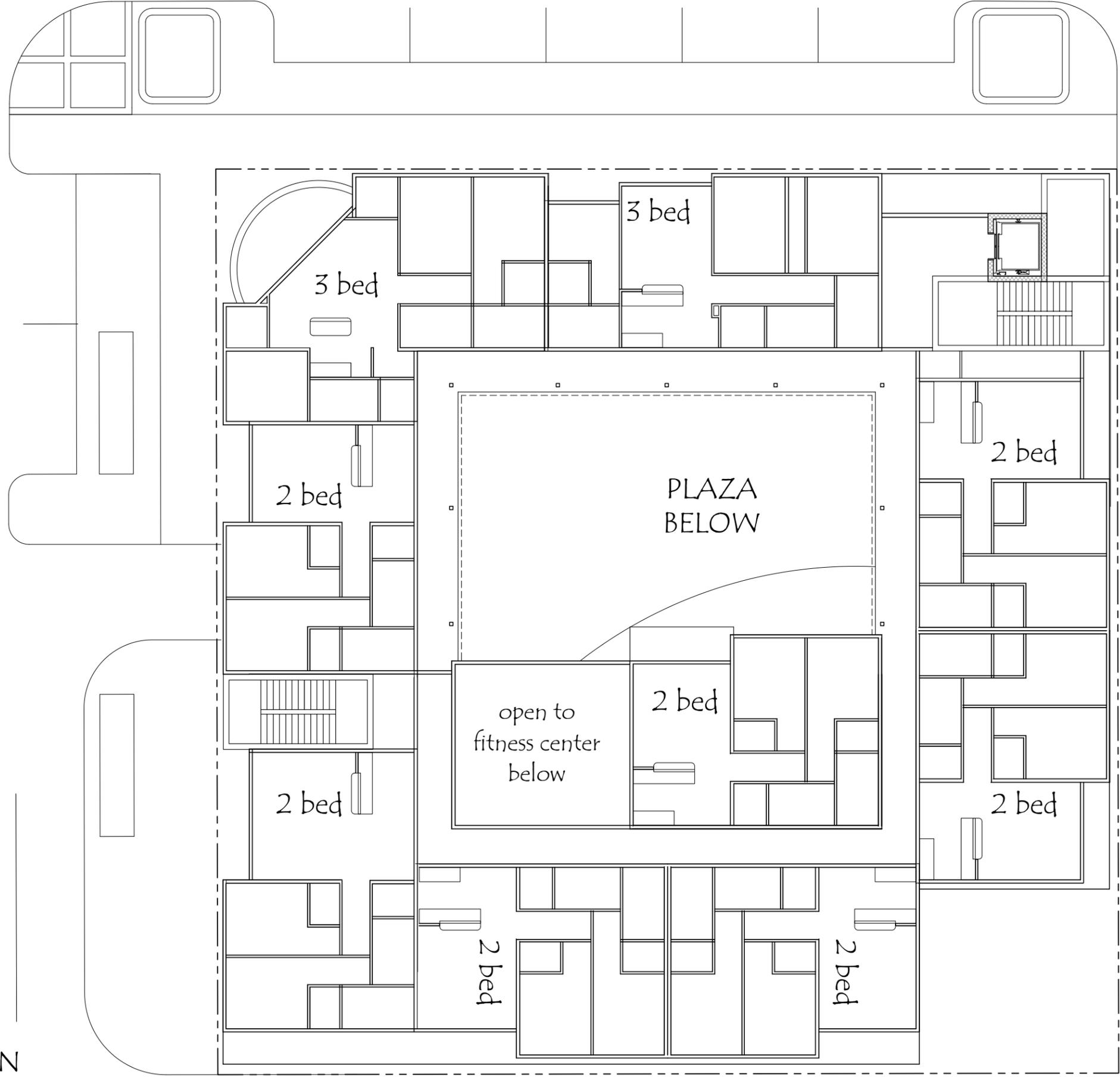
Bloomington, Indiana

**STUDIO  
THREE  
DESIGN**

interior design  
architecture  
www.studiodesign.net  
317 572-1236 fax  
8604 Allisonville Road, Suite 330, Indianapolis, IN 46250

1  
A4

LEVEL 3 FLOOR PLAN  
1/16" = 1'-0"





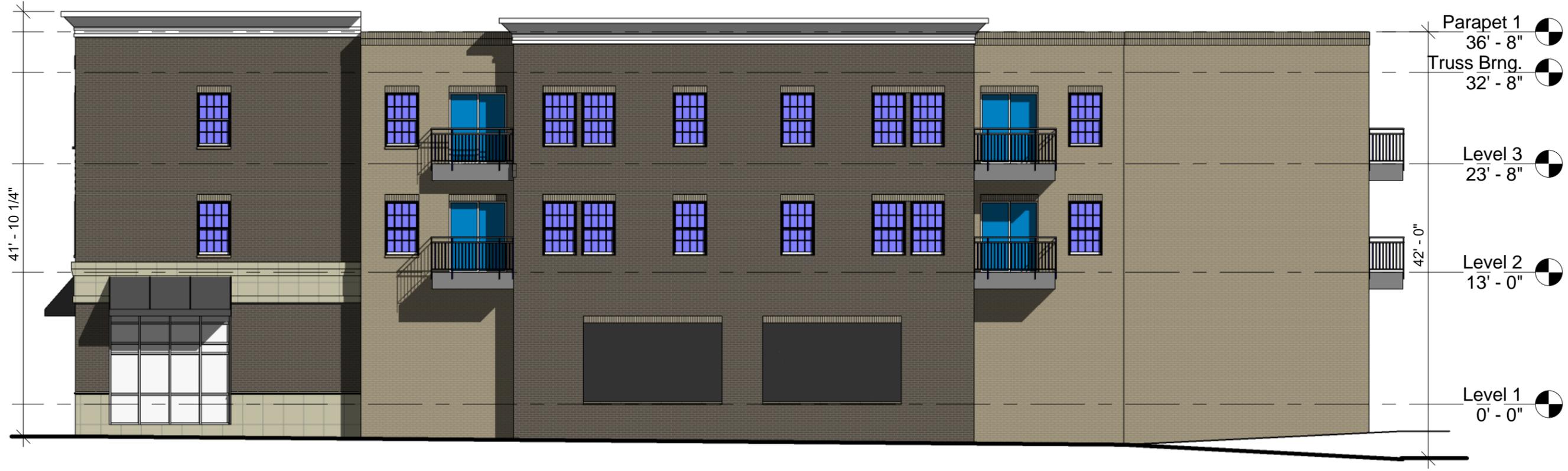
1 NORTH ELEVATION  
3/32" = 1'-0"

PROJECT NO. 13018.02		DATE 9/10/2013	
GMS-PAVILION PROPERTIES, LLC.		PAVILION CENTER ON KIRKWOOD	
BLOOMINGTON, INDIANA			
SHEET DESCRIPTION NORTH ELEVATION		SHEET NUMBER A5	



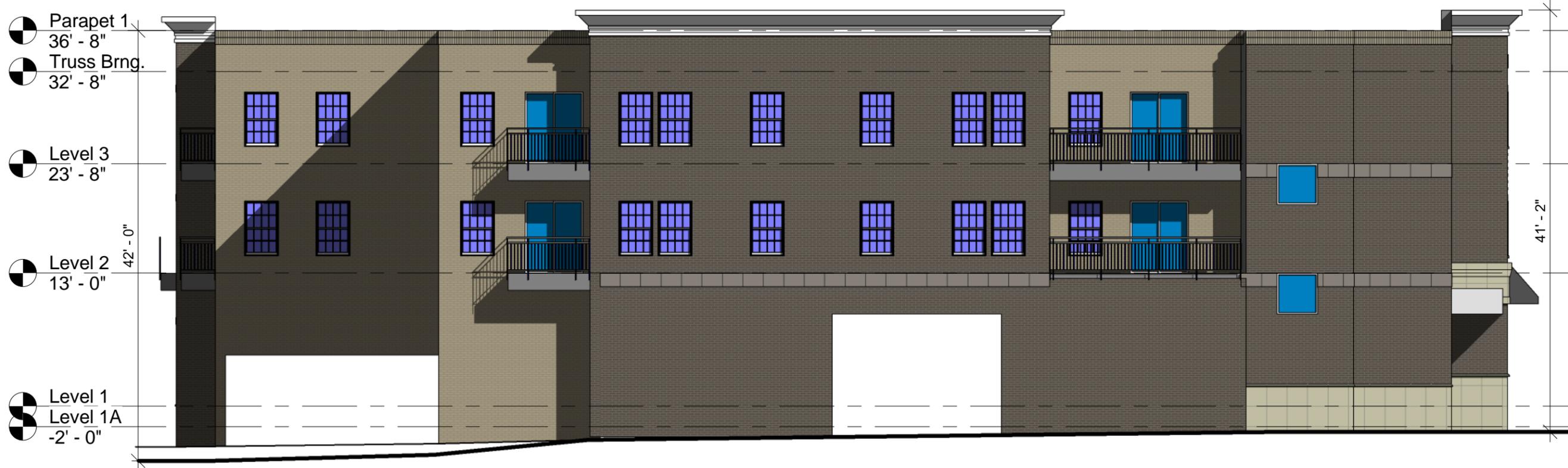
1 WEST ELEVATION  
3/32" = 1'-0"

SHEET NUMBER <b>A6</b>	
SHEET DESCRIPTION <b>WEST ELEVATION</b>	
PROJECT NO. <b>13018.02</b>	DATE <b>9/10/2013</b>
<b>GMS-PAVILION PROPERTIES, LLC.</b> <b>PAVILION CENTER ON KIRKWOOD</b> BLOOMINGTON, INDIANA	
<b>STUDIO THREE DESIGN</b>	



1 SOUTH ELEVATION  
 3/32" = 1'-0"

PROJECT NO. 13018.02		DATE 9/10/2013	
GMS-PAVILION PROPERTIES, LLC. PAVILION CENTER ON KIRKWOOD BLOOMINGTON, INDIANA			
SHEET DESCRIPTION SOUTH ELEVATION		SHEET NUMBER A7	

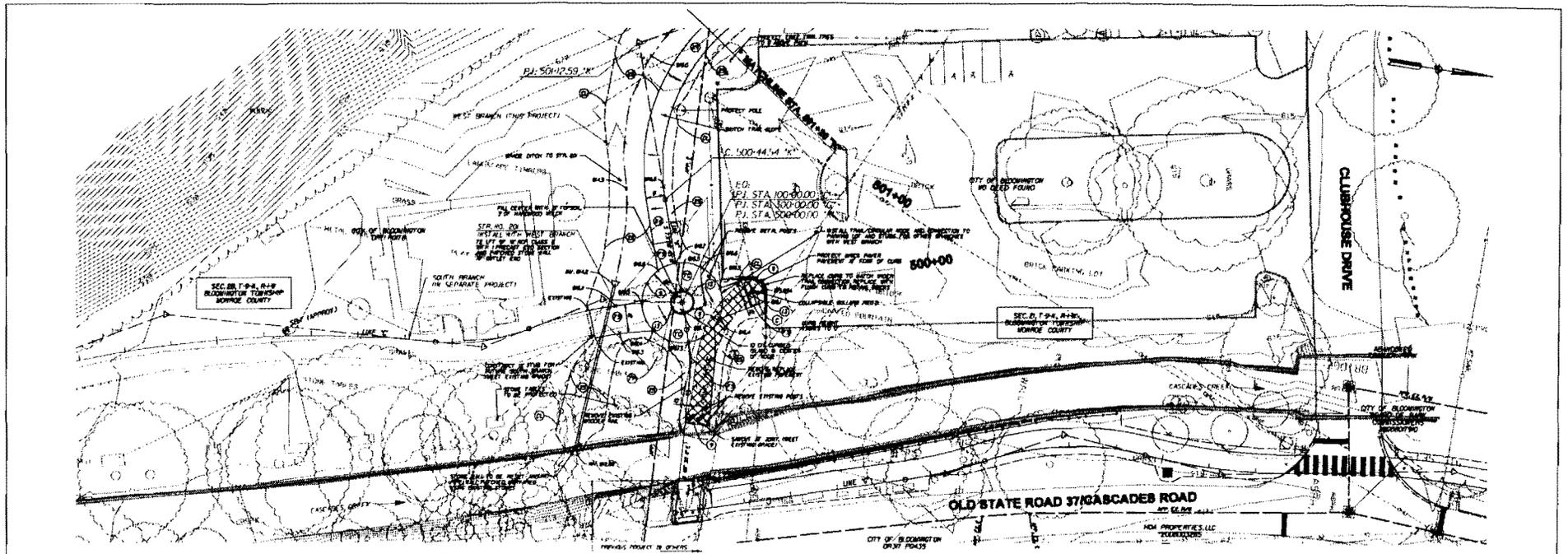


1 EAST ELEVATION  
 $\frac{3}{32}'' = 1'-0''$

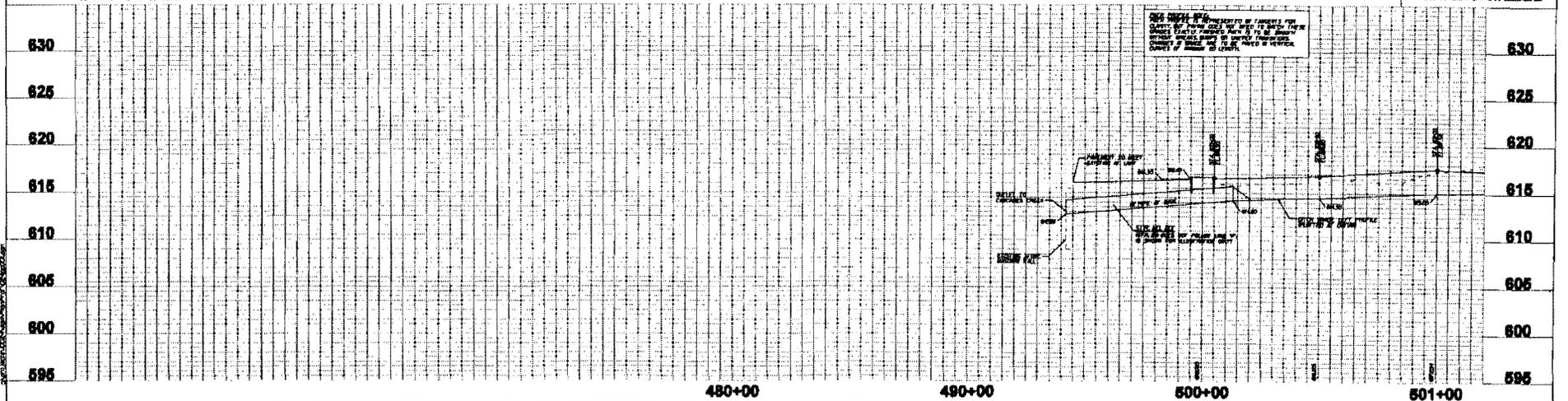
PROJECT NO. 13018.02		DATE 9/10/2013	
SHEET DESCRIPTION WEST ELEVATION		SHEET NUMBER A8	
GMS-PAVILION PROPERTIES, LLC. PAVILION CENTER ON KIRKWOOD BLOOMINGTON, INDIANA			
			

# **CASCADES TRAIL**



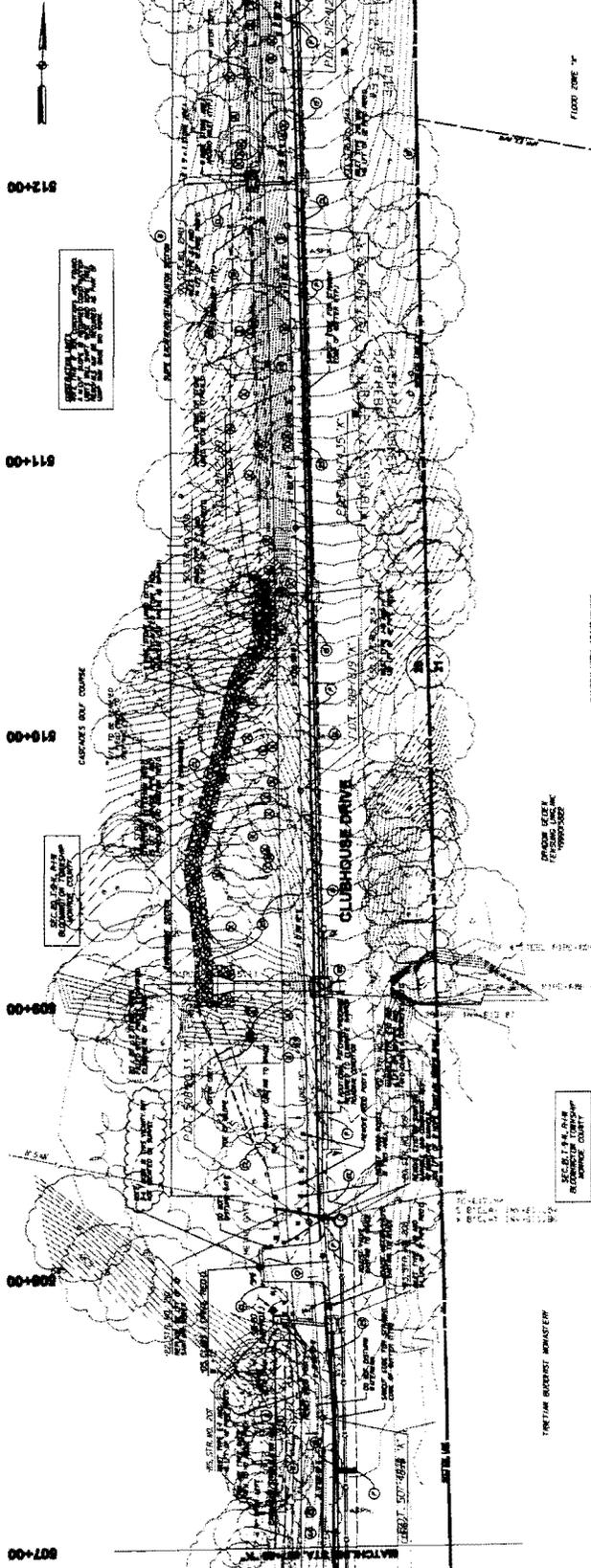


EXISTING UTILITIES SHOWN ON PLANS ARE APPROXIMATED IN ACCORDANCE WITH AVAILABLE RECORDS AND PHYSICAL EVIDENCE. OTHER UTILITIES MAY BE PRESENT. ACTUAL LOCATIONS AND ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR.

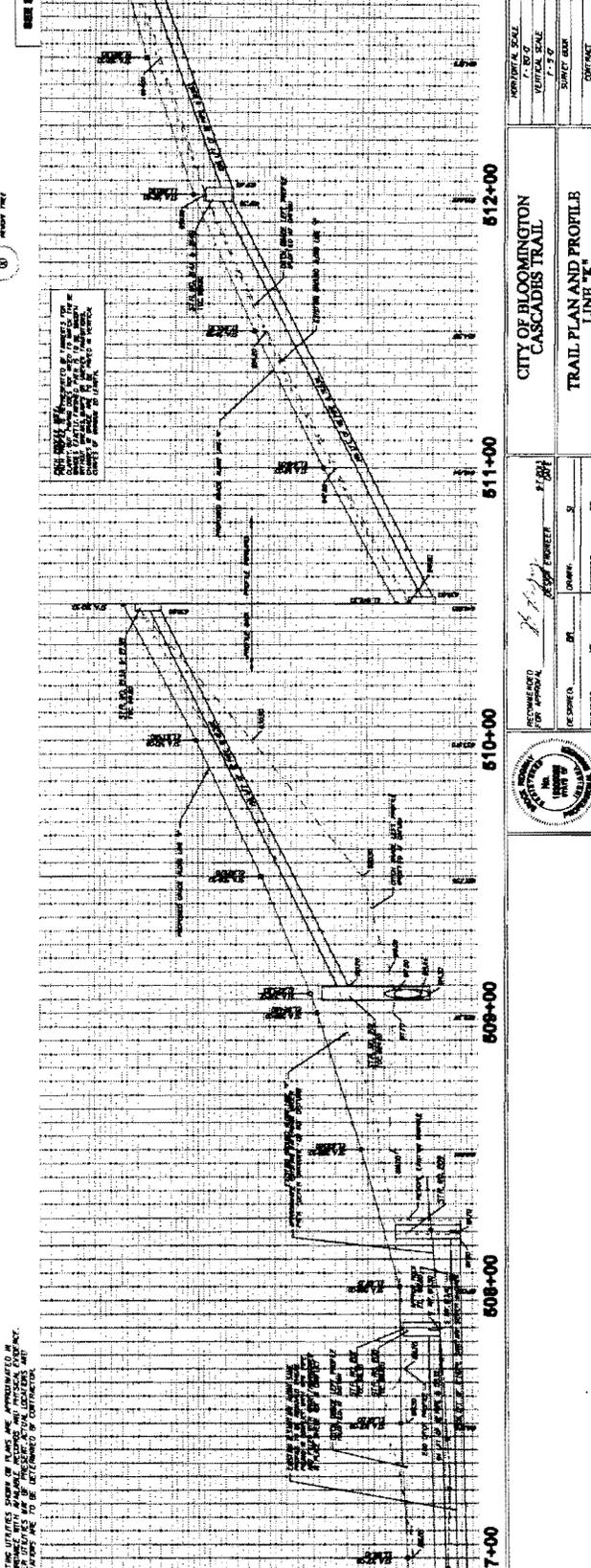


	PREPARED FOR APPROVAL: <i>[Signature]</i> DATE: <i>[Date]</i>	<b>CITY OF BLOOMINGTON</b> <b>CASCADES TRAIL</b>  <b>TRAIL PLAN AND PROFILE</b> <b>LINE "K"</b>	HORIZONTAL SCALE 1" = 40'-0"	GNDS. FILE N/A
	DESIGNED BY: <i>[Initials]</i>		VERTICAL SCALE 1" = 5'-0"	DESCRIPTION 
	CHECKED BY: <i>[Initials]</i>		SURVEY BOOK 	SHEETS 1 OF 1
	DATE: <i>[Date]</i>		CONTRACT 	PROJECT CASCADES TRAIL





807+00 808+00 809+00 810+00 811+00 812+00



810	815	820	825	830	835	840	845	850	855	860	865	870
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SEE SHEET 2 FOR LEGEND

CITY OF BLOOMINGTON  
CASCADIS TRAIL  
TRAIL PLAN AND PROFILE  
LINE "A"

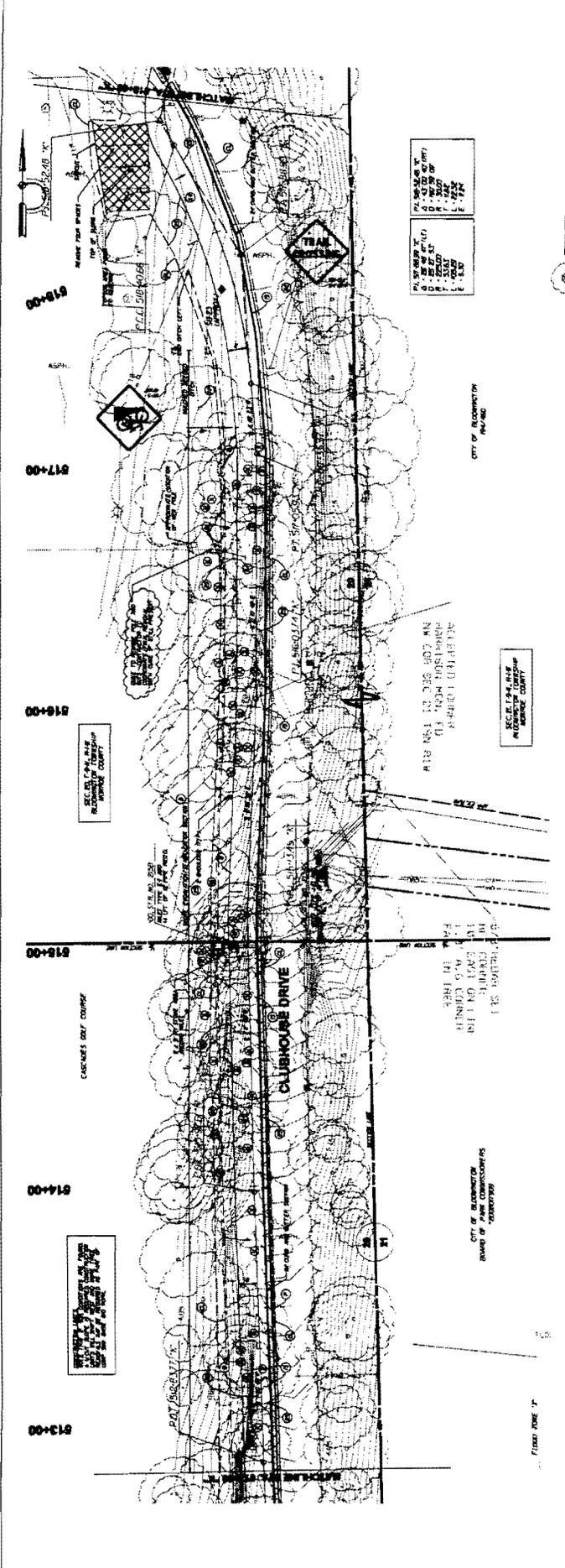
RECOMMENDED FOR APPROVAL: [Signature] DATE: 1/28/21  
 ENGINEER: [Signature] DATE: 1/28/21  
 CHECKED: [Signature] DATE: 1/28/21  
 DESIGNED: [Signature] DATE: 1/28/21  
 DRAWN: [Signature] DATE: 1/28/21  
 PROJECT: [Signature] DATE: 1/28/21  
 CONTRACT: [Signature] DATE: 1/28/21

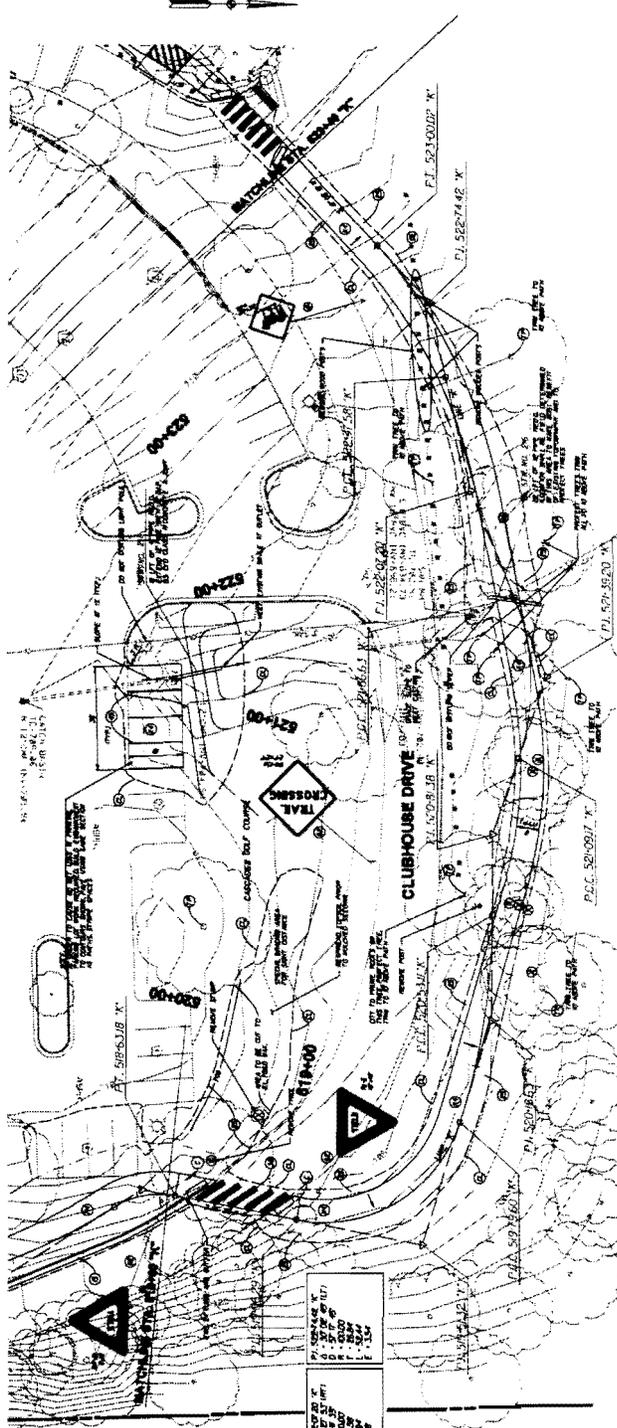
SCALE: HORIZONTAL SCALE 1" = 40' VERTICAL SCALE 1" = 10'

DATE: 1/28/21

PROJECT: CASCADIS TRAIL

ENGINEER'S NOTE: ALL ELEVATIONS ARE APPROXIMATED IN OTHER AREAS AND ARE NOT TO BE USED FOR CONSTRUCTION. ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR.





PI 518+00.00 X 1. 518+00.00 2. 518+00.00 3. 518+00.00 4. 518+00.00 5. 518+00.00 6. 518+00.00 7. 518+00.00 8. 518+00.00 9. 518+00.00 10. 518+00.00	PI 519+00.00 X 1. 519+00.00 2. 519+00.00 3. 519+00.00 4. 519+00.00 5. 519+00.00 6. 519+00.00 7. 519+00.00 8. 519+00.00 9. 519+00.00 10. 519+00.00	PI 520+00.00 X 1. 520+00.00 2. 520+00.00 3. 520+00.00 4. 520+00.00 5. 520+00.00 6. 520+00.00 7. 520+00.00 8. 520+00.00 9. 520+00.00 10. 520+00.00	PI 521+00.00 X 1. 521+00.00 2. 521+00.00 3. 521+00.00 4. 521+00.00 5. 521+00.00 6. 521+00.00 7. 521+00.00 8. 521+00.00 9. 521+00.00 10. 521+00.00	PI 522+00.00 X 1. 522+00.00 2. 522+00.00 3. 522+00.00 4. 522+00.00 5. 522+00.00 6. 522+00.00 7. 522+00.00 8. 522+00.00 9. 522+00.00 10. 522+00.00	PI 523+00.00 X 1. 523+00.00 2. 523+00.00 3. 523+00.00 4. 523+00.00 5. 523+00.00 6. 523+00.00 7. 523+00.00 8. 523+00.00 9. 523+00.00 10. 523+00.00	PI 524+00.00 X 1. 524+00.00 2. 524+00.00 3. 524+00.00 4. 524+00.00 5. 524+00.00 6. 524+00.00 7. 524+00.00 8. 524+00.00 9. 524+00.00 10. 524+00.00
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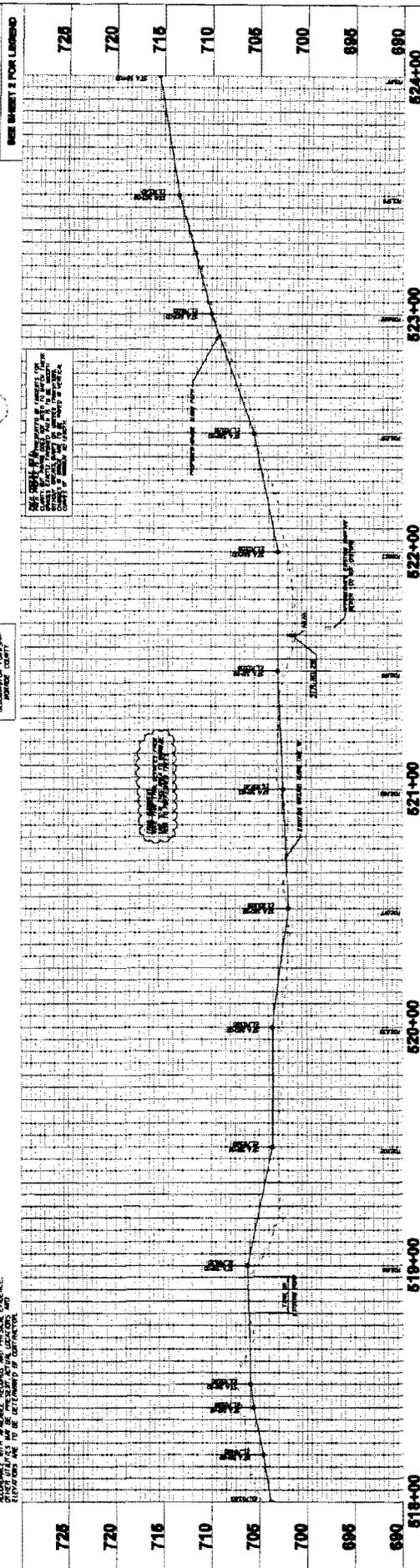
SEE PLAN FOR  
AUTOMATIC TIE-INS  
SOURCE: COUNTY

EXISTING UTILITIES SHOWN ON PLANS ARE APPROXIMATED BY  
CONDUCTING SURVEY. THE LOCATION OF ALL UTILITIES  
SHOWN ON THESE PLANS IS FOR INFORMATION ONLY. THE  
CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE  
LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION.

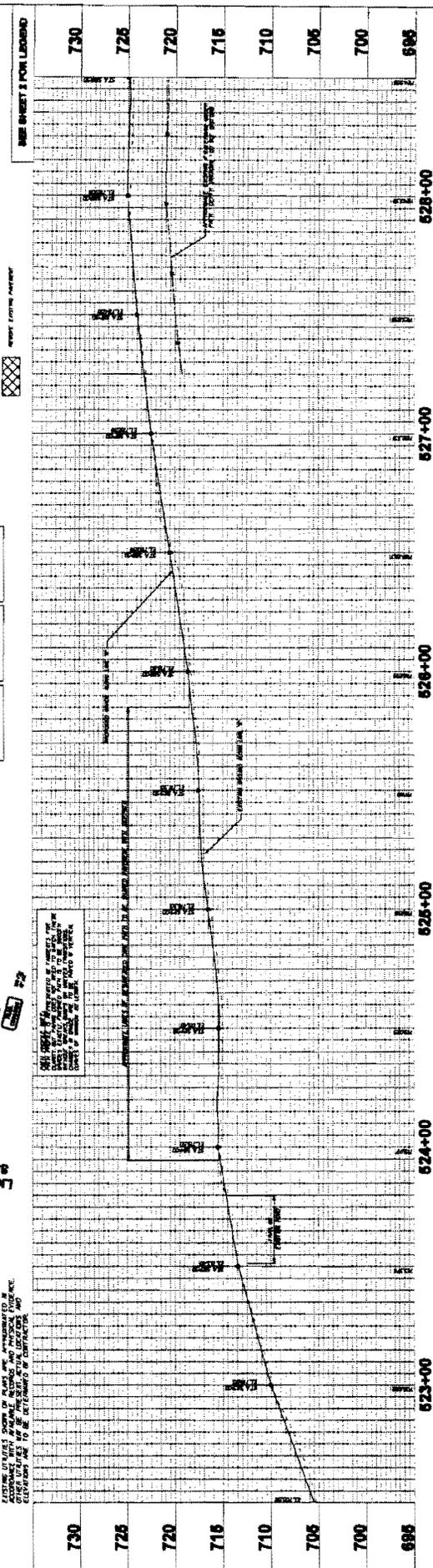
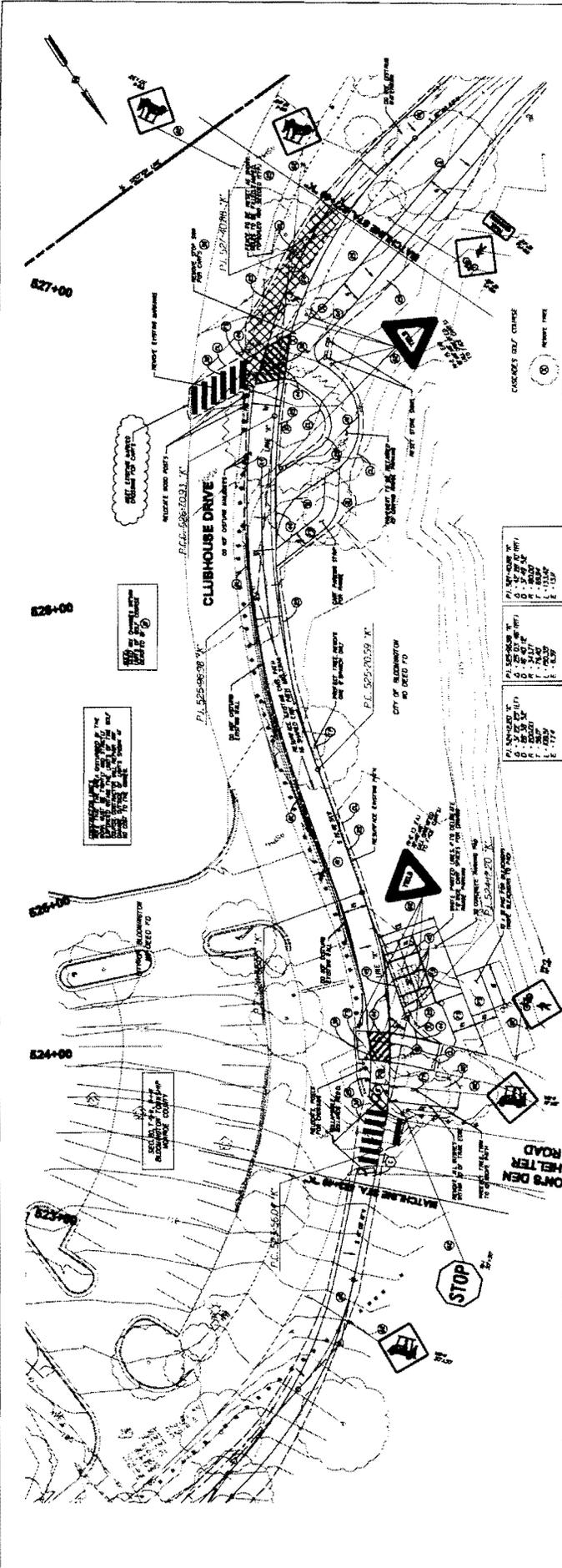
SECTION 1.1.1.1.1.1  
SOURCE: COUNTY

SECTION 1.1.1.1.1.1  
SOURCE: COUNTY

SECTION 1.1.1.1.1.1  
SOURCE: COUNTY



725	720	715	710	705	700	695	690	518+00	519+00	520+00	521+00	522+00	523+00	524+00
CITY OF BLOOMINGTON CASCADES TRAIL TRAIL, PLAN AND PROFILE LINE "K"														
RECOMMENDED FOR APPROVAL: <i>[Signature]</i> DATE: 10/15/2014 PROJECT NO: 14-001														
DESIGNED BY: <i>[Signature]</i> CHECKED BY: <i>[Signature]</i> DRAWN BY: <i>[Signature]</i> DATE: 10/15/2014														
PROJECT NO: 14-001 SHEET NO: 1 OF 1 TOTAL SHEETS: 1														
CONTRACT: <i>[Signature]</i> CONTACT: <i>[Signature]</i> PROJECT: <i>[Signature]</i> DRAWING TITLE: CASCADES TRAIL														



730	527+00	696
726	527+00	700
720	527+00	706
716	527+00	710
710	527+00	716
706	527+00	720
700	527+00	726
696	527+00	730

NORTH TO SCALE  
 PLAN TO SCALE  
 VERTICAL SCALE  
 HORIZONTAL SCALE  
 SHEET NO. 1 OF 1  
 PROJECT NO. 100-100-100  
 CONTRACT NO. 100-100-100  
 DATE: 10/1/2010

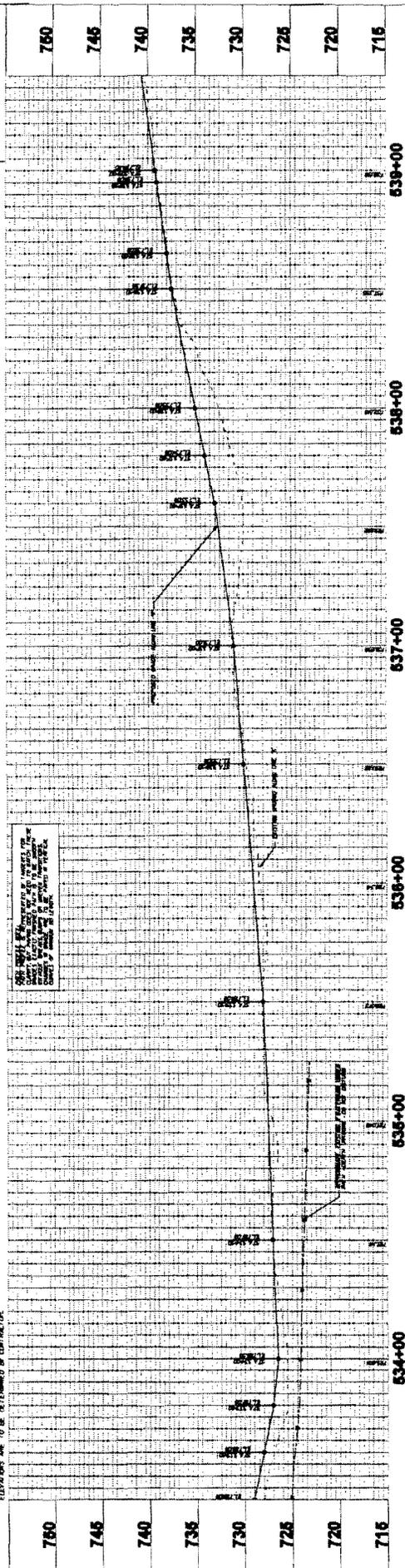
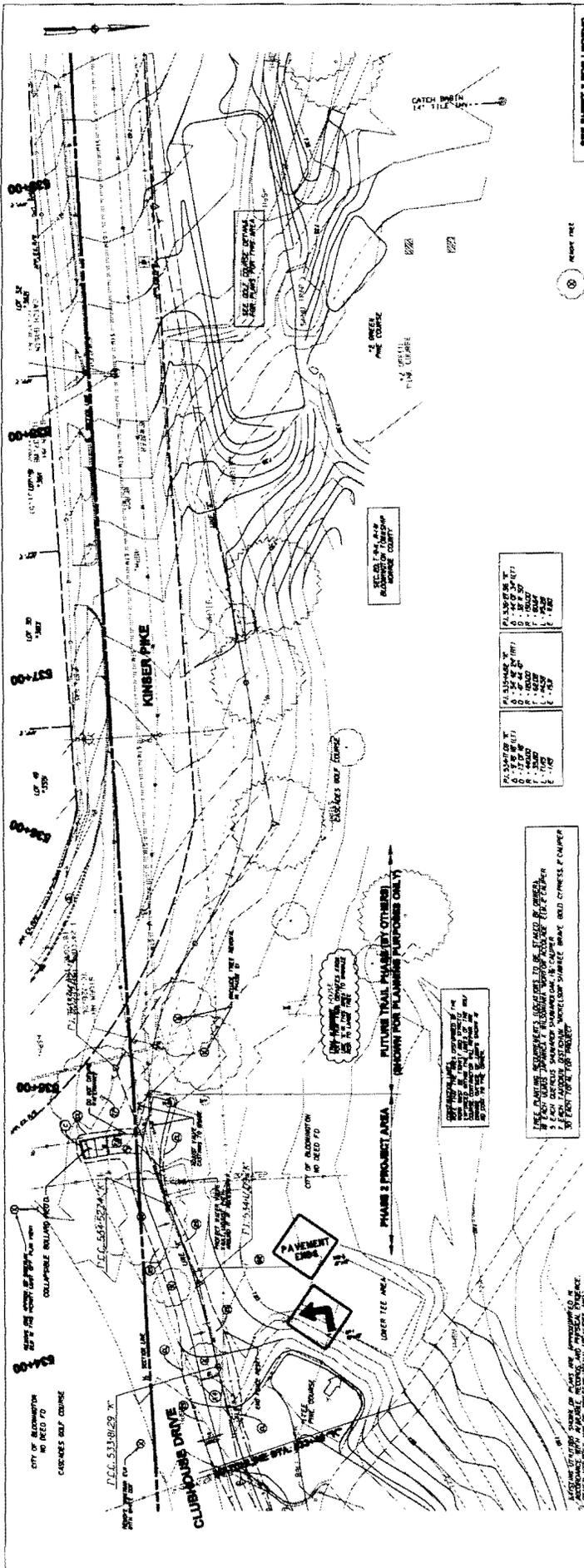
CITY OF BLOOMINGTON  
 CASCADES TRAIL  
 TRAIL PLAN AND PROFILE  
 LINE "K"

REVIEWED FOR APPROVAL: [Signature]  
 DESIGN ENGINEER: [Signature]  
 CHECKED: [Signature]  
 DATE: 10/1/2010



ALL NOTES ON THIS DRAWING ARE SUBJECT TO THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2009 EDITION, AS AMENDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2010 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2011 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2012 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2013 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2014 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2015 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2016 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2017 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2018 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2019 EDITION, AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, 2020 EDITION.





534+00	536+00	537+00	538+00	539+00
<b>CITY OF BLOOMINGTON</b> <b>CASCADES TRAIL</b> <b>TRAIL PLAN AND PROFILE</b> <b>LINE "K"</b>				
DESIGNED BY: <i>[Signature]</i> CHECKED BY: <i>[Signature]</i>		DRAWN BY: <i>[Signature]</i> CHECKED BY: <i>[Signature]</i>		
PROJECT NO.: 533-00 SHEETS: 10 CONTRACT: 533-00		SHEETS: 10 CONTRACT: 533-00		
HORIZONTAL SCALE: 1" = 20' VERTICAL SCALE: 1" = 5'		HORIZONTAL SCALE: 1" = 20' VERTICAL SCALE: 1" = 5'		



ALL DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.  
 DIMENSIONS SHALL BE AS SHOWN UNLESS OTHERWISE NOTED.  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.  
 THE CITY OF BLOOMINGTON IS NOT RESPONSIBLE FOR THE ACCURACY OF THE DATA PROVIDED.  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE DATA AND FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.