

CITY OF BLOOMINGTON



**NOVEMBER 7, 2011 @ 5:30 p.m.
COUNCIL CHAMBERS #115
CITY HALL**

**CITY OF BLOOMINGTON
PLAN COMMISSION AGENDA
Nov. 7, 2011 @ 5:30 p.m.**

❖ City Hall Council Chambers, #115

ROLL CALL

MINUTES TO BE APPROVED: Oct. 10, 2001

REPORTS, RESOLUTIONS AND COMMUNICATIONS:

- **GPP Update status report**

PETITIONS CONTINUED TO DEC 5 MEETING:

- PUD-20-11 McDoel Garden (First Capital Mgmt)**
1140 S. Morton St.
Preliminary Plan Amendment to allow multifamily residential usage in former Thomson/RCA PUD and final plan approval for a commercial building and 16 multifamily units. *(Case manager: James Roach)*
- SP-23-11 ERL-11, LLC**
626 N. Morton St.
Site plan approval to allow construction of a 40-unit apartment building.
(Case manager: Patrick Shay)

PETITIONS:

- ZO-27-11 Cheryl Underwood**
718-720 E. 8th St.
Rezoning request from Institutional (IN) to Residential Multifamily (RM).
(Case manager: Tom Micuda)
- PUD-28-11 NSSX Properties, LLC (Warehouse Community Center)**
1525 S. Rogers
An amendment to the preliminary plan to amend the list of uses on Parcel E of the Thomson PUD. Also requested is final site plan approval. *(Case manager: Eric Greulich)*
- PUD-29-11 Tommy and Lesli Berry**
1800 W. Tapp Rd.
Outline plan amendment to add gymnasium as a use to the Woolery PUD. *(Case manager: James Roach)*
- PUD-31-11 Woolery Ventures, LLC**
1800 W. Tapp Rd.
Preliminary and Final Plat approval for an 8-lot subdivision. *(Case manager: James Roach)*
- UV-30-11 Max & Gilda Lauchli**
545, 535 E. Southern Dr., 570, 580, 586 E. Hillside, 1506 S. Henderson St.
PC recommendation to the BZA re: Use Variances to allow multifamily units on the ground floor of a Commercial Limited (CL) zoning district and commercial use within a Residential Multifamily (RM) zoning district. *(Case manager: Patrick Shay)*

End of Agenda

***Next Plan Commission hearing scheduled for Dec. 5, 2011*

**BLOOMINGTON PLAN COMMISSION
PRELIMINARY REPORT
LOCATION: 718-720 East 8th Street**

**CASE #: ZO-27-11
DATE: November 7, 2011**

PETITIONER: Cheryl Underwood
718 East 8th Street, Bloomington, IN

REQUEST: The petitioner is requesting that the property be rezoned from Institutional (IN) to Residential Multifamily (RM).

BACKGROUND:

Area: 0.18 acres
Current Zoning: IN
GPP Designation: Core Residential
Existing Land Use: 5 multifamily units over 2 structures
Surrounding Uses: **North** – Residential; IU Trustees
South – Residential; IU Foundation
East – Residential; Private Ownership
West – History Department offices; IU Trustees

REPORT: The petitioner owns a 5-unit rental property located at the southwest corner of East 8th Street and Park Avenue. The property contains two structures and is registered for 5 multifamily units containing 9 bedrooms and a 14 person occupant load. The petitioner has a residence at this address. The current zoning of the property is Institutional (IN). The property is located in the University Courts subdivision, which is located at the eastern end of the Old Northeast neighborhood.

Between 1973 and 2007, the property was zoned medium density multifamily. The petitioner has been an owner or agent of the property since approximately 1993. In 2007, as part of the City's creation of the Unified Development Ordinance as well as the update of the City-wide zoning map, the property was rezoned from RM to IN. This zoning change was made for two principal reasons:

- 1) The property is located in an area designated by Indiana University as part of its campus master plan. The area, which is depicted in Exhibit #1, encompasses a portion of the Old Northeast Neighborhood and the University Courts subdivision. Between 7th Street, Indiana Avenue, 10th Street, and Woodlawn Avenue, Indiana University currently owns approximately 82 lots while approximately 39 lots are owned by private parties.
- 2) The block face on the south side of 8th Street, between Park and Fess, contains 5 parcels. Four of the 5 parcels are owned by Indiana University.

Given that the petitioner's site is located within the University's Master Plan area, the University is the dominant lot owner in this section of the Old Northeast

Neighborhood, and the University is the prevailing owner on many individual block faces, Planning staff opted to zone this property and some other privately held properties Institutional rather than Multifamily. Essentially, staff reviewed property ownership on a block by block basis. In cases where Indiana University ownership was more dominant, the block face was zoned Institutional. The petitioner argues, and staff acknowledges, that this zoning decision could have been made strictly by property ownership. In such case, Institutionally zoned property could have been limited strictly to those lots owned by Indiana University. However, staff followed a reasonable, systematic process to give deference to the Indiana University's master plan and dominant ownership in this area. An aerial map of the area with its current zoning designations can be found in Exhibit #2.

During the UDO code and zoning map update process, the Planning Department did not opt to notify all property owners who could potentially be affected by zoning map changes. Rather, staff relied on substantial media coverage, public open house meetings, and the lengthy public review process to give property owners an opportunity to bring up concerns about any potentially negative zoning map proposals. The petitioner was not specifically notified, nor did she know that the property was being rezoned from Residential Multifamily to Institutional.

The petitioner's rezoning request was initiated as part of an enforcement case. In fall of 2010, City staff and the County Building Department discovered that two bedrooms each were added to two of the five units on the property. These bedrooms were added without any permit or approvals from City Planning, HAND, and County Building.

From a zoning perspective, this was a violation under either Institutional or Multifamily zoning. With the current Institutional zoning, residential uses are not permitted. Therefore, the creation of more bedrooms in two of the units constituted an illegal expansion of a lawful nonconforming use. If the property was still zoned Residential Multifamily, residential use is clearly permitted. However, the five units on 0.18 acre translate to a density of 28 units per acre. RM zoning allows 7 units per acre of density. Although the property was clearly grandfathered for the 5 existing units and 9 bedrooms, the addition of 2 bedrooms to 2 of these units required zoning approval. Such approval would never have been granted.

While this zoning violation is still being contested in the legal process, the petitioner has opted to exercise her right to rezone the property even though staff has indicated that the additional bedrooms are still illegal and would not have been permitted under either zoning designation.

GROWTH POLICIES PLAN: The GPP designates this lot as Core Residential. A GPP and zoning map for this particular area can be found in Exhibit #3. Obviously, there are discrepancies in how the zoning designations correspond to

the GPP recommended land use designations of Core Residential and Public/Semi-Public/Institution. However, this should not be surprising or of concern to the Plan Commission because the actual property usage is still residential in nature. The area within the western edge of the IU Campus Master Plan and the eastern edge of the Old Northeast Neighborhood has always been a fine grained mix of public versus private ownership, private rental property and IU rentals, and a mix of institutional style buildings and older historic homes. With the University's ownership in the area gradually increasing over the years, determining Institutional versus Multifamily zoning has always been a difficult zoning decision to make for the Planning staff. If these zoning decisions are only based on property ownership, the zoning map would become quickly out of date once properties change from private to public ownership (or visa versa). A comparison map showing how the area was zoned prior and after the 2007 UDO adoption can be found in Exhibit #4.

In order to aid the Plan Commission's decision making to determine the appropriate zoning for this property, staff is including the following text from Page 30 of the GPP (Core Residential Land Use Policy).

Land Use

The predominant land use for this category is single family residential; however, redevelopment has introduced several uncharacteristic uses such as surface automobile parking, apartments, offices, retail space and institutional activities. This district is designed primarily for higher density single family residential use. The existing single family housing stock and development pattern should be maintained with an emphasis on limiting the conversion of dwellings to multi-family or commercial uses, and on encouraging ongoing maintenance and rehabilitation of single family structures. Multi-family (medium and high-density) residential and neighborhood-serving commercial uses may be appropriate for this district when compatibly designed and properly located to respect and compliment single family dwellings. Neighborhood-serving commercial uses, and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations. More specific land use policies include:

- *Allow multi-family redevelopment along designated major streets, in transition areas between the downtown and existing single family residential areas, and when appropriately integrated with adjacent uses per adopted form district requirements.*
- *Explore opportunities to introduce nodes of appropriately designed, neighborhood scaled commercial uses within the core neighborhoods.*
- *Discourage the conversion of single family homes to apartments.*

From reviewing these recommendations, it's clear from staff's point of view that the Core Residential policy guidance simply doesn't speak to the zoning decision

in question. Because of this, staff recommends that the Commission consider the following issues when making this recommendation to the City Council:

- 1) Did the Planning staff follow a logical and transparent process when determining Institutional versus Residential Multifamily zoning in this area of the community and for the property under consideration? Staff believes that a reasonable zoning decision was made.
- 2) Should the zoning designation for this property and others in this area be based on public versus private ownership, or, conversely, should it be made based on dominant public sector ownership and through reasonable deference to the Indiana University Master Plan? In this case, there are arguments for both approaches. While staff wishes to get guidance from the Plan Commission, we also believe that the University's dominant ownership and Master Plan are factors to consider.
- 3) Does the zoning decision and requested solution have a significant affect on the petitioner's property rights? On this issue, staff points out that the petitioner's 5-unit rental property is approximately four times more dense than what could be developed under the requested RM zoning. This is a 'grandfathered' property whose use and current density can be continued indefinitely by both the petitioner and any future owners. However, the addition of bedrooms to a property with an excessive density would not be permissible under either zoning designation.

RECOMMENDATION: Staff recommends that this rezoning request be forwarded to the December 5 Plan Commission meeting.

Cheryl L. Underwood
825 N Walnut St, Suite A
Bloomington, IN 47404
(812)334-0094
cunderwo@homefinder.org

6 October 2011

Planning & Zoning Department
City of Bloomington
401 N Morton St
Bloomington, IN 47404

To Whom It May Concern:

Is it my wish that my property at 718-720 E 8th be returned to its original zoning of Residential-Multi-Family. It was the only property of all the privately owned properties on 8th St between Fess & Woodlawn that was rezoned to INSTITUTIONAL. The intent of INSTITUTIONAL zoning as stated in the Unified Development Ordinance is "for publicly owned properties."

Sincerely,

Cheryl L. Underwood

Cheryl L. Underwood

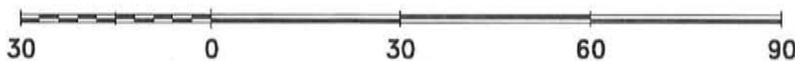
ZO-27-11 Petitioners Statement

received by
10/6/11 *Phoebe B. Miller* 5



20-27-11 Site Plan

By: micudat
6 Oct 11



For reference only; map information NOT warranted.

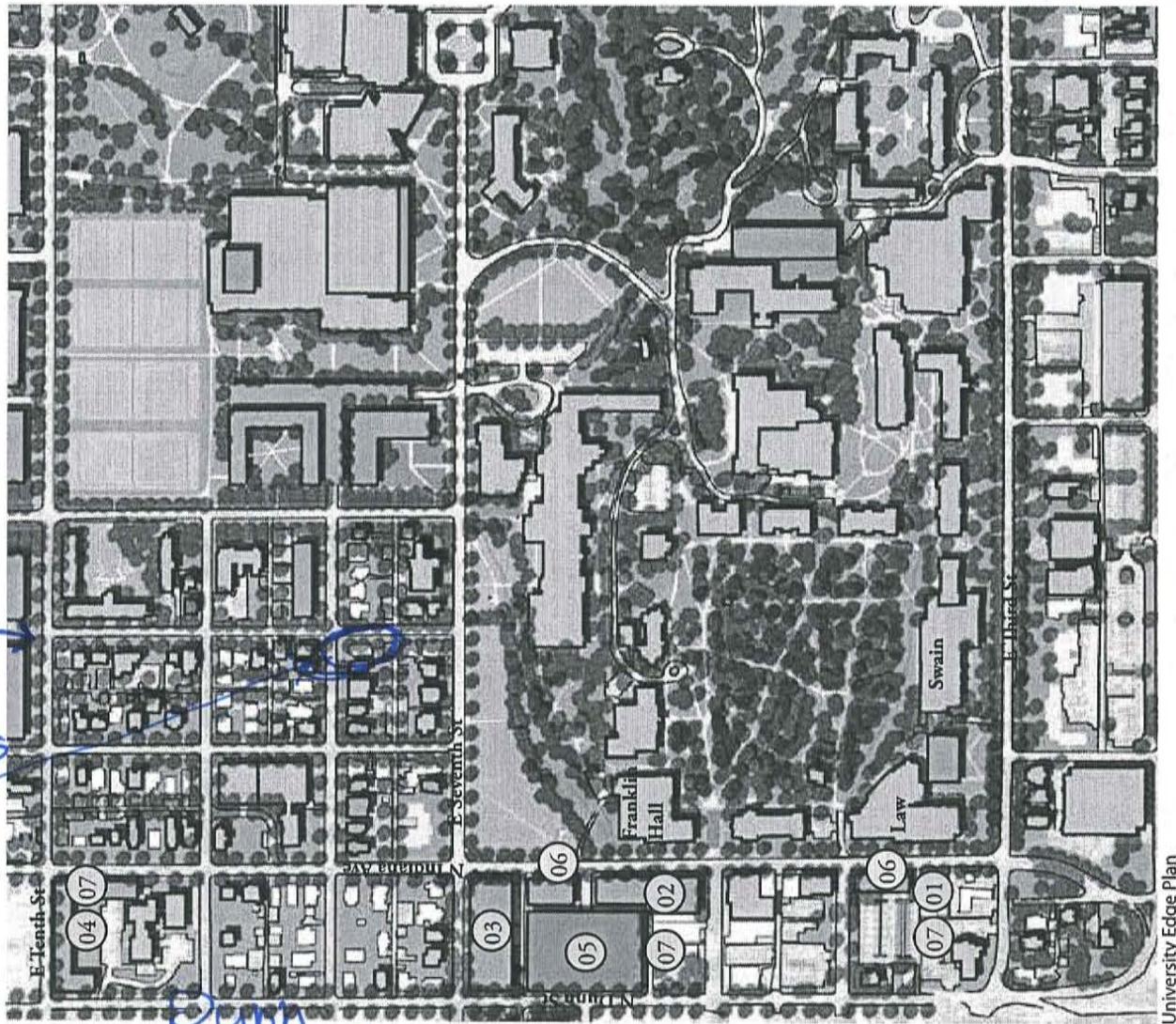


City of Bloomington
Planning



Scale: 1" = 30'

Park Ave
 Petitioner's Site



Dunn Street
 (western boundary of Master Plan)

Exhibit #1
 IU Master Plan Map for
 Westside Campus area

- KEY
- Existing Building
 - Building Opportunity
 - Parking Opportunity

University Edge Plan

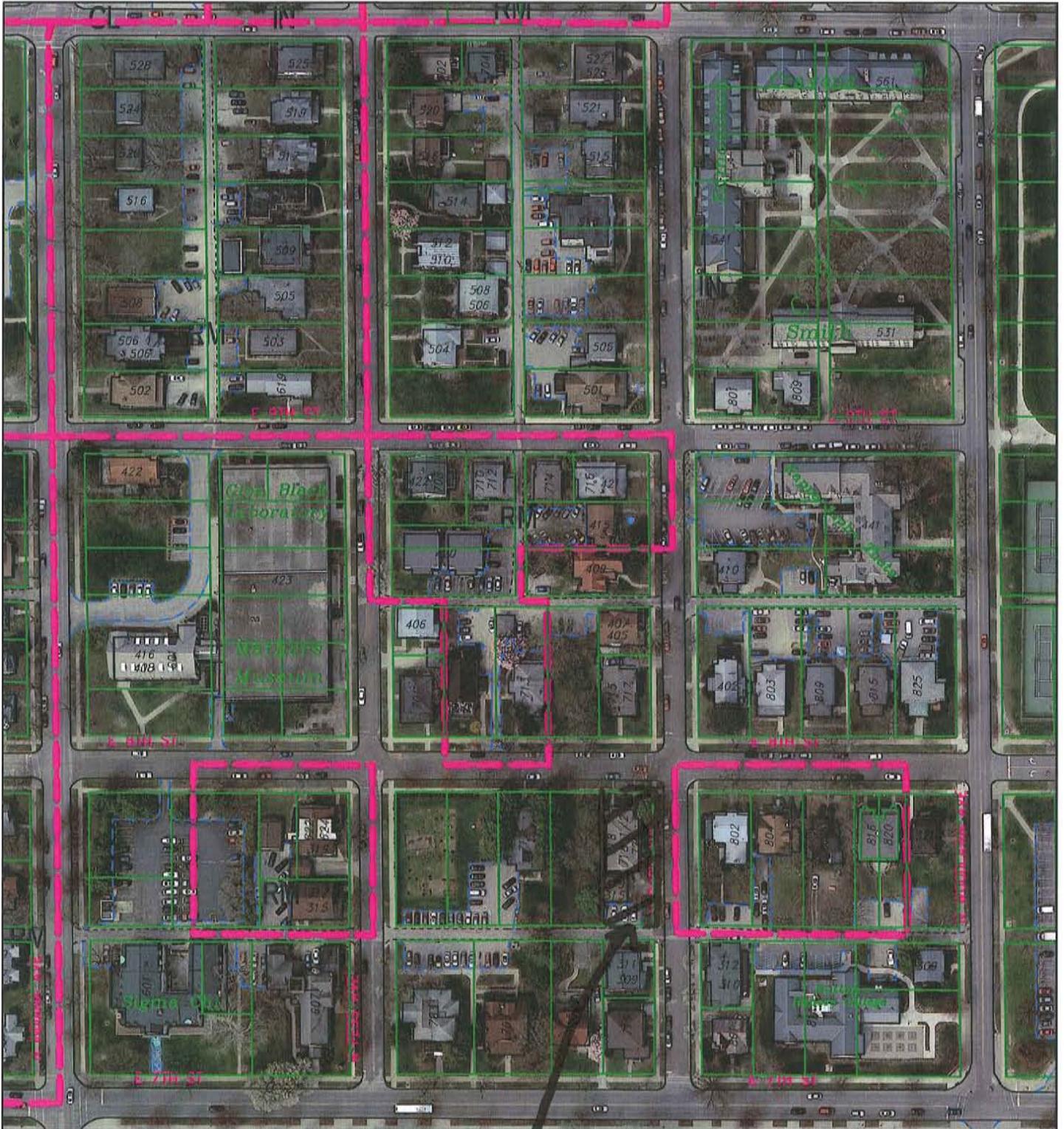
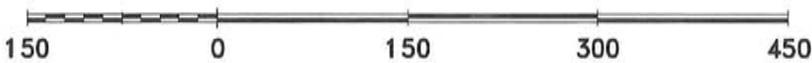


Exhibit # 2
 20-27-11 Aerial & Zoning Map

site

By: micudat
 3 Nov 11



City of Bloomington
 Planning



Scale: 1" = 150'

For reference only; map information NOT warranted.

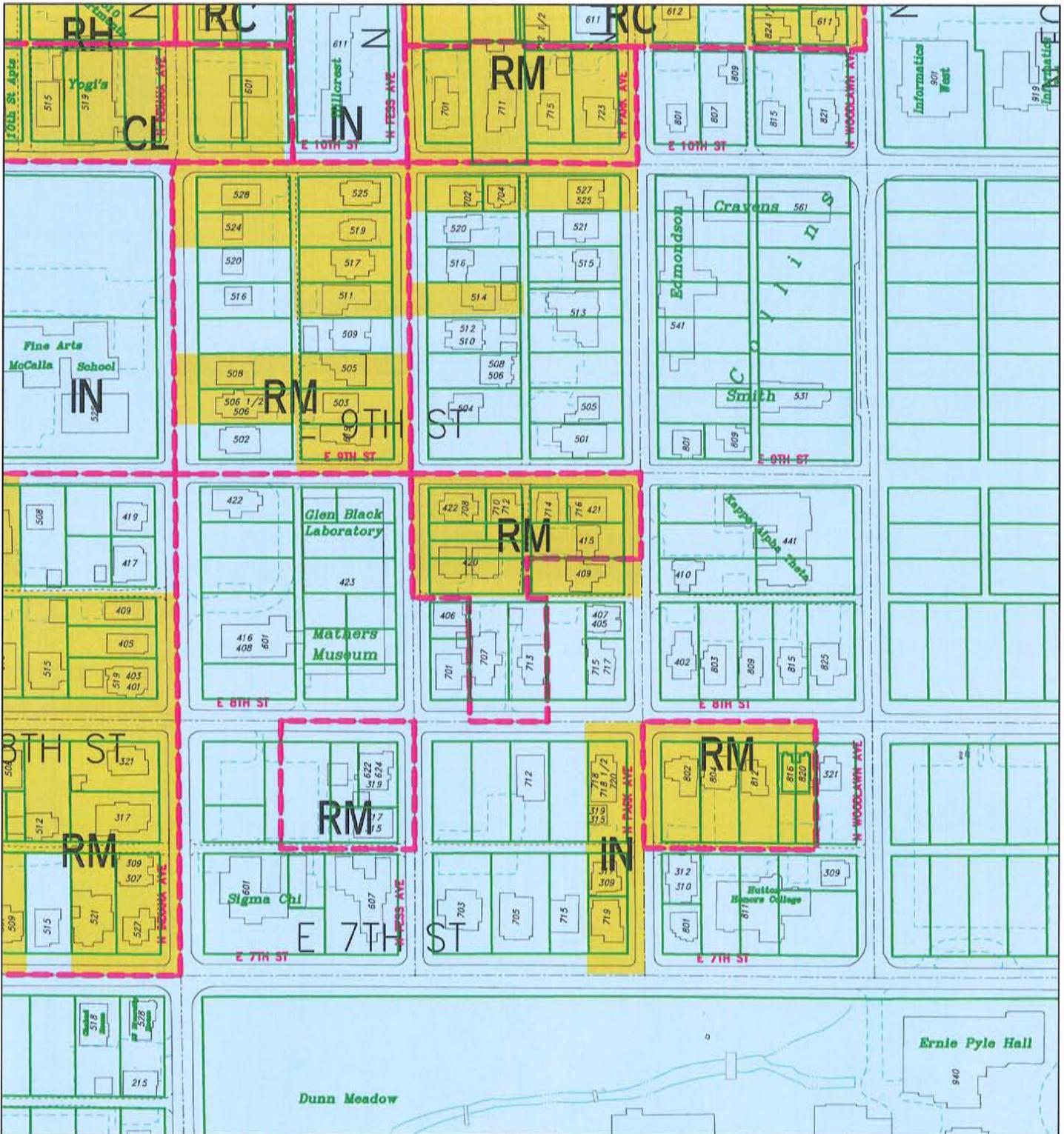


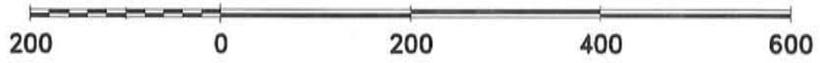
Exhibit # 3

Zoning and GPP Map

Zoning boundaries marked by dotted lines
 Core Residential shaded in yellow
 Institutional shaded in blue

By: micudat

3 Nov 11



City of Bloomington
 Planning



Scale: 1" = 200'

For reference only; map information NOT warranted.

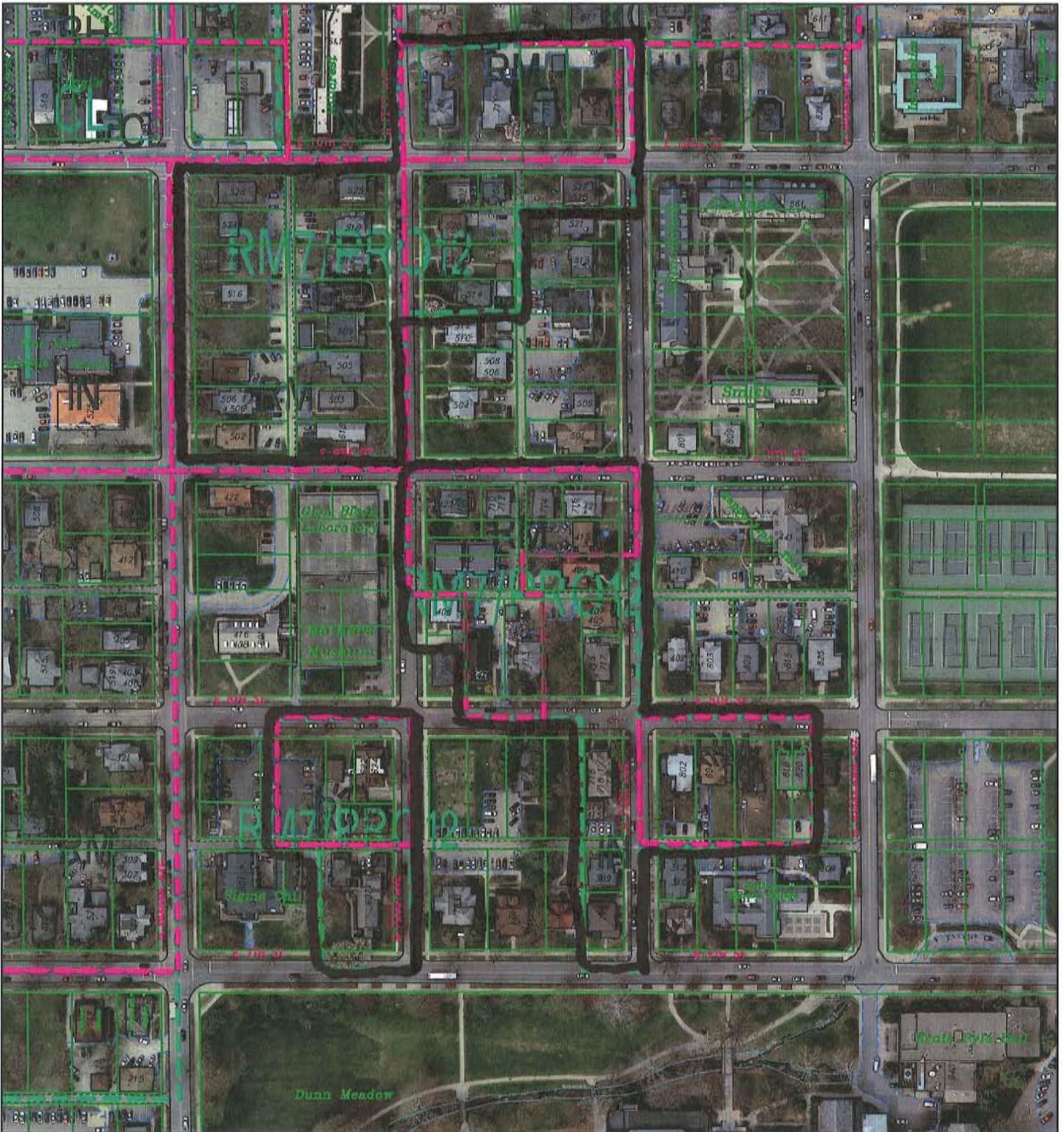
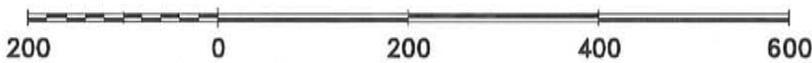


Exhibit #4 - Zoning Comparison
 Black Areas - Pre-UBD zoning
 Dotted Pink Areas - New Zoning

By: micudat
 3 Nov 11



City of Bloomington
 Planning



Scale: 1" = 200'

For reference only; map information NOT warranted.

PETITIONER: Warehouse Community Center
1525 S. Rogers Street, Bloomington

COUNSEL: Michael L. Carmin
400 W. 7th Street, Bloomington

REQUEST: The petitioner is requesting a preliminary plan amendment to amend the list of uses within Tract E of the Thomson Planned Unit Development. Also requested is a PUD Final Plan approval.

SITE INFORMATION:

Lot Area: 8.56 Acres
Current Zoning: Planned Unit Development (Thomson PUD)
GPP Designation: Employment Center
Existing Land Use: Warehouse/Offices
Proposed Land Use: Community Center
Surrounding Uses: North – Single Family (McDoel Gardens neighborhood) & Industrial
South – Warehouse/Semi-tractor storage
East – B-Line Trail, Commercial and Residential
West – Irving Materials

REPORT: The petition site is located on Tract E of the Thomson PUD and has been developed with a 200,000 sq. ft. warehouse. The property is surrounded by industrial uses to the west, south, and north with the McDoel neighborhood also to the north and the B-Line Trail/Switchyard property to the east. The property has several large trees on the south side of the property along the West Branch of Clear Creek. The floodplain of the West Branch of Clear Creek and Clear Creek encroaches along the west, south, and east sides of the property. The building itself is not located in the floodplain.

The petitioner is seeking approval to allow the redevelopment of a portion of Tract E of the Thomson Planned Unit Development also known as the Indiana Enterprise Center. This PUD was created in 1998 by the City to help guide future redevelopment of the Thomson Consumer Electronic site that had recently closed. The intent of this PUD was to recognize the former industrial use of the property and create incentives to redevelop this area with employment and ancillary uses.

Since that time, the PUD has slowly developed to reuse several existing buildings (Cook Pharmica, Upland/Indiana Warehouse, Schulte) and construct new buildings such as The McDoel Building (Sweetgrass, Clendening Johnson & Bohrer), Best Beers, Social Security, and two medical office buildings. Several public investments have also been made to the area, including street

construction within the PUD, streetscape improvements along Rogers St, and riparian buffer improvements, all designed to help promote development in the area. Additional public improvements in the area include the recently completed Phase 2 of the B-Line Trail that runs along the east side of this property. Additional improvements and plans for the recently acquired Switchyard property are being developed through a master plan process being conducted by the City.

The petitioner is requesting to amend the list of uses for this property, located within Tract E, to allow for a new community center. In addition to allowing a community center, the proposed list of uses has also been expanded to allow retail, office, and recreational uses within the building or on the property. Also requested is final plan approval of the community center including a 213 space parking lot and related site improvements.

With this petition there would be substantial improvements to the interior and exterior of the building. Exterior building improvements would consist of new siding and finishing materials on all four sides, the addition of a tower structure on the west side of the building, and construction of a theatre and fly loft on the east side of the building. Additional site improvements include installing parking and landscaping, as well as the installation of rain gardens to provide stormwater quality and detention requirements. A permit from the Department of Natural Resources for any work within the floodplain is required.

The building would be used as a community center with a wide range of services and amenities. Interior features include a 1,500 seat theater/place of worship, approximately 30,000 sq. ft. of office space, skateboard park, bocce ball court, soccer court, basketball court, climbing wall, day care center, and several retail spaces. An outdoor patio area is being created around the retail spaces on the southeast corner of the building facing the B-Line trail. An outdoor stage is proposed on the east side of the building facing the B-Line trail that is connected to and extends from the internal stage to provide an opportunity for events utilizing the adjacent park property.

Growth Policies Plan: This property is located within the Employment Center land use category of the Growth Policies Plan (GPP). The GPP states that Employment centers should be located in close proximity or contain commercial and housing opportunities to minimize the traffic generated by their employment base. (page 37)

The GPP also notes that Employment Centers should include “supporting commercial uses” and the commercial uses should be “integrated within an employment center [and be] at a scale that services the employment center but does not generate significant additional business from the community at large.” (page 37)

The GPP specifically notes that “former Thomson property” is an important site for redevelopment. (page 21) The GPP’s “McDoel Switchyard Subarea” states that the City should “promote mixed-use development adjacent to the rail corridor that encourages retail services, new housing opportunities, and recreational

amenities.” It goes on to recommend that “In order to beautify the trailway, [the City should] explore redevelopment opportunities of industrial sites along the Morton Street corridor.” (page 66)

PUD PRELIMINARY PLAN REVIEW ISSUES:

Permitted Uses: The approved PUD anticipated that Tract E would be developed with mostly industrial or office uses. Therefore, the 1998 permitted use list included a narrow range of industrial uses. Because this PUD was adopted under the previous zoning ordinance, the list of permitted uses does not match the current UDO use names. The petitioner has worked with Staff to develop a use list using the current UDO use names that includes a wider range of commercial uses. The use list included with this petition would replace the list of uses originally approved in the PUD for this property. The list of proposed permitted uses was chosen to avoid potential conflict with the adjacent single family residences as well as to fit with the future public park on the CSX property.

Development Standards: The development standards used in the original PUD for height, bulk, density, and setbacks were either the existing conditions or the applicable development standard set forth in the PUD for that use, whichever is the lesser. Since some of the uses and zoning districts used in the Thomson PUD are no longer present in the UDO, the petitioner is updating and expanding this section for this property specifically. With this petition, the development standards on this property would be those of the CG district, unless stated otherwise in the preliminary plan.

ROW Dedication: With this petition, there would be 40’ of right-of-way dedicated along Rogers Street. A 5’ wide concrete sidewalk and street trees are required as well.

Floodplain: This property is at the confluence of two floodplains that come together at the south end of the property. The floodplain of Clear Creek runs along the east side of the property and the floodplain of the West Branch of Clear Creek is to the south and west. The PUD anticipated redevelopment of the areas within the floodplain and required that all necessary local, state, and federal permits be obtained prior to work within the floodplain. A previous approval was granted to allow an even larger parking lot than proposed by this petition. That parking lot was never constructed.

SITE DESIGN/PUD FINAL PLAN REVIEW ISSUES:

Stormwater: The petitioner has submitted drainage and utility plans to City of Bloomington Utilities for review. The only major increase of impervious surface coverage on the property will be from the new parking area on the south side of the property. The petitioner is proposing several interior rain gardens to provide stormwater quality improvements and detention requirements.

Signage: The petitioner has proposed sign limitations for exterior wall signs. No box signs are allowed for external signage. In addition, external illumination for

wall signs will be prohibited. All wall signs must be internally illuminated or utilize back-lit lettering.

Architecture: The petitioner is proposing to refinish the entire exterior of the building on all four sides. New siding and finishing will be installed and will consist of limestone, brick, horizontal and vertical corrugated metal with split face block around the foundation. A list of allowable exterior materials has been proposed in the preliminary plan.

Parking: The Thomson PUD recognized the constraints on this property in regards to the large warehouse building and adjacent creeks and floodplain. The petitioner has worked with staff to provide a 25' riparian buffer from the top of the bank of the creek and the adjacent parking. The petitioner has designed a parking area that provides a total of 213 parking spaces on the site. Permeable pavers will be utilized for 43 of the parking spaces to reduce stormwater detention requirements and improve water runoff quality. The petition will also be utilizing buses to provide transportation for special events. In addition, the petitioner has contacted some of the adjacent property owners about the possibility of leasing parking spaces when necessary.

Height: The standard height for the CG districts is 50 feet. The petitioner is proposing to amend this limit to allow for a 55' tower on the west side of the building and a 74' tall flyloft for the theatre on the east side of the building. Staff requests guidance from Plan Commissioners on whether this proposed height increase is appropriate in a location that is approximately 70' from the B-Line Trail.

Impervious Surface Coverage: The property will have approximately 78% impervious surface coverage after development.

Landscaping: The Thomson PUD specifically stated that "due to the necessity to gain every available parking space on this parcel, landscaping opportunities will be limited. Perimeter parking lot landscaping/screening shall be installed where feasible, given site constraints. Parking lot landscaping code requirements are waived, given site constraints." The petitioner has submitted a landscape plan that places as much landscaping as possible around the site. Staff would recommend additional landscaping between the parking area and driveway on the west side of the property to buffer the view from Rogers Street and a future B-Line trail spur.

QUESTIONS FOR PLAN COMMISSION/GUIDANCE FOR SECOND HEARING:

1. **Uses** – Does the Plan Commission agree with the list of permitted uses that has been submitted for this parcel? Should there be a cap on the maximum amount of space for an individual retail space?
2. **Architecture** – Is the proposed architecture appropriate? Does the Plan Commission have concerns regarding the height of the building and specifically the theater flyloft?

3. Final Plan – Should final plan approval be given now or delegated to Staff?

PRELIMINARY CONCLUSIONS: Overall staff is supportive of the proposed use and modifications proposed. Staff finds the redevelopment of this property will greatly improve the look of the building and property from the B-Line trail. Leveraging the B-Line Trail as an economic development tool is an extremely important goal for the City.

RECOMMENDATION: Staff recommends forwarding this petition to the required second hearing.

MEMORANDUM

Date: October 27, 2011
To: Bloomington Plan Commission
From: Bloomington Environmental Commission
Through: Linda Thompson, Senior Environmental Planner
Subject: PUD-28-11: Warehouse Community Center

This memorandum contains the Environmental Commission's (EC) recommendations regarding a request to amend the list of acceptable uses in Parcel E of the Thomson Area Planned Unit Development (PUD), and also a request for Final Site Plan approval. The EC recommends denial of these requests for many reasons. Below, please find listed the major reasons for the EC's decision.

- 1.) The entire site outside of the building's foundation sits within a Special Flood Hazard Area, specifically a floodway, based on the Federal Emergency Management Agency (FEMA) Digital Flood Insurance Rate Maps (DFIRM), which restricts the uses allowed by the City of Bloomington Indiana, the Indiana Department of Natural Resources, and the Federal Emergency Management Agency.
- 2.) A Construction Within a Floodway Permit has not yet been granted by the DNR. Additionally, a specific PUD District Ordinance for development in a floodplain (the rules for what can and cannot be done within the floodplain in Tract E(a) of the Thomson PUD), has not yet been approved by DNR and FEMA. Furthermore, after DNR and FEMA approval, the Bloomington City Council will also have to approve the PUD District Ordinance. To approve this Final Site Plan now, which may go through changes from multiple organizations or possibly not get state or local approval at all, does not seem prudent.
- 3.) The EC is opposed to allowing the petitioner to clear-cut a wooded floodplain in order to construct a parking lot. The site affords no room for any type of tree replacement ratio to make up for all those removed for the parking. The warehouse is sufficiently large enough to create parking inside of it. If the requested new uses cannot accommodate indoor parking and must have the parking in the floodplain, then the EC recommends denial of the use amendment.
- 4.) No investigations for hazardous substances or history of this aging factory/warehouse have been provided for this Brownfield Site. The EC has no knowledge of what sort of activities or storage has occurred throughout the years here, and believe a Phase 1 Environmental Site Assessment (ESA) Report (in accordance with American Society for Testing (ASTM) "Standard Practice for Environmental Site Assessments: Phase 1 Environmental Site Assessment Process" E 1527-05) should be conducted before any uses can be determined. The purpose of an ESA is to conduct due diligence activities to determine the presence or likely

presence of any hazardous substances or petroleum products on the property, inside the building, outside, in groundwater, etc that indicate an existing release, a past release, or a material threat of a release, and to determine if the site is a Brownfield Site. A Brownfield Site is defined as real property, the expansion, redevelopment, or reuse of which may be complicated by the presence or potential presence of a hazardous substance, pollutant, or contaminant (Section 101 of the Comprehensive Environmental Response, Compensation, and Liability Act of 1980 (42 U.S.C. 9601) as amended). The allowed uses could be limited dependant on what is found and what level of potential cleanup is required.

EC RECOMMENDATION:

The EC recommends denial of the Site Plan and denial of the PUD Amendment if the Site Plan remains as it is.

PUD PRELIMINARY PLAN AMENDMENT AND FINAL SITE PLAN STATEMENT

Petition

NSSX Properties, LLC (“Petitioner”) petitions for preliminary plan amendment to the Thomson PUD and final Site Plan approval for redevelopment of the real estate at 1525 S. Rogers Street (“Real Estate”).

Current Use

The Real Estate is an existing warehouse building with associates parking located on 8.56 acres.

Current Zoning

The Real Estate is a part of Parcel E, Thomson PUD. Permitted uses in the Thomson PUD Plan for Parcel E are limited and generally relate to industrial uses.

Petitioner’s Use

Petitioner intends to remodel and renovate the existing warehouse building to a community center with additional mixed uses.

Changed Conditions

The major part of the Thomson PUD is located west of Rogers Street. Parcel E is east of Rogers Street. Parcel E and surrounding areas are not developing or being redeveloped for industrial uses. The area south of the Real Estate remains existing warehouse facilities primarily used in past years for a trucking, warehousing and transportation center. North of the Real Estate is the property recently acquired and remodeled by Community Kitchen. North of the Community Kitchen are mixed uses, including a recently developed commercial building with a restaurant. East of the Real Estate is the B-Line Trail and east of the trail is a large parcel owned by Parks & Recreation. The redevelopment of properties east of Rogers Street, particularly with the creation of the B-Line Trail, are tending to mixed uses more consistent with a Commercial Arterial Zone.

Petitioner’s Redevelopment of the Warehouse

Petitioner will remodel and redevelop the warehouse building into a covered mall type arrangement allowing for interior offices, recreational uses, community center uses, restaurant and other compatible uses. Petitioner will preserve and adapt the existing warehouse building for the mixed commercial and related uses. The project will be known as The Warehouse. Petitioner proposes to reserve the core character of the building as a warehouse in the selection of exterior materials and preserving most of the existing roof line and exterior features. The existing building and site conditions limit and restrict opportunities to redesign the site. The Real Estate is bordered on the south by an existing stream. On the east is the B-Line Trail and on the west is the Rogers Street right-of-way.

Adaption of the existing building will include raising the height of a portion of the roof line on the existing building to accommodate the interior modifications for a mall design and pedestrian corridor and a proposed stage and recreational area.

PUD Amendments

1. Permitted Uses: The following list of permitted uses is extracted from the table of permitted uses for Commercial General Zone (omitting or deleting selected permitted uses in the CG zone deemed incompatible for this location).

- antique sales
- apparel and shoe sales
- art gallery
- artist studio
- arts/crafts/hobby store
- assisted living facility
- bank/credit union
- banquet hall
- barber/beauty shop
- bicycle sales/repair
- billiard/arcade room
- bookstore
- bowling alley
- brewpub
- business/professional office
- cellular phone/pager services
- community center
- computer sales
- convenience store (without gas)
- copy center
- day-care center, adult
- day-care center, child
- drugstore
- dry-cleaning service
- dwelling, upper floor units
- fitness center/gym
- fitness/training studio
- florist
- gift shop/boutique
- government office
- government operations (non-office)
- grocery/supermarket
- group care home for developmentally disabled*
- group care home for mentally ill*
- group/residential care home*
- hardware store
- health spa
- jewelry shop
- library
- license branch
- lodge
- miniature golf

- museum
- music/media sales
- musical instrument sales
- nursing/convalescent home
- park
- pet grooming
- pet store
- photographic studio
- place of worship
- police, fire or rescue station
- radio/TV station
- recreation center
- research center
- restaurant
- restaurant, limited service
- retail, low-intensity
- school, preschool
- school, primary/secondary
- school, trade or business
- shoe repair
- skating rink
- social service
- sporting goods sales
- tailor/seamstress shop
- tanning salon
- theater, indoor
- video rental

2. Design Standards: Adopt the commercial general design and development standards as applicable to the Real Estate, except as follows:

- a. Building height. The 50-foot maximum building height increased to 74 feet for a portion of the roofline of the existing building and tower structure as depicted on Petitioner's Development Plan.
- b. Riparian Buffer to be 25 feet measured from the top of the stream bank closest to the Real Estate.
- c. Exterior Finish Building Materials shall consist of limestone, masonry or brick, painted steel, cedar or other wood materials, and glass block. Split face cmu is restricted for use on exposed foundation walls. Synthetic stucco is restricted for use in sign face/panel areas.
- d. Signs. No box signs will be permitted. No exterior illumination exclusive for signs. Sign letters to be internally illuminated or backlit lettering.

NSSX Properties, LLC is the owner of the real estate located at 1525 S. Rogers Street, described as:

Tract 1

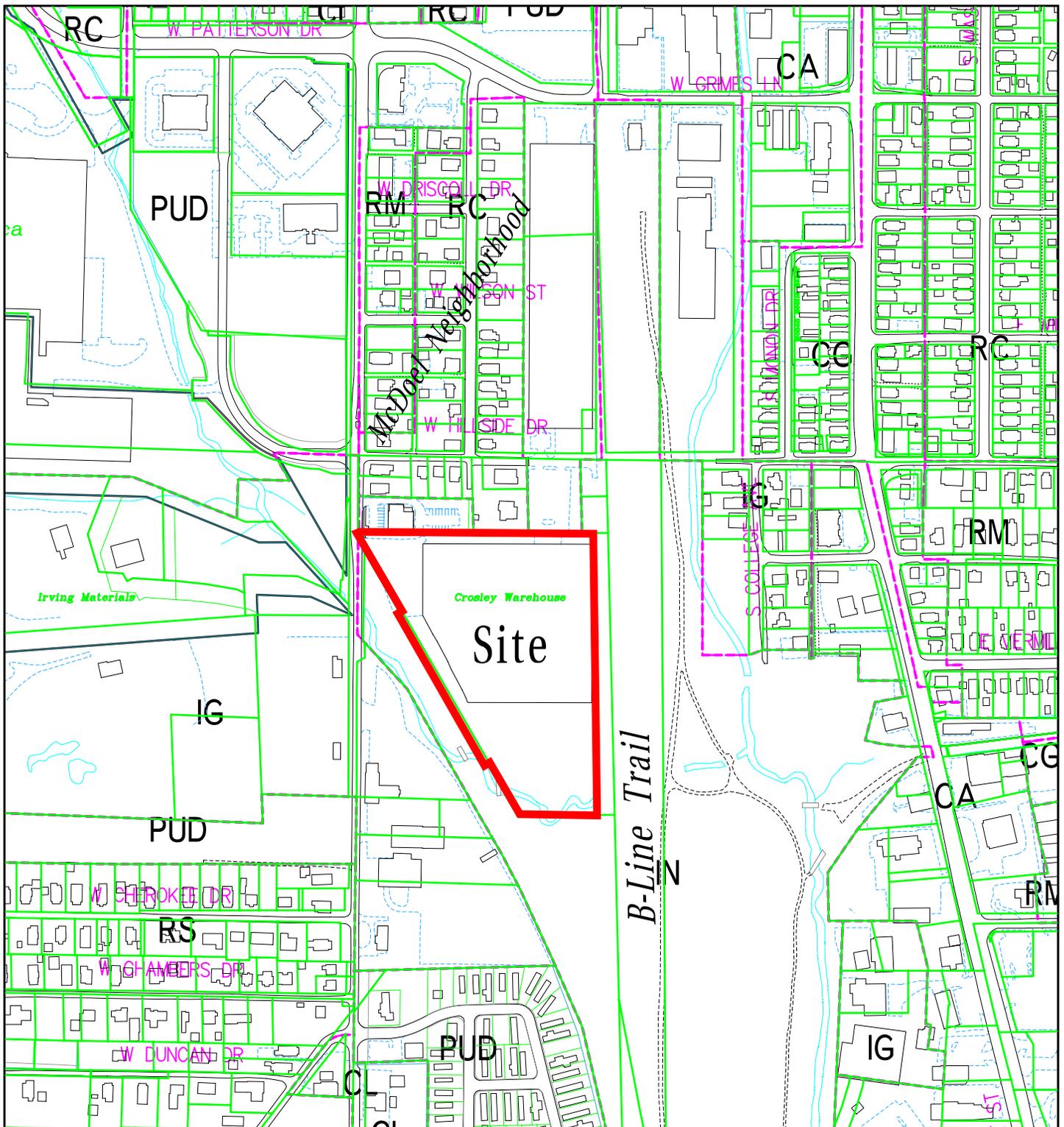
A part of the Northeast quarter of the Northeast quarter of Section 8, Township 8 North, Range 1 West, Monroe County, Indiana, described as follows:

Beginning at a point which is 220.5 feet South and 37.0 feet west of the Northeast corner of the aforesaid quarter-quarter, said point being 7 feet West of the West right-of-way of the Monon Railroad and on the South line of the property deeded to A. Helton Pauley and John L. and Lucretia H. Shirley, thence South, over and along a line 7 feet West and parallel to the West right-of-way line of the Monon Railroad, for a distance of 580.0 feet, thence East for a distance of 7 feet, and to the West right-of-way of the Monon Railroad, thence South, over and along the West right-of-way line of the Monon Railroad, for a distance of 222.5 feet, thence West for a distance of 218.9 feet and to the East right-of-way of the Illinois Central Railroad, thence North 31 degrees and 16 minutes West, over and along the East right-of-way of the Illinois Central Railroad for a distance of 933.7 feet, and to the centerline of South Rogers Street, thence North, over and along the centerline of South Rogers Street, for a distance of 7 feet, thence East, over and along the South line of the property deeded to A. Helton Pauley and John L. and Lucretia H. Shirley, for a distance of 697.5 feet, and to the place of beginning.

Tract 2

A parcel of land located in the Northeast Quarter of the Northeast Quarter of Section 8, Township 8 North, Range 1 West of the Second Principal Meridian, Monroe County, Indiana, more particularly described as follows:

Beginning at a point which bears South 31 degrees 16 minutes East a distance of 250 feet from a point which is 7 feet south, as measured along the Center line of Rogers Street, of the intersection of the north line of the Arrow Construction Company land, formerly owned by Mary Burke, deceased, and said center line of Rogers Street; thence South 58 degrees 44 minutes West a distance of 15 feet; thence South 31 degrees 16 minutes East a distance of 500 feet; thence North 58 degrees 44 minutes East a distance of 15 feet; thence North 31 degrees 16 minutes West a distance of 500 feet to the point of beginning, containing an area of 7500 square feet, more or less.



PUD-28-11 Warehouse Community Center

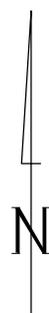
1525 S Rogers Street

Plan Commission

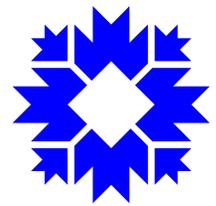
Site Location, Zoning, Parcels, Land Use

By: greulice

3 Nov 11

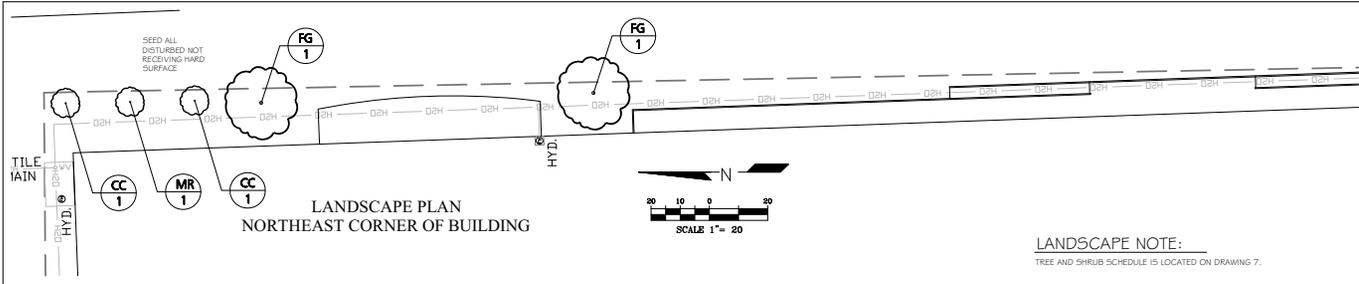


City of Bloomington
Planning



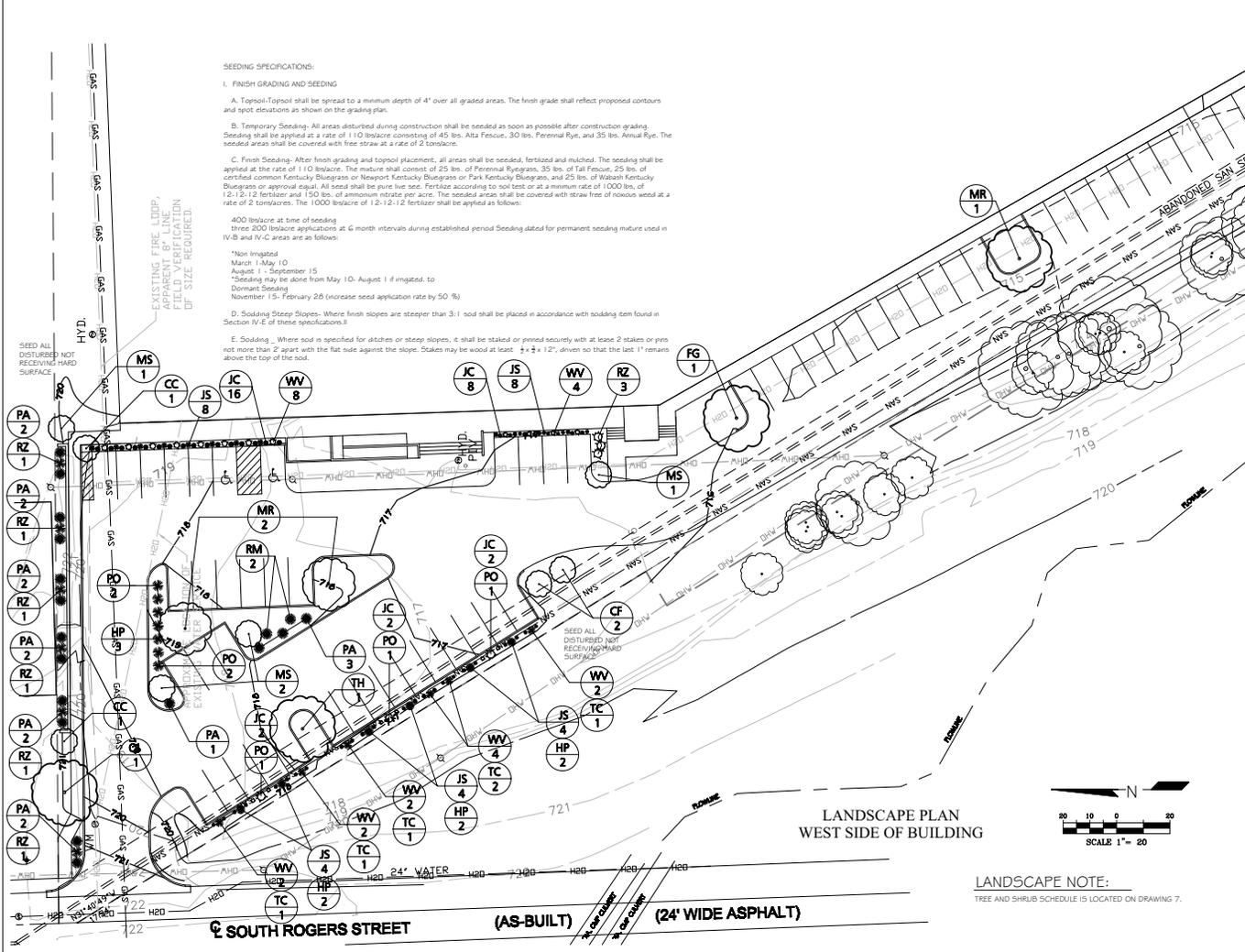
Scale: 1" = 400'

For reference only; map information NOT warranted.



LANDSCAPE PLAN
NORTHEAST CORNER OF BUILDING

LANDSCAPE NOTE:
TREE AND SHRUB SCHEDULE IS LOCATED ON DRAWING 7.



LANDSCAPE PLAN
WEST SIDE OF BUILDING

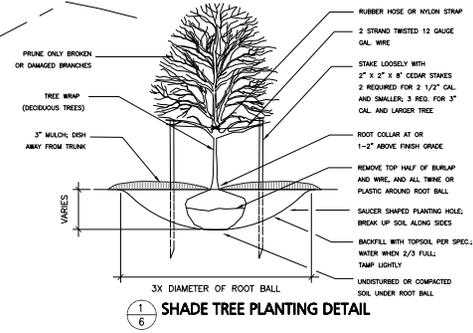
LANDSCAPE NOTE:
TREE AND SHRUB SCHEDULE IS LOCATED ON DRAWING 7.

GENERAL SITE NOTES

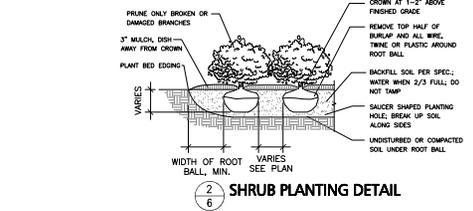
1. THE CONTRACTOR SHALL LOCATE ALL UTILITIES AND VERIFY SITE CONDITION INFORMATION ON DRAWINGS PRIOR TO STARTING WORK. PROMPTLY REPORT ANY DISCREPANCIES OR DEVIATIONS FROM THE INFORMATION SHOWN ON THE PLAN TO DESIGNER AND OWNER. THE OWNER IS NOT RESPONSIBLE FOR UNAUTHORIZED CHANGES OR EXTRA WORK REQUIRED TO CORRECT UNREPORTED DISCREPANCIES.
2. THE CONTRACTOR SHALL SUPPLY ALL PLANT MATERIAL IN QUANTITIES AND CONDITIONS SUFFICIENT TO COMPLETE THE PLANTING AS SHOWN ON THE LANDSCAPE PLAN. PLANT MATERIAL QUANTITIES SHOWN ON PLAN TAKE PRECEDENCE TO THOSE ON THE PLANT LIST.
3. ALL PLANT MATERIAL SHALL CONFORM TO ANSI Z 60-1996 AND CURRENT ASSOCIATION OF AMERICAN NURSERYMEN STANDARDS. NO PARK GRADE MATERIAL SHALL BE ACCEPTED.
4. SPECIMEN SIZES INDICATED ON PLANT SCHEDULE ARE MINIMUM ACCEPTABLE SIZES. LARGER SPECIMENS MAY BE UTILIZED.
5. ALL PLANTING MASSES TO BE CONTAINED WITHIN MULCH BEDS AND RECEIVE 4" THICK, SHREDED, HARDWOOD MULCH OVER WEED BARRIER. ALL TREES OUTSIDE PLANTING BED AREAS SHALL RECEIVE A 5" MIN. DIAMETER MULCH RING AT BASE.
6. ALL LANDSCAPED AREAS NOT MULCHED SHALL BE SEEDED OR SOODED AS INDICATED, UNLESS NOTED OTHERWISE. ALL OTHER AREAS DISTURBED, GRADED OR OTHERWISE MODIFIED BY NEW CONSTRUCTION SHALL RECEIVE 6" OF TOP SOIL AND SEED. SEE SPECIFICATIONS FOR SEEDING REQUIREMENTS.
7. ALL PLANT MATERIAL SHALL HAVE THE SAME RELATIONSHIP TO FINISH GRADE AS THE PLANTS ORIGINAL GRADE PRIOR TO DIGGING. ALL PLANTS SHALL BE SET PLUMB, UNLESS NOTED OTHERWISE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASSURE PLANTS REMAIN PLUMB UNTIL THE END OF CONTRACTED GUARANTEE PERIOD.
8. ALL PLANTINGS SHALL BE THOROUGHLY WATERED DURING THE FIRST 24 HOUR PERIOD AFTER PLANTING TO ENSURE ALL AIR POCKETS ARE REMOVED AROUND ROOT BALL.
9. CONTRACTOR IS RESPONSIBLE FOR WATERING AND MAINTAINING PLANT MATERIAL UNTIL TIME OF ACCEPTED ESTABLISHMENT.
10. ALL PLANTING BED EDGES TO BE SPADE CUT UNLESS SPECIFIED WITH MOW STRIP OR OTHER INSTALLED EDGES.
11. PLACE BIODEGRADABLE STRAW BLANKET NORTH AMERICAN GREEN 5' HIGH OR APPROVED EQUAL ON SLOPES OF OR EXCEEDING 5:1 OR AS OTHERWISE INDICATED OR DIRECTED BY ENGINEER.
12. COORDINATE LANDSCAPE PLAN WITH ALL WATER QUALITY AND EROSION CONTROL MEASURES.
13. IF OWNER REQUESTS, PLANTS SHALL BE STAKED AND/OR FIELD LOCATED BY DESIGNER WITH CONTRACTOR. FINAL LOCATIONS OF TREES SHOULD BE ADJUSTED IF NEEDED TO AVOID OVERHEAD AND UNDERGROUND UTILITIES.
14. ANY PLANT MATERIAL SUBSTITUTIONS INSTALLED WITHOUT APPROVAL FROM DESIGNER, AND/OR OWNER SHALL BE REPLACED AT CONTRACTOR'S EXPENSE.
15. ALL PLANTING MATERIALS, REGARDLESS OF SIZE SPECIFIED, SHALL BE OF SIZES THAT COMPLY WITH THE CITY OF BLOOMINGTON UNIFORM DEVELOPMENT ORDINANCE SECTION 20.05.05.2, LANDSCAPING STANDARDS.

SEEDING SPECIFICATIONS:

- I. FINISH GRADING AND SEEDING
 - A. Topsoil/Topsoil shall be spread to a minimum depth of 4" over all graded areas. The finish grade shall reflect proposed contours and spot elevations as shown on the grading plan.
 - B. Temporary Seeding- All areas disturbed during construction shall be seeded as soon as possible after construction grading. Seeding shall be applied at a rate of 1 1/2 lbs/acre consisting of 45 lbs. Alta Fescue, 30 lbs. Perennial Rye, and 35 lbs. Annual Rye. The seeded areas shall be covered with straw at a rate of 5 tons/acre.
 - C. Final Seeding- After final grading and topsoil placement, all areas shall be seeded, fertilized and mulched. The seeding shall be applied at the rate of 1 1/2 lbs/acre. The mixture shall consist of 25 lbs. of Perennial Ryegrass, 35 lbs. of Tall Fescue, 25 lbs. of certified common Kentucky Bluegrass or Newport Kentucky Bluegrass or Park Kentucky Bluegrass or Park Kentucky Bluegrass, and 25 lbs. of Watson Kentucky Bluegrass or approved equal. All seed shall be pure live seed. Fertilize according to soil tests or at a minimum rate of 1000 lbs. of 12-12-12 fertilizer and 150 lbs. of ammonium nitrate per acre. The seeded areas shall be covered with straw free of noxious weed at a rate of 5 tons/acre. The 1000 lbs/acre of 12-12-12 fertilizer shall be applied as follows:
 - *Non Irrigated
 - March 1-May 10
 - August 1 - September 15
 - *Irrigated may be done from May 10- August 1 if irrigated, to
 - Soonest Seeding
 - November 15- February 28 (increase seed application rate by 50 %)
- II. SOODING STEEP SLOPES- Where finish slopes are steeper than 3:1 soil shall be placed in accordance with sodding stem found in Section IV-C of these specifications.
 - A. Sodding - Where sod is specified for ditches or steep slopes, it shall be staked or pinned securely with at least 2 stakes or pins not more than 2' apart with the flat side against the slope. Stakes may be wood at least 1 1/2" x 1 1/2", driven so that the last 1" remains above the top of the sod.



SHADE TREE PLANTING DETAIL



SHRUB PLANTING DETAIL

Date	
By	
Revised	

FILED

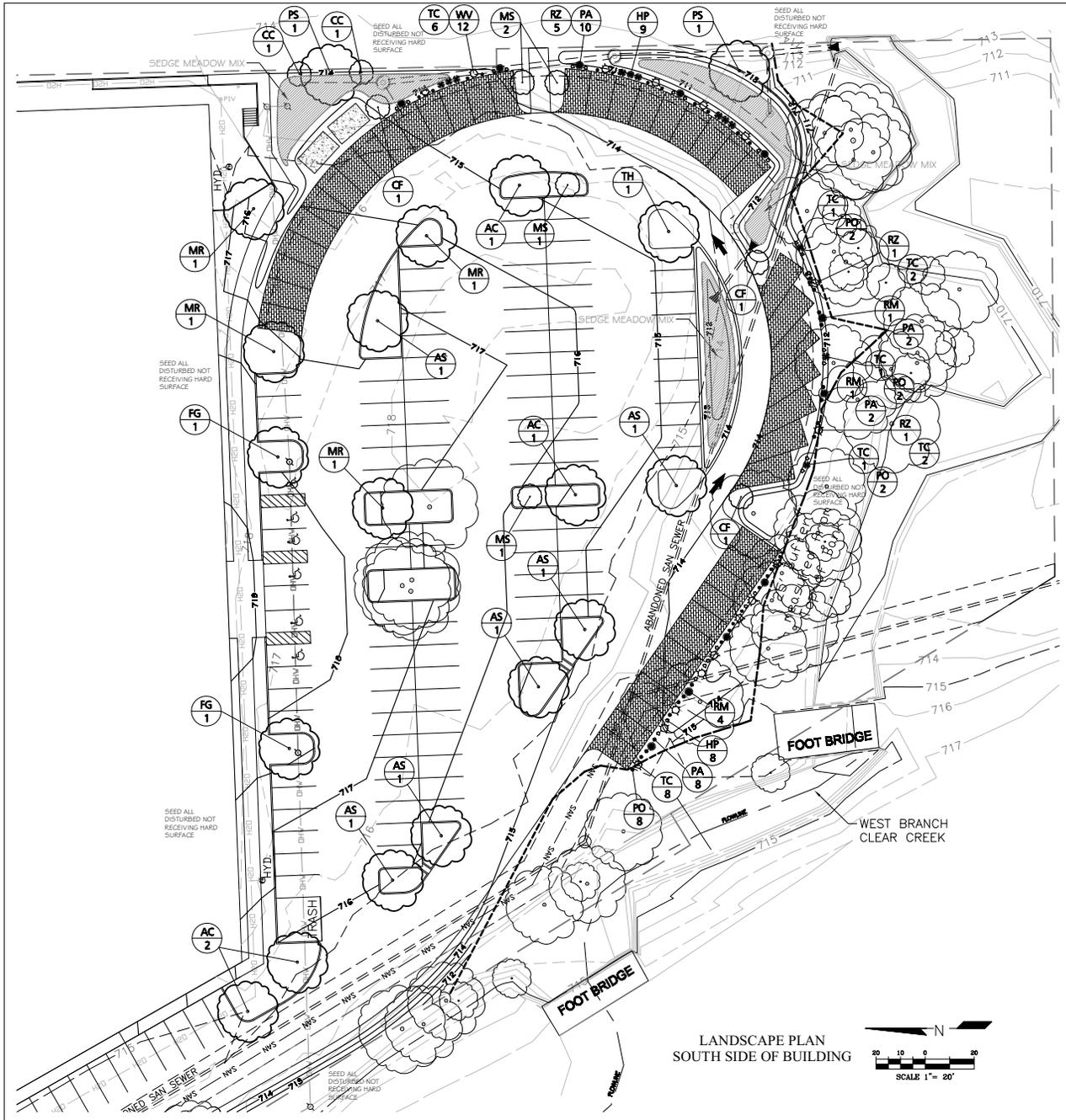
Bledsoe Rispent Guertter
LANDSCAPE ARCHITECTS, P.A.
100 West New York Street, Suite 200
Bloomington, IN 47404
P: 317-326-8877
F: 317-326-8877

The Warehouse
by
Family Center
1525 South Rogers Street
Bloomington, Indiana

SURVEYED BY:
DRAWN BY: GSK
CHECKED BY:
DATE: OCT. 2011

LANDSCAPE PLAN

SHEET
6 OF **11**
PROJECT NO. 6671



LANDSCAPE PLAN
SOUTH SIDE OF BUILDING



TREE / SHRUB SCHEDULE

SYD	SCIENTIFIC NAME (PRES)	COMMON NAME	SIZE	CONDITION	# OF PLANTS
AC	ACERUCARIA GOMPHRA	RED HORN OAKBERRY	2 1/2" CALIPER	B & B	4
AD	ACER SACCHARINUM	SUGAR MAPLE	2 1/2" CALIPER	B & B	6
CC	CORNUS COSSAGRIANA	SWEET GUM	2 1/2" CALIPER	B & B	6
CP	CORNUS FLORIDA	FLORIDAN DOGWOOD	1 1/2" CALIPER	B & B	3
FC	FALGUS GRANDIFOLIA	AMERICAN BEECH	2 1/2" CALIPER	B & B	6
MR	MAGNOLIA STELLATA VERNAL STAR	ROYAL STAR MAGNOLIA	2 1/2" CALIPER	B & B	8
MR	MALUS SPECIOSA	FLORIDIAN CRAB APPLE	1 1/2" CALIPER	B & B	7
FR	FALGUS STREBOS	WHITE PINE	1 1/2" CALIPER	B & B	2
DI	DIERIS LAMPROCARPA	THIMBLE CLIM	2 1/2" CALIPER	B & B	1
TH	THELMA GRANDIFOLIA	EASTERN HEMLOCK	1 1/2" CALIPER	B & B	2

SYD	SCIENTIFIC NAME (SWAMP)	COMMON NAME	SIZE	CONDITION	# OF PLANTS
IF	IPOMOEPA TRUNCATA SUBPURA	SHAMPO WORMWEED	30" HEIGHT	B & B	24
IC	IMPATIENS OCHRACEA OLD GOLD	OLD GOLD IMPATIEN	3 GALLON	CONTAINER	30
IC	IMPATIENS OCHRACEA SPARKLE	SPARKLE IMPATIEN	3 GALLON	CONTAINER	28
PA	PERSEA ABBEY	RED BERRY SPINEL	3 GALLON	CONTAINER	30
PD	PHYTOLAPPA OPIUNIFOLIA	POISONWOOD	3 GALLON	CONTAINER	21
RM	RHOISODENDRON NARAYAN	ROSEMARY RHODODENDRON	3 GALLON	CONTAINER	8
RZ	RHOISODENDRON NOVA ZIBELIA	NOVA ZIBELIA RHODODENDRON	3 GALLON	CONTAINER	16
TC	THELMA CLYDEANA CUPRESSA	UPRIGHT YEW	3 GALLON	CONTAINER	27
WV	WEGELIA VANDERI	CARDINAL THYRUS	3 GALLON	CONTAINER	34

SEEDGE MEADOW SEED MIX

GRASSES & SEEDS: *oz* / *acre*

- 1 Carex comosa (Bristly Sedge)
- 1 Carex cnstata (Crested Sedge)
- 5 Carex frankii (Frank's Sedge)
- 2 Carex hystrix (Foncagine Sedge)
- 2 Carex lurida (Lund Sedge)
- 1.5 Carex stipitata (Awl Fruited Sedge)
- 1 Carex tribuloides (Pointed Oval Sedge)
- 4 Carex vulpinoidea (Fox Sedge)

- 64 Elymus virginicus (Virginia Wild Rye)
- 4 Glycena striata (Fowl Mania Grass)
- 4 Lerna cryzoides (Rice Cut Grass)
- 4 Panicum virgatum (Switchgrass)
- 0.5 Scopus atrovirens (Dark Green Bulrush)
- 2 Spartina pectinata (Fraine Cordgrass)

FORBES: *oz* / *acre*

- 2 Angelica atropurpurea (Angelica)
- 2 Asclepias incarnata (Swamp Milkweed)
- 1 Aster firmus (Shining Aster)
- 1 Aster novae-angliae (New England Aster)
- 1 Aster puniceus (Swamp Aster)
- 1 Aster umbellatus (Flat-topped Aster)
- 1 Boltonia latispinosa (False Aster)
- 3 Cassia hebecarpa (Wild Sienna)
- 2 Eupatorium perfoliatum (Boneset)
- 2 Helianthus autumnale (Autumn Sneezeweed)
- 2 Latis spicata (Dense Blazing Star)
- .25 Lobelia cardinalis (Cardinal Flower)
- .25 Lobelia siphilitica (Great Blue Lobelia)
- .5 Mimulus ringens (Monkey Flower)
- 1 Penstemon digitalis (Foxglove Beardtongue)
- 1 Pycnanthemum virginianum (Mountain Mint)
- 3 Rudbeckia fulgida speciosa (Showy Black-eyed Susan)
- 1 Rudbeckia hirta (Black-eyed Susan)
- 3 Rudbeckia subtomentosa (Sweet Black-eyed Susan)
- 2 Silphium integrifolium (Rosenweed)
- 2 Silphium perfoliatum (Cupplant)
- 2 Silphium terebinthaceum (Fraine Dock)
- 1 Solidago patula (Swamp Goldenrod)
- 2 Solidago riddiellii (Ruddell's Goldenrod)
- 2 Verbena hastata (Blue Vervain)
- 2 Vernonia fasciculata (Smooth Ironweed)
- 1 Veronicastrum virginicum (Culver's Root)
- 1 Zizia aurea (Golden Alexanders)

Date	
By	
Revised	

FILING

Bledsoe Rispett Guerretaz
LANDSCAPE ARCHITECTS P.A.
100 West Main Street, Suite 200
Bloomington, IN 47404
P: 317.344.8777
F: 317.344.8777

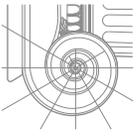
The Warehouse
by
Family Center
1525 South Rogers Street
Bloomington, Indiana

SURVIVED BY:
DRAWN BY: CLK
CHECKED BY:
DATE: OCT. 2011

LANDSCAPE PLAN

SHEET
7 OF **11**

PROJECT NO. **6671**



**ARCHITECTURAL
DESIGN STUDIO**
INCORPORATED

1525 South Rogers Street, Bloomington, Indiana
317.344.2225 FAX
317.344.2225 TEL

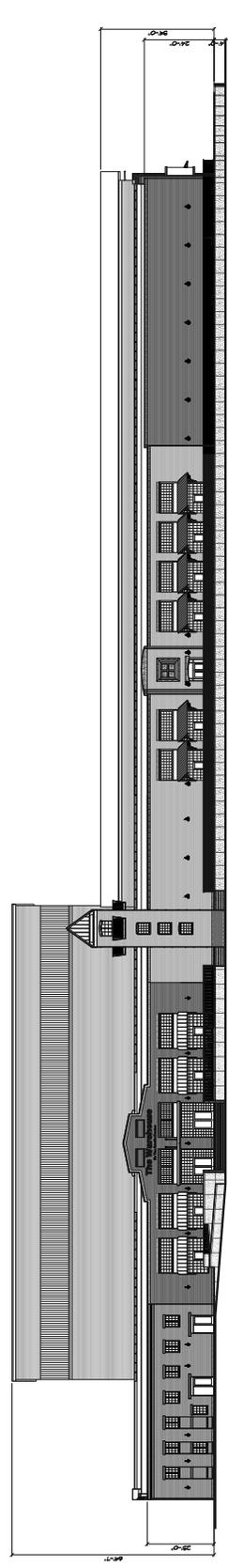
**ARCHITECTURE
PLANNING
DESIGN**

COPYRIGHT NOTICE
THIS DOCUMENT IS THE PROPERTY OF ARCHITECTURAL DESIGN STUDIO, INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. ANY REUSE OR REPRODUCTION OF THIS DOCUMENT WITHOUT THE WRITTEN PERMISSION OF ARCHITECTURAL DESIGN STUDIO, INC. IS STRICTLY PROHIBITED.

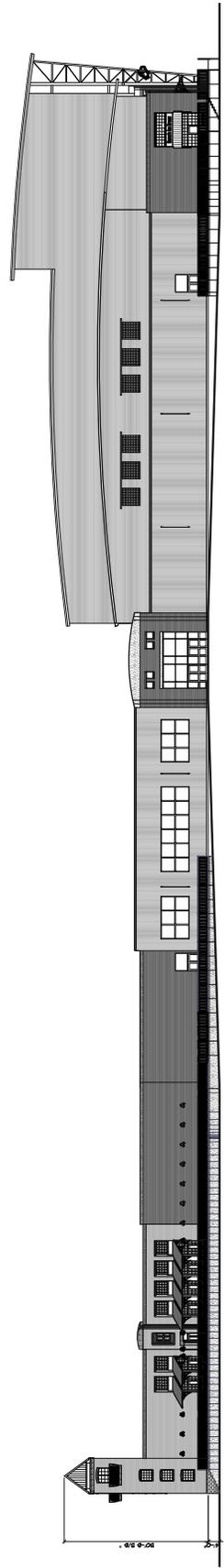
- REVISIONS**
- 1
 - 2
 - 3
 - 4
 - 5
 - 6
 - 7
 - 8
 - 9
 - 10
 - 11
 - 12
 - 13
 - 14
 - 15
 - 16
 - 17
 - 18
 - 19
 - 20

The Warehouse
by the Family Center

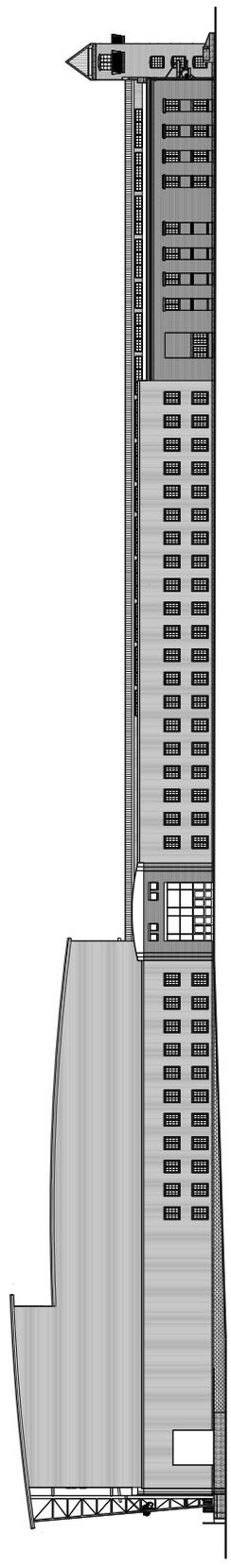
CONSTRUCTION
2009.01
DRAWN BY: B. Perdue
CHECKED BY: B. Perdue
DATE: 01/01/09



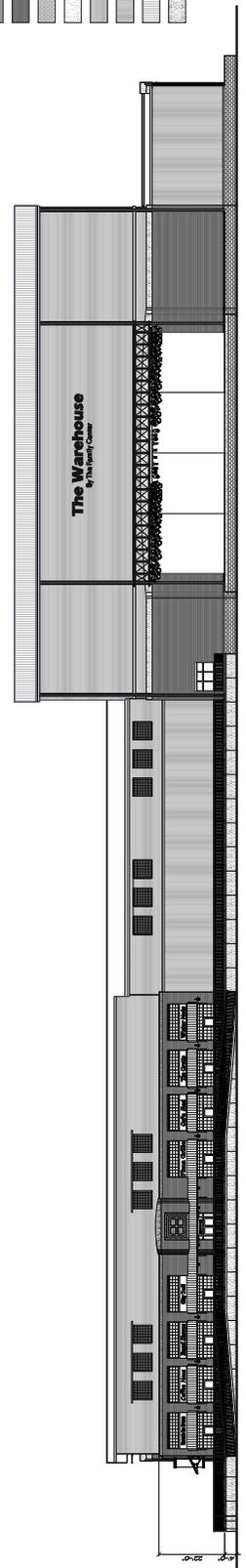
WEST ELEVATION



SOUTH ELEVATION

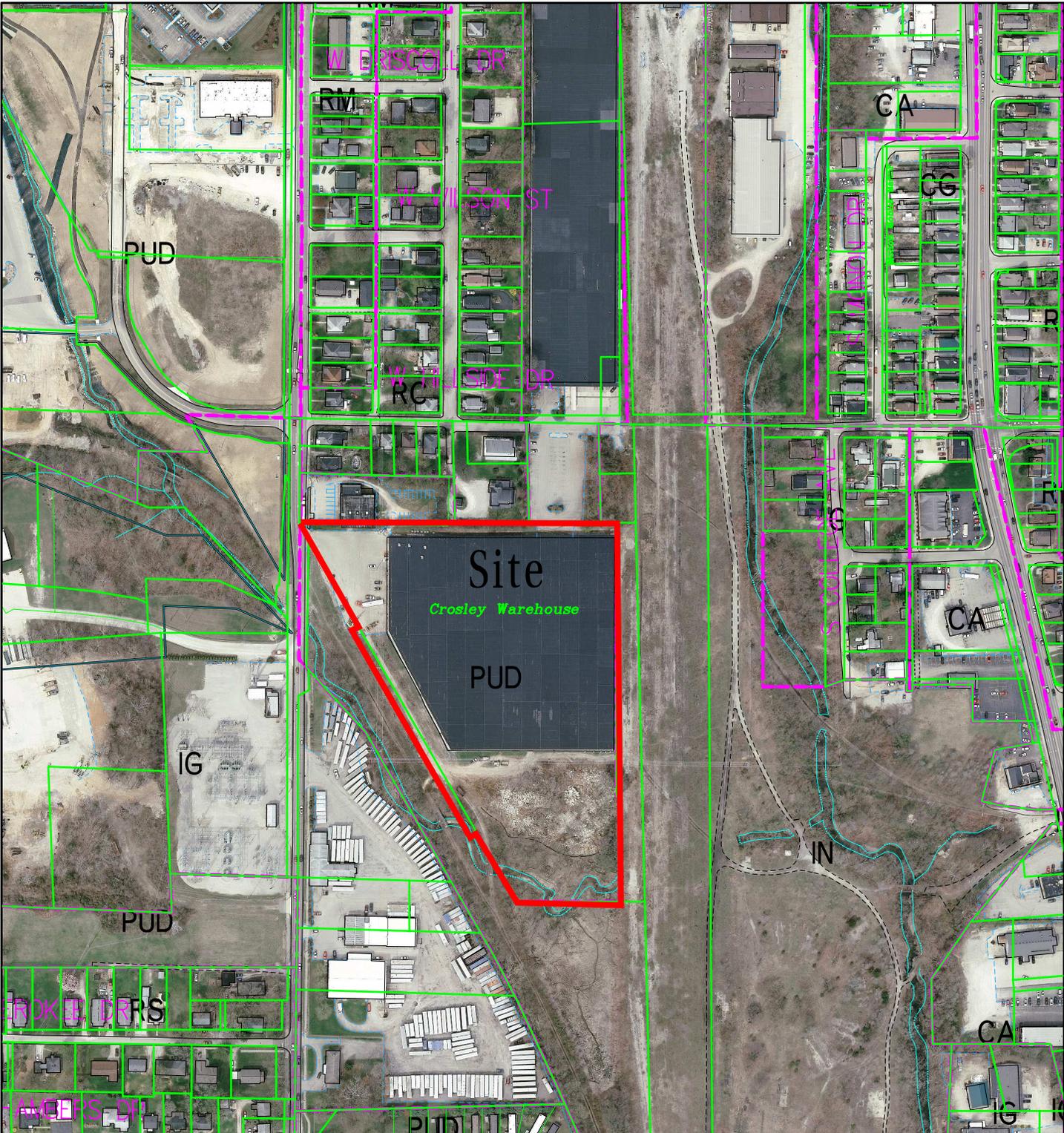


NORTH ELEVATION



EAST ELEVATION

- BRICK COLOR A
- BRICK COLOR B
- BRICK COLOR C
- SPLIT FACE BLOCK
- LIMESTONE TRIM
- HORIZONTAL CORRUGATED METAL SIDING
- VERTICAL CORRUGATED METAL SIDING
- METAL ROOFING
- EXPOSED CONCRETE



PUD-28-11 Warehouse Community Center

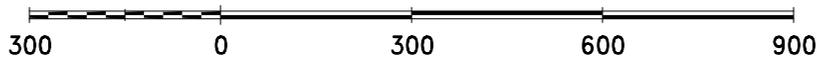
1525 S Rogers Street

Plan Commission

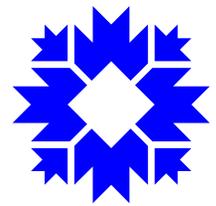
2010 Aerial Photograph

By: greulice

3 Nov 11



City of Bloomington
Planning



Scale: 1" = 300'

For reference only; map information NOT warranted.

PETITIONER: Tommy and Lesli Berry
2001 S. Renwick Blvd., Bloomington

COUNSEL: Bledsoe, Riggert and Guerrettaz
1351 W. Tapp Road, Bloomington

REQUEST: The petitioners are requesting a PUD Preliminary Plan amendment to allow Recreation Center as a permitted use within Parcel I of the Woolery Planned Unit Development.

SITE INFORMATION:

Lot Area: 1.68 Acres
Current Zoning: Planned Unit Development (Woolery PUD)
GPP Designation: Community Activity Center
Existing Land Use: Vacant
Proposed Land Use: Recreation Center
Surrounding Uses: North – Multi-family, Cassady Electric
South – Quarry
East – Vacant, multi-family
West – Vacant, future mixed use Woolery Mill properties

REPORT: The petitioners are seeking approval to allow the development of a portion of Tract I of the Woolery Planned Unit Development. This PUD was created in 1994 by case #PUD-64-94. Parcel I was originally 13.5 acres in size, was approved as a neighborhood shopping center, and is located on the north side of W. Tapp Road, northeast of the intersection of Tapp Rd. and S. Weimer Road and southeast of the former Woolery Stone Mill. The approved uses for Parcel I ranged from small scale shops and services to larger scale commercial uses like groceries, restaurants and gas stations, but nothing has been developed yet.

This portion of Parcel I is approximately 1.68 acres and is currently vacant. The site is bordered by a constructed, but not yet dedicated public street (to be called Keg Road) to the west, the remainder of Parcel I to the east and the south, and the Cassady Electric offices and the former Woolery Stone Mill to the north.

The petitioners currently operate an indoor gymnastic school (Bloomington United Gymnastics School) and an indoor soccer school (Bloomington United Soccer School) on two adjacent buildings in an industrial area on Yost Avenue within the Monroe County Planning Jurisdiction. They have been in business since 2002 and would like to consolidate their operations in a single building in a location closer to the City's greenway system.

The petitioners propose to construct a 26,000 square foot building on the north side of a lot to be created in Parcel I. This lot is at the southeast corner of W. Sunstone Drive and S. Keg Road. A PUD Preliminary Plan Amendment is required to change the permitted use list for Parcel I to include "Recreation Center." This request requires two Plan Commission meetings and is ultimately decided by the Common Council. Also requested is delegation of the PUD Final Plan to staff level for the Recreation Center.

Growth Policies Plan: This property is located within the Community Activity Center (CAC) land use category of the Growth Policies Plan (GPP). The GPP states that the primary intent of the CAC is to "provide community-serving commercial opportunities in the context of a high density, mixed use development." (page 35)

The GPP makes several recommendations associated with CACs that can apply to this PUD Preliminary Plan Amendment.

- [The CAC should] be designed to serve not only pedestrian traffic from nearby neighborhoods, but also a community-wide group of users that may drive a personal vehicle to the CAC.
- The primary land use in the CAC should be medium scaled commercial retail and service uses.
- Community Activity Centers should be connected to a future city-wide greenway system in order to create adequate public recreation space as well as alternative means to access the development.
- A Community Activity Center should be located at an intersection which is made up of designated Collector or Arterial streets, in order to provide automobile access without overwhelming the pedestrian aspects of the development.
- The CAC should be sensitive to the surrounding context.
- An increased emphasis must be placed on Urban Design and the creation of a distinctive design style in each area.
- Buildings should be developed with minimum street setbacks to increase pedestrian and transit accessibility.
- Parking should be located and designed with an emphasis on minimizing pedestrian obstacles to accessing businesses.

This portion of the Woolery PUD has already met many of the recommendations of the CAC including use of on-street parking, access to a greenway and access to open space (Clear Creek floodplain).

PUD PRELIMINARY PLAN REVIEW ISSUES:

Permitted Uses: The approved PUD anticipated that Parcel I would be developed with a traditional medium scale shopping center, likely anchored by a grocery store, with out-lots and B-shops. In the 17 years since this PUD was approved there have been no petitions to develop these uses on Parcel I. While the permitted use list included a wide range of commercial uses, it did not contain many non-retail uses like the proposed Recreation Center or offices, churches or

daycare centers. If this use is approved, the remainder of Parcel I will still contain almost 9.5 acres of land for development of commercial use. In comparison, this is similar in size to the Kroger center at E. Rhorer Road and S. Old SR 37 and the Walnut Square center at S. Walnut Street and W. Country Club Drive. Staff finds the proposed use to be consistent with the GPP recommendations for Community Activity Centers.

Development Standards: Parcel I was originally approved with Commercial Arterial (CA) zoning district development standards. As part of this request, the petitioners have proposed few changes to the CA development standards (see below). While the petitioners have not developed a complete PUD Final Plan, they have designed the site to a level of detail to show that they can comply with the CA standards for maximum impervious surface coverage, sidewalks, street trees, bike parking, signage, setbacks, maximum parking, and landscaping requirements.

SITE DESIGN/PUD FINAL PLAN REVIEW ISSUES:

Architecture: The petitioners propose to utilize an industrial aesthetic for the design of the building. The building would have a low-pitched metal roof and would utilize vertical metal siding and windows placed high on the wall. The petitioners' architect designed the building to compliment the former industrial Woolery Stone Mill and the yet to be constructed along Keg Rd. These buildings were 2-3 stories tall and included vertical metal siding, large square windows and little ornamentation.

In addition, the nature of the proposed use may not lend itself well to the architectural requirements of the UDO, especially the requirements for changes in modulation and large windows. Other similar style buildings include the Twin Lakes Recreation Center and the YMCA.

Approval of this PUD amendment, with this architecture, would have to include changes to the Architecture Standards in the UDO. The proposed building violates the following UDO architectural requirements:

1. Materials: Metal siding not permitted
2. Blank wall: Areas of blank wall greater than 40 feet not permitted
3. Patterns: Changes in color and texture required and not provided
4. Roofing: Parapet or roof eaves required and not provided
5. Entrances: Incomplete pedestrian entry detailing

Environmental Constraints: Few environmental constraints exist on this lot. There are a handful of trees at the north end of the site that will be preserved with this petition. There are no known karst features, wetland, steep slopes or floodplain on this lot.

Access: Main access to the site would be from a new drive cut off of Keg Rd. This drive would be shared with a proposed lot to the south, at the corner of Keg Rd. and Tapp Rd. The property is separated from Keg Rd. by a limestone mill

block retaining wall. The new drive and a new pedestrian staircase would necessitate removal of parts of this wall. In addition to the main drive, a secondary Fire Department access drive is shown on the north side of the building with access from Sunstone Dr.

Utilities: Water and sanitary sewer plans have been submitted to City Utilities and are under review. The lot can be adequately served by the existing water and sanitary sewer mains within Keg Rd. and Sunstone Dr.

Stormwater: The petitioners have submitted preliminary stormwater calculations to City Utilities for review. While City Utilities has not yet completed their review, the primary issue is whether the large pond to the west was approved to handle the required stormwater detention and water quality requirements for this site. If so, the petitioners may not be required to provide on-site detention. If not, the petitioners would need to meet stormwater detention and water quality requirements independent of the pond. The petitioners' consultant has indicated to Staff that this could be handled through underground detention.

Regardless of whether City Utilities permits the ponds to count toward requirements for water quality, the UDO requires any parking lot of 16 spaces or more to either utilize pervious pavers or natural stormwater filtration (such as bio-swales or rain gardens) to filter parking lot run-off. The petitioners would like the existing Woolery Stone Mill ponds to count toward this requirement also. The Environmental Commission believes this requirement should be met on the site. In general, staff agrees with the Environmental Commission recommendation. Staff is seeking feedback from the Plan Commission on this issue.

Parking: The UDO does not require any parking for this commercial use. The UDO permits a maximum parking for this use of 104 spaces. The petitioners are proposing 39 on-site spaces. Spill-over parking would be handled with a combination of street parking and parking at the Woolery Mill site.

Developer Track Record: The petitioners have no development history within the City of Bloomington. They currently rent buildings outside the City limits.

QUESTIONS FOR PLAN COMMISSION/GUIDANCE FOR SECOND HEARING:

1. **Uses** – Is a recreation center an appropriate use for this site? Is the use compatible with the already constructed uses in this PUD, the anticipated uses in Parcel I and the stated CAC policies of the GPP?
2. **Final Plans** – Does the petition contain enough details to allow delegation of the PUD Final Plan to staff level?
3. **Architecture** – Is the proposed architecture appropriate? Does the Plan Commission believe that the industrial aesthetic approved for the new mixed use buildings on the Wooley Mill site should be continued to Parcel I? If not appropriate, what changes should be made to the proposed building? At this point, Staff recommends that changes be made to the building to bring it more in line with the UDO architectural requirements.

4. **Stormwater quality:** Should the ponds on the common lot be utilized to meet the water quality requirements for parking lots of more than 16 spaces instead of meeting this requirement on-site?

PRELIMINARY CONCLUSIONS: Staff is supportive of the proposed use of the property. Staff finds the GPP's policies toward Community Activity Centers, approved after creation of the use list for this PUD, supports larger community scale uses like the one proposed. The other main issues of architecture and general development standards need to be further developed prior to the second hearing.

RECOMMENDATION: Staff recommends forwarding this petition to a second hearing.

MEMORANDUM

Date: October 27, 2011
To: Bloomington Plan Commission
From: Bloomington Environmental Commission
Through: Linda Thompson, Senior Environmental Planner
Subject: PUD-29-2011 BUGS & BUSS

This memorandum contains the Environmental Commission's (EC) input and recommendations regarding an amendment to the PUD District Ordinance to add gymnasium as a use to the Woolery Planned Unit Development (PUD), and to designate Site Plan approval to the planning staff.

The EC is in favor of adding the gymnasium as a use to the PUD District Ordinance, thus recommends a positive recommendation from the Plan Commission to the City Council for a PUD amendment.

ISSUES OF SOUND ENVIRONMENTAL DESIGN:

1.) GREEN BUILDING:

The EC recommends that the developer design the building with as many best practices for energy savings as possible. The EC recommends that enhanced insulation; high efficiency heating and cooling; Energy Star doors, windows, lighting, and appliances; and a white roof with a reflective coating be used. Some other green-building suggestions for this project include high efficiency toilets, programmable thermostats, sustainable floor coverings, and recycled products such as carpet and counter tops.

Green building supports Bloomington's overall commitment to sustainability and is being actively promoted by the City (<http://bloomington.in.gov/greenbuild> and UDO green building incentives 20.05.049 GD-01). Green building is also called for by the Mayors' Climate Protection Agreement and City Council resolution 06-05, which support the Kyoto Protocol and reduction of our community's greenhouse gas emissions, and resolution 06-07, which recognizes and calls for planning for peak oil.

Additionally, a metal pole-barn building is generally considered neither attractive or energy efficient. The EC would encourage the developer to re-design the building to fit better with the esthetics of the surrounding area rather than distract from the unique picturesque look of the rest of the PUD.

2.) LOW IMPACT DEVELOPMENT:

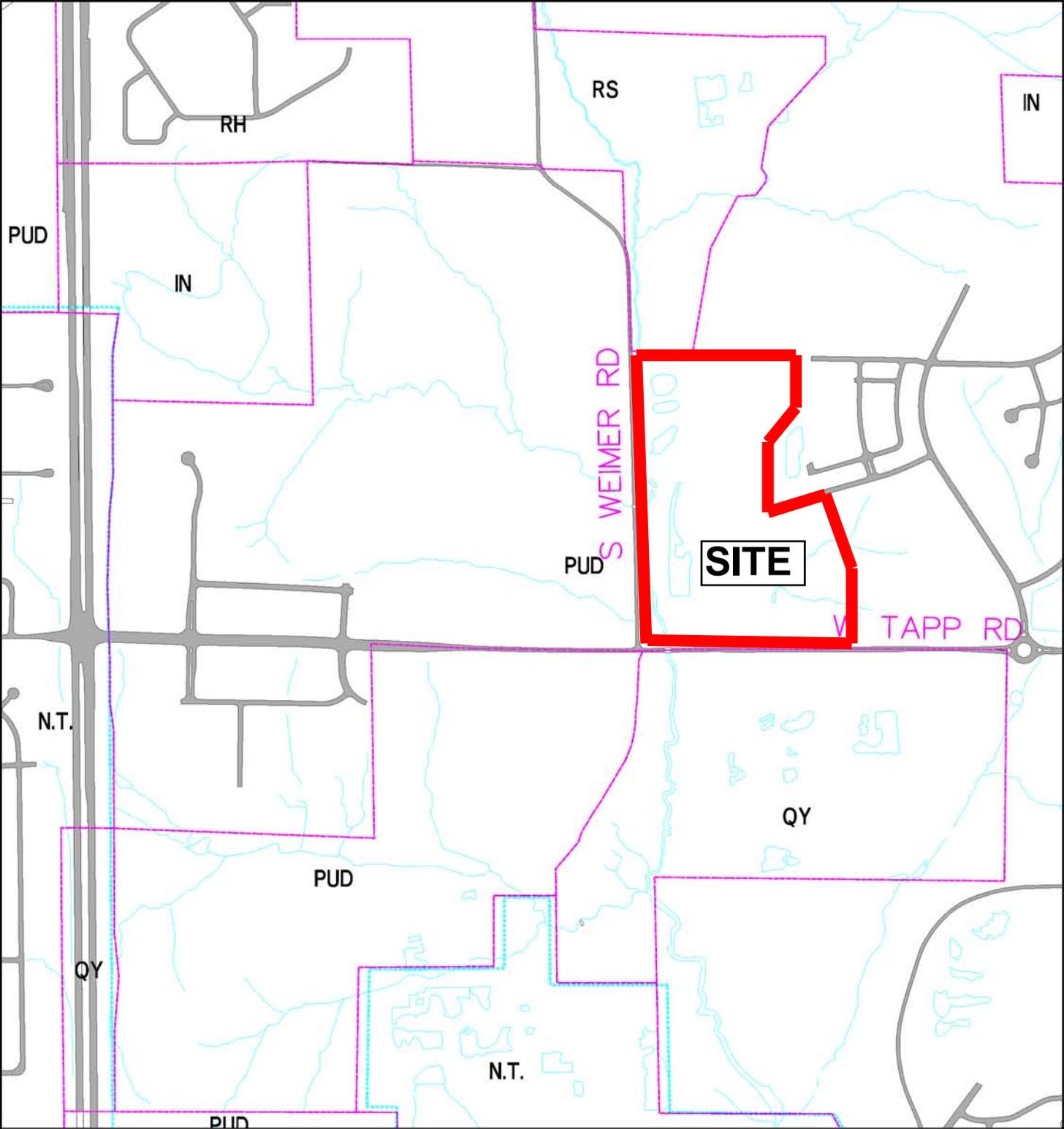
The EC recommends that the areas north and west of the building, which are planned with traditional landscaping, be re-designed as biofiltration swales. The islands in the parking lot should also be redesigned to be biofiltration swales, thus stormwater from the building and parking lots can be slowed down and filtered before it enters the floodplain as described in UDO 20.05.070 (m) Parking Standards; Surface Material.

The EC promotes Low Impact Development (LID) for all new development and renovations. LID includes sustainable stormwater management strategies that prevent environmental degradation from erosion, heat island

effect, pollutants, and floods. The strategies promote water infiltration that recharge aquifers, filter out pollutants, regulate temperature, and allow financial savings. The idea of LID embraces a holistic approach to manage water at its source such that stormwater can be distributed across a site and avoid channelized networks that pipe water downstream into a large stormwater management facility such as a detention or retention pond.

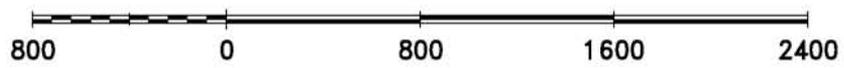
EC RECOMMENDATIONS:

- 1.) The Petitioner should apply green building practices to create a high performance and low carbon-footprint structure.
- 2.) The Petitioner should construct bioswales for water quality and quantity north and west of the building and in the parking lot islands.



PUD-29-11
Location and Zoning Map

By: roachja
3 Nov 11



City of Bloomington
Planning



40
Scale: 1" = 800'

For reference only; map information NOT warranted.

October 24, 2011

Jim Roach
City of Bloomington, Planning Department
401 N. Morton St., Suite 160
PO Box 100
Bloomington, IN 47402-0100

Re: BUGS-BUSS Training Center
Petitioner's Statement
BRG Project No. 7303

Dear Jim,

On behalf of Tommy and Lesli Berry, owners of BUGS and BUSS Youth Training facilities, we are requesting consideration by the Plan Commission and City Council for the approval of a PUD amendment to add "recreation center" to the approved list of uses for the Woolery PUD. Also, we are requesting that site plan approval for the BUGS/BUSS site be designated to staff level. This petition is being parallel processed with a request for preliminary and final plat approval for phase IX of the Woolery Planned Community, within which the proposed facility will be developed.

Proposed for the BUGS-BUSS site is a single building with adjacent parking lot. The Woolery PUD, which controls use of the site, does not include "recreation center" in its list of approved uses, hence this request. We would like to be placed on the November 5, 2011, Plan Commission meeting agenda and subsequently on the City Council agenda at the earliest dates possible.

The subject property, Lot 3, is approximately 1.68 acres. Public streets, already constructed, exist on the west and north sides of the site. They extend from an intersection with Tapp Road and connect to Sunstone Drive to the east. The proposed development is accessed from the public street to the west and shares a common drive with Lot 2, a .75 acre lot to the south, which has frontage on Tapp Rd. and shall remain undeveloped at this time. The proposed building is a 26,000 sq. ft. and is dedicated primarily to training for youth soccer and gymnastics. Included in the plan are 39 on-site parking spaces. There are also a number of parallel parking spaces provided on both frontages along the public streets and a larger common parking lot in the Woolery Planned Community just to the north of this site. We believe the proposed use is fully compatible with the existing PUD and with what the Growth Policies Plan envisions for this area.

Stormwater, sanitary and water services is available at the site and have the capacity to service the proposed use. We believe that both stormwater detention and treatment have already been provided in the common area of the existing development. Discussions with the City of Blooming Utilities are underway. We have had preliminary discussions with Fire Inspection Officer Tim Clapp with regards to fire protection service. It appears we will be able to provide what is needed to obtain his approval.

Tommy and Lesli Berry are Bloomington natives, local business owners and coaches. Their businesses, Bloomington United Gymnastics School (BUGS) and Bloomington United Soccer School (BUSS), are primarily centered on youth recreation. Both businesses have been in existence for the past decade, in adjacent leased warehouses on Yost Ave. Together their businesses service approximately 600 children per week in the community, ages 2-18, and yearly employ 40 part-time employees. The busiest time of operation is from 4-9pm on weekdays, and throughout the day on the weekends. BUGS specializes in preschool, recreation, and competitive team gymnastics. There is also a competitive cheer team. They host birthday parties as well as gymnastic meets and these bring users to BUGS beyond their traditional members. The soccer facility is centered on youth soccer training and provides supplemental winter training for the local Cutters Soccer Club. There is also an adult soccer population that uses the facility several times per week in the late evenings.

PUD-29-11 PUD District Ordinance

Since opening their doors in January 2002, their dream has been to combine both BUGS and BUSS, under one roof, in a building that they own to build equity towards retirement. Over the past 10 years they have earnestly pursued four different projects to achieve this dream, but have unfortunately hit insurmountable obstacles in each case. Lesli has been wonderfully persistent and hopefully they've found the right spot to make a final successful attempt. They are very excited about the Woolery Mill location, viewing its proximity to the Clear Creek trail as a great way to provide more healthy living opportunities for their gymnastics and soccer families. They also greatly value the idea of preserving the mill and are excited to be a part of the project as it moves toward its full potential. They feel their businesses could be a positive catalyst for the development of the mostly unoccupied property.

Next year is the summer Olympics and the gymnastics competition always spikes enrollment at BUGS. Their ultimate goal is to be able to open the doors to their new facility in August 2012. Lesli and Tommy are community coaches, and not developers, so this has all been a bit overwhelming, but they are dedicated to give their best effort towards completing this dream.

Included with this petitioners statement is the application form and fee, as well as the site, grading and drainage, utility and landscape plans for the site and conceptual elevations of the proposed building. Also included is tabulation information regarding the landscape plan requirements and the filing fee. Notices will be sent to adjacent property owners as required.

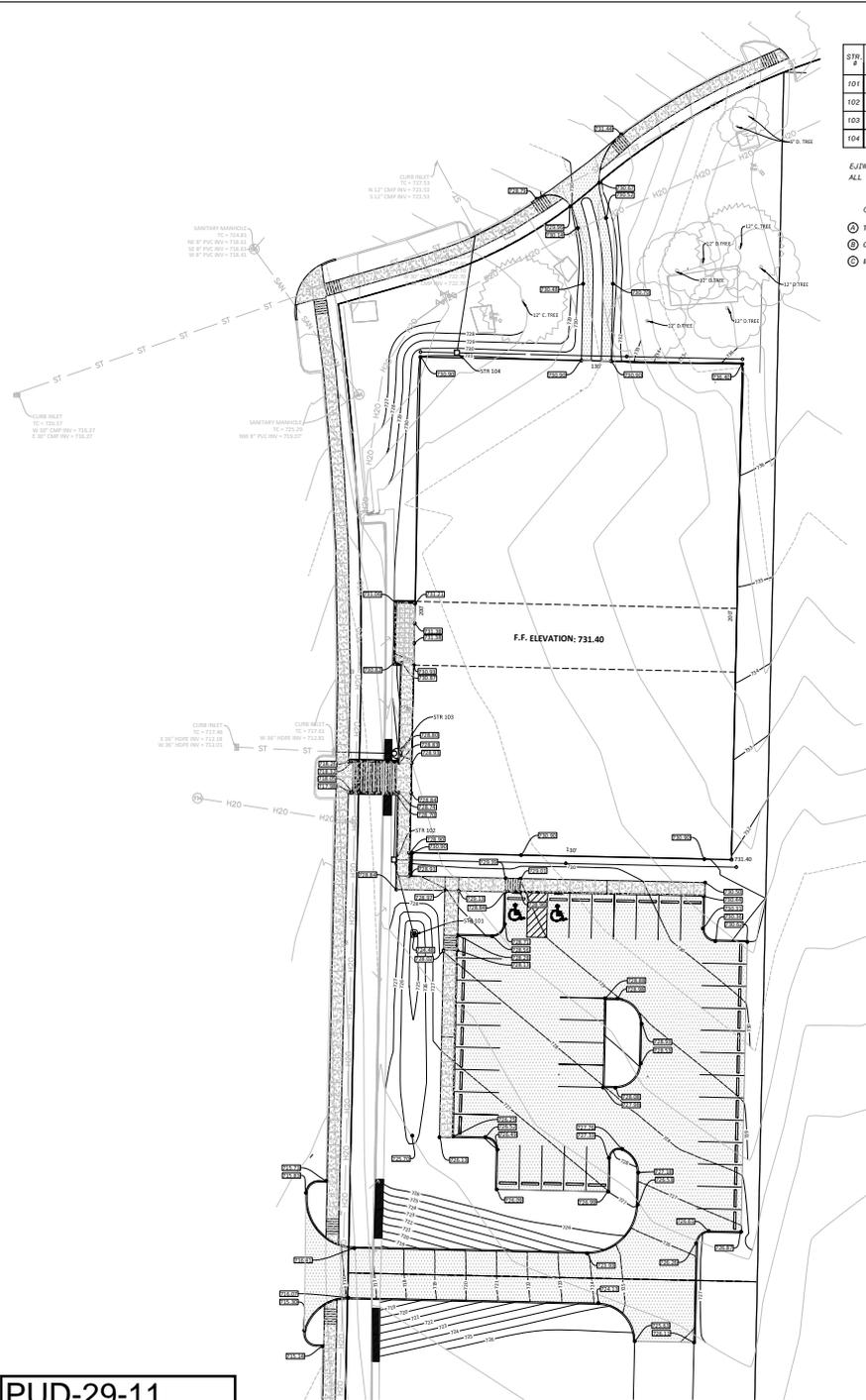
Please contact me if you have any questions.

Sincerely,

Dan Neubecker, Project Manager
Bledsoe Riggert Guerrettaz

xc: Tommy and Leslie Berry, BUGS-BUSS
File – Project No. 7303

PUD-29-11
Grading Plan



STORM STRUCTURE DATA TABLE

STR. #	CASTING	STRUCTURE TYPE	SP. STR. #	IN. STR. #	PIPE SPECIFICATIONS			TOP OF CASTING	R/W. ELEVATION	NORTHING	EASTING	COMMENTS			
					LENGTH	SIZE	SLOPE								
101	EJW 6340	INLET TYPE "A"	-	102	31'	12"	HDPPE TYPE C	2.23%	723.00	721.00	725.00	724.50	1415975.31	3101191.31	⊙
102	EJW 8309	INLET TYPE "A"	101/101	103	43'	12"	HDPPE TYPE C	9.65%	721.00	716.85	728.70	728.70	1417005.38	3101153.28	⊙
103	EJW 1030	48" CONE TOP MANHOLE	102	EXIST	25'	15"	HDPPE TYPE C	6.00%	716.85	712.85	728.85	728.85	1417048.22	3101154.41	⊙
104	EJW 8308	INLET TYPE "A"	RD/101	EXIST	49'	12"	HDPPE TYPE C	10.41%	727.90	722.80	730.40	730.40	1417209.83	3101178.82	⊙

EJW = EAST JORDAN IRON WORKS OR APPROVED EQUAL
ALL COORDINATES ARE TO CENTER OF STRUCTURE

COMMENTS:
 ⊙ THE HOPE TYPE 'S' PIPE SHALL BE DUAL WALL IN ACCORDANCE WITH ASD2010 M24.
 ⊙ CORRECT BUILDING ROOF DRAINS TO 8" HDPE PIPE, AND CONNECT PIPE TO STRUCTURE
 ⊙ MODIFY EXISTING DOWNSTREAM STRUCTURE TO ACCEPT NEW PIPE AND SEAL WATERTIGHT

GENERAL NOTES

- GRADE ALL AREAS TO THE FINISH GRADES SHOWN.
- CONTRACTOR TO VERIFY FIELD CONDITIONS WITH RESPECT TO THE PROPOSED GRADING PLANS AND NOTIFY ENGINEER OF ANY AND ALL DISCREPANCIES PRIOR TO BEGINNING WORK.
- INSTALL AND MAINTAIN EROSION CONTROL DEVICES AS REQUIRED AND WHERE NECESSARY TO CONTROL SEDIMENT.
- CONTRACTOR SHALL PREVENT SURFACE WATER AND GROUND WATER FROM ENTERING EXCAVATIONS FROM FLOODING OR PREPARED SURFACES AND FROM FLOODING PROJECT SITE AND SURROUNDING AREAS. PROTECT SURFACES FROM SOFTENING, UNDERMINING, WASHING AND DAMAGE BY RAIN OR WATER ACCUMULATION. THIS WILL REQUIRE SUPPLEMENTAL GRADING ABOVE AND BEYOND THAT SHOWN.
- CONTRACTOR SHALL ADJUST ALL CUTS TO FINISH GRADE.
- CONTRACTOR SHALL ESTABLISH FINISH GRADES TO ENSURE POSITIVE DRAINAGE WITH NO POOLING.
- CONVENTIONAL SIDEWALK SLOPE SHALL NOT EXCEED 1%, UNLESS NOTED OTHERWISE. TRANSVERSE SIDEWALK SLOPE SHALL NOT EXCEED 2%.
- FINISH SURFACE GRADES OF ADA PARKING AREAS SHALL NOT EXCEED 1:50 IN ALL DIRECTIONS.
- SPOT GRADES GIVEN AT THE FACE OF CURB INDICATE PAVEMENT EDGE/CURB INTERFACE FROM UNEL ELEVATION, UNLESS NOTED OTHERWISE. BOTTOM OF WALL ELEVATIONS INDICATE WHERE FINISH GRADE AND WALL MEET.

PLAN NOTES

⊙ -

Professional Engineer Seal

1351 West Tapp Road
Bloomington, Indiana 47403
Phone: 812.336.8277
Fax: 812.336.0817
www.jrgcivil.com

BLOOMINGTON BEDFORD PAOLI

PRELIMINARY
NOT FOR
CONSTRUCTION

BUGS-BUSS Youth Training Center
Street Address
Bloomington, IN 47403
BRG Project No: 7303

SITE GRADING & DRAINAGE PLAN



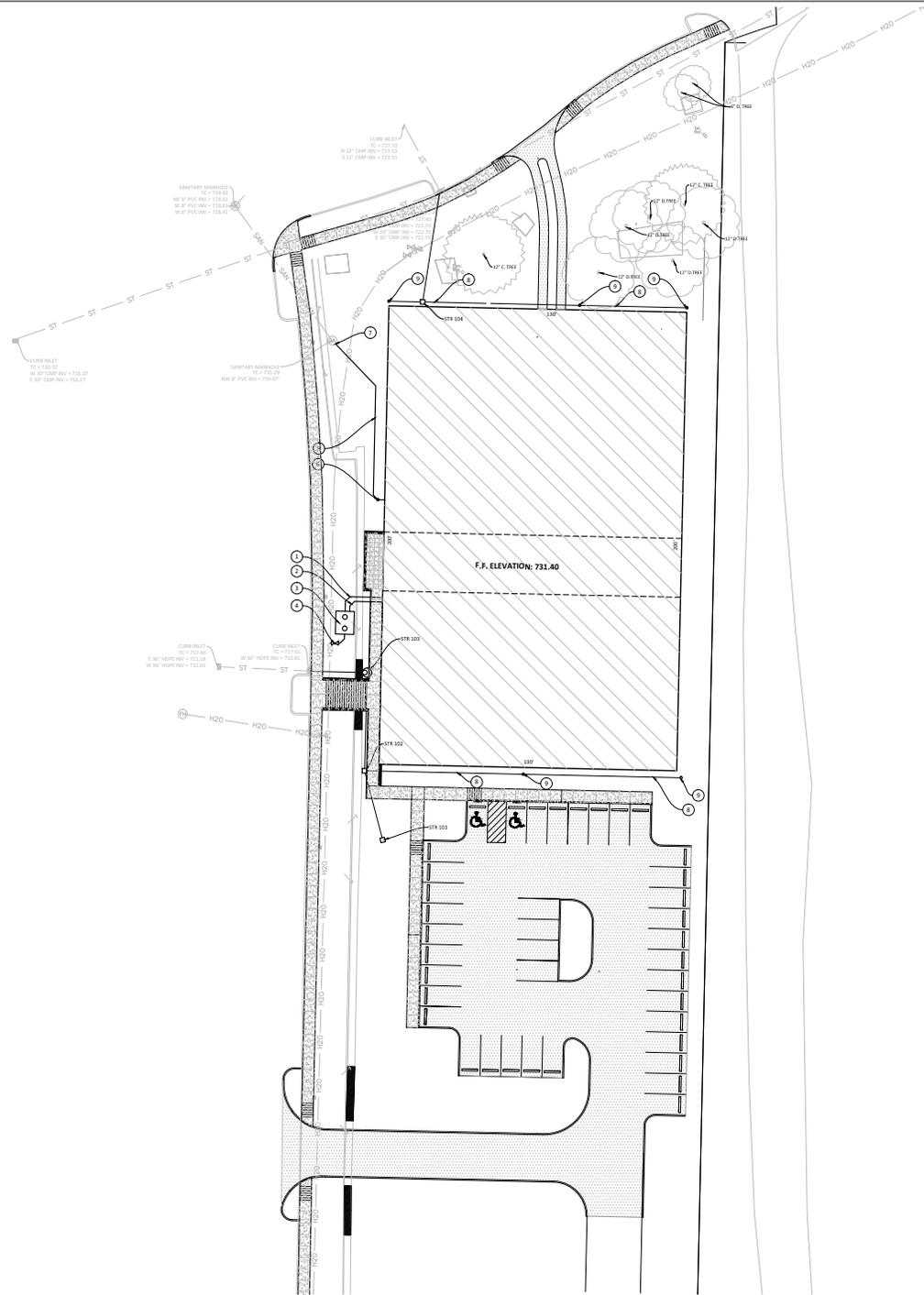
LEGEND

- PROPOSED CONTOURS
- EXISTING CONTOURS
- SPOT ELEVATION
- MSEI
- MSL
- 7R
- 8R
- TS
- BS
- FL
- TC
- LIMITS OF ASPHALT PAVEMENT
- LIMITS OF CONCRETE PAVING
- PROPOSED BUILDING

THIS IS THE LAW
 ONLY A REGISTERED PROFESSIONAL ENGINEER OR ARCHITECT CAN SEAL THESE PLANS.
 ANY VIOLATION OF THESE RULES IS A CRIME UNDER INDIANA LAW AND IS PUNISHABLE BY IMPRISONMENT AND/OR FINE.

Drawn By: DLN
Designed By: DLN
Checked By: WBR

C501



GENERAL NOTES

1. REFER TO DEMOLITION PLANS FOR SEQUENCE OF UTILITY REPLACEMENT TO ENSURE CONTRACTOR COVERS UP ALL UTILITIES.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR CUTTING AND PATCHING AS REQUIRED TO COMPLETELY REINSTALL THE WORK INDICATED.
3. CONTRACTOR SHALL COORDINATE EXACT UTILITY LOCATIONS WITH THE OWNER AND LOCAL UTILITY COMPANIES PRIOR TO COMMENCING ANY WORK. CONTACT THE REGIONAL UTILITY INFORMATION CENTER AT 317-330-3544 AND UTILITY UTILITIES PRIOR TO ANY EXCAVATION ON THE SITE.
4. ALL WORK ASSOCIATED WITH WATER AND SEWER SYSTEMS SHALL COMPLY WITH THE STANDARDS & REQUIREMENTS OF THE REGIONAL DEPT. OF ENVIRONMENTAL MANAGEMENT. THE REGIONAL DEPT. OF HEALTH, THE AMERICAN WATER WORKS ASSOCIATION (AWWA), THE GREAT LAKES UPPER MISSISSIPPI BASIN OF STATE PUBLIC HEALTH AND ENVIRONMENTAL MANAGERS (GLUMM).
5. CONTRACTOR IS REQUIRED TO VERIFY FIELD CONDITIONS AND NOTIFY ARCHITECT/ENGINEER OF ANY DISCREPANCIES PRIOR TO BEGINNING WORK.
6. CONTRACTOR SHALL SET ALL EXISTING AND PROPOSED CASTINGS AND CLEANOUT COVERS TO FINAL FINISH GRADE.
7. A MINIMUM OF 18 INCHES VERTICAL SEPARATION SHALL BE MAINTAINED BETWEEN WATER AND SANITARY SYSTEMS UNLESS OTHERWISE INDICATED, OR UNLESS WRITTEN PERMISSION IS GIVEN BY THE ENGINEER. SEWER CROSSING WATER MAINS SHALL BE LAID TO MAINTAIN A MINIMUM OF 18 INCHES VERTICAL SEPARATION BETWEEN THE CENTER OF THE WATER MAIN AND OUTSIDE OF THE SEWER MAIN. THIS SHALL BE THE CASE WHETHER THE WATER MAIN IS ABOVE OR BELOW THE SEWER. THE CROSSING SHALL BE ARRANGED SO THAT THE CENTER OF THE SEWER MAIN WILL BE EQUIDISTANT AND AS FAR AS POSSIBLE FROM THE JOINTS IN THE WATER MAIN. THE CROSSING MUST BE AT A MINIMUM ANGLE OF 45 DEGREES FROM THE CENTERLINE OF THE SEWER AND WATER MAINS. WHERE A WATER MAIN CROSSES UNDER A SEWER, ADEQUATE STRUCTURAL SUPPORT SHALL BE PROVIDED FOR THE SEWER TO MAINTAIN LINE AND GRADE.
8. A MINIMUM OF 10 FEET HORIZONTAL SEPARATION SHALL BE MAINTAINED BETWEEN WATER AND SANITARY/STORM SEWERS UNLESS OTHERWISE INDICATED, OR UNLESS WRITTEN PERMISSION IS GIVEN BY THE ENGINEER.
9. ALL SANITARY AND STORM LATERALS SHALL HAVE A MINIMUM COVER OF 24" UNLESS NOTED OTHERWISE.
10. ALL SANITARY AND STORM LATERALS SHALL HAVE A MINIMUM SLOPE OF 1/8" PER FOOT UNLESS NOTED OTHERWISE.
11. ALL WATER LINES SHALL HAVE A MINIMUM COVER OF 48". INSTALL LINES WITH NO ISOLATED HIGH POINTS.
12. WHERE DOMESTIC WATER MAINS ARE JOINED TOGETHER CONTRACTOR SHALL USE A NON-SHEAR COUPLING EQUAL TO FERRO-C.
13. CONTRACTOR SHALL REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL, PLUMBING, AND LANDSCAPE PLANS FOR ADDITIONAL UTILITY INDICATIONS AND NOTIFY ARCHITECT/ENGINEER OF ANY DISCREPANCIES PRIOR TO START OF CONSTRUCTION.
14. CONTRACTOR TO EXTEND ALL DRAINAGE, UNDERDRAIN, UNDERSEAL, INTERIOR DRAIN, ROOF DRAIN AND RETAINING WALL DRAIN PIPING TO THE NEAREST PROPOSED STORM STRUCTURE WHILE MAINTAINING SUFFICIENT RISE, UNLESS NOTED OTHERWISE. ALL CONNECTIONS SHALL BE WATER TIGHT.

PLAN NOTES

- ① FIRE PROTECTION LINE
- ② 2" DOMESTIC WATER LINE
- ⑩ FIRE VALVE WITH POST INDICATOR AND STORGE FIRE DEPARTMENT CONNECTION
- ⑪ TAPPING VALVE AND SADDLE
- ⑫ 8" SANITARY LATERAL
- ⑬ SANITARY CLEANOUT
- ⑭ MODIFY EXISTING SANITARY STRUCTURE TO ACCEPT SANITARY LATERAL
- ⑮ 8" WATER MAIN FIRE CONNECTION BRANCH AND SLOPE TO DRAIN TO NEAREST STORM STRUCTURE - 12" MINIMUM COVER
- ⑯ STORM CLEANOUT

LEGEND

- LIMITS OF NEW ASPHALT PAVEMENT
- LIMITS OF NEW 4" THICK CONCRETE PAVING
- PAVEMENT STRIPING 2" C.C.
- LIMITS OF ASPHALT PAVEMENT PATCH
- PROPOSED BUILDING



1351 West Tapp Road
Bloomington, Indiana 47403
Phone: 812.336.8277
Fax: 812.336.0837
www.brgwi.com

BLOMINGTON BEDFORD PAOLI

PRELIMINARY
NOT FOR
CONSTRUCTION

BUGS-BUSS Youth Training Center
Street Address
Bloomington, IN 47403
BRG Project No: 7303

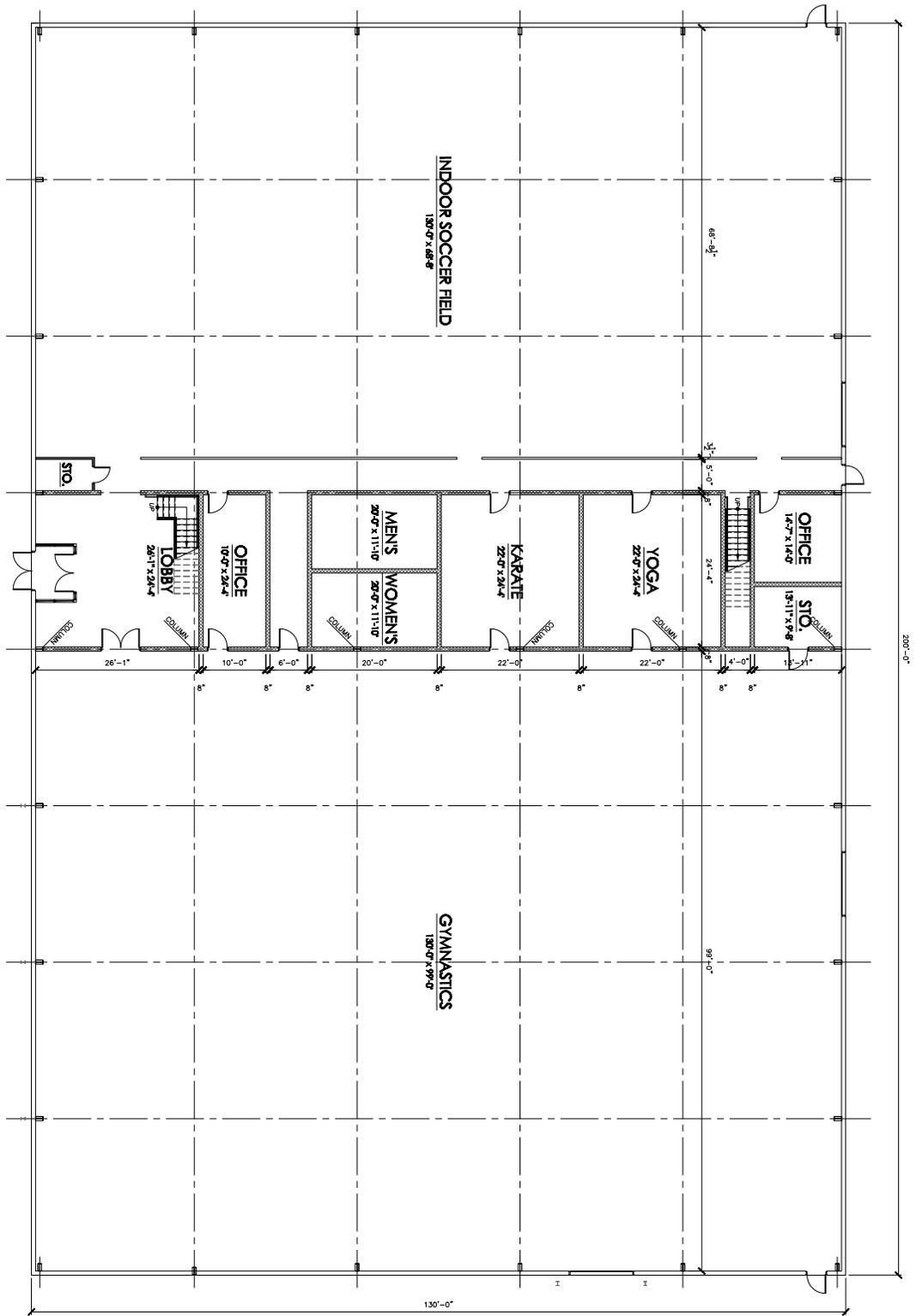
SITE UTILITIES PLAN

SCALE: 1" = 20'

Date: _____ Issue: _____

Drawn By: DBH
Designed By: DBH
Checked By: WCB

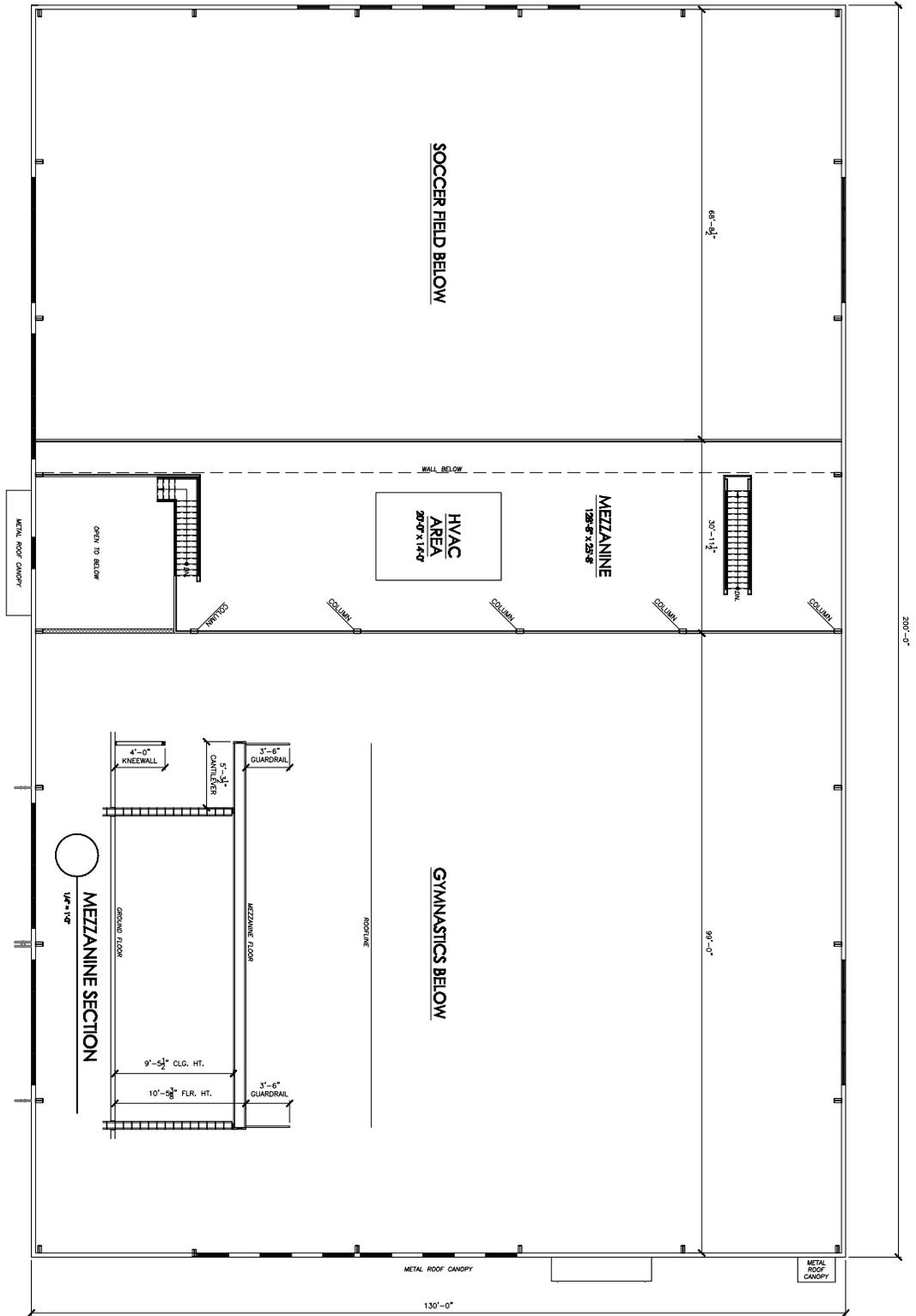
C601




FIRST FLOOR PLAN
 A NEW FACILITY FOR THE
 COMMUNITY CENTER
 & SOCCER FIELD
 1/8" = 1'-0"
 COMMUNITY CENTER ARCHITECTURAL DESIGN
 08/27/11
 CENTRAL LAYOUT



PUD-29-11
 First Floor Plan



MEZZANINE PLAN

N

1/4" = 1'-0"

A NEW FACILITY FOR
 THE
 COMMUNITY
 AND
 RECREATION
 CENTER
 IN
 THE
 CITY OF
 WESLEY,
 FLORIDA

DESIGNED BY
 CH2M HILL
 ARCHITECTURAL
 PARTNERS

DATE
 09/27/11

SCALE
 1/4" = 1'-0"

PUD-29-11
 Second Floor Plan



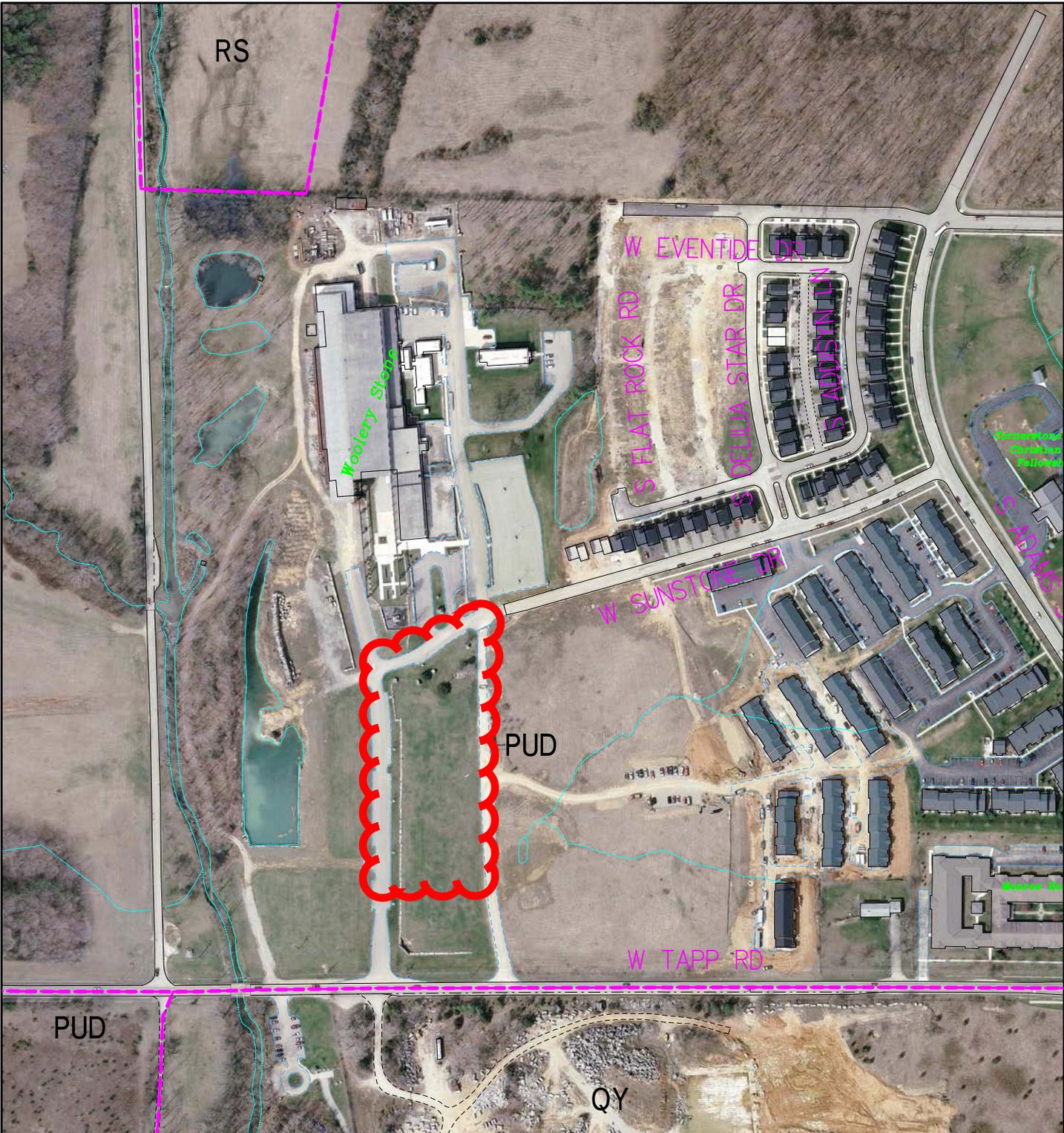
A NEW THREE STORY BUILDING
SCALE: 1/8" = 1'-0"

WOOLERY STONE MILL AT WOOLERY FARM

PREPARED BY KIRKWOOD DESIGN STUDIO P.C. ARCHITECTS

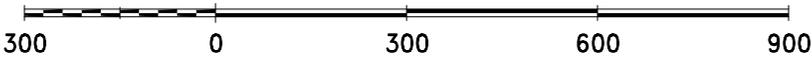
07.10.02

PUD-29-11
Buildings approved on Keg Road

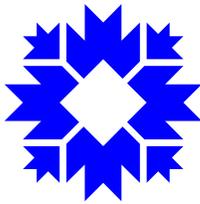


PUD-29-11
2010 Aerial Photo

By: roachja
1 Nov 11

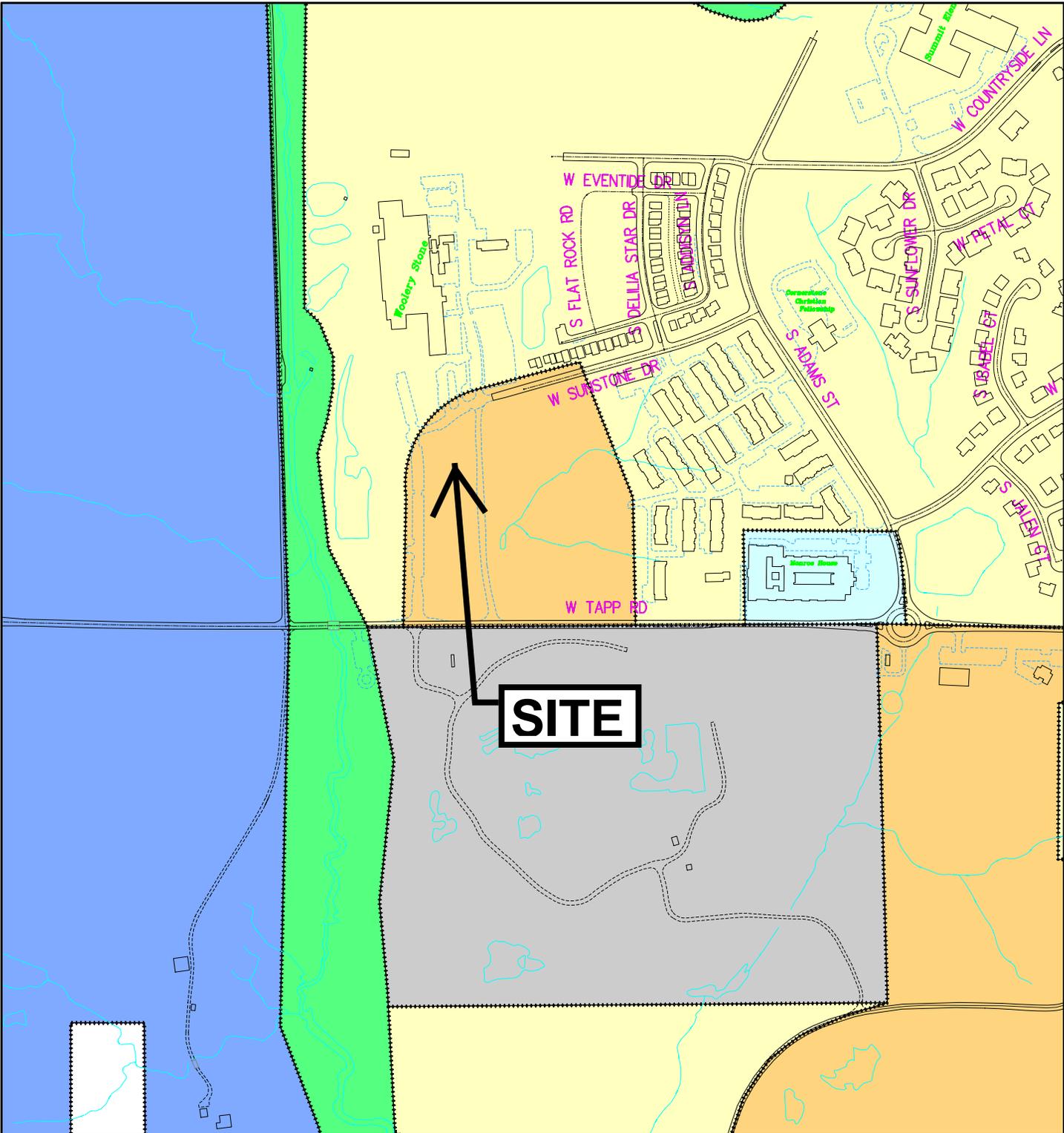


City of Bloomington
Planning



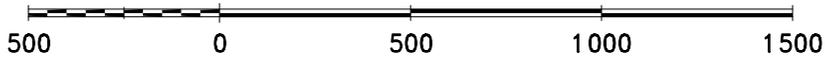
Scale: 1" = 300'

For reference only; map information NOT warranted.

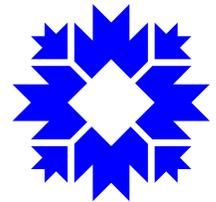


PUD-29-11
2002 GPP Map

By: roachja
3 Nov 11



City of Bloomington
Planning



Scale: 1" = 500'

For reference only; map information NOT warranted.

**BLOOMINGTON PLAN COMMISSION
STAFF REPORT FIRST HEARING
Location: 1800 W. Tapp Road**

**CASE #: PUD-31-11
DATE: November 7, 2011**

PETITIONER: Woolery Ventures, LLC (Randy Cassady)
PO Box 122, Ellettsville

COUNSEL: Bledsoe, Riggert and Guerrettaz
1351 W. Tapp Road, Bloomington

REQUEST: The petitioner is requesting a preliminary and final plat for seven lots and one common area in Parcels A and I of the Woolery Planned Unit Development (PUD).

SUMMARY: The property is located northeast of the corner of W. Tapp Road and S. Weimer Road. The property is 39.3 acres in area and is zoned Planned United Development (PUD). The Woolery PUD was created in 1994 (PUD-64-94) and was amended in 2001 to allow a mixed use redevelopment of the former Woolery Mill on Parcel A (PUD-35-01). A final plan was approved in 2002 to redevelop the stone mill with a hotel, conference center and residential units, allow for cut and fill within the floodway of the West Fork of Clear Creek and approve construction of six mixed use buildings (PUD-16-02). As of today, the Mill Administration building has been remodeled to house the offices of Cassady Electric, a new road has been built (to be called S. Keg Road) and grading within the floodplain has been completed. While no final plan has been approved for Parcel I, the Plan Commission is also currently reviewing a PUD Preliminary Plan amendment that would facilitate construction of a 26,000 recreation center on Parcel I (PUD-29-11).

This plat would allow creation of seven lots and a common lot.

- Lot 1: 8.7 acres. Remainder of Parcel I. Nothing proposed or approved for this lot
- Lot 2: 0.8 acre. Northeast corner of Keg Rd. and Tapp Rd. Nothing proposed or approved for this lot
- Lot 3: 1.7 acres. Proposed for the 26,000 square foot recreation center (PUD-29-11)
- Lot 4: 3.7 acres. Approved for 6 mixed use buildings. Not yet constructed.
- Lot 5: 7.7 acres. Contains former Woolery Stone Mill. Approved for a hotel/conference center and condominiums. Not yet constructed.
- Lot 6: 1.8 acres. Contains Mill Administration building. Currently contains the office of Cassady Electric.
- Lot 7: 1.7 acres. Approved for a warehouse. No final plan approval yet.
- Common Lot: 9.5 acres. Contains the floodplain and channel of Clear Creek. Development is prohibited on this lot.

Because this petition involves dedication of right-of-way for a new public street, two hearings is required.

PLAT ISSUES:

Preliminary plat discrepancies: There are parts of the proposed preliminary plat that do not match the actually constructed property. The petitioner's engineers are working on accurately surveying the property to ensure that there are no setback encroachments of other issues with the plat. In addition, the existing contour lines of Lot 1 and required street trees have not been indicated on the preliminary plat. A revised preliminary plat must be provided prior to the second hearing.

Trail Extension: PUD-35-01 and the subsequent PUD Final Plan in 2002 required the construction of a 12 foot wide extension of the Clear Creek Trail. The trail was to be placed within a 50 foot wide right-of-way and dedicated to the City. The trail has not yet been built. Since that time, the plans for the extension of the Clear Creek trail have changed. The City no longer wishes the petitioner to construct a trail on the east side of the creek. Instead, the City intends to relocate Weimer Rd. to the west and down-grade the existing Weimer Rd. so that the existing Weimer Rd. pavement can be used as the trail extension. Because of this, the city will no longer accept dedication of a trail constructed by the petitioner on the east side of the creek. The petitioner may choose to still build the trail, but it would remain a private trail.

One exception to this is at the north end of the property. Immediately north of the Mill Building, a public trail extension is planned as part of the Sudbury PUD. This extension would parallel the Clear Creek floodplain and dead end into the north end of Lot 5. The petitioner has provided a 50 foot wide trail right-of-way along the north side of Lot 5 that will allow the City in the future to connect the downgraded Weimer Rd./future Clear Creek Trail to the Sudbury PUD Trail. This connection will require the construction of a bridge over Clear Creek. The petitioner is not responsible for construction of this trail or bridge.

Access: Parcel I was approved for a single new access point onto Tapp Rd. with PUD-64-94. An early version of this plat showed this drive immediately east of Lots 2 and 3, in the location of an existing gravel service drive. Staff believes that this location is too close to the intersection of Keg Rd. and Tapp Rd. and would create a drive in an awkward location for the use of Lot 1. The petitioner's engineers have shown that this drive can be moved further to the east and still meet sight distance requirements, but ask that the final location of this drive be determined with a future PUD Final Plan for Lot 1. Lots 2 and 3 gain access to Keg Rd. via a shared drive along the property line. Lots 4, 5, 6 and 7 gain access from the internal streets, Keg Rd. and Sunstone Dr.

In addition, the petitioner has provided a private road stub to the north, along the east side of the mill, to allow for connectivity with the future residential uses in the Sudbury PUD. This is in compliance with the approved PUD Final Plan.

Environmental Protection: The common area “park” contains most of the environmentally sensitive features on the property. It contains the majority of the floodplain for Clear Creek, the creek itself and its riparian corridor, many large trees, a series of ponds and land that was to be replanted as part of a floodplain mitigation plan. This property will be preserved with a drainage and conservation easement and controlled with a facilities maintenance plan. The floodplain elevations must also be indicated on both the preliminary and final plats.

While the common area contains the majority of the floodplain, it does not contain all of it. Some areas of the floodplain are located on Lot 4. These areas were approved for construction of buildings and parking lots with the PUD Preliminary Plan amendment. These areas are not included in either drainage or conservation easements.

Lot 8 contains a wooded area and a series of karst of “pusedo-karst” features. The PUD Preliminary Plan approved a building in this area to be used as a warehouse for Cassady Electric, but a Final Plan has not yet been approved for this building. The Preliminary Plan also required preservation of the trees not impacted by construction of the building and associated parking lot. In addition, this lot contains several features that appear to be karst. These features were studied by Earth-Tech in 2001, but they did not give a definitive assessment as to whether or not they should be regulated as karst features and protected by a buffer. The report recommends either avoidance, as required by the UDO, or additional study. If the petitioner at the time of a future PUD Final Plan desired to not protect the features as karst, additional study of the features and their hydrology will be required. Since the building and parking lot have not yet been designed or approved, staff recommends deferral of platting tree preservation and karst conversancy easements on Lot 8 until after approval of a PUD Final Plan.

Facilities Maintenance Plan: The UDO requires a facilities maintenance plan for the common area lot, here labeled as a “park.” The plan must spell out the ownership and ongoing maintenance responsibility of the lot. This lot includes most of the floodplain of Clear Creek as well as the ponds used for detention and water quality for at least Lots 4-7. This plan must be submitted for review and approved by the Plan Commission with the Final Plat approvals.

Street trees and sidewalks: The PUD Preliminary Plan amendment waived sidewalk requirement for the east side of Weimer Rd. because of the future trail extension in this area. The City will be rebuilding Tapp Rd. and installing a new sidepath along the south side of this property this coming summer.

The east side of Keg Rd. and all of Sunstone Dr. require a five-foot wide sidewalk and street trees spaced no further than 40 feet on center. These features are not yet shown on the preliminary plat.

The west side of Keg Rd. was approved with a ten-foot wide sidewalk with street trees within tree grates as part of the PUD Final Plan. The approved buildings were to be built immediately at the edge of the right-of-way. Sidewalks and street trees are not yet shown on the preliminary plat.

All sidewalks and street trees along the public right-of-way must be installed or bonded for with this plat. Staff recommends that bonding for the south side of Sunstone Dr. adjacent to Lot 1 be deferred until there is a PUD Final Plan approved for development on this lot.

The petitioner has requested deferral of bonding on the 10 foot wide sidewalk, street trees and tree grates on the west side of Keg Rd. This streetscape was required with the PUD Final Plan for the new construction which hasn't been built yet. The petitioner contends that because of the building design, with the structures built at the right-of-way line, that construction of the sidewalk and the buildings at the same time would be most efficient. Staff requests discussion by the Plan Commission on this issue. Absent Commissioner input, this request is supportable to staff.

Right-of-way: A 50 foot right-of-way dedication is required from the center of Tapp Road, a 35-foot dedication is required from the center of Weimer Road and a 60-foot total right-of-way is required for Keg Rd. and Sunstone Dr. All required right-of-way is shown on the plat.

Utilities: Water and sanitary sewer plans and easements have been submitted to City Utilities and are under review. All water and sanitary sewer mains associated with this project have already been installed. No specific utility plan has been prepared for Lot 1 because no development has yet to be proposed.

Developer Track Record: This petition comes to the Plan Commission in an attempt to clean-up an illegal subdivision of land that took place in 2002. The petitioner illegally subdivided Parcel A into four parcels through recording of deeds. In addition, the petitioner and owner of Parcel I at the time, SFT Properties LLC, illegally split Parcel I into two lots. This plat will correct these illegal subdivisions and create a lot for sale for the proposed Recreation Center use.

In addition, the petitioner has failed to comply with some of the commitments and conditions of PUD-16-02, including failure to provide a "dry hydrant" for Fire Department use and failure to complete an approved floodplain mitigation planting plan. These items are not specifically platting issues and will be handled by the Planning Department separately.

Items for discussion:

1. **Status of Lot 7:** Is it appropriate to delay platting of required tree preservation and karst conservancy easements on Lot 7 until after a PUD Final Plan is approved?
2. **Sidewalks and street trees:** Should bonding for the sidewalks and street trees along Lot 1 be delayed until there is a PUD Final Plan approved for this lot? Should bonding for the sidewalk, street trees and tree grates be deferred until the new mixed use building are built on Lot 4?

RECOMMENDATIONS: Staff recommends that this petition be forwarded to a second hearing.

MEMORANDUM

Date: November 2, 2011
To: Bloomington Plan Commission
From: Bloomington Environmental Commission
Through: Linda Thompson, Senior Environmental Planner
Subject: PUD-31-2011 Woolery Planned Community, Phase IX

This memorandum contains the Environmental Commission's (EC) input and recommendations regarding a Preliminary and Final Plat for the Woolery Planned Unit Development (PUD).

ISSUE OF SOUND ENVIRONMENTAL PLANNING:

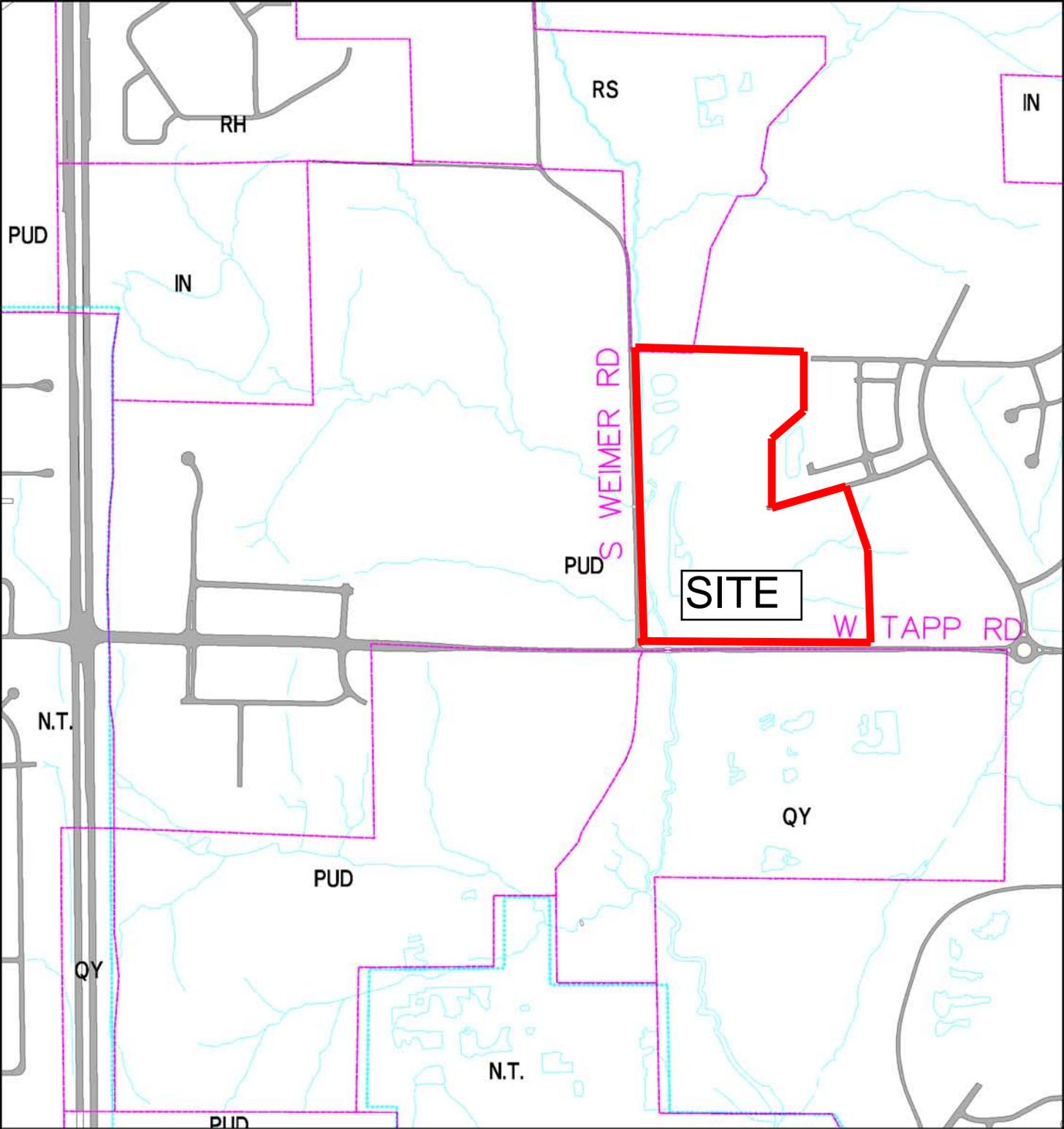
The site is home to several karst or pseudokarst* features in its northeast corner. These sinkhole-looking features were briefly looked at and considered during review of the Woolery Mill Complex PUD-35-01 in 2001. At that time it was unclear if they were naturally-occurring sinkholes, or if they were pseudokarst sinkholes developed from soil piping through fill. Whether or not the soil piping resulted from naturally-occurring dissolution of limestone, or migration through watercourses between unconsolidated materials is not relevant. With either scenario, the features behave like sinkholes in that they are direct conduits to groundwater and should have conservation buffers surrounding them.

The petitioner should take one of two actions before a future Final Plan is submitted for the northeast corner of the site. One choice is to conduct a geologic study of the subsurface to determine the validity and extent of the sinkholes prior to delineating a Karst Conservancy Easement. The other choice is to delineate a Karst Conservancy Easement, with an associated 10 foot building setback, based on the surface expression and regardless of the provenance of the sinkholes.

EC RECOMMENDATION:

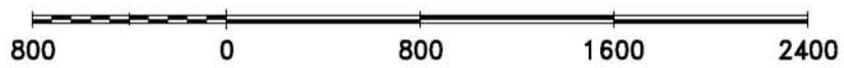
The EC recommends that before a future Final Plan is approved, the size and shape of the Karst Conservancy Easement, and an associated 10 foot building setback, are defined by either a geologic study of the area, or a topographic interpretation of the last closed contours of the sinkholes.

*pseudokarst: Terrane with features similar to karst but produced by a process other than the dissolving of bedrock producing depressions and pipes.



PUD-31-11
Location and Zoning Map

By: roachja
3 Nov 11



City of Bloomington
Planning



59
Scale: 1" = 800'

For reference only; map information NOT warranted.

October 24, 2011

Jim Roach
City of Bloomington, Planning Department
401 N. Morton St., Suite 160
PO Box 100
Bloomington, IN 47402-0100

Re: Woolery Planned Community Phase IX preliminary and final plat
Petitioner's Statement
BRG Project No. 7303

Dear Jim,

On behalf of Woolery Ventures and other interested parties, we are requesting consideration by the Plan Commission for the approval of a preliminary and final plat for Phase IX of the Woolery Planned Community. This petition is being parallel processed with a request for a PUD Amendment to add the use "recreation center" to the approved list of uses in the controlling PUD to allow for the development of the BUGS-BUSS Youth Training Center on lot 3. Plat approval is required to allow for the sale of the lot.

The BUGS-BUSS facility is proposed for Lot 3 (approximately 1.68 acres) of Phase IX of the Woolery Planned Community. Public streets, already constructed, exist on the west and north sides of the site. They extend from an intersection with Tapp Road and connect to Sunstone Drive to the east. The proposed development is accessed from the public street to the west and shares a common drive with Lot 2, a .75 acre lot to the south, which has frontage on Tapp Rd. and shall remain undeveloped at this time. A 20' shared drive easement is provided between lots 2 and 3, which also makes Lot 1 accessible from the west.

Stormwater, sanitary and water services are available at the site and all have the capacity to service the proposed uses. Easements are provided for all existing and known proposed utilities. There is also a 40' public R/W provided for along a portion of the northern boundary of this plat to facilitate expansion of the multi-use trail, eventually to be located in the existing Weimer Road R/W. We believe that both stormwater detention and treatment have already been provided on the proposed common area lot, which was designed cover development of Lots 2-8. Lot 1 will be required to provide its own detention and treatment facilities. Discussions with the City of Bloomington Utilities are underway in this regard. A facilities maintenance plan, as well as CC&R's will be provided prior to final plat approval.

Included with this petitioner's statement are the application form and filing fee, as well as preliminary and final plat drawings. Notices will be sent to adjacent property owners as required.

Please contact me if you have any questions.

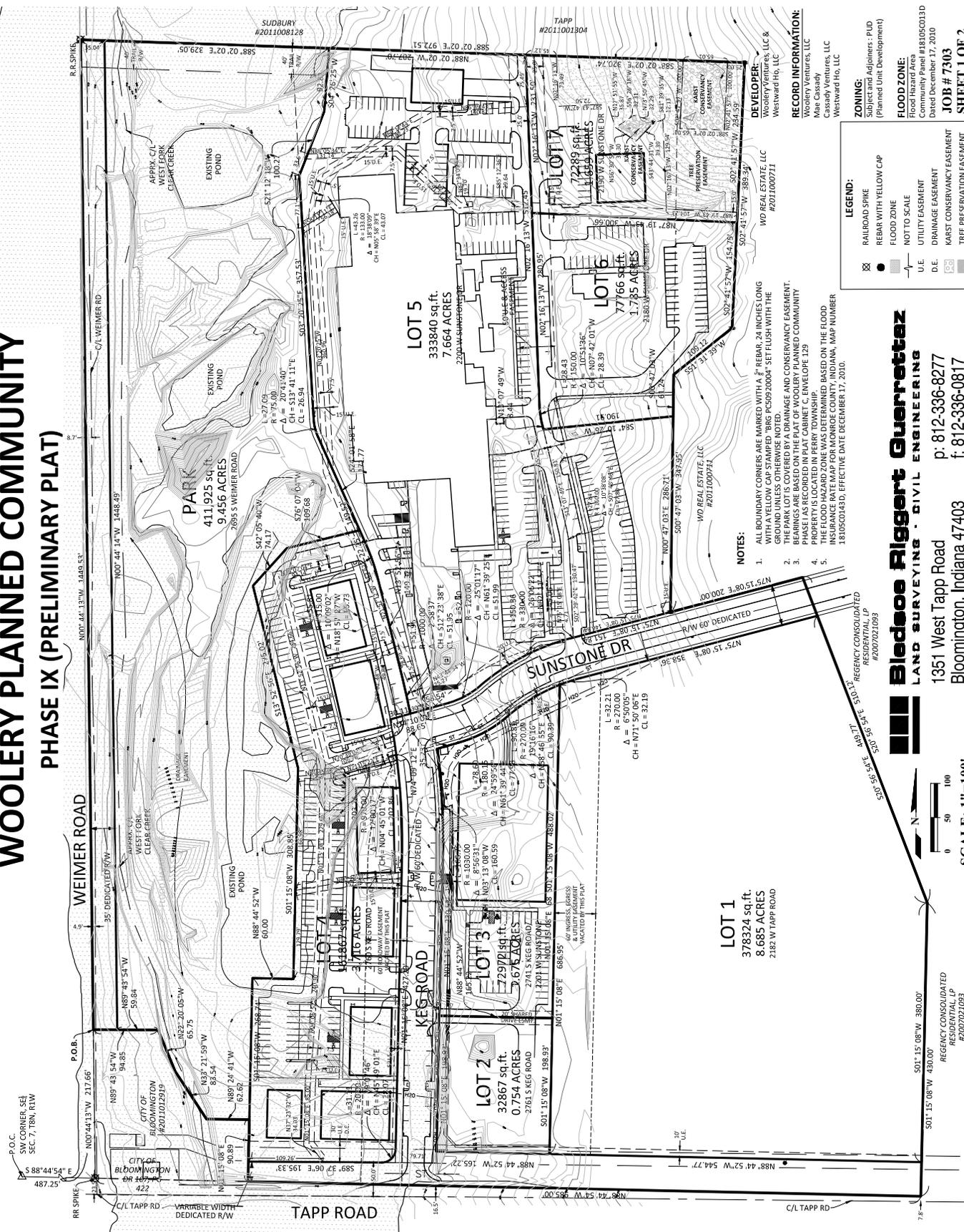
Sincerely,

Dan Neubecker, Project Manager
Bledsoe Riggert Guerrettaz

xc: Randy Cassady, Woolery Ventures
File – Project No. 7303

PUD-31-11
Petitioner's Statement

WOOLERY PLANNED COMMUNITY PHASE IX (PRELIMINARY PLAT)



NOTES:

- ALL BOUNDARY CORNERS ARE MARKED WITH A 3" REBAR, 24 INCHES LONG WITH A YELLOW CAP STAMPED "BRG FC509.20004". SET FLUSH WITH THE GROUND UNLESS OTHERWISE NOTED.
- THE PARK LOT IS COVERED BY A DRAINAGE AND CONSERVANCY EASEMENT.
- THE PROPERTY IS LOCATED IN PERRY TOWNSHIP.
- PROPERTY IS LOCATED IN PLAT CABINET, C.C. ENVELOPE 25.
- THE FLOOD HAZARD ZONE WAS DETERMINED BASED ON THE FLOOD INSURANCE RATE MAP FOR MONROE COUNTY, INDIANA, MAP NUMBER 18105043D, EFFECTIVE DATE DECEMBER 17, 2010.

DEVELOPER:
Wooley Ventures, LLC
Westward Ho, LLC

RECORD INFORMATION:
Wooley Ventures, LLC
Westward Ho, LLC
Cassady Ventures, LLC
Westward Ho, LLC

ZONING:
Subject and Adjacent: PUD
(Planned Unit Development)

FLOOD ZONE:
Flood Hazard Area
Community Panel #18105013D
Dated December 17, 2010

JOB # 7303

SHEET 1 OF 2

LEGEND:

- RAILROAD SPIKE
- REBAR WITH YELLOW CAP
- FLOOD ZONE
- NOT TO SCALE
- UTILITY EASEMENT
- DRAINAGE EASEMENT
- KARST CONSERVANCY EASEMENT
- TREE PRESERVATION EASEMENT

SCALE: 1"=100'

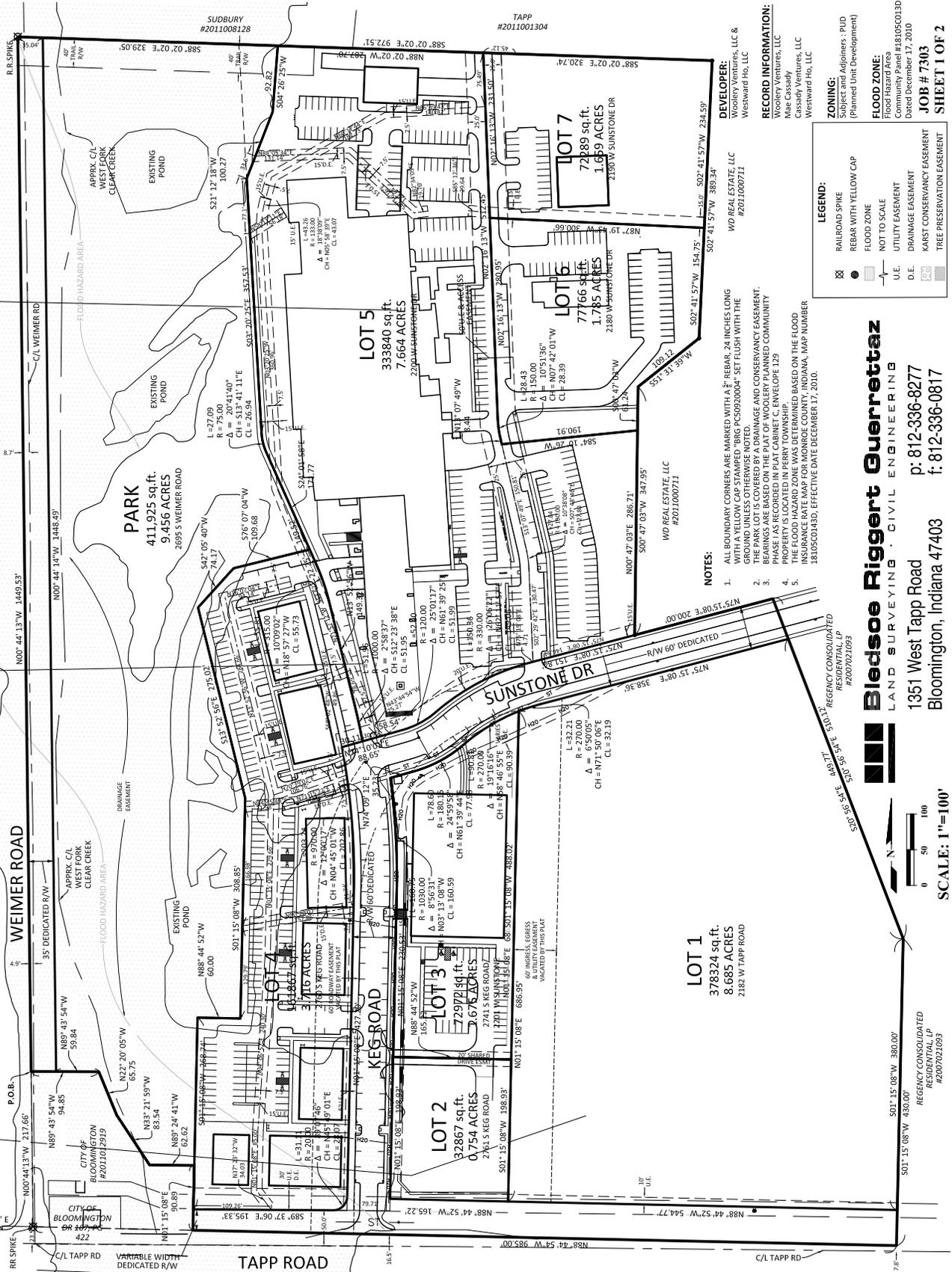
Bledsoe Riggert Guerrettez
LAND SURVEYING · CIVIL ENGINEERING

1351 West Tapp Road
Bloomington, Indiana 47403

p: 812-336-8277
f: 812-336-0817

PUD-31-11
Preliminary Plat with contour lines

WOOLERY PLANNED COMMUNITY PHASE IX (PRELIMINARY PLAT)



LOT 1
378324 sq. ft.
8.685 ACRES
2182 W TAPP ROAD

LOT 2
32867 sq. ft.
0.754 ACRES
2761 S KEG ROAD

LOT 3
72972 sq. ft.
1.676 ACRES
2721 W SUNSTONE DR

LOT 4
3716 ACRES
2745 S KEG ROAD

LOT 5
333840 sq. ft.
7.664 ACRES
2230 W SUNSTONE DR

LOT 6
77766 sq. ft.
1.785 ACRES
2188 W SUNSTONE DR

LOT 7
72889 sq. ft.
1.659 ACRES
2150 W SUNSTONE DR

PARK
411,925 sq. ft.
9.456 ACRES
2695 S WEIMER ROAD

- NOTES:**
- ALL BOUNDARY CORNERS ARE MARKED WITH A 3" REBAR, 24 INCHES LONG WITH A YELLOW CAP STAMPED "BRG PC50920004". SET FLUSH WITH THE GROUND UNLESS OTHERWISE NOTED.
 - THE PARK LOT IS COVERED BY A DRAINAGE AND CONSERVANCY EASEMENT.
 - PROPERTY IS LOCATED IN PERRY TOWNSHIP, MONROE COUNTY, INDIANA.
 - THE FLOOD HAZARD ZONE WAS DETERMINED BASED ON THE FLOOD INSURANCE RATE MAP FOR MONROE COUNTY, INDIANA, MAP NUMBER 18105G0149D, EFFECTIVE DATE DECEMBER 17, 2010.

DEVELOPER:
Wooley Ventures, LLC & Westward Ho, LLC

RECORD INFORMATION:
Wooley Ventures, LLC
Westward Ho, LLC
Mary Cassidy
Cassidy Ventures, LLC
Westward Ho, LLC

ZONING:
Subject and Adjacent: PUD (Planned Unit Development)

FLOOD ZONE:
Flood Hazard Area
Community Panel #18105G0149D
Dated December 17, 2010

JOB # 7303
SHEET 1 OF 2

LEGEND:

☒	RAILROAD SPIKE
●	REBAR WITH YELLOW CAP
○	FLOOD ZONE
—	NOT TO SCALE
—	UTILITY EASEMENT
—	DRAINAGE EASEMENT
—	KARST CONSERVANCY EASEMENT
—	TREE PRESERVATION EASEMENT

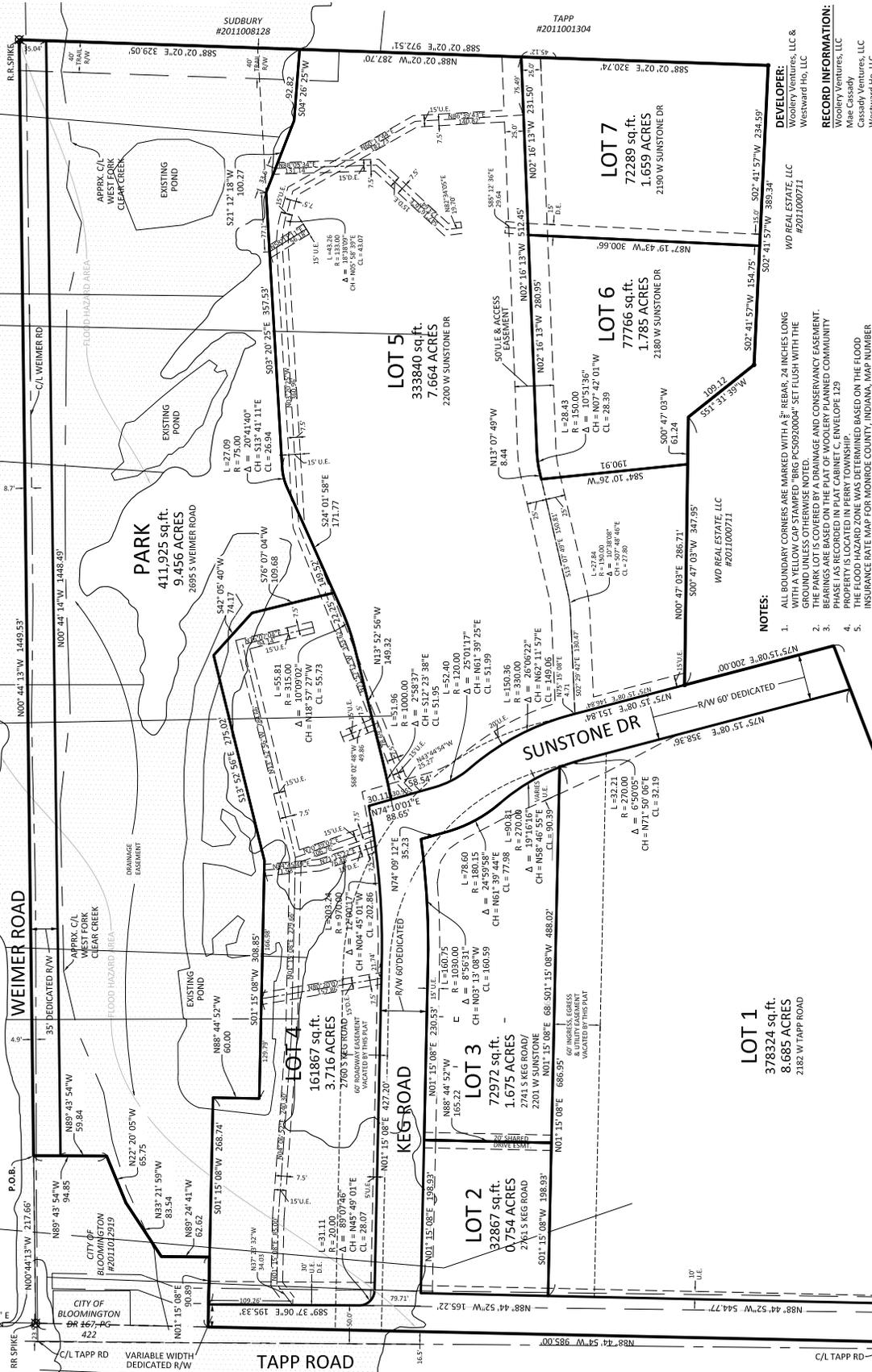
Bledsoe Riggert Guerrettaz
LAND SURVEYING · CIVIL ENGINEERING
151 West Tapp Road
Bloomington, Indiana 47403
p: 812-336-8277
f: 812-336-0817

REGISTRY CONSOLIDATED
RESIDENTIAL LP
#2007021093

REGISTRY CONSOLIDATED
RESIDENTIAL LP
#2007021093

SCALE: 1"=100'

WOOLERY PLANNED COMMUNITY PHASE IX (FINAL PLAT)



DEVELOPER:
Wooley Ventures, LLC &
Westward Ho, LLC

RECORD INFORMATION:
Westward Ho, LLC
Wooley Ventures, LLC
Mary Cassidy
Cassidy Ventures, LLC
Westward Ho, LLC

ZONING:
Subject and Adjacent: PUD
(Planned Unit Development)

FLOOD ZONE:
Flood Hazard Area
Community Panel #180105013D
Dated December 17, 2010

JOB # 7303

SHEET 1 OF 2

LEGEND:

- RAILROAD SPIKE
- REAR WITH YELLOW CAP
- FLOOD ZONE
- NOT TO SCALE
- UTILITY EASEMENT
- DRAINAGE EASEMENT
- KARST CONSERVANCY EASEMENT
- TREE PRESERVATION EASEMENT

NOTES:

- ALL BOUNDARY CORNERS ARE MARKED WITH A 3" REBAR, 24 INCHES LONG WITH A YELLOW CAP STAMPED "BRG PC50920004". SET FLUSH WITH THE GROUND UNLESS OTHERWISE NOTED.
- THE PARK LOT IS COVERED BY A DRAINAGE AND CONSERVANCY EASEMENT.
- PHASE IS LOCATED IN PERRY TOWNSHIP, MONROE COUNTY, INDIANA.
- PROPERTY IS LOCATED IN PERRY TOWNSHIP, MONROE COUNTY, INDIANA.
- THE FLOOD HAZARD ZONE WAS DETERMINED BASED ON THE FLOOD INSURANCE RATE MAP FOR MONROE COUNTY, INDIANA, MAP NUMBER 18105G0143D, EFFECTIVE DATE DECEMBER 17, 2010.

SCALE: 1"=100'

Bledsoe Riggert Guerrettaz
LAND SURVEYING · CIVIL ENGINEERING

151 West Tapp Road
Bloomington, Indiana 47403

p: 812-336-8277
f: 812-336-0817

PUD-31-11
Final Plat

5010 Stone Mill Road, Bloomington, Indiana 47408

December 3, 2001

Mr. Phillip Tapp
Tapp, Bledsoe & Riggert, Inc.
1351 W. Tapp Road
Bloomington, IN 47403

Subject: Inspection of topographic depressions on Woolery Farm Property

At your request, I performed an inspection this morning of a portion of the Woolery Farm property located northeast of the intersection of Tapp Road and Weimer Road, Bloomington, Indiana. The purpose of this inspection was to determine if certain topographic depressions on the property should be regarded as sinkholes. A few small topographic depressions occur in shallow drainageways on a gentle slope in a wooded area, north of the former Mill Office in the extreme northeast corner of the property. Two of these features had open holes in their bottoms that expose the underlying unconsolidated materials. I noted that at both of these locations the exposed material consisted of anthropogenic fill. This material contained mixed soil, mill fines, small pebble-sized fragments of Salem Limestone, and larger pieces of cut mill stone. These depressions are not developed in natural soil.

Telephone
812-336-0972
Facsimile
812-336-3991

The features resemble natural sinkholes, and clearly have formed from soil piping activity. However, the presence of the exposed fill material in the bottom of at least two features suggests that piping may have occurred through coarse fill material placed on the slope and not through natural solution conduits in the limestone. In this situation, the depressions would more correctly be regarded as *pseudokarst* features, as defined in *A Lexicon of Cave and Karst Terminology with Special Reference to Environmental Karst Hydrology (1999)*. EPA/ 600/R-99/006. I have seen this type of piping, not related to karst solution conduits occur at other stone mill sites in Monroe County. In one case, we have documented for the Indiana Department of Transportation that the entire low flow of Stout's Creek is diverted underground through coarse mill waste limestone material and flows several hundred feet through the subsurface to emerge at the downstream toe of the fill as a spring.

It is not clear to me at this point if the requirements of Section 20.06.05.02 of the City of Bloomington Municipal Code regarding an environmental review plan for karst terrain are applicable to this situation. If the soil piping involves ground water flow through the limestone bedrock, then this is a karst situation, and the environmental review plan requirements would, in my opinion, apply. If these are *pseudokarst* features that only involve piping through coarse fill placed on the slope, then those aspects of Section 20-06.05.02 related to the protection of natural karst features are probably not applicable.



c:\hold\woollery_inspection.doc

PUD-31-11
2001 Karst inspection

12/3/2001
Page 2

If the presence or absence of karst terrain becomes a land use or rezoning issue I would recommend that additional investigations such as air photo review, karst inventory and soil testing be conducted. Otherwise, I would recommend that a thorough geotechnical investigation of any building site on the fill area be conducted to identify potential soil piping areas that could be detrimental to construction.

Please contact me if you have any questions.

Sincerely,

JOHN BASSETT

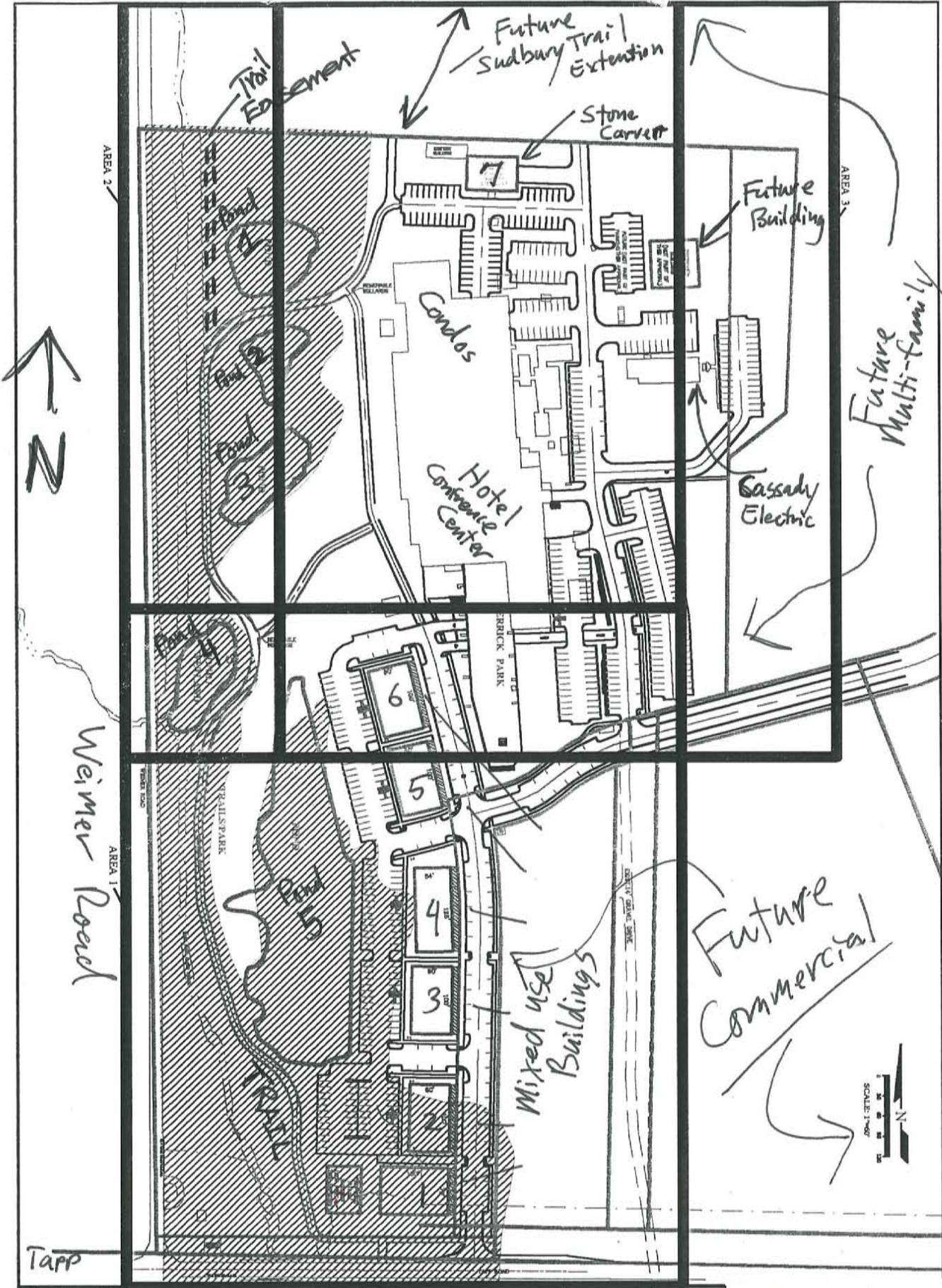
John Bassett
Licensed Professional Geologist



A **tyco** INTERNATIONAL LTD. COMPANY

c:\hold\wollery_inspection.doc

PUD-31-11
2001 Karst inspection



PUD-31-11
 Approved Final Plan for Parcel A

Bledsoe Tapp & Riggert, Inc.
 Quality Land Surveying and Civil Engineering Services
 1311 West Tapp Road
 Bloomington, IL 61703
 (312) 336-8271
 FAX (312) 336-0817

1324 "K" Street, Room 130
 Bedford, MA 01730
 (312) 275-0801
 FAX (312) 275-8251

Overall
 Site Plan
 PUD-16-02

28

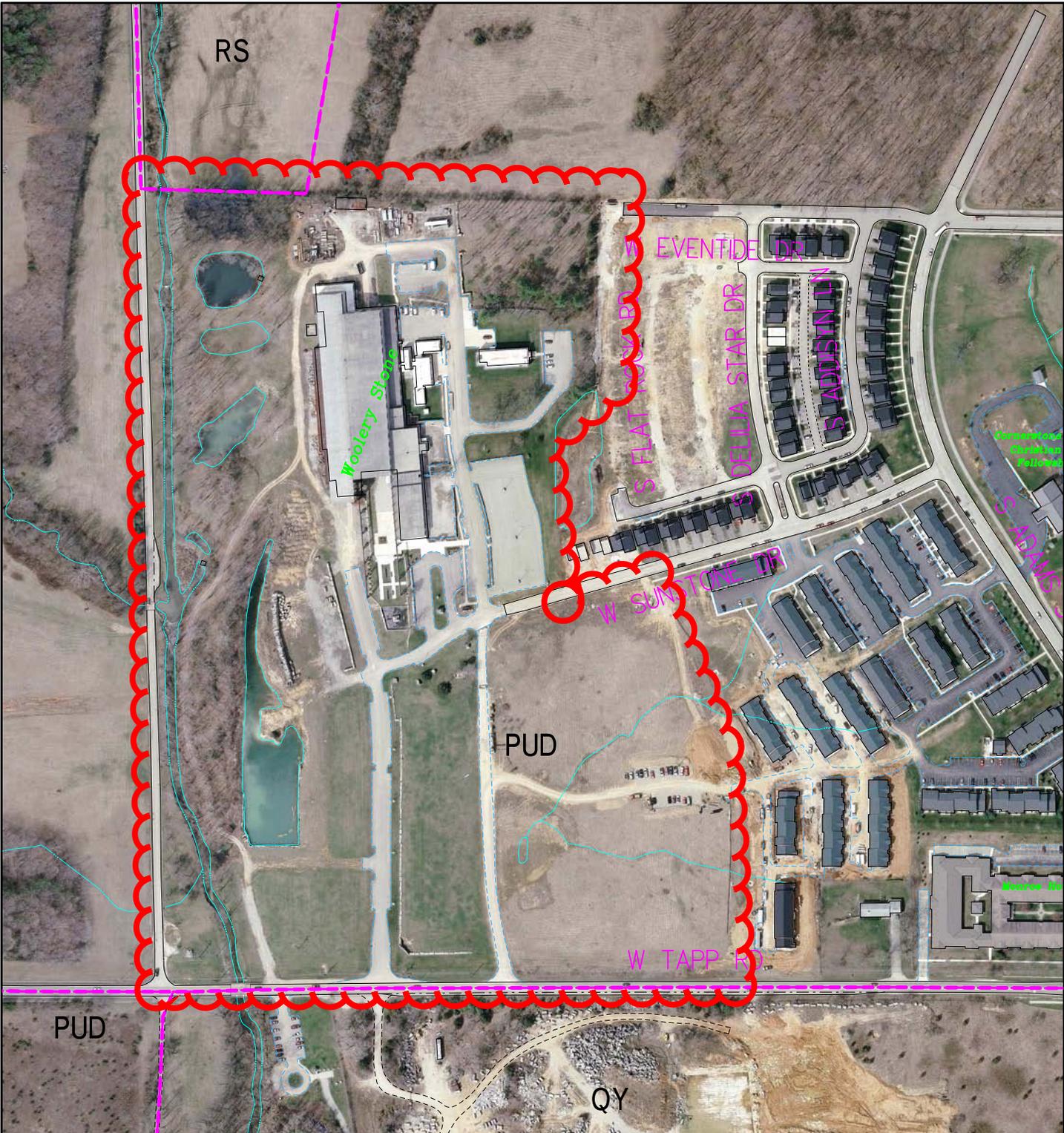


WOOLERY STONE MILL AT WOOLERY FARM - STREET VIEW TOWARDS STONE MILL

PREPARED BY KIRKWOOD DESIGN STUDIO, PC AND BLEDSOE TAPP & RIGGERT, INC.

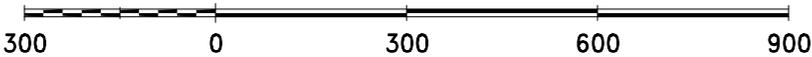
KIRKWOOD DESIGN STUDIO, PC COPYRIGHT 2001

PUD-31-11
Rendering of new buildings on west side of Keg Rd.



PUD-31-11
2010 Aerial Photo

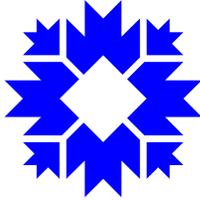
By: roachja
1 Nov 11



For reference only; map information NOT warranted.



City of Bloomington
Planning



Scale: 1" = 300'

**BLOOMINGTON PLAN COMMISSION
STAFF REPORT**

**CASE #: UV-30-11
DATE: November 7, 2011**

Location: 535 & 545 E. Southern Drive; 570, 580, and 586 E. Hillside Drive; and 1506 S. Henderson Street

PETITIONER: Max and Gilda Lauchli
570 E. Hillside Drive

COUNSEL: Bynum Fanyo & Associates, Inc.
528 N. Walnut Street, Bloomington

REQUEST: The petitioner is requesting use variance approvals to allow multi-family residential units on the ground floor within the Commercial Limited (CL) zoning district and commercial space within a Residential Multifamily (RM) zoning district. This use variance request requires Plan Commission review and recommendation to the Board of Zoning Appeals.

Zoning:	CL and RM
Acreage:	CL – 0.275 Acres, RM – 0.549 Acres
GPP Designation:	Neighborhood Activity Center and Urban Residential
Existing Land Use:	Single Family – Residential Rentals
Proposed Land Use:	Multi-family and Commercial
Surrounding Uses:	North - Mixed-Use (South Dunn Street PUD) South - Office and Residential East - Single Family West - Single Family

SUMMARY: The petitioners have accumulated 6 parcels that include three properties at the southwest corner of E. Hillside Drive and S. Henderson Street and three properties at the northwest corner of E. Southern Drive and S. Henderson Street. The 6 properties are separated by an east/west public alley that divides the parcels fronting on Hillside Dr. from those fronting on Southern Dr. All three of the Hillside lots are zoned Residential Multifamily (RM) and have existing structures on them, two of which are vacant. The eastern two lots on Southern Dr. are zoned Commercial Limited (CL) and the third Southern Dr. lot is zoned RM. There are two existing structures on these three lots, one of which is currently vacant.

The petitioners are proposing to raze the existing structures and develop the lots with three two-story structures. Two of the structures would be solely residential buildings while the proposed structure located adjacent to the Hillside Dr. and Henderson St. intersection would be a mixed-use building with approximately 2000 square feet of commercial space.

The current zoning on the properties would require non-residential use on the entirety of the first floor of the two CL zoned parcels located on Southern Dr. and would not allow any commercial use on any of the lots that front on Hillside Drive. The petitioners are proposing to essentially flip the commercial portion of their project to the Hillside

frontage. Staff finds the Hillside frontage to be more desirable for the commercial development and would complement the mixed-use buildings located in the South Dunn Street development to the north. To allow for the commercial to be placed on the RM portion and to not have any commercial on the CL portion of the property, a use variance is necessary. The petitioners are also requesting a package of variances from the Board of Zoning Appeals to allow their redevelopment project to move forward. The variances include setbacks, density (to allow for a more even distribution of the allowable units across the entire property), impervious surface coverage, and landscaping.

The proposed site plan includes three new structures, all of which are proposed to be two-stories in height. A mixed-use structure at the Hillside Dr. and Henderson St intersection would have approximately 2072 square feet of commercial space and 9 one-bedroom units. The second building would be located immediately west of the mixed-use building and would house 6 one-bedroom units. The last building would be located on the 3 parcels along Southern Dr. This structure is proposed to have 15 one-bedroom units. The total number of units proposed is 30 one-bedroom units or 7.5 units after Dwelling Unit Equivalents (DUEs) are applied. The current zoning would allow 4.1 units on the CL portion of the property and 3.8 units on the RM portions of the site. Although the most likely use of the commercial space would be a restaurant, staff will recommend to the BZA that all uses permitted within the CL zoning district be permitted for the commercial space.

The mixed-use building has been changed to include a flat roof similar to what has been shown across Hillside Dr. It will have a large amount of storefront glass and a raised entry. The façade of this structure is proposed to be brick. Individual residential entries for the two units fronting on Henderson St. have also been incorporated into the design.

The larger residential building along Southern Dr. would provide pedestrian entry through an open “mouse hole” entryway that would access an internal courtyard. Prior to the Board of Zoning Appeals, staff is requesting that the petitioner explore the addition of a more defined base to this structure along both Henderson St. and Southern Dr. Staff would also recommend that the petitioner explore the use of canopies to further enhance the commercial space on Hillside Dr. The petitioners have also designed the site to accommodate an outdoor patio area in anticipation of a potential restaurant use. This area would be further detailed with the construction of a trellis system between the two structures on Hillside Dr.

The building architecture for the residential buildings is also two stories in height with a pitched roof allowing for vaulted ceilings in the second floor units. The structures would have several exposed balconies and would be clad with cementitious siding. They would mostly be accessed from an internal courtyard with external entryways.

Parking for this site will be provided off of the unimproved alley that bisects the lots. The petitioners are not proposing to vacate the alley, but will widen and shift access slightly to the south to provide access to parking spaces on both sides of the alleys. They are not required to provide any parking with this petition, but have developed a

site plan that has 25 spaces off the alley. They are also proposing to add 8 on-street parking spaces to Hillside Dr. similar to those found on the north side of the street, and formalize 5 on-street parking spaces along Southern Dr. At the request of the City Engineering Department, a pull-off for delivery vehicles has been included along the Henderson St. frontage.

GPP ANALYSIS: The Growth Policies Plan (GPP) designates the eastern portion of this site as a Neighborhood Activity Center (NAC) and the western portion as Urban Residential. The GPP provides significant guidance on these designations. The main points that pertain to this project are as follows:

NAC (Intent)

- is a mixed commercial node that serves as the central focus of each neighborhood.
- must be designed so that it serves the neighborhood adequately without attracting an influx of usage from surrounding areas
- located so that it is easily accessible by pedestrians, minimizing automotive traffic throughout the neighborhood
- will provide small-scale retail and business services within the context of neighborhoods while maintaining compatibility within the existing fabric of development

NAC (Land Use)

- should contain a mix of neighborhood scale retail and office space, as well as services such as day care and higher density housing
- Housing elements are ideally integrated with nonresidential elements such that housing units are situated above commercial and office space
- located ...most probably through the redevelopment of an existing nonresidential use
- The main focus of the NAC should be commercial uses at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes.
- Residential uses should be limited to multifamily development, ideally on floors above street level commercial uses.

NAC (Site Design)

- Compatibility with surrounding established neighborhoods is one of the most important factors in the development of a Neighborhood Activity Center
- must relate to surrounding residential neighborhoods and not adversely affect the livability of these neighborhoods through traffic, lighting, noise, litter or other impacts
- The height of new commercial structures in a NAC shall be limited to three stories in order to minimize the impact of such uses on surrounding residents
- Bus stops, bus pull-offs, or shelters shall be incorporated to maximize transit trips to the NAC
- In order to define the center, buildings should be pushed to the front edge of the site

- Any parking that is provided for a NAC should be primarily serving any residential units that are a part of the development rather than used as an attractor for commercial users
- Parking should be located in the side or rear of buildings, and can be made accessible from an improved alley system in order to minimize street cuts in front of buildings
- All parking areas should also be heavily landscaped in order to soften their impact on the neighborhood

Urban Residential (Intent)

- This category identifies existing residential areas with densities generally ranging from 2 units per acre to 15 units per acre.
- The fundamental goal for these areas is to encourage the maintenance of residential desirability and stability. Where new infill development is proposed, it should be consistent and compatible with preexisting development

Urban Residential (Land Use)

- Single family residential development is the primary land use activity for this category with some additional uses such as places of religious assembly, schools, home occupations, and multifamily housing

Urban Residential (Site Design)

- contain a mixture of densities, housing types (single family vs. multifamily)
- Redevelopment or rehabilitation of existing structures or development of single lots or small parcels should respect the unique character and development pattern of the neighborhood. The development should emphasize building and site compatibility with existing densities, intensities, building types, landscaping and other site planning features

Based on the clear compatibility between the proposal and this GPP guidance, staff finds that the petition does not substantially interfere with the GPP. Conversely, it furthers many of the guiding principles of the GPP such as Compact Urban Form, Conserve Community Character, Leverage Public Capital, and Mitigate Traffic.

ENVIRONMENTAL COMMISSION REPORT: The Environmental Commission has expressed support of the proposed use variance, but has concerns and does not anticipate support of the proposed variances from impervious surface coverage and landscaping. These issues will be resolved at the BZA hearing.

NEIGHBORHOOD INPUT: The petitioners have met with the Bryan Park Neighborhood Association on two occasions to discuss the proposed project. Overall, the neighborhood has been very receptive to the proposed redevelopment of the site and the proposed uses. They have also worked with the petitioner to revise the proposed elevations to be more consistent with the surrounding area. They still have a preference to have more of a “front” for the smaller residential building along Hillside Drive. They have also expressed a concern with the amount of impervious surface coverage, density and believe there is generally not enough parking.

CONCLUSION: Staff finds the proposed use variances to move the commercial space closer to the intersection of Hillside Dr. and Henderson St. to be appropriate and desirable. Furthermore, staff finds the proposed redevelopment of the property will provide an appropriately scaled urban infill project that is in character and scale with the surrounding area. Furthermore, staff finds no substantial interference with the GPP. In fact, this project would achieve many of the goals of the GPP.

RECOMMENDATION: Staff recommends forwarding this request to the Board of Zoning Appeals with a positive recommendation.

MEMORANDUM

Date: October 27, 2011
To: Bloomington Plan Commission
From: Bloomington Environmental Commission
Through: Linda Thompson, Senior Environmental Planner
Subject: UV-30-2011 Max & Gilda Lauchli

This memorandum contains the Environmental Commission's (EC) input and recommendations regarding a Use Variance request for multifamily units on the ground floor of a Commercial Limited (CL) zoning district, and commercial use within a Residential Multifamily (RM) zoning district. The purpose of this hearing is to render a recommendation from the Plan Commission to the BZA for this variance.

The EC is in favor of changing the uses for this site, thus recommends that the Plan Commission provide a positive recommendation to the Board of Zoning Appeals (BZA) for a Use Variance. The EC does have concerns about the other variances that the BZA will have to decide on in the future.

ISSUES OF CODE COMPLIANCE:

1.) LANDSCAPE PLAN:

The Petitioner will be requesting a variance from the Unified Development Ordinance (UDO) landscape regulations. The EC believes that the footprint of the buildings and parking lots should be reduced if that is what is needed to provide adequate land for greenspace. This site is not confined to already-built urban density, therefore the EC sees no public or environmental reason to allow impervious surface to have priority over landscaped greenspace, thus will be recommending against it.

The built environment (sometimes referred to as gray infrastructure) impacts health, economy, ecological services, and the overall quality of life. Therefore, too much of it leaves little opportunity for preserving or enhancing any green infrastructure, or permeable, vegetated space that supports functioning ecosystems and associated services (e.g. climate control, aesthetic enrichment) that impact health, economy, and the overall quality of life.

The EC suggests that the petitioner use a diverse mix of native tree, shrub and prairie species. Besides enhancing our city's sense of place and its native biodiversity, these efforts will attract residents and shoppers, thus helping to stimulate the economic vitality of the area. Native species do not require inputs of chemical fertilizers or pesticides, are water efficient once established, and provide habitat for birds, butterflies and other beneficial insects promoting biodiversity in the city. For suggestions, please see the EC's Natural Landscaping materials at www.bloomington.in.gov/beqi/greeninfrastructure.htm under 'Resources' in the left-hand column. For excellent photos of native prairie species, see:

http://www.prairiemoon.com/store/template/product_display.php?NID=88&SID=04303bb59359492983a1d255f50dd2d2.

For additional suggestions plus an excellent guide to Midwest sources of native prairie and other species see:

<http://www.inpaws.org/landscaping.html>.

2.) IMPERVIOUS SURFACE COVERAGE:

The Petitioner will also be asking the BZA for a variance from the UDO regulations regarding the percent of the site that is allowed to be covered by impervious material. As with the Landscape Plan, the EC will be recommending that this variance not be granted. The EC sees no justification for bypassing the UDO limits on impervious surfaces for this proposal. Maintaining pervious landscaped areas on this site will not result in any practical difficulties in the use of the property, or any practical difficulty that would be peculiar to the property, nor will it result in any practical difficulties by denying the variances.

ISSUES OF SOUND ENVIRONMENTAL DESIGN:

1.) GREEN BUILDING:

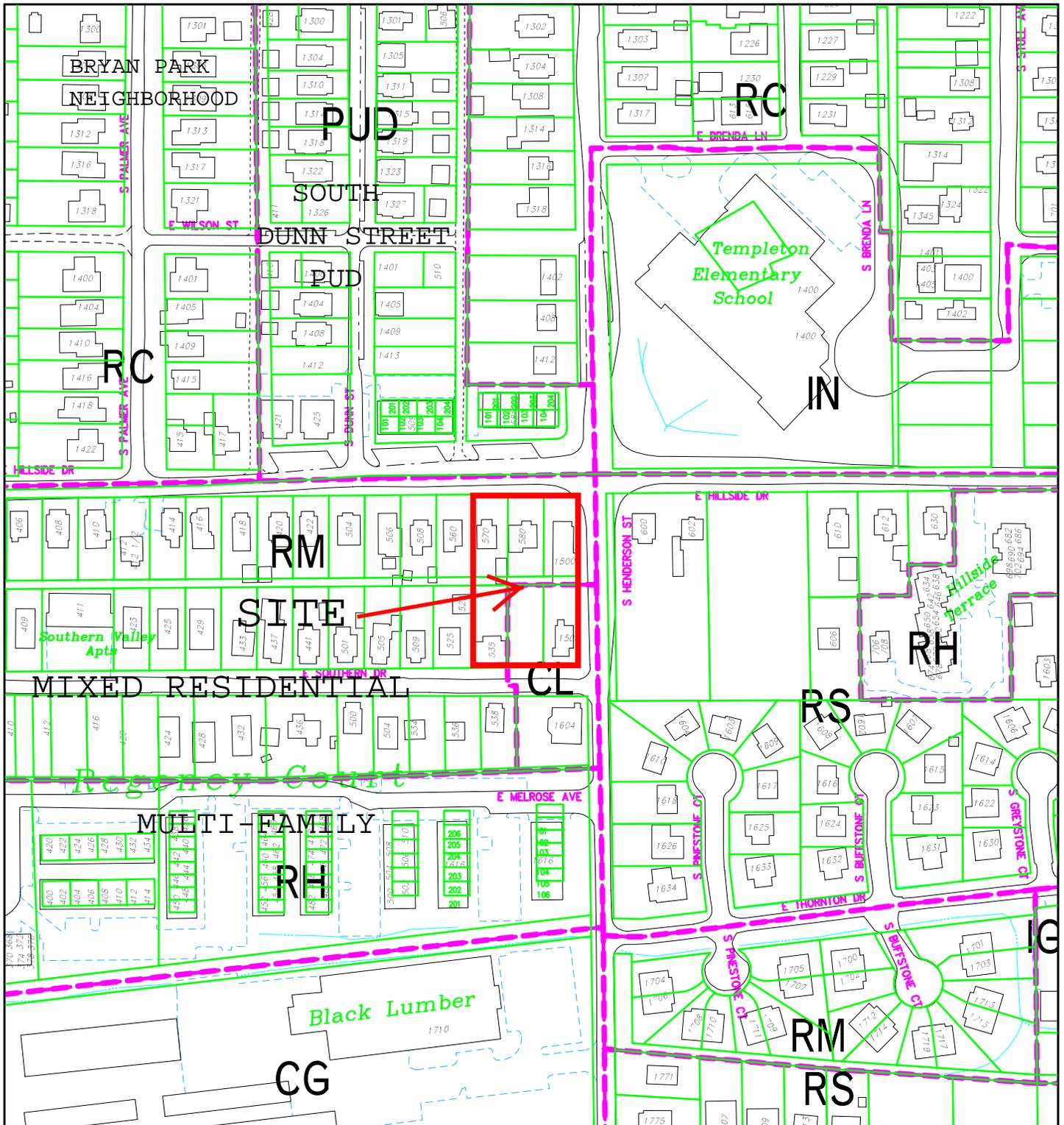
The EC recommends green building features. Green building can provide substantial savings in energy costs to a building over its life cycle and is thus an especially prudent investment in this time of rising energy prices. Green building features are consistent with the spirit of the UDO and supported by Bloomington's overall commitment to sustainability and its green building initiative (<http://Bloomington.in.gov/greenbuild>). Sustainable building practices are explicitly called for by the Mayors' Climate Protection Agreement signed by Mayor Kruzan, by City Council resolution 06-05 supporting the Kyoto Protocol and reduction of our community's greenhouse gas emissions, and by City Council resolution 06-07, which recognizes and calls for planning for peak oil.

2.) RECYCLING SPACE:

Space should be provided for recyclable-material collection, which will consequently reduce the development's carbon footprint and promote healthy indoor and outdoor environments. Lack of recycling services is the number one complaint that the EC receives from apartment dwellers in Bloomington. Recycling has become an important norm that has many benefits in energy and resource conservation. Recycling is thus an important contributor to Bloomington's environmental quality and sustainability and it will also increase the attractiveness of the apartments to prospective tenants.

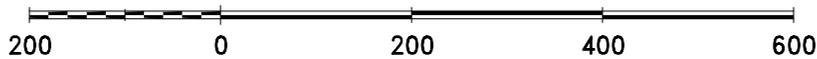
EC RECOMMENDATIONS:

1.) The EC recommends that the Use Variance go forward with a positive recommendation.

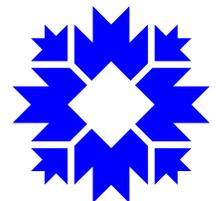


UV-30-11 MAX AND GILDA LAUHLI
 LOCATION/ZONING/LAND-USE MAP

By: shayp
 3 Oct 11



City of Bloomington
 Planning



Scale: 1" = 200'

For reference only; map information NOT warranted.

REVISIONS:

ARCHITECTURE
CIVIL ENGINEERING
PLANNING

BBB
BYNUM FAYO & ASSOCIATES, INC.

528 North Indiana Street
Bloomington, Indiana 47404
(317) 339-2990 (Fax)

certified by:

Proposed by:
HILLSIDE & HENDERSON APARTMENTS

Bloomington, Indiana

THE SITE PLAN
SCALE: 1"=20'

designed by: JBT
drawn by: DDB
checked by: JBT
sheet no.: 2
project no.: 401133

GENERAL LEGEND

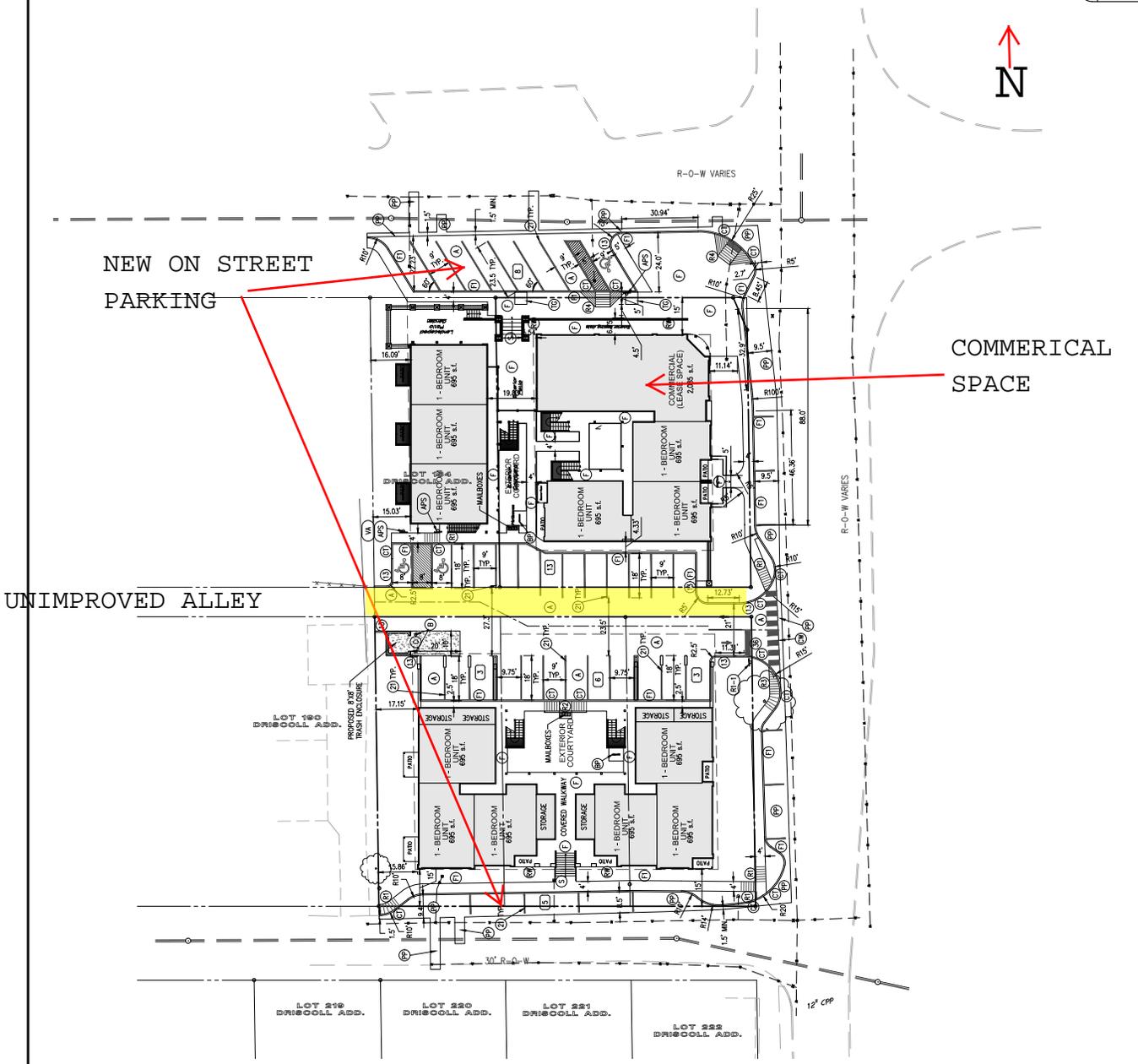
PROPERTY LINE	XX/XX
CONSTRUCTION LIMITS	XX/XX
DEED BOOK AND PAGE	XX/XX
TO BE REMOVED	---
TO REMAIN UNDISTURBED	---
SETBACK LINE	X-58'
PROPOSED 20"X12" DUMPSTER PAD	---
SHOWING BY 6" SOLID WOOD	---
FRONT YARD SETBACK	---
PROPOSED ACCESSIBLE PARKING	---
SANITARY SEWER EASEMENT	---
GAS EASEMENT	---
WATER LINE EASEMENT	---
ELECTRIC EASEMENT	---
DRAINAGE EASEMENT	---
UTILITY EASEMENT	---

SITE LEGEND

(1)	PROPOSED BRUSHING PAVING
(2)	PROPOSED 8" REINFORCED CONCRETE PAVING
(3)	PROPOSED 4" REINFORCED CONCRETE PAVING
(4)	PROPOSED 2" REINFORCED CONCRETE PAVING
(5)	PROPOSED ACCESSIBLE RAMP, TYPE "X"
(6)	PROPOSED ACCESSIBLE RAMP, TYPE "Y"
(7)	PROPOSED ACCESSIBLE RAMP, TYPE "Z"
(8)	PROPOSED PAINTED STEEL ROLLUP
(9)	PROPOSED 8" REINFORCED CONCRETE PAVING
(10)	PROPOSED MARKING, PAINTED, SOLID, WHITE "A"
(11)	PROPOSED STOP BAR MARKING, THERMOPLASTIC, REFER TO DETAIL FOR ACCESSIBLE MARKING AREAS
(12)	PROPOSED STOP BAR MARKING, THERMOPLASTIC, SOLID, WHITE, 10' LONG FROM EDGE OF PAVEMENT, REFER TO DETAIL FOR ACCESSIBLE MARKING AREAS
(13)	PROPOSED MARKING, THERMOPLASTIC, 24" WIDE, 24" SPACING, WHITE, 4' LONG (CROSSWALK PER FOOT STANDARD 885.07)
(14)	PROPOSED MONOLITHIC CURB AND SIDEWALK
(15)	PROPOSED CURB TRANSITION, 6" LENGTH FROM 0" TO 6" CURB HEIGHT TO THE RETAINING WALL AND CURB PAVING, REFER TO ARCH PLANS FOR MORE INFO.
(16)	PROPOSED CURB TRANSITION, 6" LENGTH FROM 0" TO 6" CURB HEIGHT TO THE RETAINING WALL AND CURB PAVING, REFER TO ARCH PLANS FOR MORE INFO.
(17)	PROPOSED TREE GRATE, LEAF CASTING, J
(18)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(19)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(20)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(21)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(22)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(23)	PROPOSED ONE PARKING AREA, ORIGINAL CYCLOSOLS MODEL 270-9 RACK WITH 857X72" CONC. PAD
(24)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(25)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(26)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(27)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(28)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(29)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(30)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(31)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(32)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(33)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(34)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(35)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(36)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(37)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(38)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(39)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(40)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(41)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(42)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(43)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(44)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(45)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(46)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(47)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(48)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(49)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(50)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(51)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(52)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(53)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(54)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(55)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(56)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(57)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(58)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(59)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(60)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(61)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(62)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(63)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(64)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(65)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(66)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(67)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(68)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(69)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(70)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(71)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(72)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(73)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(74)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(75)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(76)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(77)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(78)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(79)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(80)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(81)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(82)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(83)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(84)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(85)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(86)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(87)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(88)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(89)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(90)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(91)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(92)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(93)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(94)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(95)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(96)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(97)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(98)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(99)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS
(100)	PROPOSED VAN ACCESSIBLE PARKING SPOT ACCORDING TO ADA STANDARDS

NOTE TO CONTRACTOR
SCALE: 1"=20'

CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS & DEPTHS AND NOTIFY ALL UTILITIES PRIOR TO ANY EXCAVATION. NO PARKING SHALL BE MADE TO CONTRACTOR FOR UTILITY DESTRUCTION OR UNDERGROUND CHANGES REQUIRED DUE TO CHANGING ELEVATIONS.



revisions:

ARCHITECTURE
 CIVIL ENGINEERING
 PLANNING
 BRYUM FAYO & ASSOCIATES, INC.
 528 North Indent Street
 Bloomington, Indiana
 (812) 332-8030

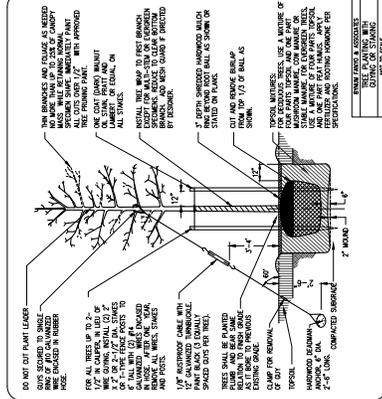
certified by:

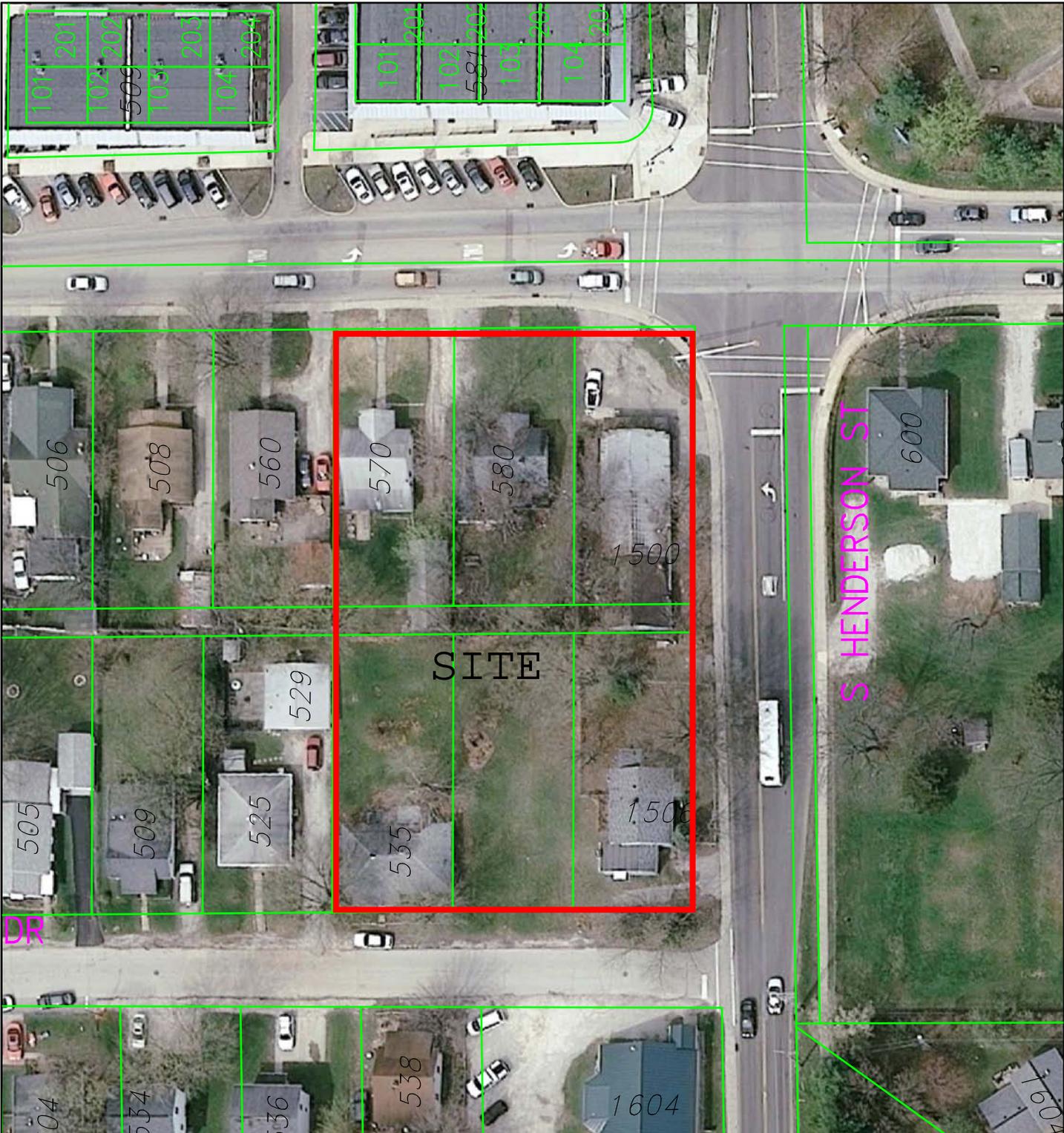
Proposed:
HILLSIDE AND HENDERSON APARTMENTS
 Bloomington, Indiana

THE LANDSCAPE PLAN
 designed by: JR
 drawn by: JR
 checked by: JSE
 sheet no.:
 project no.: 401133

PLANT LIST

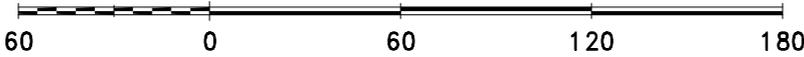
LEGEND	KEY	BOTANICAL NAME	COMMON NAME	QTY	SIZE & CONDITION
LARGE CANOPY DECIDUOUS TREES					
	1P	CELANIA SPINOSA	CELANIA OAK	8	2" CAL. B & B
	2P	QUERCUS ALBINA	WHITE OAK	8	2" CAL. B & B
	3P	FRAXINUS AXILLARIS	RED BASKET HAZE	8	2" CAL. B & B
SMALL MEDIA DECIDUOUS TREES					
	4P	BETULA NERA	RIVER BIRCH	7	2" CAL. B & B
EVERGREEN TREES					
	5P	PRUNUS SPINOSA	WINE VINE	6	3/4" HIGH
DECIDUOUS SHRUBS					
	6P	ARUNDA DOBSONIA	BLACK DOGWOOD	4	2 1/2" SPREAD
	7P	MOERHUSIA	MOORHUSIA	4	2 1/2" SPREAD
EVERGREEN SHRUBS					
	8P	HYDRANGEA	HYDRANGEA	4	2 1/2" SPREAD
	9P	ROSA	ROSA	4	2 1/2" SPREAD
LEGEND KEY					
	10P	PLANT TYPE	PLANT TYPE	PLANT TYPE	PLANT TYPE
	11P	PLANT TYPE	PLANT TYPE	PLANT TYPE	PLANT TYPE
	12P	PLANT TYPE	PLANT TYPE	PLANT TYPE	PLANT TYPE





UV-30-11 MAX AND GILDA LAUCHLI
AERIAL PHOTO

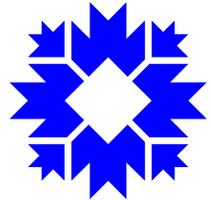
By: shapp
3 Oct 11



For reference only; map information NOT warranted.



City of Bloomington
Planning



Scale: 1" = 60'

