

CITY OF BLOOMINGTON



August 19, 2015 @ 2:00 p.m.
CITY HALL -
KELLY CONFERENCE ROOM #155

CITY OF BLOOMINGTON
HEARING OFFICER
August 19, 2015 at 2:00 p.m.

***Kelly Conference Room #155**

PETITION:

- V-28-15 **McDonald's Real Estate Company**
2819 E. 3rd St
Request: Variance from entrance and drive standards to install a drive-through aisle within the front parking setback.
Case Manager: Beth Rosenbarger

**BLOOMINGTON HEARING OFFICER
STAFF REPORT
LOCATION: 2819 E. 3rd Street**

**CASE #: V-28-15
DATE: August 19, 2015**

**PETITIONER: McDonalds Real Estate Co
250 W. 96th Street Suite 500, Indianapolis**

**CONCULTANT: Stantec, David Call
3754 Ranchero Drive, Ann Arbor, Michigan**

REQUEST: The petitioner is requesting a variance from driveway design standards.

REPORT SUMMARY: The property is located at the northeast corner of E. 3rd Street and S. Pete Ellis Drive and is zoned Commercial Arterial (CA). It is surrounded to the west, north, and east by other fast food restaurants. To the south is a funeral home and to the southwest is the College Mall. The property has been developed with a single-story McDonald's restaurant.

The petitioner proposes to construct a second drive-through ordering station on the northeast side of the building, adjacent to the existing drive-through. A drive-through is a permitted use within the CA zoning; however, a variance is required from the entrance and drive standards due to the location within the front parking setback and parallel orientation to the street right-of-way.

The petitioner wishes to add the new drive-through ordering station to alleviate some traffic flow issues and safety concerns on the site that arise when traffic backs-up onto Pete Ellis Drive. The drive is in the location of an existing parking aisle. The drive-through bailout lane is in the location of existing parking. The amount of impervious surface on site will actually be reduced due to increased landscaping, a reduced drive cut, and a reduction in width of a different drive aisle. Additionally, the majority of business for fast food restaurants in general and McDonalds in particular is conducted via the drive-through.

The property was developed prior to the adoption of the Unified Development Ordinance (UDO), and most of the improvements on the lot do not meet current standards. In order to install the drive-through, the petitioner has provided a site plan that will bring the lot closer to conformity with current standards. In the area adjacent to the proposed drive-through, existing parking within the front parking setback will be removed and required landscaping will be added, which will serve to reduce the impervious surface coverage and add a landscaped screen between the sidewalk and the drive-through. Other site improvements focus on improving the parking, landscaping, and bicycle and pedestrian access.

CRITERIA AND FINDINGS

20.09.130 (e) Standards for Granting Variances from Development Standards:

A variance from the development standards of the Unified Development Ordinance may be approved only upon determination in writing that each of the following criteria is met:

1. *The approval will not be injurious to the public health, safety, morals, and general welfare of the community.*

Staff's Finding: Staff finds no injury to the general welfare. The variance will allow the removal of parking within an existing setback as well as other site improvements. The impervious surface coverage will be reduced. The drive cut on E. Pete Ellis drive will be reduced, resulting in safety improvements for pedestrians, bicyclists, and automobiles. The additional drive-through window will result in improved efficiency, traffic flow, and safety on the site.

2. *The use and value of the area adjacent to the property included in the variance will not be affected in a substantially adverse manner.*

Staff's Finding: Staff finds the use and value of the area adjacent to the property will not be affected in a substantially adverse manner. The closest adjacent residential use is nearly 700 feet, as the crow flies, northeast of the north property line of the site. The area contains other similar development and uses.

3. *The strict application of the terms of the Unified Development Ordinance will result in practical difficulties in the use of the property; that the practical difficulties are peculiar to the property in question; that the variance will relieve practical difficulties.*

Staff's Finding: Staff finds peculiar conditions for the variance in the existing driveway locations and the median on E. 3rd Street. The entrance drive from E. 3rd Street is exit-only, which means all auto traffic must enter the site from S. Pete Ellis Dr. Fast food restaurants in auto areas rely on drive-throughs, and the entrance forces cars to either loop all the way around the building, or more often, form a queue that backs up onto S. Pete Ellis and blocks the pedestrian crosswalk. The peculiar condition of the entrance drives and traffic flow on the property are exacerbated by the intensity of the drive-through use of this establishment. The variance will relieve these practical difficulties, improve the site, and resolve the traffic-flow issues and safety concerns.

RECOMMENDATION: Based on the written findings, staff recommends approval of the variance with the following conditions.

1. The landscaping plan and all plant species must be approved by staff before release of the Certificate of Zoning Compliance.
2. All signage must receive a sign permit prior to installation, including the menu board.
3. A right-of-way excavation permit is required for all work within a right-of-way such as the drive-cut reduction and crosswalk striping.
4. As discussed with petitioner, the bicycle rack will be relocated to be adjacent to the E. Pete Ellis entrance and will include a sidewalk connection.

Ms. Beth Rosenbarger
City of Bloomington, IN
Planning and Transportation
401 N. Martin St. Suite 130
Bloomington, IN 47404

Re: Petitioner's Statement regarding the retro-fitting of the existing McDonald's at 2819 E. 3rd Street, Bloomington

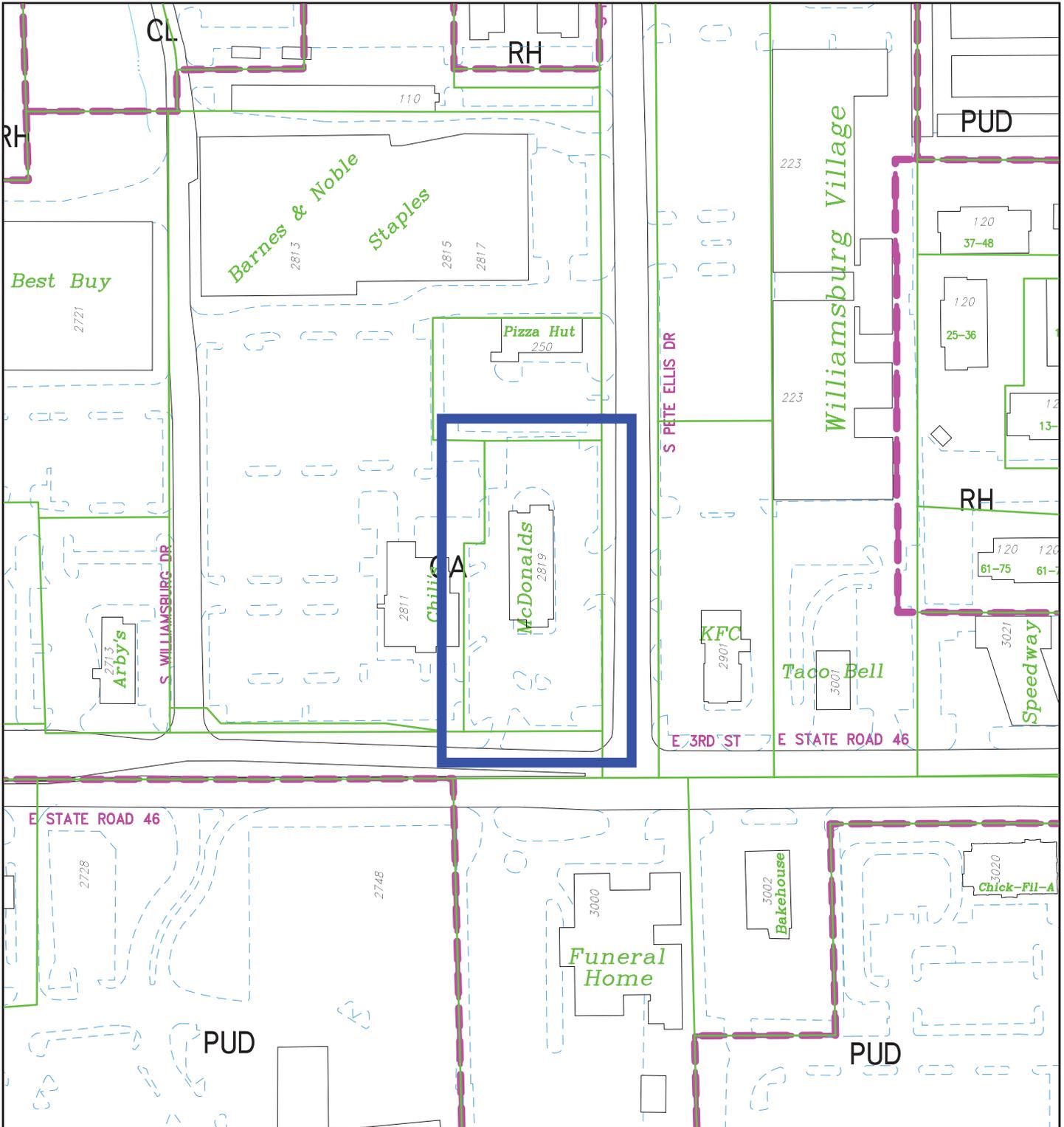
Ms. Rosenbarger;

A site variance is required from the entrance and drive standards in order to permit a drive-thru aisle within the front parking setback. This proposed project provides a second drive-thru adjacent to the existing drive-thru at the north end of this site. The practical difficulty with providing this second drive-thru is that it will not perform as a drive-thru unless located as proposed. Due to the property size, shape, and existing layout of the currently functioning McDonald's all contribute to the cause of the practical difficulty which is peculiar to this site. The Development Standards variance will relieve the practical difficulty.

Being sensitive to the surrounding area, McDonald's has agreed to modifications to the existing site as well as to provide increased landscaping along the east side of the property to buffer the proposed second drive-thru area. Three proposed parking spaces planned for the east side of the site, north of the access drive, have been removed and will accommodate the required bicycle parking as well as to provide more pervious area that will be landscaped. Additionally the driveway from South Pete Ellis Drive has been narrowed approximately 10 feet to come into conformance with the required standards.

Based on the efforts made to modify the plans as requested by Bloomington staff, and the interest McDonald's has in re-investing financially into this existing site by making it easier and quicker for the public to be serviced we believe the approval we seek is not injurious to the public health, safety, morals, and general welfare of the community.

V-28-15
Petitioner's Statement



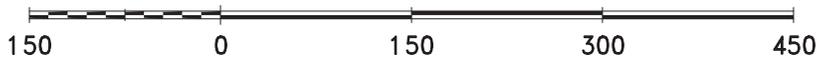
V-28-15 Hearing Officer

Location Map

Zoning, Location, Land Use, Buildings

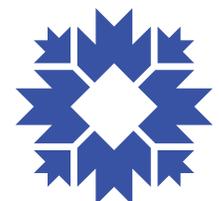
By: rosenbab

14 Aug 15

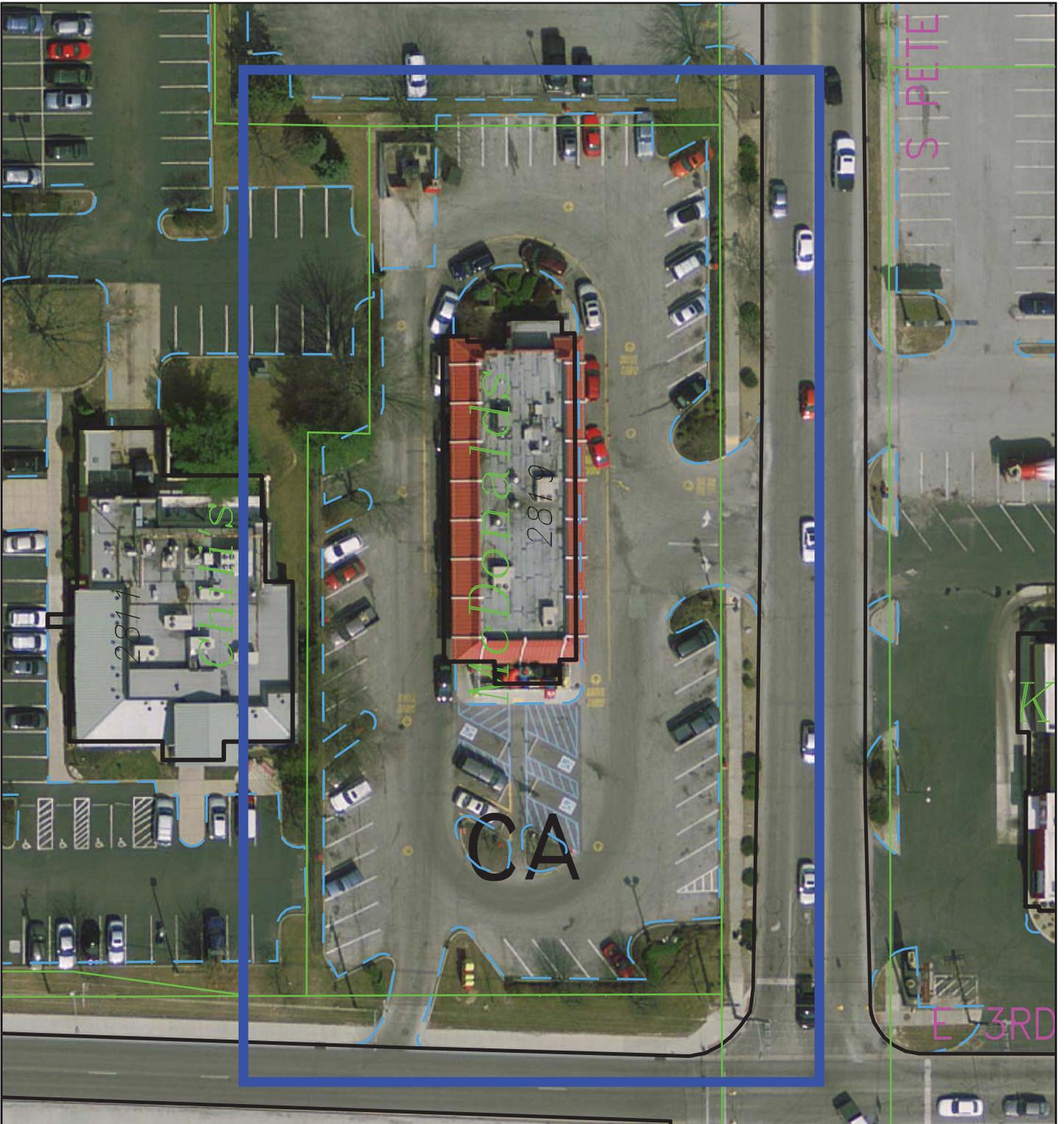


For reference only; map information NOT warranted.

City of Bloomington
Planning & Transportation



Scale: 1" = 150'



V-28-15 Hearing Officer
 2014 Aerial Photography

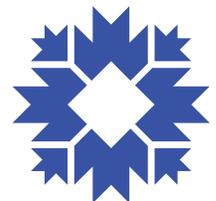
By: rosenbab
 14 Aug 15



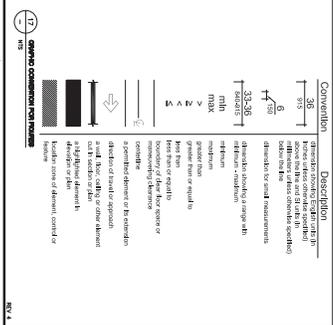
For reference only; map information NOT warranted.

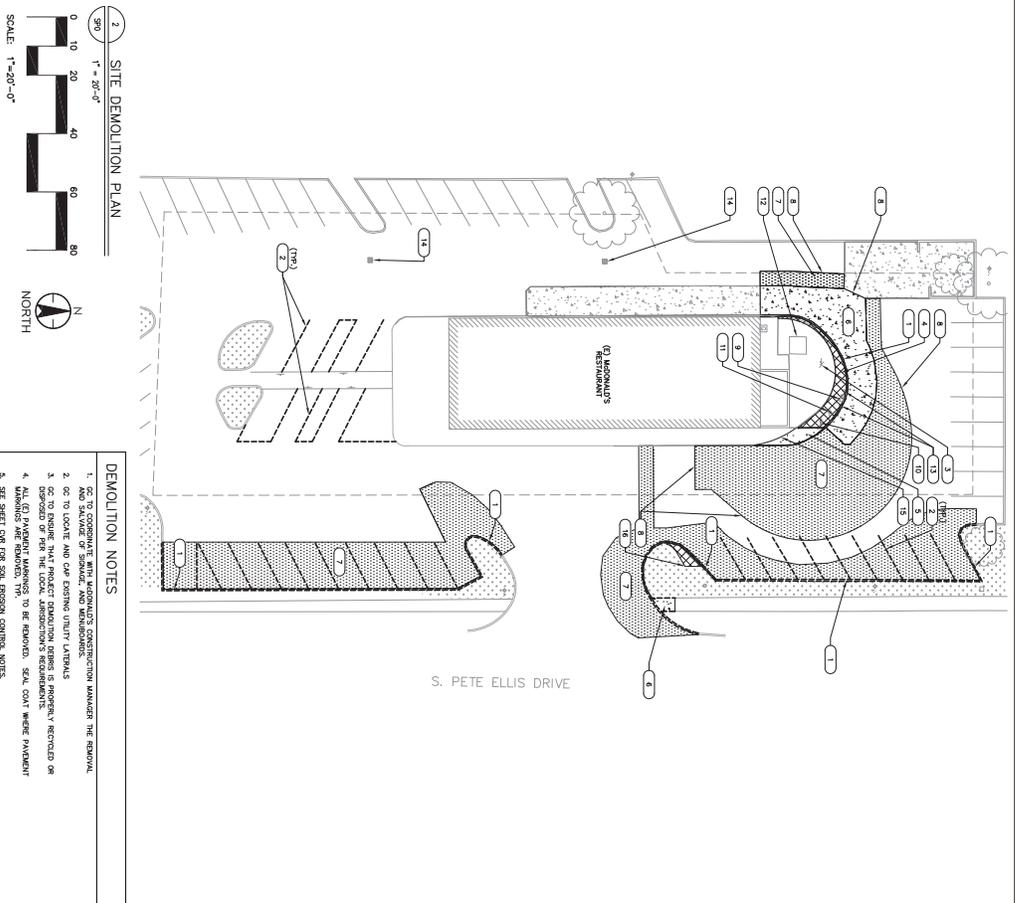
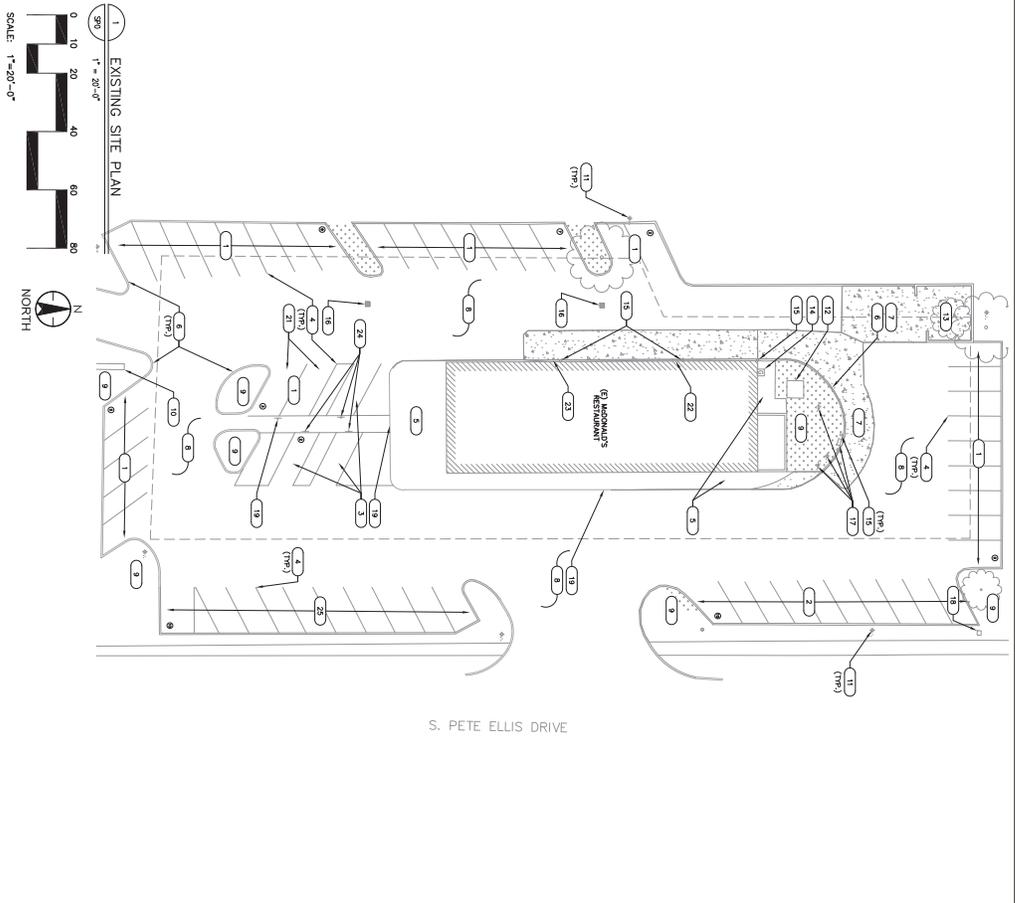


City of Bloomington
 Planning & Transportation



Scale: 1" = 50'





- KENNOTES**
- 1 (E) PARKING STALLS TO REMAIN.
 - 2 (E) PARKING STALLS TO BE EXAMINED.
 - 3 (E) VAN ACCESSIBLE STALLS BE RECOMMENDED.
 - 4 (E) STRIPING (TYP.)
 - 5 (E) SIDEWALK
 - 6 (E) CONCRETE CURB
 - 7 (E) ASPHALT PAVING
 - 8 (E) ASPHALT PAVEMENT
 - 9 (E) ASPHALT PAVING TO BE RELOCATED.
 - 10 (E) ASPHALT PAVING TO BE RELOCATED.
 - 11 (E) ASPHALT PAVING TO BE RELOCATED.
 - 12 (E) ELECTRICAL TRANSFORMER TO REMAIN.
 - 13 (E) GAS METER
 - 14 (E) SLOTTED
 - 15 (E) SLOTTED
 - 16 (E) SLOTTED
 - 17 (E) SLOTTED

- KENNOTES**
- 1 REMOVE (E) CONCRETE CURB.
 - 2 REMOVE (E) ASPHALT PAVING AS SHOWN. SEAL COAT WHERE PAVEMENT AND SAND/GRAD OR SHADDE AND MANDIBOLDS.
 - 3 REMOVE (E) ASPHALT PAVING AND CONCRETE BASE. CONCRETE WILL REMAIN AND BE REBUILT AT THEIR BEST CONDITIONS.
 - 4 REMOVE PORTION OF (E) SIDEWALK.
 - 5 REMOVE PORTION OF (E) SIDEWALK.
 - 6 REMOVE PORTION OF (E) ASPHALT PAVING.
 - 7 REMOVE PORTION OF (E) ASPHALT PAVING.
 - 8 REMOVE PORTION OF (E) ASPHALT PAVING.
 - 9 REMOVE (E) ASPHALT PAVING.
 - 10 REMOVE (E) ASPHALT PAVING.
 - 11 REMOVE (E) ASPHALT PAVING.
 - 12 REMOVE (E) ASPHALT PAVING.
 - 13 REMOVE (E) ASPHALT PAVING.
 - 14 REMOVE (E) ASPHALT PAVING.

DEMOLITION NOTES

- SEE TO CONSULT WITH ARCHITECT'S CONSTRUCTION MANAGER THE REMOVAL AND SALVAGE OF SHADDE AND MANDIBOLDS.
- SEE TO LOCATE AND CAP EXISTING UTILITY LINES.
- DO NOT REMOVE THAT PROJECT DEMOLITION DEBRIS IS PROPERLY RECYCLED OR REUSED.
- ALL (E) REMAINING MANDIBOLDS TO BE REMOVED. SEAL COAT WHERE PAVEMENT REMAINS ARE REMOVED. TYP.
- SEE SHEET C01 FOR SOIL EROSION CONTROL NOTES.

PLAN LEGEND

[Symbol]	GENERAL LINE	[Symbol]	POINT OF CURVATURE
[Symbol]	EXISTING SIDEWALK	[Symbol]	POINT OF WAY
[Symbol]	EXISTING ASPHALT PAVING	[Symbol]	EXISTING ASPHALT PAVING
[Symbol]	EXISTING CONCRETE CURB	[Symbol]	EXISTING CONCRETE CURB
[Symbol]	EXISTING ASPHALT PAVING TO BE REMOVED	[Symbol]	EXISTING ASPHALT PAVING TO BE REMOVED
[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED	[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED
[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED	[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED
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[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED	[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED
[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED	[Symbol]	EXISTING ASPHALT PAVING TO BE RELOCATED

BRAND REVIEW

PLANNING	DATE	BY
PERMIT	06/29/2015	BD
CONSTRUCTION		

McDonald's USA, LLC
 2810 E. 39th ST.
 BLOOMINGTON, IN. 47401

Stantec
 3754 RANCHERO DRIVE
 ANN ARBOR, MI 48108

Tel. 734.761.010
 Fax. 734.761.1200
 www.stantec.com

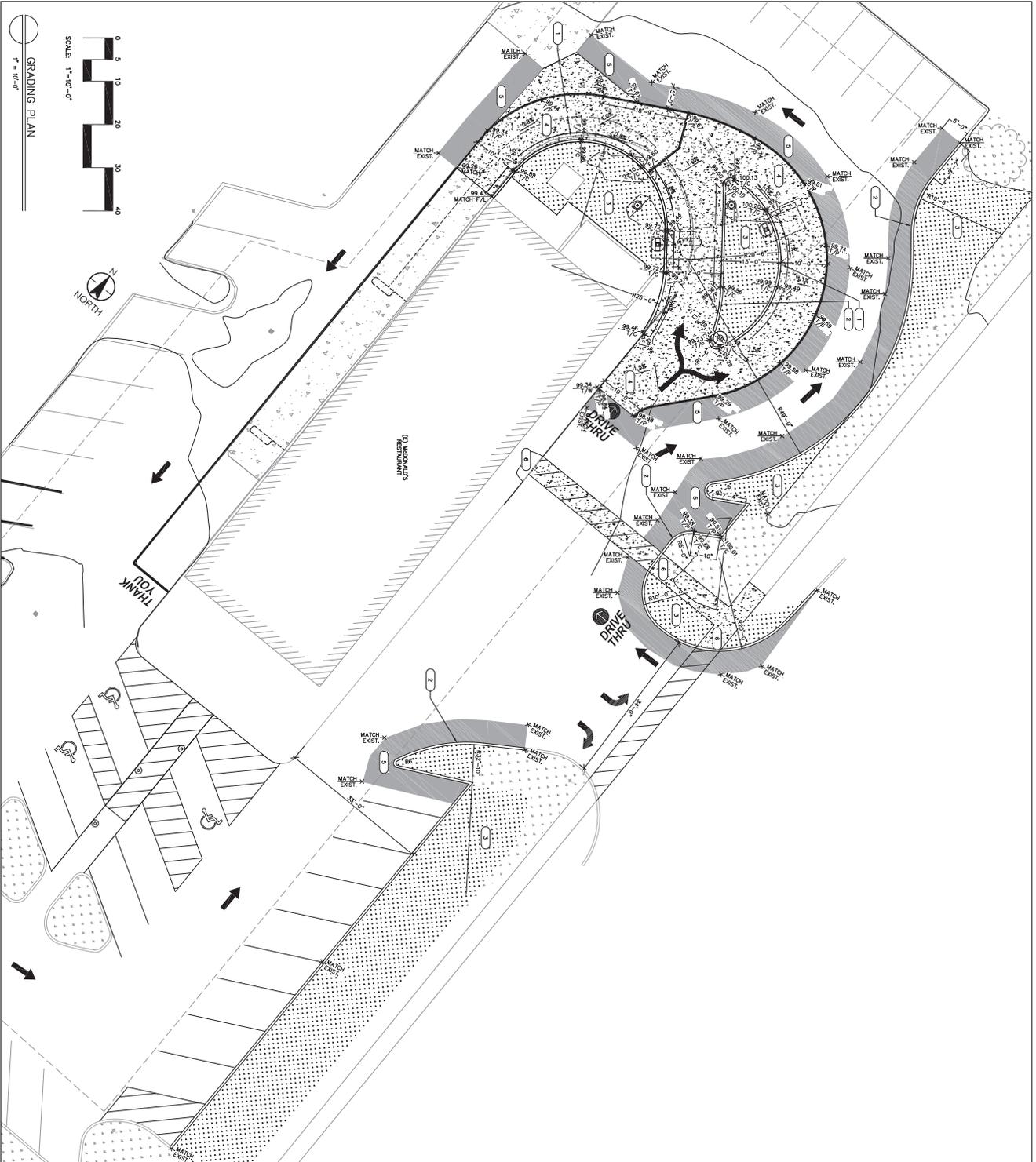
REV	DATE	DESCRIPTION	BY
2	8/3/15	PERMIT SET	RES
1	07/29/15	REVIEW SET	MDP

SHEET NO. SPO

DRAWING TITLE: EX. SITE & DEMOLITION PLAN

PROJECT ADDRESS: 2810 E. 39th ST. BLOOMINGTON, IN. 47401

PROJECT CAD FILE: 2075\26700 STATE SITE CODE: 013-0064



GENERAL NOTES

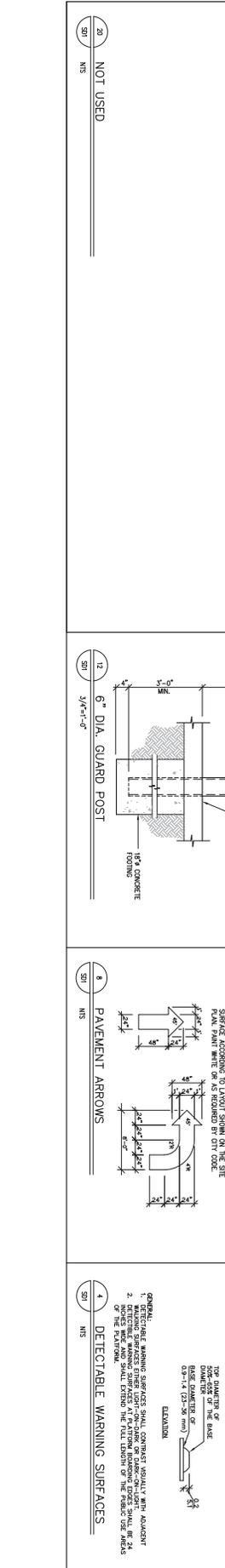
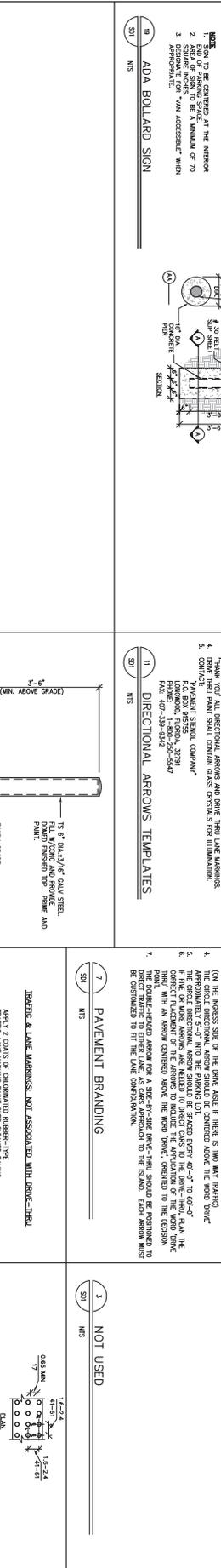
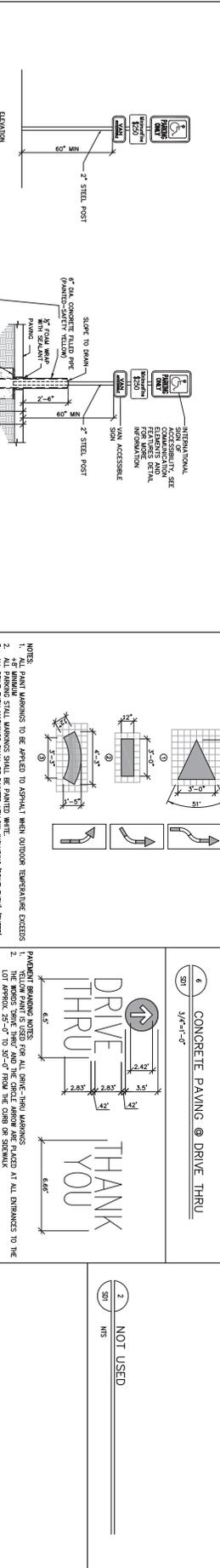
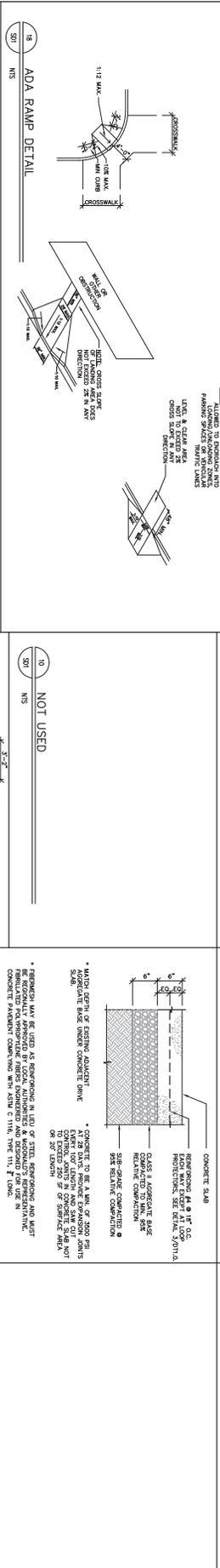
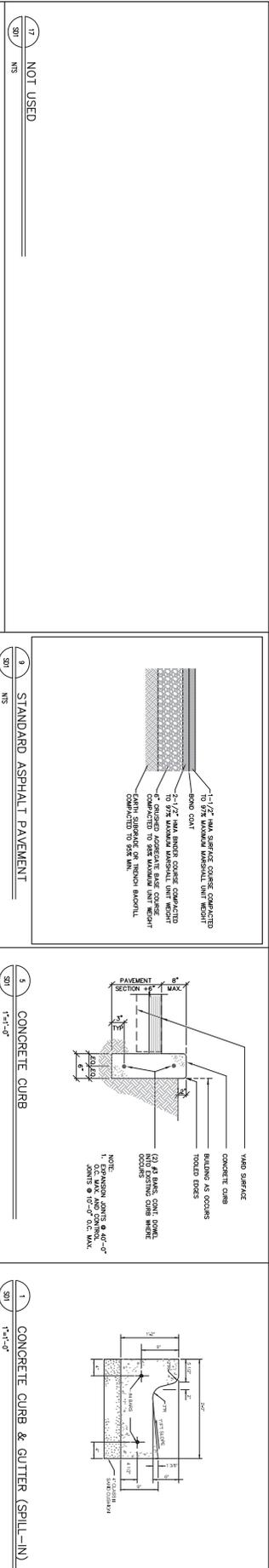
1. SURFACE OF DRIVE AND FLARES SHALL BE SIF RESISTANT AND COMPACTED FRESH FROM
2. DRIVE SHALL BE IN SLOPE STATE FLARE 1/4" IN. SURFACE MARKING AND CROSS SLOPE
3. DRIVE MARKING SHALL HAVE A CROSS SLOPE OF 2% MAX. IN ALL DIRECTIONS.
4. MARKING SLOPE IN ACCESSIBLE PARKING SPACES & ACCESSIBLE DRIVE TO BE 2% IN ANY DIRECTION.
5. ALL PARKING LOT IMPROVEMENTS SHALL MATCH SURROUNDING EXISTING SPACES.

KEYNOTES

- (1) (N) CONCRETE DRIVE & DRIVEWAY. SEE DETAIL 18/201.
- (2) (N) CONCRETE DRIVE. SEE DETAIL 9/201.
- (3) (N) LANDSCAPING TO MATCH EXISTING. SEE NOTE 17 ON L10.
- (4) (N) CONCRETE PAVING AT DRIVE THRU. SEE DETAIL 9/201.
- (5) (N) ASPHALT PAVING. SEE DETAIL 9/201.
- (6) (N) ADA MARK. SEE DETAIL 18/201.

PLAN LEGEND

(N)	NOTHING	(1)	CONCRETE DRIVE
1/4"	FACE OF CURB	(2)	CONCRETE DRIVE
B/C	BACK OF CURB	(3)	CONCRETE DRIVE (9/21-10)
1/8"	TOP OF PAVEMENT	(4)	CONCRETE DRIVE (9/21-10)
		(5)	CONCRETE DRIVE
		(6)	CONCRETE DRIVE
		(7)	CONCRETE DRIVE
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		(100)	CONCRETE DRIVE



REV	DATE	DESCRIPTION	BY
2	8/3/15	PERMIT SET	RES
1	07/29/15	REVIEW SET	MOP

PREPARED FOR: ©2012 McDonald's USA, LLC

McDonald's USA, LLC

Stantec
3754 RANCHERO DRIVE
ANN ARBOR, MI 48108

Tel. 734.761.010
Fax. 734.761.1200
www.stantec.com

BRAND REVIEW

PLANNING

PERMIT

6/29/2015

SD1

CONSTRUCTION

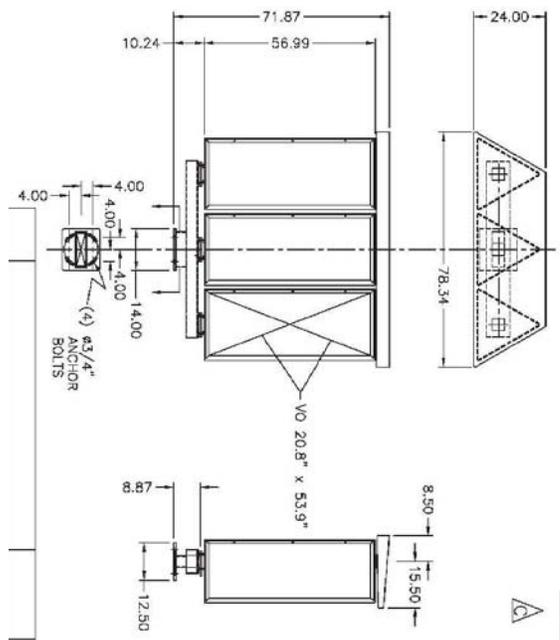
SHEET NO. 13

DRAWING TITLE: SITE DETAILS

PROJECT ADDRESS: 2810 9TH ST, BLOOMINGTON, IN. 47401

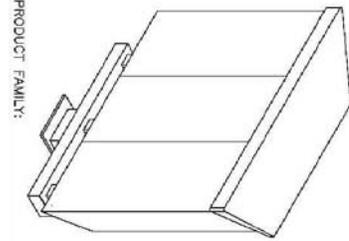
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SD1



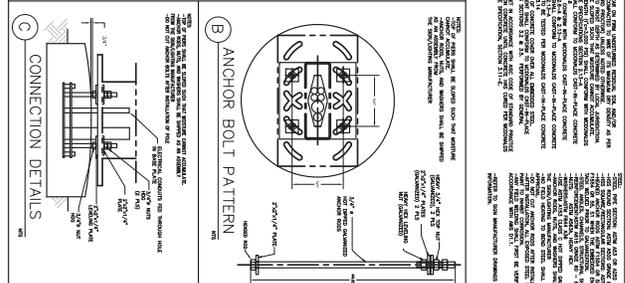
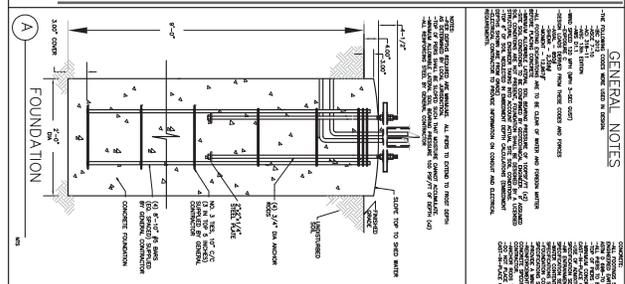
PRODUCT COMPONENTS - AS SHIPPED

QTY	PART #	DESCRIPTION	CARTON SIZE	WEIGHT
1	A740355	BASE	60x20x14	242#
3	A740181P	TRIANGLE ASSYS	72x22x25	143#
1	A740358	RAINCAP KIT	60x20x10	ND
1	A740353	RAINCAP	79x25x5	29#



SEE DETAIL #
ENGINEERING PRODUCT FAMILY:

10 MENU BOARD DETAIL
NOT TO SCALE



GENERAL NOTES

1. ALL DIMENSIONS ARE IN INCHES. ALL DIMENSIONS TO FACE UNLESS OTHERWISE NOTED.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITY.

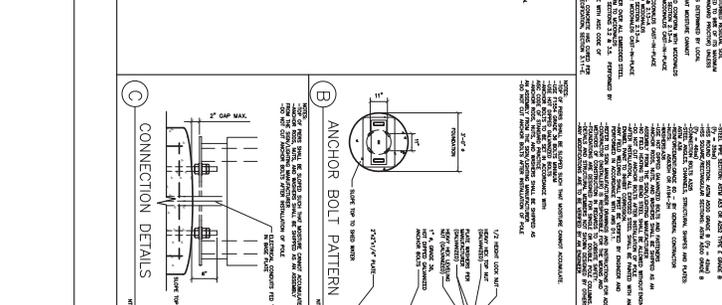
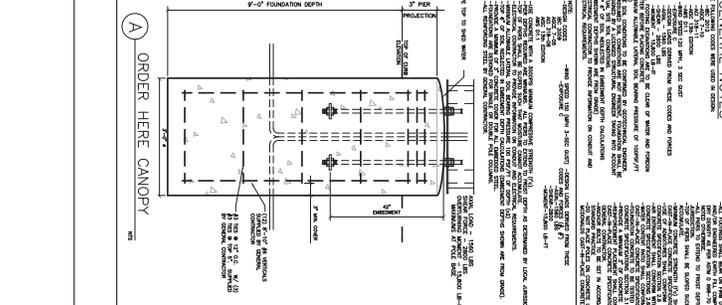
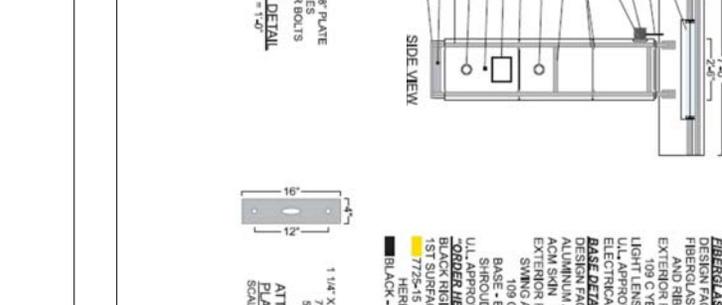
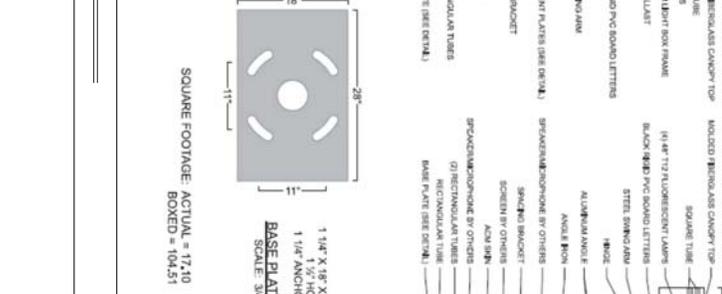
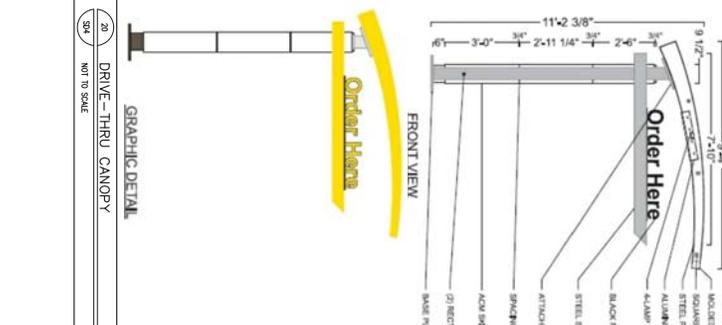
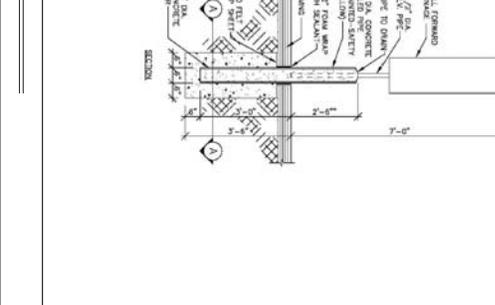
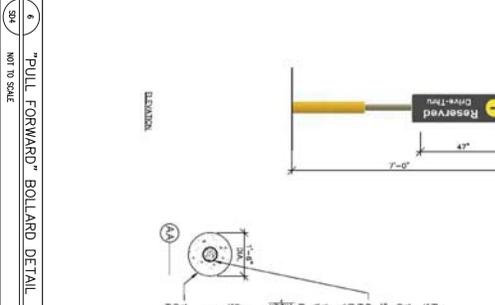
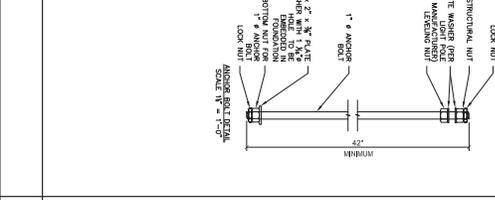
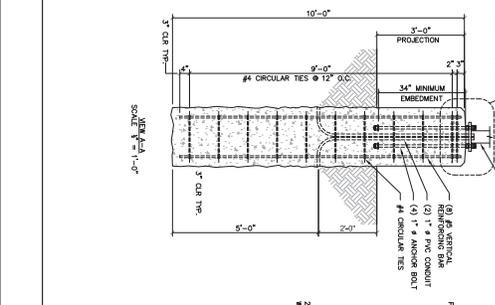
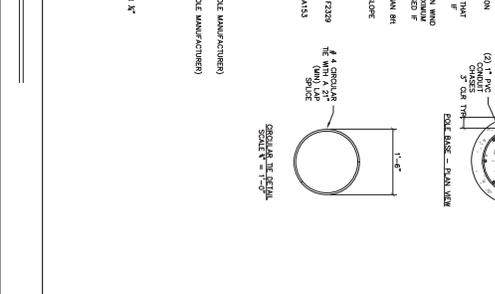
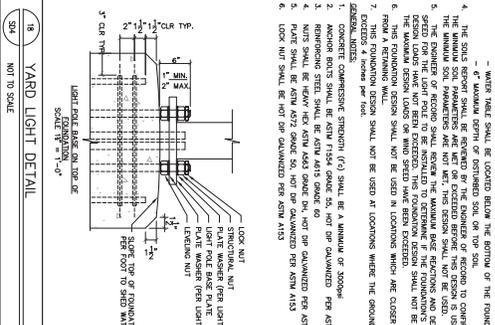
20 NOT USED
N/S

12 NOT USED
N/S

<p>SD2</p>	<p>DRAWING TITLE: MENU BOARD AND PRE-SELL DETAILS</p>	<p>BRAND REVIEW: 4/22/2015 PLANNING - PERMIT 6/29/2015 BD - CONSTRUCTION -</p>	<p>PREPARED FOR: ©2012 McDonald's USA, LLC McDonald's USA, LLC</p>	<p>Stantec Stantec 3754 RANCHERO DRIVE ANN ARBOR, MI 48108 Tel. 734.761.010 Fax. 734.761.1200 www.stantec.com</p>	<p>REV 2 8/3/15 PERMIT SET PES REV 1 07/29/15 REVIEW SET MDP BY</p>
	<p>PROJECT ADDRESS: 2819 E. 3RD ST. BLOOMINGTON, IN. 47401</p>	<p>PROJECT CAD FILE: 2075126700</p>	<p>STATE SITE CODE: 013-0064</p>	<p>McDonald's USA, LLC These drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be copied or reproduced without written authorization. The contract documents are prepared for use on this specific site in conjunction with its issue date and are not suitable for use on a different site or at a later date. Use of these drawings for reference or example on another project requires the services of properly licensed architects and engineers. Reproduction of the contract documents for reuse on another project is not authorized.</p>	<p>Stantec Stantec 3754 RANCHERO DRIVE ANN ARBOR, MI 48108 Tel. 734.761.010 Fax. 734.761.1200 www.stantec.com</p>

MINIMUM SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LIGHTS AND TRAFFIC SIGNALS, FROM EDITION, 2009
 CONSTRUCTION SPECIFICATIONS FOR HIGHWAY SIGNS, LIGHTS AND TRAFFIC SIGNALS, FROM EDITION, 2009

1. MINIMUM DESIGN WIND SPEED = 120 MPH
2. MINIMUM DESIGN WIND PRESSURE = 16.40 psf
3. MINIMUM DESIGN WIND FORCE = 1200 LB
4. MINIMUM DESIGN WIND MOMENT = 12000 LB-FT
5. MINIMUM DESIGN WIND TORQUE = 12000 LB-FT
6. MINIMUM DESIGN WIND LOAD ON SIGN = 1200 LB
7. MINIMUM DESIGN WIND LOAD ON LIGHT = 1200 LB
8. MINIMUM DESIGN WIND LOAD ON TRAFFIC SIGNAL = 1200 LB
9. MINIMUM DESIGN WIND LOAD ON TRAFFIC SIGNAL HOUSING = 1200 LB
10. MINIMUM DESIGN WIND LOAD ON TRAFFIC SIGNAL MOUNTING = 1200 LB
11. MINIMUM DESIGN WIND LOAD ON TRAFFIC SIGNAL MOUNTING HOUSING = 1200 LB
12. MINIMUM DESIGN WIND LOAD ON TRAFFIC SIGNAL MOUNTING HOUSING HOUSING = 1200 LB
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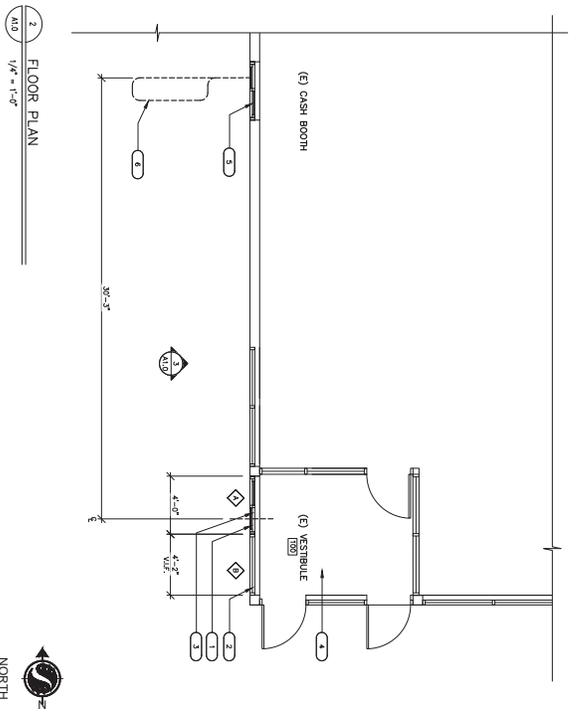
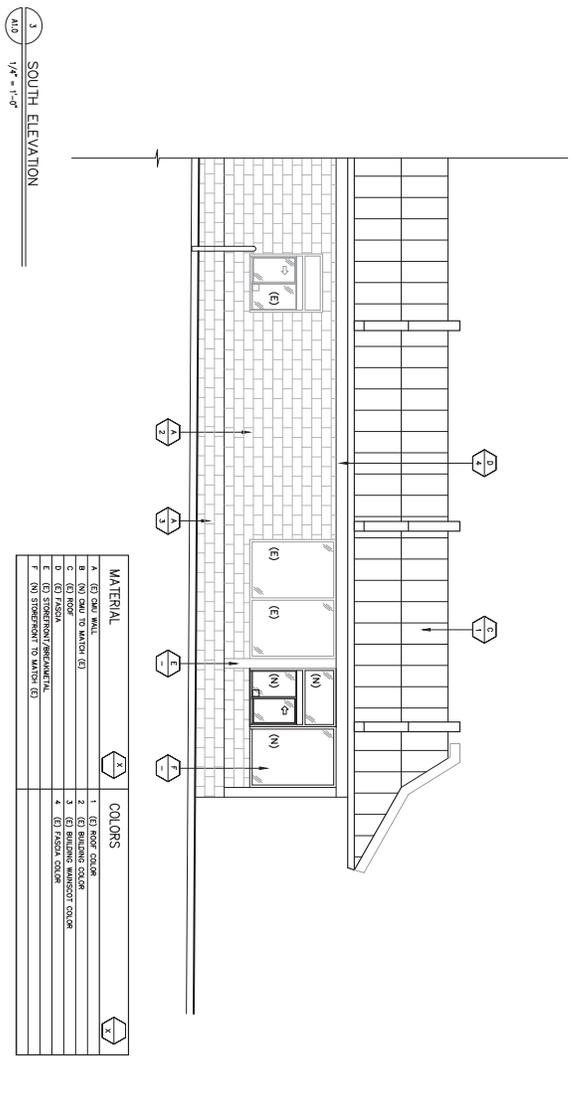


SHEET NO. SD4
 DRAWING TITLE: CANOPY AND PULL FORWARD DETAIL
 PROJECT ADDRESS: 2815 3RD ST, BLOOMINGTON, IN. 47401
 PROJECT CAD FILE: 2075126700 STATE SITE CODE: 013-0064

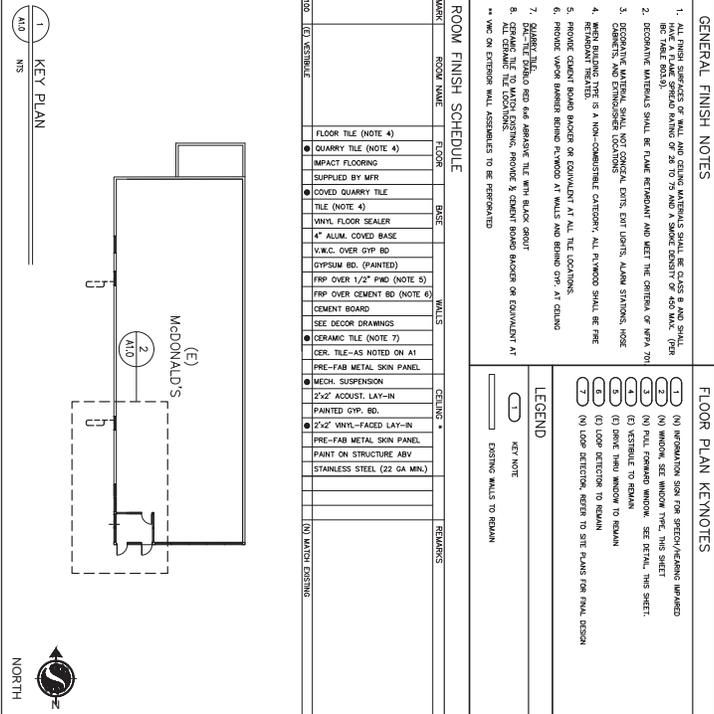
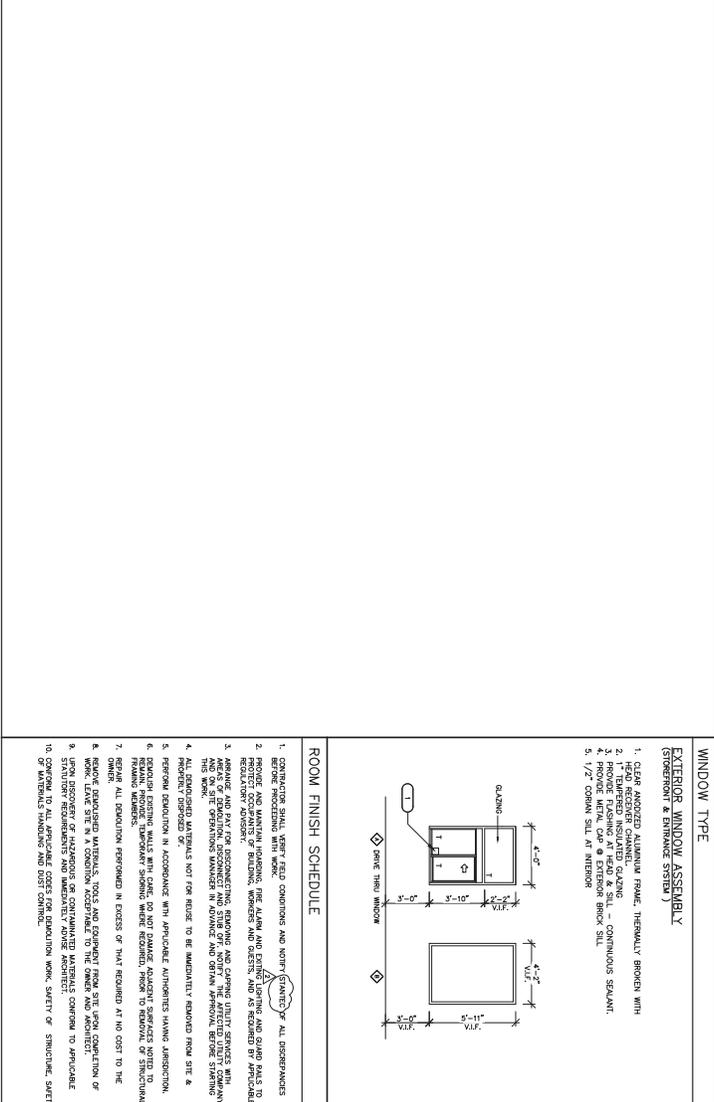
BRAND REVIEW: 6/29/2015
 PLANNING: BO
 PREPARED FOR: McDonald's USA, LLC
 ©2012 McDonald's USA, LLC
 MCDONALD'S USA, LLC
 PREPARED FOR: Stantec
 3754 RANCHERO DRIVE
 ANN ARBOR, MI 48108
 Tel. 734.761.010
 Fax. 734.761.1200
 www.stantec.com

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REV	DATE	DESCRIPTION	BY
2	8/3/15	PERMIT SET	RES
1	07/29/15	REVIEW SET	MOP

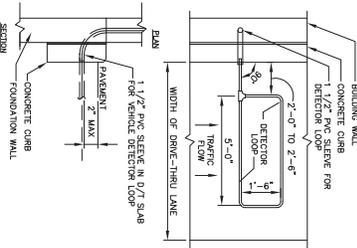
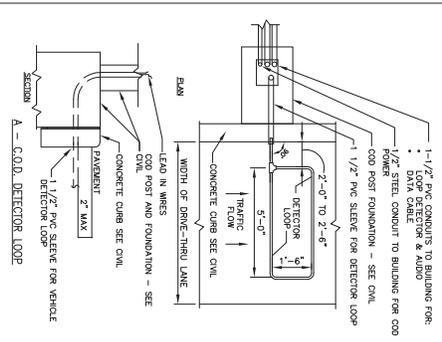


REV	DATE	DESCRIPTION	BY
1	7/6/15	REVISES	AMZ
2	6/30/15	100% CD - PERMIT SUBMITTAL SET	EW

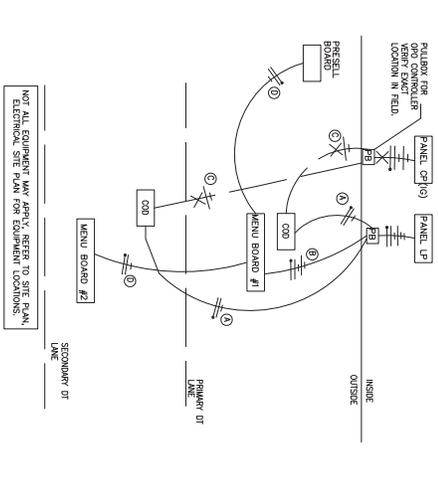
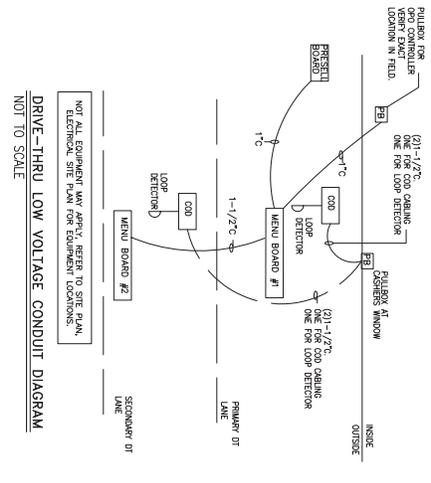


MARK	FLOOR	WALLS	CEILING	REMARKS
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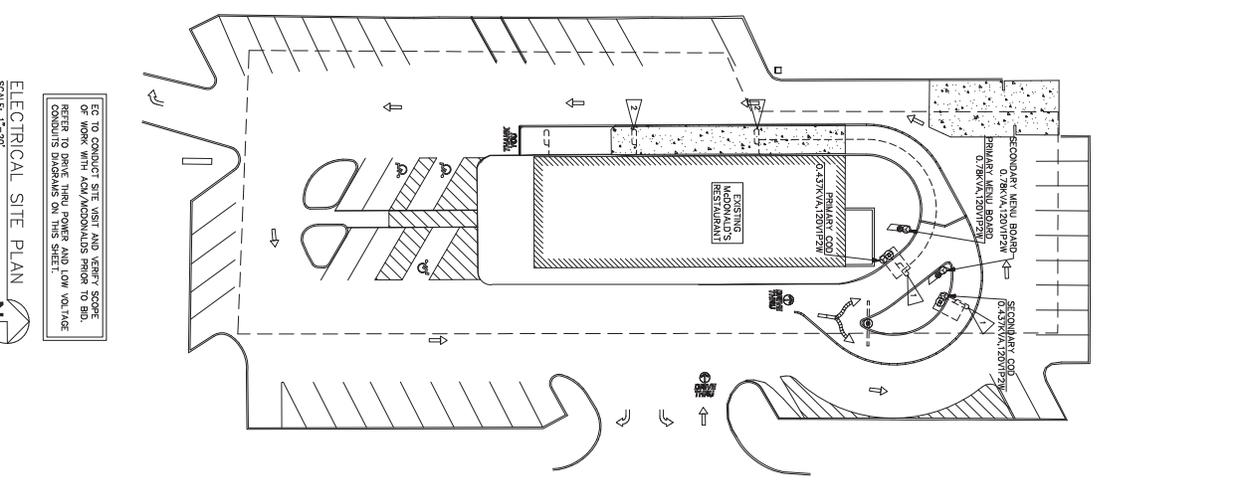
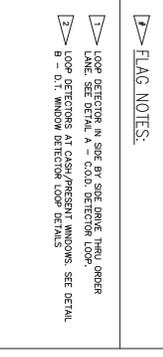
SHEET NO. A10	DRAWING TITLE: FLOOR PLAN & ELEVATION	BRAND REVIEW: PLANNING - PERMIT - BC - CONSTRUCTION	PREPARED FOR: McDonald's USA, LLC	© 2012 McDonald's USA, LLC	Stantec	Stantec Architecture and Engineering, LLC 1383 N. McDowell Blvd St. 250 Petaling, CA U.S.A. 94954 Tel. 707.774.8344 Fax. 707.765.9908 www.stantec.com
PROJECT ADDRESS: 281 1/2 St. BLOOMINGTON, IN. 47401	STATE SITE CODE: 013-0064	CONSTRUCTION: -	These drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be copied or reproduced without the prior written consent of McDonald's USA, LLC. The contractor shall be responsible for obtaining all necessary permits and for compliance with all applicable laws and regulations. Use of these drawings for reference or example on another project requires the express written consent of the architect and engineer. Reproduction of the contract documents for reuse on another project is not authorized.			



- NOTES**
- VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
 - CENTER VEHICLE DETECTOR LOOP IN DRIVE THRU LANE. INSTALL PER MFR'S RECOMMENDATIONS.
 - SEE CIVIL FOR DIMENSIONS OF DRIVE-THRU LANE CONCRETE PAD FOR DETECTOR LOOP.
 - NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2' OF LOOP.
 - DETECTOR LOOP MANUFACTURERS: DETECTOR LOOPS MAY BE OBTAINED FROM THE FOLLOWING COMPANIES OR EQUAL: MFR: 1-800-848-4468
 - DETECTOR LOOP MATERIAL: DETECTOR LOOP WIRE SHALL BE 100 PSI LOOP WIRE FROM ONE END OF THE PULVERIZED GRADE STRANDED CABLE TO THE OTHER END OF THE PULVERIZED GRADE STRANDED CABLE. LENGTH OF PVC CONDUIT, CONDUIT & PULVERIZED AS SHOWN.



- NOTES**
- 2412 & 1412 END TO SPARE CIRCUIT WITHIN LIGHTING PANEL FOR COO OVERUP LIGHTING.
 - 4412 & 1412 END TO SPARE CIRCUIT WITHIN LIGHTING PANEL FOR ONE MENU BOARD #1 LIGHTING AND P.C.
 - 2412 & 1412 END & 1412 ISOLATED END TO SPARE CIRCUIT WITHIN LIGHTING PANEL FOR ONE MENU BOARD #2 LIGHTING AND P.C.
 - 2412 & 1412 END TO SPARE CIRCUIT WITHIN LIGHTING PANEL FOR COO OVERUP LIGHTING.
- DRIVE-THRU POWER DIAGRAM**
- NOTE-VERIFY EXACT CIRCUITS & QUANTITIES OF CIRCUITS WITHIN LIGHTING PANEL AND EXISTING ELECTRICAL PANELS.
 - NOTE-PER MANUFACTURER'S INSTALLATION INSTRUCTIONS, SEPARATE WIRING METHODS ARE REQUIRED FOR EACH CIRCUIT (P.C. AND LIGHTING).



<p>SHEET NO. EO.1</p>	<p>DRAWING TITLE: ELECTRICAL SITE PLAN</p>	<p>BRAND REVIEW: PLANNING</p>	<p>PREPARED FOR: McDonald's USA, LLC</p>	<p>© 2012 McDonald's USA, LLC</p>	<p>Stantec</p>	<p>6/6/2013</p>
	<p>PROJECT ADDRESS: 2819 E. 3RD ST. BLOOMINGTON, IN. 47401</p>	<p>PERMIT: BD</p>				
<p>PROJECT CAD FILE: 2079126760</p>	<p>STATE SITE CODE: 013-0064</p>	<p>DATE: 06/30/15</p>	<p>DESCRIPTION: ISSUED FOR PERMIT</p>	<p>REV: RD</p>	<p>BY:</p>	<p>DATE:</p>



McDonalds at 2819 E. 3rd Street. The landscaped stripe will be expanded.



View of the existing drive-through aisle and facing the drive-cut that connects to S. Pete Ellis Drive



Most of the parking on the left will be removed and a larger landscaped area will be added.



Existing drive-through aisle. The additional ordering station will be adjacent to this on the right side of the photo, parking to be removed is not visible in photo.