

CITY OF BLOOMINGTON



**MAY 11, 2015 @ 5:30 p.m.
COUNCIL CHAMBERS #115
CITY HALL**

**CITY OF BLOOMINGTON
PLAN COMMISSION
May 11, 2015 @ 5:30 p.m.**

❖ **City Hall Council Chambers - Room #115**

ROLL CALL

MINUTES TO BE APPROVED: April 2015

REPORTS, RESOLUTIONS AND COMMUNICATIONS:

PETITIONS:

SP-2-15 **Monroe County Commissioners**
312 N Morton St
Site plan to allow construction of an 8-story parking garage.
Case Manager: Patrick Shay

**BLOOMINGTON PLAN COMMISSION
THIRD HEARING STAFF REPORT**
Location: 312 N. Morton Street

**CASE #: SP-2-15
DATE: May 11, 2015**

PETITIONERS: Monroe County Commissioners
100 W. Kirkwood Ave, Bloomington

CONSULTANT: Bynum Fanyo & Associates, Inc
528 N. Walnut St, Bloomington

REQUEST: The petitioners are requesting site plan approval to allow construction of a 7-story parking garage in the Commercial Downtown (CD) Zoning District.

Area:	Approximately 0.4 Acres								
Zoning:	Commercial Downtown (CD)								
Downtown Overlay:	Downtown Core Overlay (DCO)								
GPP Designation:	Downtown								
Existing Land Use:	Parking lot								
Proposed Land Use:	Parking Garage and Emergency Evacuation Site								
Surrounding Uses:	<table> <tr> <td>East</td> <td>- Government (Monroe County Justice Building)</td> </tr> <tr> <td>South</td> <td>- Office (Curry/Fiscus), Mixed-Use</td> </tr> <tr> <td>West</td> <td>- Government, Office (Showers Complex)</td> </tr> <tr> <td>North</td> <td>- Mixed-Use (Smallwood, Smallwood Annex)</td> </tr> </table>	East	- Government (Monroe County Justice Building)	South	- Office (Curry/Fiscus), Mixed-Use	West	- Government, Office (Showers Complex)	North	- Mixed-Use (Smallwood, Smallwood Annex)
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South	- Office (Curry/Fiscus), Mixed-Use								
West	- Government, Office (Showers Complex)								
North	- Mixed-Use (Smallwood, Smallwood Annex)								

PROPERTY SUMMARY: The petitioners own a surface parking lot located at the southeast corner of W. 8th Street and N. Morton Street. This property is zoned Commercial Downtown (CD) and is further located within the Downtown Core Overlay (DCO). The property is currently fenced and used to park a variety of Monroe County vehicles and is separated from additional properties owned by the petitioners by two adjacent public alleys.

PROJECT SUMMARY: The petitioners wish to replace the surface parking lot with a new 7-story building that would principally house a parking garage for approximately 240 parking spaces. The building would also function as an emergency evacuation center for the adjacent Correctional Center.

SECOND HEARING SUMMARY: The Plan Commission voted at its March 9 meeting to continue this petition to a third hearing. Several concerns were cited regarding the proposed height of the building. Some of the Plan Commission encouraged the petitioners to continue to work with the City to seek alternative parking arrangements that would allow the proposed garage to be further reduced in height.

SUMMARY OF REVISIONS TO PETITION: Between the first and second hearings for this petition, several revisions were made to the proposed parking facility. Additionally, the conditional use approval for a work-release center was withdrawn. The petitioners have further revised their petition to address some of the comments made at the second hearing.

The following changes have been made to the petitioners' proposal:

- The entire 8th floor has been removed, effectively lowering the overall height of the building by approximately 10 feet.
- The acid-etched precast concrete base has been wrapped and is now included on the south elevation.
- The exposed concrete of the parking decks on the south façade is now brick and includes additional vertical supports creating a more “punched opening” look.
- The southeast stair tower has changed materials from brick to the acid-etched precast concrete to match the northwest stair tower and create more visual contrast to the brick that has been added to the south façade.
- The north elevation along 8th Street has been revised to cover the exposed concrete parking decks with brick and add vertical supports.
- Additional architectural detail has been added between several sections of the acid-etched precast concrete and the brick to create articulation between the materials so the facade doesn't have as much of a flat planed look through the height of the building.
- The first level floor plan has been revised to better reflect the removal of the work release function from the petition.

Plan Commission Site Plan Review: Four aspects of the proposal require the Plan Commission to hear this petition for site plan approval. These aspects are:

- The project includes more than 25,000 square feet of non-residential space
 - The proposal is adjacent to a residential use (Smallwood Plaza and Annex)
 - The proposal includes a Parking Garage as a primary structure
 - The petitioners are requesting waivers to the standards in BMC 20.03.120 & 130.
- The following waivers are being requested:

- Building height
- Building setback
- Ground floor non-residential use
- Building cap requirements
- Void-to-solid requirements
- Window design
- Building modulation
- Building step back

SITE PLAN REVIEW: DEVELOPMENT STANDARDS

Height: After the submitted revisions, the official height of the proposed building as measured by the UDO, would be approximately 84 feet (measured from the elevation at the intersection of 8th Street and Morton Street to the top of the southeast stair tower. The main wall along Morton Street has been reduced from 80 feet to approximately 70 feet in height. The perceived height along the 8th Street frontage (as it relates to the adjacent

topographic grade) ranges from approximately 68 feet (east) to 80 feet at the corner stair tower. The DCO allows for a maximum of 50 feet without an associated waiver. As mentioned at the first hearing, several surrounding buildings exceed 50 feet in height. The immediate height context includes:

1. Smallwood – 97 feet (western façade); 80 feet on College Avenue
2. Morton 400 (the Smallwood Annex building) – Approx. 45 feet
3. Charlotte Zietlow Justice Center – 69 feet (western façade); 60' on College
4. Hilton Garden Inn – 70 feet
5. Morton Street Garage – 78 feet (84 feet with tower)
6. Curry Building – Approx. 30 feet
7. Fiscus Building – Approx. 20 feet
8. Mercury Building - Approx. Range from 60 to 78 feet

Since the UDO restriction of 50 feet was adopted in 2007, multiple waivers have been granted in this overlay. Two of these were hotel petitions. Waivers granted in close proximity to this site include:

1. Springhill Suites Hotel – 71 feet (5-6 stories)
2. Hyatt Place Hotel – 74 feet (7 stories)
3. 10 North – 68 feet (5 stories)
4. 304 W. Kirkwood (Elmore-Orego petition) – 62 feet (4 stories recessed)
5. Studio 531 (Elliot Lewis petition) – 54 feet (4 stories recessed)

Height Waiver - 20.03.120(b): The City's *Downtown Vision and Infill Strategy Plan*, which must be evaluated to justify any waiver request, provides the following guidance in regards to building height.

“Building Heights vary substantially in Downtown Bloomington and yet there is a strong sense of similarity in scale. This is in part because most buildings are within two to four stories in height. In addition, most buildings have features at the lower levels that are similar in scale. ...A variety of building heights in new construction is, therefore, appropriate. However, the dominant scale of two to four stories should be maintained. This may be accomplished by literally constructing a building within this traditional height range; in other cases, design elements that reflect traditional height may be incorporated into larger structures. Setting upper floors back from the building front also may be considered.

Staff Commentary: The proposal submitted for Plan Commission approval is in conflict with this policy guidance regarding preferred downtown heights for new construction. As a result, the Commission should consider the following issues in making this finding: 1) the unique height need in constructing a parking garage, 2) the combined challenge faced by the petitioners in adequately addressing employee parking needs with a property that is less than an ideal size for a parking garage, 3) taller building context in this area as compared to other areas of downtown, and 4) the overriding importance of the petitioners being able to control their long-term

parking needs and remain viable as a key civic employer downtown.

In previous discussions with the petitioner, staff has noted that its preferred height of the building in reference to the guidance of the Downtown Plan would be no more than 7 stories. This has now been accomplished due to the removal an additional building story. Staff had made an offer to the petitioner to enter into negotiations with the City for leased parking in the 7th and Morton and 7th and Walnut Garages. There are approximately 100 spaces available to be leased to the County. These discussions are ongoing, and have resulted in the petitioners removing the 8th floor from the garage proposal. As a result, while staff cannot find strict compliance with the Downtown Plan, the City Administration supports the height of the building proposal due to the mitigating factors noted in this section.

Bicycle Parking: The petitioners are required to place a minimum of four covered bicycle parking spaces. The petitioners have proposed to place 20 secured bicycle parking spaces within the first level of the parking garage.

Setbacks: The proposed building is located at the build-to line along Morton Street. Along 8th Street, the proposed building is set back approximately 2 feet from the 8th St. right-of-way line to allow for a full sidewalk with tree grates. The proposed structure meets all other setback standards.

Front Setback Waiver (8th St.) – 20.03.120 (d)(1): Staff is supportive of this waiver. This waiver would allow the petitioners to achieve a desired design element by allowing adequate width to install street trees grates along 8th St. The proposed structure will still have the feeling of a structure built at the property line since the sidewalk will extend to the structure. An easement must be placed on the portion of the sidewalk that is outside of the right-of-way or it must be dedicated as right-of-way.

Ground Floor Non-Residential Uses: This project is in an area that requires non-residential use along both the 8th and Morton Street frontages. In addition, a minimum 50% of the entire ground floor level must be used for non-residential uses. For the purposes of this requirement, structured parking is not considered as a non-residential use within the CD district.

One of the main intents of this requirement is to create a more vibrant streetscape that creates pedestrian interest at the street level. Although the proposal does not meet this requirement, the petitioners have continued to revise their proposal to relocate the vehicular entry from Morton Street to 8th St. and to increase the openings and pedestrian interest of the streetscape with particular attention to the Morton St. façade. As mentioned, the petitioners have replaced the spandrel glass along the entirety of Morton St. with clear glass that opens to an internal corridor that will now allow light to flow from the internal space to the external. This also creates an opportunity to utilize this space in a gallery fashion for revolving art displays. A waiver is required for the absence of display windows along the 8th Street frontage and a non-residential floor area of approximately 45% for the

first floor. The need for this waiver is driven by the petitioners' desire to maximize the amount of parking on the first level as well as the security requirements of the emergency evacuation center use.

Ground Floor Non-residential Waiver – 20.03.120(e): Due to the large amount of structured parking associated with this project, it does not meet the ground floor non-residential requirements. With the submitted changes, this waiver is supported. Although the Downtown Plan would guide the petitioners to include more active first level space, the size of the property coupled with the need for parking makes retail space impossible on the first floor. The desired emergency evacuation use also creates difficulty in using this space as office because of security orientation needs. The petitioners have worked with staff to find an alternative treatment to the Morton St. streetscape.

ARCHITECTURAL STANDARDS: The petitioners are proposing a 7-story structure in a conventional parking garage form. The garage will be accessed from 8th Street and be clad primarily in precast concrete, brick, metal, and glass. Several areas of exposed concrete decks have been revised to include brick and additional vertical supports. These changes have created more of a punched opening appearance. Revised architectural elevations and a color model of the proposed structure have been included in your packet.

Building Frontage, Alignment, Orientation and Entrances: The structure is proposed to be constructed with a building forward design that fills the corner of 8th Street and Morton Street. The petitioners have designed the proposed garage with the required two primary pedestrian entries. These entries (within 3 feet of the adjacent sidewalk elevation) are both located near the intersection of 8th Street and Morton Street and enter into a combined entry vestibule. Although this corner entry will be heavily used, the petitioners have also shown a stairwell and entry at the southeast corner of the structure that will provide access to floors 2-7 of the garage.

Streetscape: The petitioners are proposing to reconstruct both of the adjacent street frontages. The proposed plan would retain the existing on-street parking while adding protective bump-outs that will narrow the distances at sidewalk crossings. The reconstruction will also replace the existing tree plots and sidewalks with new integrated sidewalks and 5' X 5' tree grates. This will likely require the relocation or reinstallation of several parking meters and street lights.

Additional decorative street lights will also be added near the alley intersections with both 8th Street and Morton Street. Staff is also exploring the potential for a formalized pedestrian crossing from the 8th Street intersection across Morton Street to an existing pedestrian ramp.

Roof/Building Cap: Due to the style of building chosen, the building does not have a pronounced cap. There is a small metal cap shown.

Building Cap Waiver – 20.03.130(b)(5)(A): This waiver is supported. Although this

standard could be met, the waiver is requested to allow for flexibility in design. The stair towers are the main aesthetic focus and have incorporated caps. The addition of a continuous cap would not enhance the aesthetic of the structure.

Void-to-Solid Percentage – The DCO gives guidance for the first level of a building to have a minimum of 60% void to solid ratio and upper floors to have a minimum of 20% void-to-solid. The petitioners meet the 20% upper level requirement as well as the required void along Morton Street. However, due to the lack of transparent windows along 8th Street as previously mentioned, the project has fallen short of the required void along 8th Street. Staff finds a preference to the Morton Street frontage to have more void openings.

Void-to-Solid Waiver – 20.03.130(b)(2): As a parking garage, there is ample void in the upper levels of the structure. Per Plan Commission guidance, the petitioners increased the amount of solid along Morton St. to better mimic traditional windows and minimize the aesthetic impact of the parking decks. With the petitioners' revisions to the Morton St. façade, the main issue remaining is the lack of void along 8th St. The wall along 8th Street buffers the street from both interior parking and secured space. With the lack of pedestrian traffic along 8th St. and the desired use of the property, the use of spandrel glass on 8th St. only is found to be supportable.

Windows: The DCO requires that windows have a 1:1.5 ratio of width to height and have distinctive sills and lintels. Due to the nature of the proposed use for the building as a garage, there are relatively few windows in lieu of large openings at each garage level. The petitioners are proposing glass curtain walls at the stair towers. These curtain walls may be appropriate, but do require waivers from the design requirements for ratio and detailing.

Window Design Waiver – 20.03.130(b)(3): The main windows that fall under this requirement are those found in the stairwells. The larger curtain wall design were revised prior to the second hearing to reduce the amount of glass on the stairwells, but these windows still have a more modern design that do not utilize sills and lintels. In this case, the curtain wall approach does improve the aesthetic quality of the building, and the City has also used this design in its parking garage structures.

Materials: Prior to the second hearing, the petitioners revised their architecture to include more variety of materials and colors. This included utilizing additional acid-etched precast concrete panels to create a more defined base as well as breaking the Morton St. façade up to create additional visual interest and reduce the massing of a single material/color. The proposal still includes a partial green wall feature on the western facade facing Morton Street. The elevations have been further revised to removed the exposed poured concrete parking decks on the north and south facades. The petitioners are not proposing any non-permitted materials, and they have greatly reduced the amount of exposed concrete from the initial submittal.

Entrance Detailing: This site requires primary pedestrian detailing for both of the entries on 8th Street and Morton Street. The petitioners have committed to including the required signage, address, and lighting to primary entries. Furthermore, the petitioners have revised

the entry design to include the requirement for a recessed entry of 4 feet.

MASS, SCALE, AND FORM:

Building Façade Modulation: The UDO requires building offsets of a minimum 3% of the total façade width on all the Morton Street and 8th Street facades. Based upon the 132 feet of frontage along both adjacent streets, the proposed building is required to have a minimum modulation of approximately 4 feet. No modulation has been proposed with this petition.

Building Height Step Back: The DCO guides structures over 45 feet in height to step back upper levels 15 feet from the lower levels along street frontages. The proposed structure is approximately 70 feet in height along Morton Street and reduces in height as it goes east due to the grade changes on the site. Step backs of 15 feet have not been incorporated into either of these taller building facades.

Façade Modulation Waiver and Building Height Step Back Waivers – 20.03.130(c)(1&3): Support of these waivers is found in the use of this building. Due to the small size of the lot, a traditional wrap is not possible. The small lot size also does not allow for significant articulation as it greatly impacts the efficiency and practicality of the parking design. Parking garages usually have consistent parking layouts on each floor to maximize efficiency and provide adequate parking stall and aisle widths. Modulation and step-backs would make a parking garage impractical at this location.

Additional Downtown Plan Guidance:

The Downtown Plan offers the following specific guidance for parking structures being proposed in the downtown:

- *“Parking Structures should be designed to enhance pedestrian activity at street level. At a minimum, a parking structure should help to animated the street, contribute to the integrity if the streetscape and be compatible with the surrounding built environment. “*
- *“Design a parking structure that contributes to the desired character of the community, which results in a visibly attractive and active street edge.”*
- *“When feasible, a parking structure should be wrapped with retail, commercial or other uses that generate pedestrian activity along the street edge. The building should screen internally parked and moving vehicles from the street by locating access ramps to the rear of the building, preferably along an alley and by using façade treatments that mitigate rows of parked cars.”*
- *“Mitigation elements typically include, but are not limited to:*
 - *Retail/commercial “wrap” or leasable space that camouflages the parking structure*

- *Murals or public art*
- *Landscape elements such as window boxes and street trees*
- *Product display cases and windows”*

Although staff still finds that the proposed structure does not achieve a level of retail/commercial wrap, pedestrian activity, height, or upper story step backs intended by the Downtown Plan, staff acknowledges that the petitioners are restricted by need to achieve a reasonable number of parking spaces on a property of less than ideal size for a structured parking facility. In addition, the desired use of the lower level for an emergency evacuation center for the adjacent correctional center further complicates achieving all of the stated goals of the Downtown Plan. Since the second hearing, the petitioners have continued to work with the City to bring their proposal more into alignment with these specific goals with the revisions as described in this report. This proposal is being brought forward to utilize a small property that is already under the petitioners’ ownership and is one of the few sites that can accommodate the parking needs of County employees both in terms of location and number.

ENVIRONMENTAL COMMISSION RECOMMENDATIONS: The Bloomington Environmental Commission (EC) has made 2 recommendations concerning this development.

1. The Petitioner should plant street trees every twenty (20) feet along Morton and 8th Streets.

Staff response: Although desirable, the petitioners’ proposal for street trees meets the UDO requirements.

2. The Petitioner should provide design details for the green wall prior to any approvals.

Staff response: Staff has addressed this recommendation as a proposed condition of approval.

CONCLUSION: Although staff cannot find strict compliance with some elements of the Downtown Plan, the City Administration supports the proposal for the reasons outlined in this report. Essentially, staff believes the Commission must determine whether the building is of an acceptable height. Additionally, the Commission must determine whether the building architecture has been sufficiently modified to be consistent with the Downtown Vision and Infill Strategy Plan.

RECOMMENDATION: Staff recommends approval of SP-2-15 with all the associated waivers and the following conditions of approval:

1. The petitioners must work with staff to find an appropriate species for the green wall feature.
2. Any portions of the proposed public sidewalk along W. 8th Street, but outside of the

right-of-way must be placed within a pedestrian easement or dedicated as right-of-way prior to issuance of final occupancy.

3. The primary pedestrian entrance along Morton Street must include the required lighting, signage and address.
4. A parabolic mirror or similar measure for drivers to clearly see pedestrians must be evaluated and incorporated as needed into the vehicle entry along W. 8th St.
5. The proposed building must be constructed with materials and finishings as outlined in the petitioner's statement, renderings, and elevations.
6. The petitioner must work with City staff to develop an art concept to be used for the first floor interior space facing Morton Street.

MEMORANDUM

Date: February 26, 2015

To: Bloomington Plan Commission

From: Bloomington Environmental Commission

Through: Linda Thompson, Senior Environmental Planner

Subject: SP-2-15 Monroe County Commissioners
312 N. Morton Street

This memorandum contains the Environmental Commission's (EC) input and recommendations regarding the request of a Site Plan and Variances for a 9-story parking garage. The 0.4 acre parcel is in the Downtown Commercial zoning district and the Downtown Core Overlay district. The site is currently used as a surface parking lot and an emergency evacuation site for the Monroe County Jail. Eleven parking spaces will have electric vehicle charging stations, and there will be solar photovoltaic panels on the top floor.

ISSUES OF SOUND ENVIRONMENTAL DESIGN:

1.) STREET TREES:

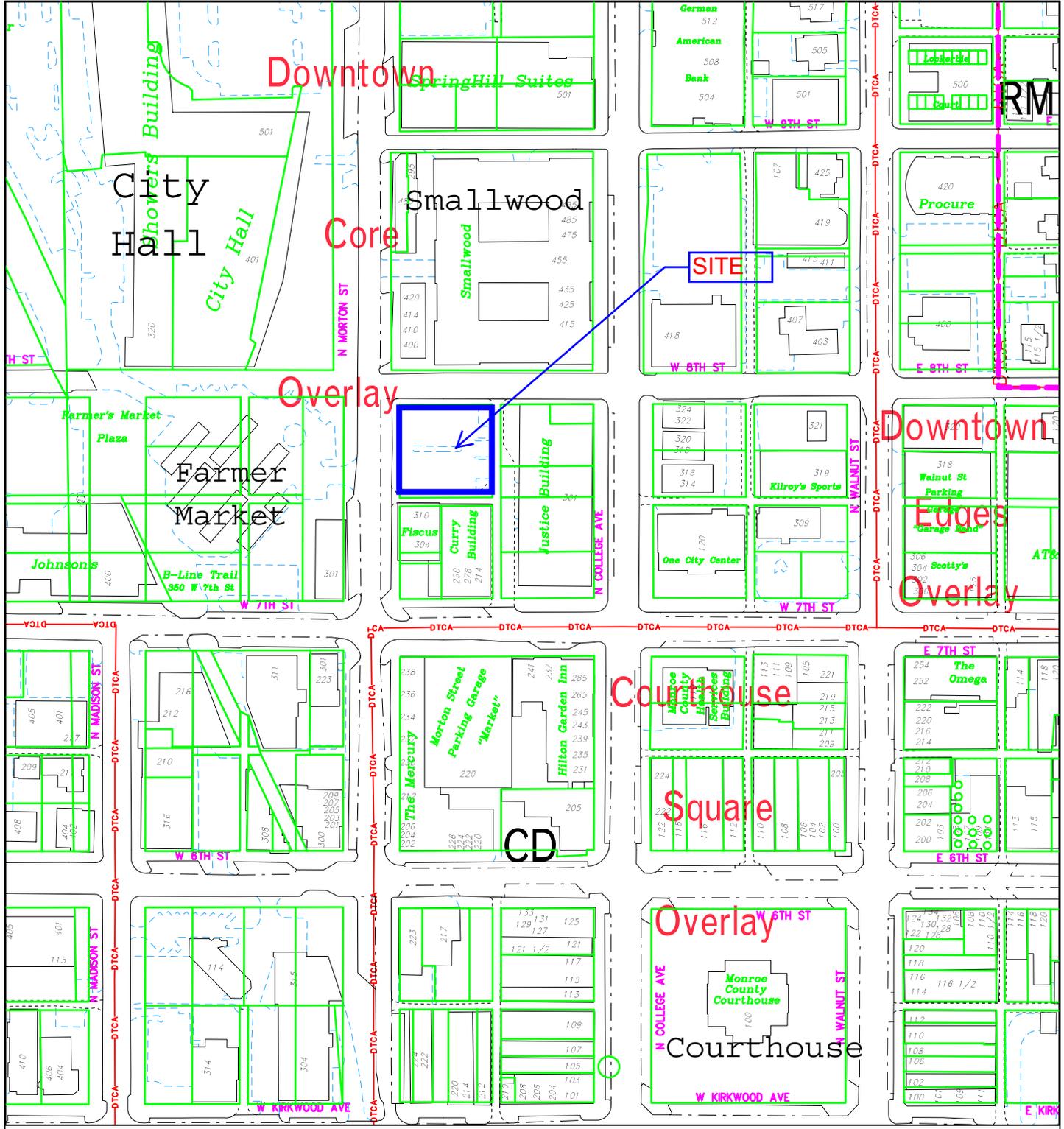
Because of the use of this building, it may be assumed difficult to install the kind of pedestrian-friendly streetscape that the city embraces. This is a high-density pedestrian location across from Showers City Hall, the Farmers' Market, and other large festivals that merits a design that enhances human usability and environmental benefits. Therefore, the EC recommends that the Petitioner plant street trees more densely than the minimum amount (one tree every 40 feet), and recommends that the street trees be planted every twenty (20) feet (the closest they are allowed) along both 8th and Morton Streets. Not only will more trees visually screen the building, it will add a calming social benefit, and provide habitat for birds and pollinators.

2.) GREEN WALL:

The EC applauds the petitioner for planning a green, or vegetated wall. The EC would like to know more details about the design and the type of system planned to be used. Additionally, the EC would like to know what type of plants will be used on the green wall.

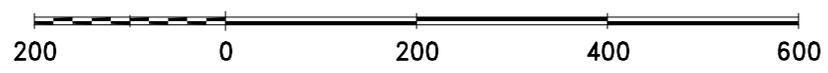
EC RECOMMENDATIONS:

- 1.) The Petitioner should plant street trees every twenty (20) feet along Morton and 8th Streets.
- 2.) The Petitioner should provide design details for the green wall prior to any approvals.



SP-2-15 Monroe County
Commissioners
Location/Zoning Map

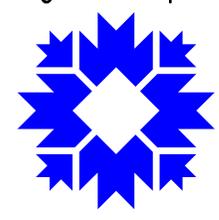
By: shapp
5 Feb 15



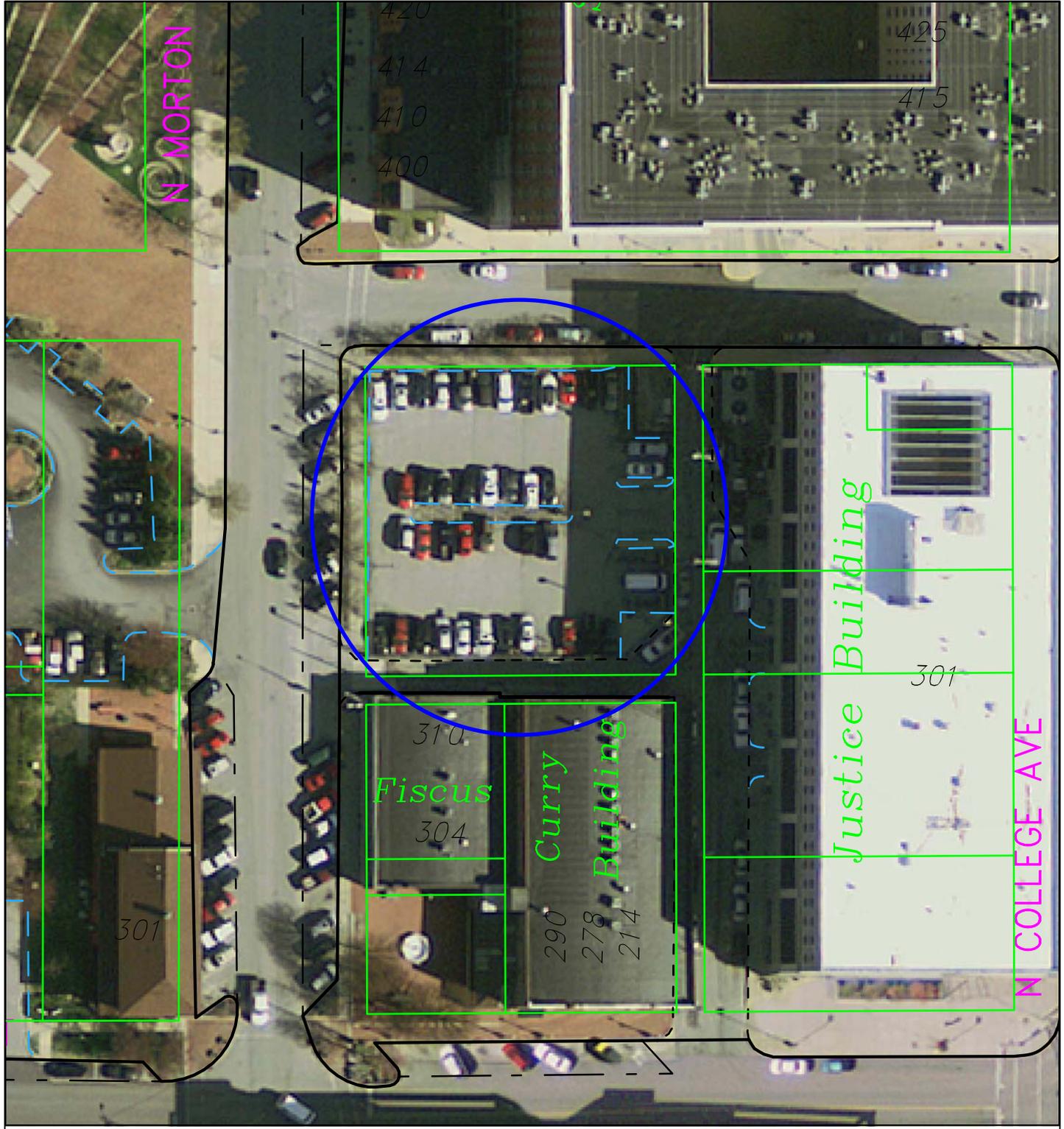
For reference only; map information NOT warranted.



City of Bloomington
Planning & Transportation

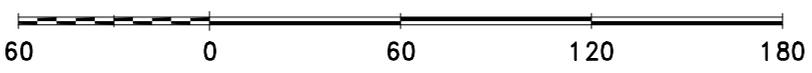


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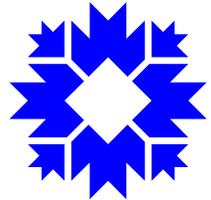


SP-2-15 Monroe County Commissioners
Aerial Photo

By: shayp
31 Dec 14



City of Bloomington
Planning & Transportation



Scale: 1" = 60'

For reference only; map information NOT warranted.

March 30, 2015

City of Bloomington Plan Commission

401 N. Morton Street

Bloomington, IN 47403

Re: Monroe County Parking Garage

Dear Plan Commission Members:

Our client, The Monroe County Commissioners, respectfully request site plan approval to construct a 7 level parking garage and emergency inmate shelter located at the southeast corner of the intersection of 8th and Morton Streets. The property is zoned CD downtown commercial and is in the Downtown Core Overlay Zone. The site is currently used as a surface parking lot and contains an emergency generator for the Justice Building located east of this property.

Our request has been continued twice. Since the previous plan commission meeting, the County Commissioners have been working closely with the City to develop a successful collaborative solution that we submit today and subsequently plan to present to you on April 13th. Please review our list of program and architectural revisions that have been made as a result of this collaboration between the County and the City.

Building Program Revisions:

In the previous two presentations, one of the main points of contention has been the height of the building. Honorably, the County has presented their need for downtown parking and the number of parking spaces required to the City and the Plan Commission. Those requirements are the reasoning behind the previously proposed heights of the building. On March 9th, a revised submission was presented to the Plan Commission that eliminated one level of the garage and reduced the average height of the building by ten feet. Although the revised submission was recommended to be continued, Plan Commission members were pleased with the building's aesthetics and encouraged the County to continue discussions with the City.

A revised design for a seven level garage with 240 total parking spaces is being submitted to you today. In addition, the first floor plan has been redesigned to be utilized as an emergency inmate shelter only. The County has abandoned future plans to convert the space into a work release program.

A summary of the proposed revisions are itemized below.

SP-2-15
Revised Petitioner's
Statement

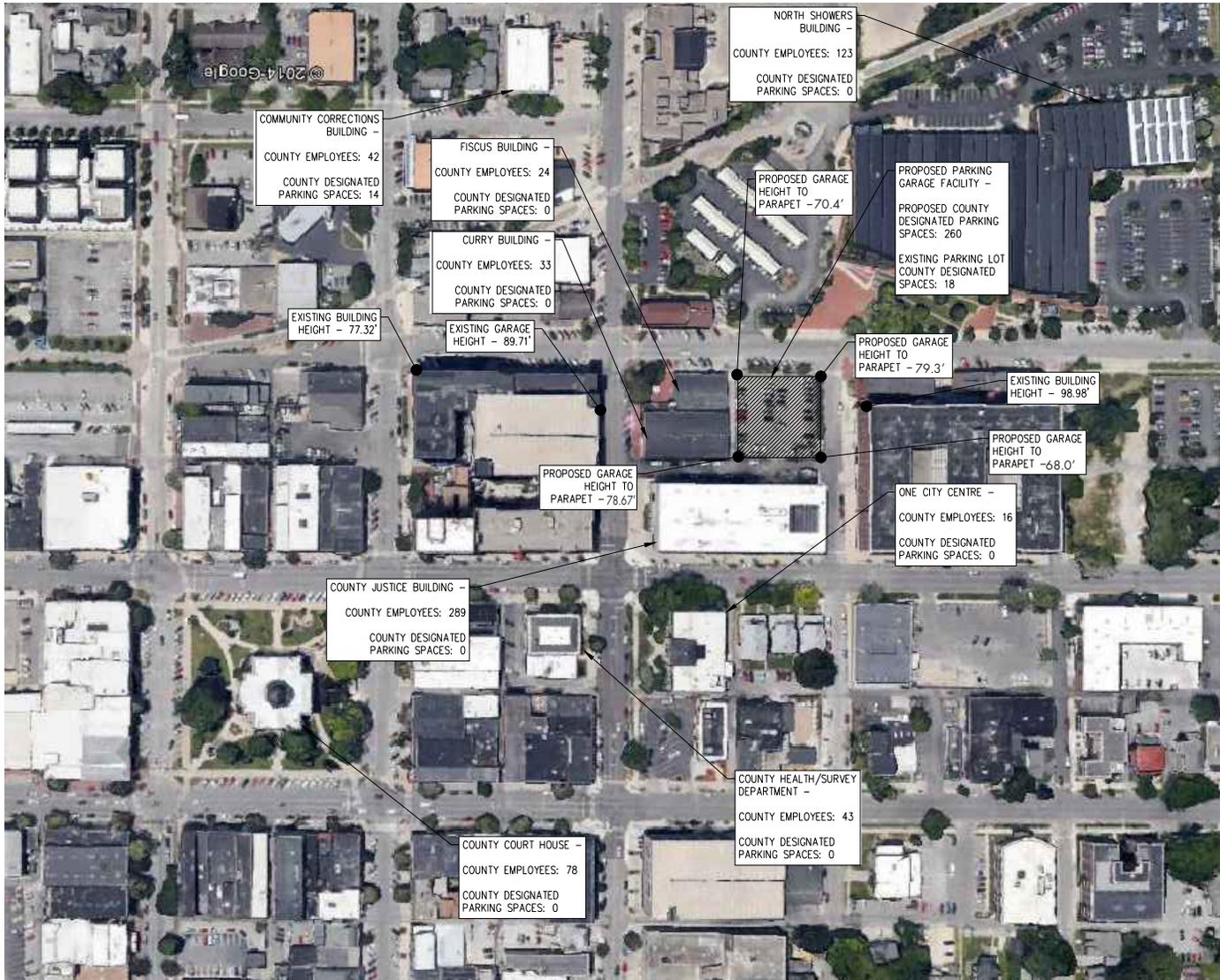
Architectural Revisions:

1. The emergency inmate shelter plan has been revised and designed to be utilized only as an emergency holding shelter. The County Commissioners have abandoned the idea that the space may be utilized in the future as a work release program. The layout of the shelter will be finalized with the Sheriff and Jail Commander following approvals.
2. Morton Street Façade (West Elevation):
 - The Morton Street's façade configuration of a two-story base, the use of two types of precast concrete panels, and a punched open vocabulary is relatively the same. A vertical live wall system continues to be proposed for the southwest corner of the building.
 - The height of the façade has been reduced from 80 feet to 70 feet. The northwest stair tower has been reduced from 90 feet to just over 79 feet.
 - Clear storefront and horizontal metal canopies are still being proposed on the first level.
 - A reduced amount of glazing and the acid etched precast are proposed for the façade of the shortened northwest stair tower.
3. Seventh Street Façade (South Elevation):
 - The height of the Seventh Street façade has been reduced from 80 feet to 70 feet and the stair tower has been reduced from 88 feet to 78 feet.
 - A two-story limestone-like base is continued down the alley and extends to the southeast stair tower. The most notable revision is the significant amount of brick veneer that has been added to the south elevation continuing the punched opening vocabulary of the Morton Street facade. In addition, the brick on the southeast stair tower has been replaced with the limestone-like precast material to contrast the added amount of brick and highlight the stair.
4. Eighth Street Façade (North Elevation):
 - The height of the Eighth Street façade has also been reduced. The new proposed height is 68 feet.
 - Similar to the Seventh Street façade, additional vertical and horizontal brick veneer material has been added to the Eighth Street façade enhancing the solid to void ratio and giving this façade a punched opening vocabulary.
 - The two-story limestone-like base and first level storefront has gone unchanged. Spandrel glazing is still being proposed to help screen cars within the garage.
5. East Elevation:

- A reduction in height for both the façade and the southeast stair tower is the most significant change to the East Elevation. Both have been reduced by approximately 10 feet.
- As noted above, the southeast stair tower is now clad with the limestone-like precast material and the ratio of glass to precast remains approximately the same.

Sustainable Features:

The same sustainable design features are still being proposed for the facility. The seventh level of the garage will include a solar PV panel array and structure that will supply a significant portion of the electricity consumed by the lighting in the open parking area of the facility. The garage will also provide 10 electric vehicle charging stations connected to the PV array. Surplus electricity generated by the panels, if any, will be metered back into the utility grid for consumption by other buildings. Energy efficient LED lighting fixtures will be utilized throughout the structure to minimize electricity consumption. An 7-story green screen trellis system will be attached to the masonry wall located along North Morton Street. Species of ivy will be planted in hanging planter boxes at each floor level and will provide a vertical element of vegetation that will be visually appealing from the streetscape. Secured bike parking has been provided on the first parking level near the 8th Street garage entrance. A total of 20 spaces have been provided.



MONROE COUNTY COMMISSIONER'S GARAGE | SURROUNDING BUILDING HEIGHTS

APRIL 2015





- ① CHARGING STATIONS
- ② BICYCLE RACKS
- ③ SECURITY GATE
- ④ EMERGENCY INMATE SHELTER - ENTRANCE
- ENTRY/ EXIT STAIRS
- PARKING GARAGE
- EMERGENCY INMATE SHELTER
- SOLAR PANEL ARRAY

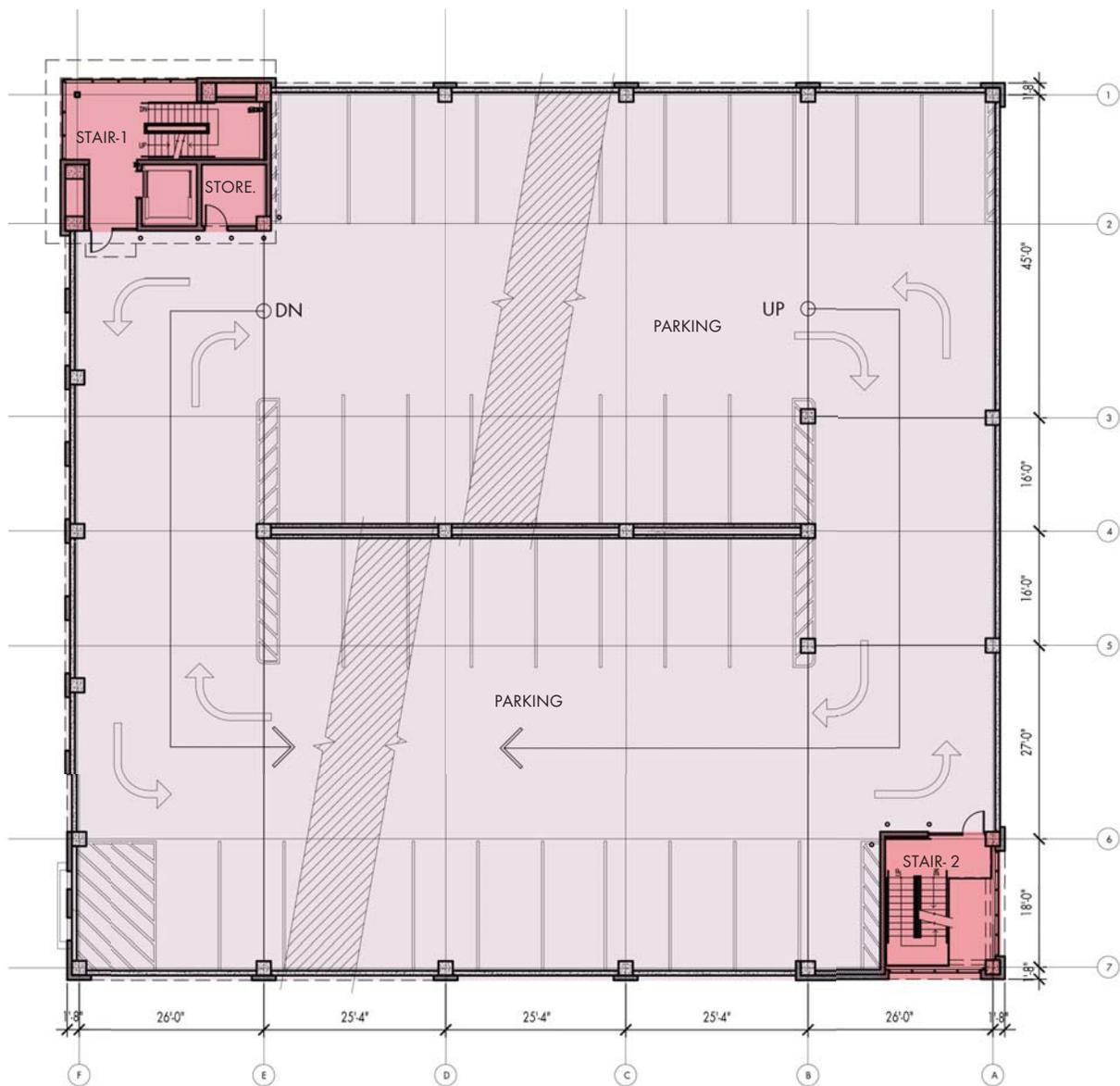
MONROE COUNTY COMMISSIONER'S GARAGE | LEVEL 01 FLOOR PLAN

APRIL 2015

SCALE: 1/16" = 1'-0"



SP-2-15
Floor Plans



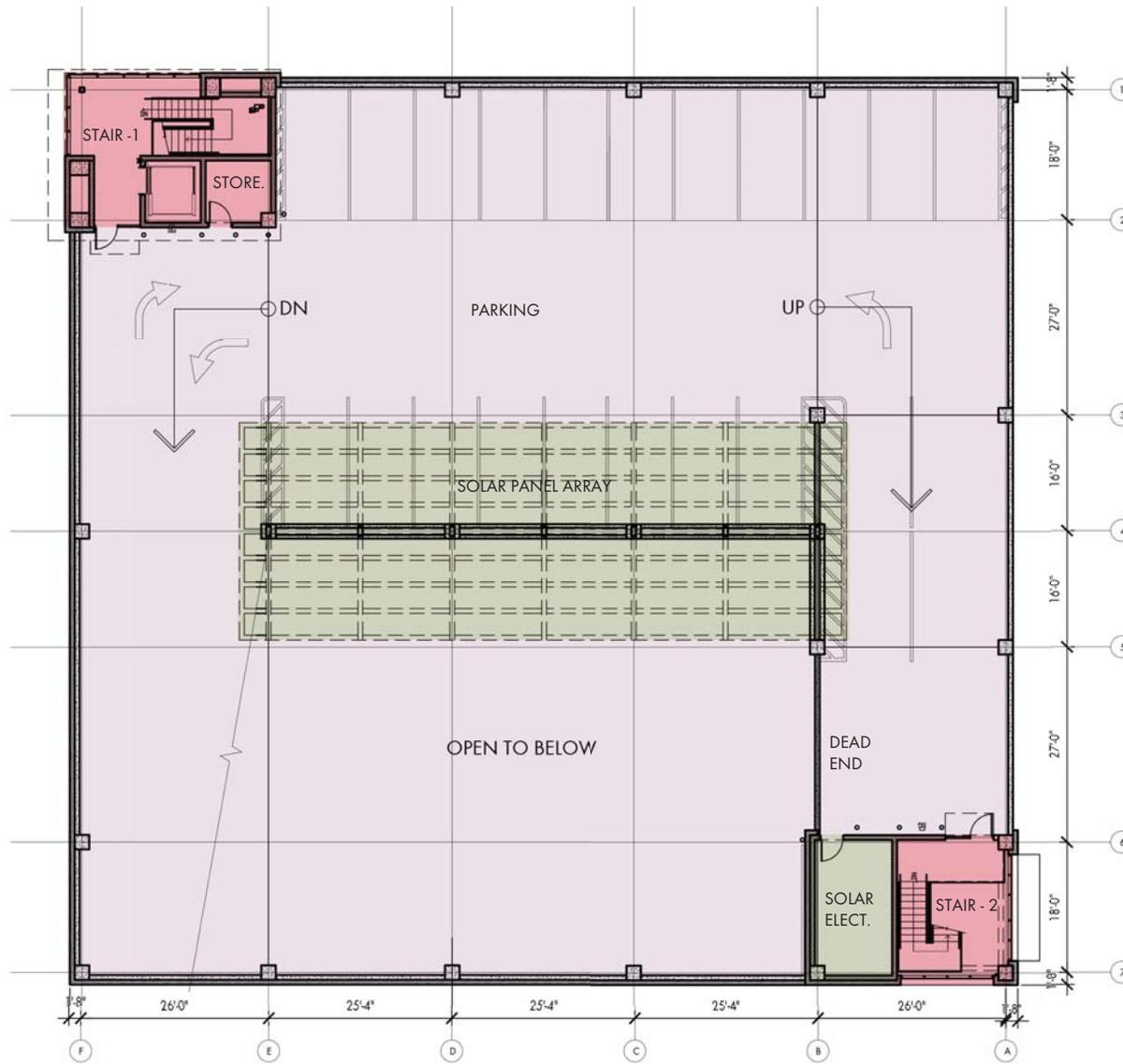
- ① CHARGING STATIONS
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- ENTRY/ EXIT STAIRS
- PARKING GARAGE
- EMERGENCY INMATE SHELTER
- SOLAR PANEL ARRAY

MONROE COUNTY COMMISSIONER'S GARAGE | LEVEL 02-06 FLOOR PLAN

APRIL 2015

SCALE: 1/16" = 1'-0"





- ① CHARGING STATIONS
- ② BICYCLE RACKS
- ③ SECURITY GATE
- ④ EMERGENCY INMATE SHELTER - ENTRANCE
- ENTRY/ EXIT STAIRS
- PARKING GARAGE
- EMERGENCY INMATE SHELTER
- SOLAR PANEL ARRAY

MONROE COUNTY COMMISSIONER'S GARAGE | LEVEL 07 FLOOR PLAN

APRIL 2015

SCALE: 1/16" = 1'-0"





MONROE COUNTY COMMISSIONER'S GARAGE | MORTON STREET AND 8TH STREET PERSPECTIVE

APRIL 2015



Looking Southeast
from Morton



MONROE COUNTY COMMISSIONER'S GARAGE | 7TH STREET AND MORTON STREET PERSPECTIVE

APRIL 2015



Looking northeast from 7th & Morton



MONROE COUNTY COMMISSIONER'S GARAGE | 7TH STREET PERSPECTIVE

APRIL 2015



Looking north from
7th Street



MONROE COUNTY COMMISSIONER'S GARAGE | MORTON STREET PERSPECTIVE

APRIL 2015



Looking northeast
from Morton

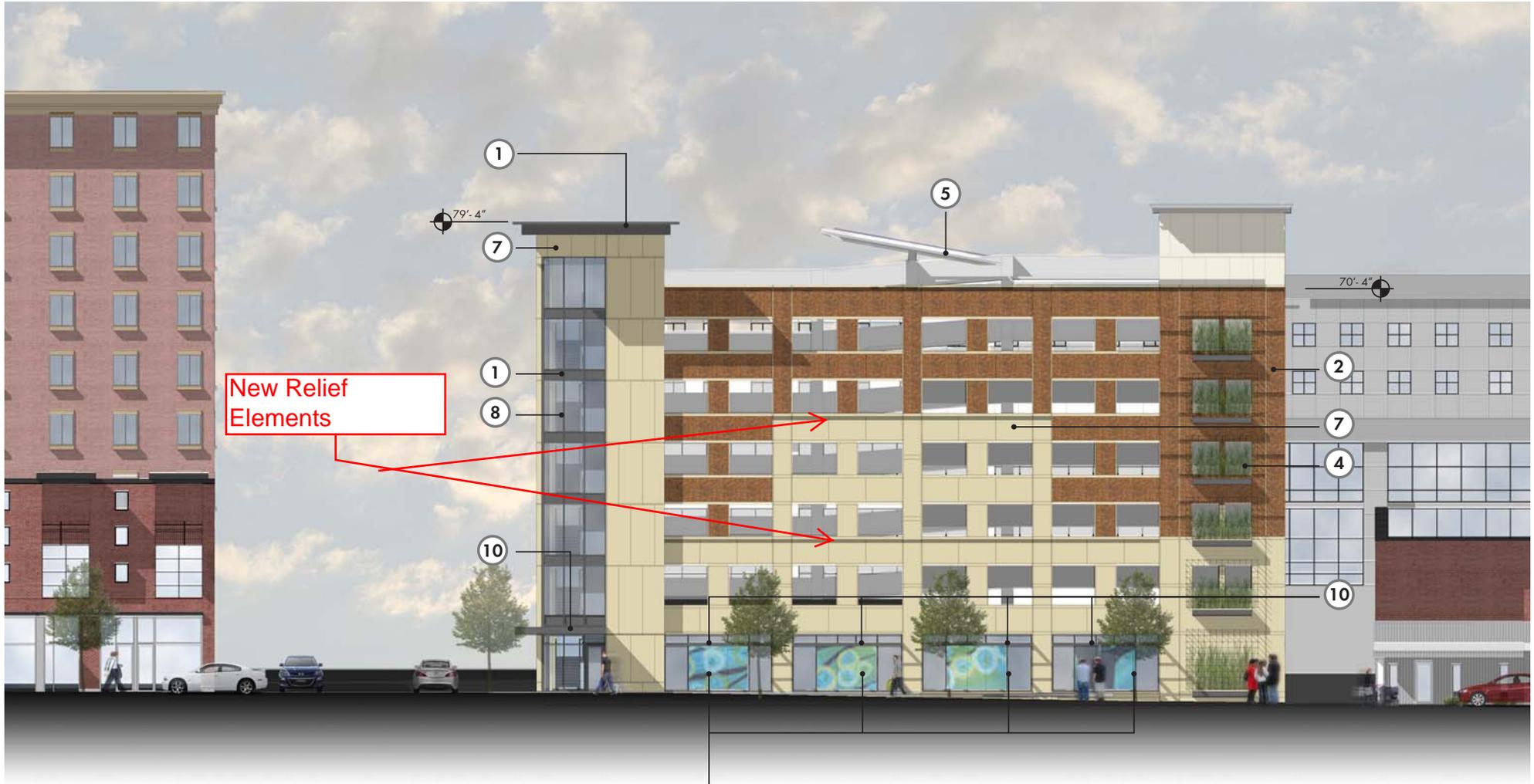


MONROE COUNTY COMMISSIONER'S GARAGE | WEST ELEVATION

APRIL 2015



Larger Morton
Streetscape



New Relief Elements

- ① METAL PANEL
- ② BRICK VEENER PRECAST PANELS
- ③ SECURITY GATE
- ④ GREEN SCREEN TRELLIS SYSTEM
- ⑤ SOLAR PANEL STRUCTURE
- ⑥ CONCRETE STRUCTURE
- ⑦ ACID ETCHED PRECAST PANEL
- ⑧ ALUMINUM STOREFRONT WITH CLEAR GLAZING
- ⑨ ALUMINUM STOREFRONT WITH SPANDREL GLAZING
- ⑩ METAL CANOPY



MONROE COUNTY COMMISSIONER'S GARAGE | WEST ELEVATION
 APRIL 2015 SCALE: 1/16" = 1'-0"

West Elevation



New Brick and vertical supports

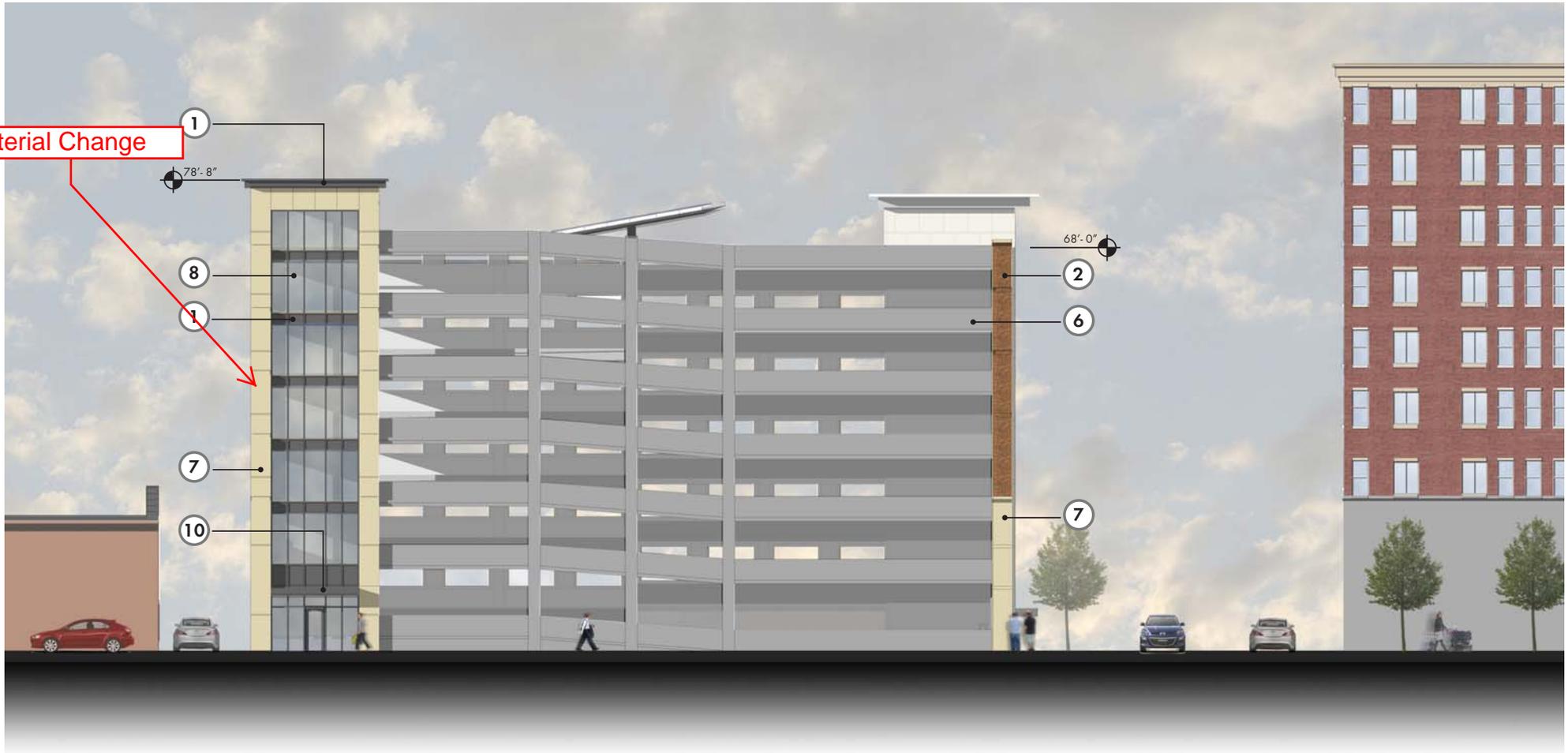
New Relief Element

- ① METAL PANEL
- ② BRICK VEENER PRECAST PANELS
- ③ SECURITY GATE
- ④ GREEN SCREEN TRELLIS SYSTEM
- ⑤ SOLAR PANEL STRUCTURE
- ⑥ CONCRETE STRUCTURE
- ⑦ ACID ETCHED PRECAST PANEL
- ⑧ ALUMINUM STOREFRONT WITH CLEAR GLAZING
- ⑨ ALUMINUM STOREFRONT WITH SPANDREL GLAZING
- ⑩ METAL CANOPY

MONROE COUNTY COMMISSIONER'S GARAGE | NORTH ELEVATION
 APRIL 2015 SCALE: 1/16" = 1'-0"

North Elevation



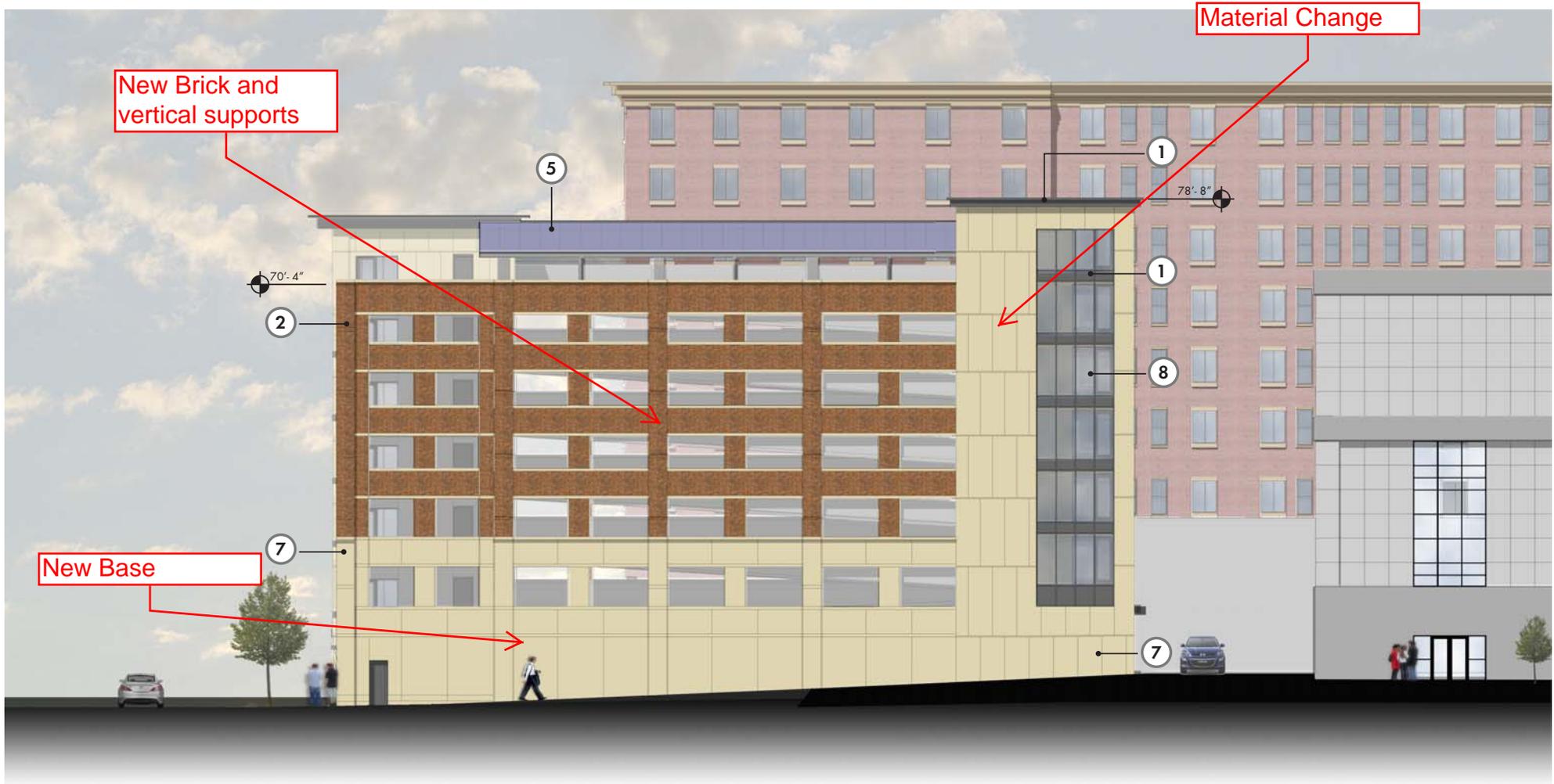


MONROE COUNTY COMMISSIONER'S GARAGE | EAST ELEVATION
 APRIL 2015 SCALE: 1/16" = 1'-0"

- | | |
|-------------------------------|---|
| ① METAL PANEL | ⑥ CONCRETE STRUCTURE |
| ② BRICK VEENER PRECAST PANELS | ⑦ ACID ETCHED PRECAST PANEL |
| ③ SECURITY GATE | ⑧ ALUMINUM STOREFRONT WITH CLEAR GLAZING |
| ④ GREEN SCREEN TRELLIS SYSTEM | ⑨ ALUMINUM STOREFRONT WITH SPANDREL GLAZING |
| ⑤ SOLAR PANEL STRUCTURE | ⑩ METAL CANOPY |



East Elevation



New Base

New Brick and vertical supports

Material Change

- ① METAL PANEL
- ② BRICK VEENER PRECAST PANELS
- ③ SECURITY GATE
- ④ GREEN SCREEN TRELLIS SYSTEM
- ⑤ SOLAR PANEL STRUCTURE
- ⑥ CONCRETE STRUCTURE
- ⑦ ACID ETCHED PRECAST PANEL
- ⑧ ALUMINUM STOREFRONT WITH CLEAR GLAZING
- ⑨ ALUMINUM STOREFRONT WITH SPANDREL GLAZING
- ⑩ METAL CANOPY

MONROE COUNTY COMMISSIONER'S GARAGE | SOUTH ELEVATION
 APRIL 2015 | SCALE: 1/16" = 1'-0"

South Elevation

