



City of Bloomington Common Council

Legislative Packet

Containing legislation and materials related to:

Wednesday, 15 February 2023

Regular Session at 6:30pm



CITY OF BLOOMINGTON COMMON COUNCIL

AGENDA AND NOTICE:
REGULAR SESSION
WEDNESDAY | 6:30 PM
15 February 2023

*Council Chambers (#115), Showers Building, 401 N. Morton Street
The meeting may also be accessed at the following link:*

<https://bloomington.zoom.us/j/81386634795?pwd=dGxld2E1TWVsa3lpWmlxcWNlZEU4QT09>

- I. **ROLL CALL**
- II. **AGENDA SUMMATION**
- III. **APPROVAL OF MINUTES:**
 - A. September 16, 2020 – Regular Session
- IV. **REPORTS** *(A maximum of twenty minutes is set aside for each part of this section.)*
 - A. Councilmembers
 - B. The Mayor and City Offices
 - i. Report from HAND Director John Zody re: Public Input on use of HOME/ARPA Funding
 - C. Council Committees
 - i. Sidewalk Committee – Recommendations re: 2023 Sidewalk Allocations
 - D. Public*
- V. **APPOINTMENTS TO BOARDS AND COMMISSIONS**
- VI. **LEGISLATION FOR SECOND READINGS AND RESOLUTIONS**
 - A. Resolution 23-04 – A Resolution Authorizing the 2023 Expanded Outdoor Dining Program in the Downtown Corridor
- VII. **LEGISLATION FOR FIRST READINGS**
 - A. Ordinance 23-03 - To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles And Traffic" - Re: Amending Section 15.12.010 to remove seven stop intersections, to add six stop intersections, and to delete one 4-way stop intersection; Section 15.12.020 to add one yield intersection; Section 15.32.030 to delete angle parking on Fourth Street between College Avenue and Gentry Street; Section 15.32.080 to add no parking spaces on Duncan Drive, Nineteenth Street, and Strong Drive and to remove no parking spaces on Grant Street and Nineteenth Street; and Section 15.32.090 to add limited parking zones to Eighth Street

(over)

*Members of the public may speak on matters of community concern not listed on the agenda at one of the two public comment opportunities. Citizens may speak at one of these periods, but not both. Speakers are allowed five minutes; this time allotment may be reduced by the presiding officer if numerous people wish to speak.

To request an accommodation or for inquiries about accessibility, please call (812) 349-3409 or e-mail council@bloomington.in.gov.

Posted: February 10, 2023

VIII. ADDITIONAL PUBLIC COMMENT *

(A maximum of twenty-five minutes is set aside for this section.)

IX. COUNCIL SCHEDULE

X. ADJOURNMENT

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**City of Bloomington
Office of the Common Council**

Minutes for Approval

16 September 2020

In Bloomington, Indiana on Wednesday, September 16, 2020 at 6:30pm, Council President Stephen Volan presided over a Regular Session of the Common Council. Per the Governor's Executive Orders, this meeting was conducted electronically via Zoom.

COMMON COUNCIL
REGULAR SESSION
September 16, 2020

Councilmembers present via Zoom: Matt Flaherty, Isabel Piedmont-Smith, Dave Rollo, Kate Rosenbarger, Susan Sandberg, Sue Sgambelluri, Jim Sims, Ron Smith, Stephen Volan
Councilmembers absent: none

ROLL CALL [6:31pm]

Council President Stephen Volan summarized the agenda.

AGENDA SUMMATION [6:31pm]

There were no minutes for approval.

APPROVAL OF MINUTES [6:33pm]

There were no reports from council members.

REPORTS

Mayor John Hamilton gave a statement and provided details about the proposed Local Income Tax (LIT) Resolution that the Council would be hearing later that evening.

- COUNCIL MEMBERS

Sgambelluri reported that the Sustainable Development Committee would be meeting to hear from Alex Crowley, Director of Economic and Sustainable Development.

- The MAYOR AND CITY OFFICES [6:34pm]

Jim Shelton spoke about the need for Court Appointed Special Advocates (CASA) volunteers and said that training was starting soon.

- COUNCIL COMMITTEES [6:47pm]

David Keppel, spokesperson for Bloomington Peace Action Coalition, spoke about the dangers related to nuclear weapons.

- PUBLIC [6:49pm]

Greg Alexander spoke about the need for better sidewalk funding.

There were no appointments to boards or commissions.

APPOINTMENTS TO BOARDS AND COMMISSIONS [6:59pm]

Piedmont-Smith moved and it was seconded that Ordinance 20-15 be read by title and synopsis only. The motion received a roll call vote of Ayes: 9, Nays: 0, Abstain: 0. Clerk Nicole Bolden read the legislation by title and synopsis.

LEGISLATION FOR SECOND READING AND RESOLUTIONS [6:59pm]

Piedmont-Smith moved and it was seconded that Ordinance 20-15 be adopted.

Ordinance 20-15 - To Rezone a 19.73 Acre Property from Employment (EM) to Residential Estate (RE) - Re: 2300 W. Tapp Road (Duncan Campbell, Petitioner)

Ryan Robling, Zoning Planner from the Planning and Transportation Department, presented the legislation. The current zoning classification of Employment (EM) did not allow for the construction of a single family dwelling. The petitioner requested the property be rezoned to Residential Estate (RE) to allow for the expansion of the currently existing structure. The Plan Commission voted 9-0 to send the legislation to the Council with a favorable recommendation to approve the request to rezone.

Duncan Campbell, Petitioner, said that he was available for any questions.

Piedmont-Smith, Chair of the Land Use Committee, shared the committee’s support of the project.

Ordinance 20-15 (cont’d)

There were no council questions.

Council questions:

Greg Alexander said the rezoning process for this project was objectionable.

Public comment:

Piedmont-Smith disagreed with Greg Alexander and said she supported the legislation.

Council comment:

Sandberg said the legislation had been approved by the Plan Commission and thanked Piedmont-Smith for reminding people about the updates to zoning maps.

Sims pointed out that the Land Use Committee recommended this legislation be forwarded to the council with a 4-0 vote to approve the request by the petitioner.

Flaherty responded to Alexander’s comments, noting that RE was used sparingly for narrow uses, and this property qualified for that use.

Volan asked if the EM zoning allowed for any type of housing.

Robling responded that it did not.

The motion to adopt Ordinance 20-15 received a roll call vote of Ayes: 9, Nays: 0, Abstain: 0.

Vote to adopt Ordinance 20-15 [7:22pm]

Piedmont-Smith moved and it was seconded that Ordinance 20-16 be introduced and read by title and synopsis only. The motion was approved by voice vote. Bolden read the legislation by title and synopsis, giving the Land Use Committee do-pass recommendation of Ayes: 4, Nays: 0, Abstain: 0.

Ordinance 20-16 - To Establish the Sustainable Development Non-Reverting Fund and To Amend Title 2 of the Bloomington Municipal Code Entitled “Administration and Personnel” (Adding Chapter 2.35 Entitled “Sustainable Development Fund Advisory Commission”) [7:23pm]

Piedmont-Smith moved and it was seconded that Ordinance 20-16 be adopted.

Flaherty summarized Ordinance 20-16 including the creation of a dedicated Sustainable Development Fund to receive all monies received by the City of Bloomington from an income tax rate that had been designated for economic development purposes. The proposal would also amend Title 2 of the Bloomington Municipal Code (BMC) titled Administration and Personnel to create a new seven member Sustainable Development Fund Advisory Commission. This Commission would, among other duties, prioritize projects to be funded with the income tax revenue, make funding recommendations, and report on the use of the fund.

Hamilton strongly welcomed collaboration between such a commission and the administration. He suggested the ordinance be tabled until after the vote for the income tax, in case it did not pass and the commission was not needed.

Rollo asked about the procedure for tabling or postponing the adoption of the ordinance.

Council questions:

Volan stated that the number of votes in favor of Resolution 20-13 would indicate the next steps.

Rollo asked Flaherty if it was ideal to wait to see if the Local Income Tax (LIT) passed.

Ordinance 20-16 (cont'd)

Flaherty said that there was uncertainty about how the funds would be used if the tax was adopted. If Ordinance 20-16 passed, it would establish a procedure for advising how the funds were spent and would affect councilmembers' votes.

Piedmont-Smith added that a motion to postpone should include a date for consideration of the postponed legislation.

Sims asked if this advisory commission would evaluate only revenues that came from this tax.

Flaherty confirmed that was correct.

Sims asked Flaherty if it was only the LIT that would be affected by Ordinance 20-16.

Flaherty confirmed that Ordinance 20-16 would only redirect LIT monies to the non-reverting sustainable development fund established by the legislation. It would not affect other city funds.

Sims stated that the LIT funds would go into the general fund and he was not sure if the investments would be separate.

Flaherty said there was no Economic Development Income Tax (EDIT) and it would be clear and isolated in the new fund.

Sims asked if the legislation would have no effect on non-EDIT funds.

Flaherty confirmed that was correct.

Sims referenced a whereas clause that stated that climate change had a disproportionate impact on the health and financial well-being of low-income communities and communities of color. He inquired how the ordinance would affect low-income communities and communities of color in Bloomington.

Flaherty said that nationally and statewide, it was clear that there was classism and racism in policy that affected low-income communities and communities of color. He said that by addressing climate action, the communities would be affected positively in Bloomington. He provided additional details regarding racial and socioeconomic injustices.

Sgambelluri asked if Ordinance 20-16 and the LIT were passed that evening, would they apply to future income taxes.

Flaherty believed it would though that was not the intent. The ordinance could be amended in the future if another income tax was adopted.

Sgambelluri asked how Flaherty envisioned measuring the impact of the recommendations of the commission.

Flaherty gave examples of potential metrics that might be used to measure the impact.

Rollo made a motion to postpone the adoption of Ordinance 20-16 until after the vote is taken for the adoption of an income tax tonight. The motion was not seconded.

Motion to postpone adoption of Ordinance 20-16

Smith asked how the commission would be authorized to say how these revenues would be spent and if it was permissible.

Council questions:

Flaherty responded that the intent of the ordinance was to create a dedicated fund for the revenues from this tax. The commission would not have the authority to decide how the funds were spent, and would only advise the council and city administration how they felt the money should be spent. Any recommendations made by the commission for capital improvement expenditures would also be advisory.

Smith asked how it would relate to the capital plan.

Flaherty explained that a capital improvement plan was required by state law. He said that as changes were made, the plan would need to be update. He provided examples.

Ordinance 20-16 (cont'd)

Flaherty moved and it was seconded that Amendment 01 to Ordinance 20-16 be adopted. Flaherty presented Amendment 01.

Amendment 01 to Ordinance 20-16

Amendment 01 Synopsis: This amendment is sponsored by Councilmember Flaherty and clarifies that the Common Council may adopt legislation for the expenditure of income tax revenue without a recommendation from the Sustainable Development Fund Advisory Commission if the Commission fails to provide a timely recommendation. The amendment also clarifies that the Mayor and Common Council should generally accept the Commission's funding recommendations but are not bound to do so.

There were no council questions.

Council questions:

Daniel Bingham wondered if Amendment 01 undermined the commission and removed the authority to dictate how the revenues from the tax would be spent.

Public comment:

David Keppel stated his support for Amendment 01 and spoke in favor of collaboration.

Alex Goodlad opposed Amendment 01 and thought it was necessary to balance spending power with the community.

Rollo asked if Amendment 01 was created to explicitly state that the commission could not make binding decisions and added a time frame for the commission's advice.

Council comment:

Flaherty stated the main purpose of the Ordinance 20-16 was to get the community and elected officials involved at an earlier stage in developing and making a recommendation for the use of the funds. He did not believe that a commission was not permitted to delegate authority over funding.

Lucas explained that state law prohibited delegating funding authority.

Rollo asked if the council had to wait until a recommendation came from the advisory commission before the administration or council took action.

Flaherty responded yes but with a time limit given to the commission to make a recommendation.

Sandberg appreciated the concept of Ordinance 20-16 and did not oppose it but would abstain on a vote to adopt due to the uncertainty of the income tax being adopted.

Rollo felt Amendment 01 was necessary when no recommendation came from the commission. He said he would vote to adopt it.

The motion to adopt Amendment 01 to Ordinance 20-16 received a roll call vote of Ayes: 7, Nays: 0, Abstain: 2 (Sandberg, Sims).

Vote to adopt Amendment 01 to Ordinance 20-16 [8:03pm]

Flaherty moved and it was seconded to adopt Amendment 02 to Ordinance 20-16. Flaherty presented Amendment 02.

Amendment 02 to Ordinance 20-16

Amendment 02 Synopsis: This amendment is sponsored by Councilmember Flaherty and removes a condition for effectiveness of Ordinance 20-16 in order to avoid improper delegation of legislative authority.

There was no council questions.

Council questions:

There was no public comment.

Public comment:

There was no council comment.

Council comments.

The motion to adopt Amendment 02 to Ordinance 20-16 received a roll call vote of Ayes: 7, Nays: 0, Abstain: 2 (Sandberg, Sims).

Vote to adopt Amendment 02 to Ordinance 20-16 [8:07pm]

Sgambelluri asked if the work of other commissions would influence the decisions of the proposed commission. She wondered if the commission would undermine other commissions.

Council questions:

Flaherty described how the commission members would be chosen, with the goal that there would be a good representation of opinions. At a later date, this question might need to be revisited and reevaluated. The commission could ask other commissions for their expertise when it was relevant.

Sgambelluri asked if any boards would change as a result of the passage of the legislation forming the new commission.

Flaherty explained that was to be determined by the commission including if they sought feedback from other commissions and boards.

Sims inquired about the language in Ordinance 20-16 that allowed undefined types of expenditures in times of economic crisis.

Flaherty included the language to provide flexibility to the commission in times of economic crisis, natural disaster, or health emergencies and provided examples. In those times, the revenues from the income tax could be used differently. He provided examples of the usage of other funds.

Sims asked if the commission could recommend using the EDIT funds to enhance public safety.

Flaherty said that needs could be identified and the commission could consider the appropriateness of the need in the context of the adopted city plans.

Rollo asked if appointments were specified in Ordinance 20-16. He felt that in the event that the mayor and councilmembers could not agree on an appointment, that the parties would make a ranked-choice vote from among the qualified candidates and should be defined clearly in the legislation. He felt the way it was currently stated was ambiguous.

Flaherty stated that it was implied in the legislation but could be clarified via an amendment.

Bolden read a Zoom chat comment from Sam Dove who asked when the bus routes were going to change.

Public comment:

David Keppel stated his support of the proposed legislation.

Ilana Stonebraker expressed her desire to allow more public comment on this legislation before it was adopted.

Greg Alexander stated that EDIT revenues should fund public transportation.

Daniel Bingham stated the Ordinance 20-16 did not give the commission enough power to dictate how revenue from an EDIT would be spent on climate change.

Alex Goodlad stated his support for the legislation because it was better than no additional public scrutiny.

Ordinance 20-16 as amended (cont'd)

Rosenbarger thanked Flaherty for Ordinance 20-16 and commented on the focus of the proposal.

Council comments:

Rollo thanked Flaherty for Ordinance 20-16 and stated his support.

Rollo made a motion and it was seconded to adopt Amendment 03 to Ordinance 20-16.

Amendment 03 to Ordinance 20-16

Amendment 03 Synopsis: Ordinance 20-16 shall be amended in Section 6, by adding text that defines that a ranked-choice vote will be done by the mayor and councilmember commissioners if they cannot agree on a city resident appointment(s) to the commission.

There were no council questions.

Council questions:

Randy Paul was in favor of the proposal before the climate action funding was removed.

Public comment:

There were no comments from the council.

Council comments:

The motion to adopt Amendment 03 to Ordinance 20-16 received a roll call vote of Ayes: 7, Nays: 0, Abstain: 2 (Sandberg, Sims).

Vote to adopt Amendment 03 to Ordinance 20-16 (8:45pm]

Piedmont-Smith stated her support of Ordinance 20-16 and stated that there would be public input via the advisory commission and commented on the need to mitigate climate change impacts.

Council discussion:

Sandberg stated her appreciation of the proposal and said that a broader conversation could be done in the future but more time was needed. She said she would vote against Ordinance 20-16.

Flaherty expressed his reasons for bringing the legislation to the council.

Smith thanked Flaherty for Ordinance 20-16 stated his support of the ordinance and believed it increased public participation. He would support the legislation.

Sgambelluri stated her support of the ordinance and thanked Flaherty for his work on Ordinance 20-16. She believed it was important to require input.

Sims was concerned about the trust with expenditure of funds. He believed the efforts needed to be broader with neighboring counties in order to impact climate change. He thanked Flaherty for his efforts but stated that he would not support Ordinance 20-16.

Volan stated his interest in the council and the public being involved in spending decisions. He described council's role in budget and spending and his concerns with the process. He urged the mayor and city staff to accept the input from council and the public. He provided additional examples.

The motion to adopt Ordinance 20-16 as amended received a roll call vote of Ayes: 7, Nays: 2 (Sandberg, Sims), Abstain: 0.

Vote to adopt Ordinance 20-16 as amended [9:01pm]

Piedmont-Smith moved and it was seconded that Resolution 20-13 be read by title and synopsis only. The motion received a roll call vote of Ayes: 9, Nays: 0, Abstain: 0. Bolden read the legislation by title and synopsis.

Resolution 20-13 - Resolution Proposing an Ordinance to Modify the Monroe County Local Income Tax Rate, Allocate the Additional Revenues to Economic Development and Cast Votes in Favor of the Ordinance

Piedmont-Smith moved and it was seconded that Resolution 20-13 be adopted.

Mayor Hamilton presented the Resolution 20-13. He explained the need to adopt an EDIT to offset losses in revenues due to the effects of the pandemic.

Sandberg asked about the meeting with state legislator Ron Thompson.

Council questions:

Hamilton responded that he had been briefed in the meeting. He was pleased to continue to collaborate with the General Assembly who would likely consider the LIT issues. He opined further about what could be forthcoming but there were many unknowns.

Rollo asked if the draft capital plan was only for considering possibilities.

Hamilton confirmed that was correct and described the process.

Rollo stated that it would be difficult to marshal some of the proposed plans.

Hamilton stated that was correct.

Bolden read a comment submitted via Zoom chat from Bryony Gomez-Palacio expressing her support of adoption of the tax due to the effects of the pandemic and provided reasons.

Public comment:

David Keppel expressed his support of the adoption of the tax.

Bolden read a comment submitted via Zoom chat from Dave Askins of B Square Beacon where he explained that state statutes regarding LIT revenues. He wondered what type of distribution each council member would prefer if the proposal passed.

Randy Paul spoke in opposition to the tax being adopted at the time and provided reasons.

David Warren spoke in opposition to the tax being adopted and listed multiple reasons.

Alex Goodlad spoke in favor of funding public transportation in Bloomington regardless of the passing of Resolution 20-13.

Erin Predmore, President and CEO of The Greater Bloomington Chamber of Commerce, appealed to the council to oppose the tax. She advocated for public engagement when contemplating adoption of future taxes.

Ross Mead spoke in favor of the tax and the proposed commission.

Jill Thurman, Office Administrator for The Greater Ellettsville Area Chamber of Commerce, spoke in opposition to the tax, and stated that the Chamber's Board of Directors voted to oppose the tax.

Geoff McKim, Monroe County Councilor, opposed the proposed tax and provided reasons.

Ilana Stonebraker spoke in opposition to the tax.

Claire Cumberland spoke in favor of the tax and stated that she supported progressive use of revenues from the tax.

Tyler Cain wondered if the proposed tax would address emissions produced by busses in the city.

Daniel Bingham spoke in favor of cutting carbon emissions, and of adopting the tax to make a step toward the goal.

Natalia Galvan urged the council to adopt the tax due to the climate emergency.

Ann Hedin spoke in favor of addressing climate change and supported the tax.

Novella Shuck supported the proposed tax and spoke in favor of funding climate solutions.

Jacob Schwartz spoke in favor of funding climate solutions and he stated his support of the tax.

Rollo addressed the question about using income taxes for public transportation.

Council comments:

Hamilton confirmed that the funding could be used for public transportation and that the county could use their portion for it too. He reminded council and the public that Bloomington Transit (BT) received about \$8 million in federal dollars in response to the Covid-19 crisis which allowed BT to not have immediate cash needs. Hamilton stated general obligation bonds would increase taxes though the city had been adverse to use bonds to fund operating expenses. It was not an approach that he felt was prudent for anything other than capital expenses and projects.

Piedmont-Smith asked Hamilton his opinion about the two ways of allocating revenue from the tax.

Hamilton anticipated that the Indiana state legislature may be changing rules around income taxes in the near future. The Local Tax Council could take a look at the way revenues would be distributed if the tax is passed. He described the two options of allocating the revenue.

Piedmont-Smith asked Hamilton to explain the standard further.

Hamilton said the simplest description was that it was the same split as the Public Safety LIT (PSLIT). He provided additional information pertaining to the four recipients of the funding; Bloomington, Monroe County, Ellettsville, and Stinesville.

Piedmont-Smith asked if, given the current legislation, the other option was to divide the funds by population.

Hamilton said that the Local Tax Council could opt to distribute based on population but would have to be done August for the following year.

Rollo asked Hamilton to elaborate more on the potential legislative impact on local income tax.

Hamilton stated that, based on past history, he was not optimistic about rules being set in favor of municipalities. He provided examples.

Piedmont-Smith asked if the tax could potentially be used to address emissions from buses in the city.

Resolution 20-13 (cont'd)

Hamilton stated it could potentially be used for that purpose and provided additional information pertaining to federal grants to BT and its plans moving forward.

Rollo spoke in support of the tax and provided reasons. The pandemic would cause a drop in revenues to the city resulting in a lack of funds to address climate change. He felt a sense of urgency regarding climate action.

Flaherty thanked members of the public who had shared their opinion. He commented on previous LITs and collaborations with the city and county, and also on the lack of a plan to reduce emissions by the county. He stated his support for the tax in the interest of the common good and provided additional details.

Smith felt the proposed LIT and its uses of revenues from the income tax were too broad. He stated his opposition to the tax and provided reasons including his consultation from community members.

Piedmont-Smith stated her decision was one of the hardest in her tenure. She spoke in opposition to the tax because there was more opportunity to address climate change than through an imposed tax. She expressed a desire to have more support if a tax was going to be adopted especially the private sector and business leaders. She commented on the General Assembly's actions.

Sandberg stated she would oppose the LIT and acknowledged that in order to recover, the city was going to need more revenue. She provided additional details. She felt there was not a consensus between the county and city council which was problematic. She opined that collaboration, and coalition-building, was necessary. Sandberg stated that the proposed LIT was different from the narrow scope of the PSLIT.

Sgambelluri discussed her reasons in opposition to the proposed LIT. She supported the values listed by the mayor, but did not feel there was a clear spending plan in place for revenues generated from the LIT. She stated that it was important to consider the pandemic and economic downturn and think ahead. She explained her reasoning for supporting the Recover Forward plan. She also applauded the previous PSLIT but stated that the proposed new tax without a sunset date was not like the PSLIT. She provided additional details against the proposed LIT.

Rosenbarger thanked everyone who engaged in the process. She wished a progressive tax, or refunds for low income families, could be proposed along with the LIT but Indiana restricted that action. She commented on other restrictions created by the state legislators. She expressed interest in funding climate change actions and helping citizens who were struggling financially. She commented on the timing of the LIT and said there was not a perfect time to ask community members for additional revenue via a tax. She also commented on the struggles that community members faced and hoped it was just the beginning of a robust conversation.

Sims reflected on his time on the council and stated that his decision to support the proposed LIT or not was the hardest decision to date. He commented on reasons that weighed on his decision including conversations with community members, the greater good, processes, and meaningful feedback from the community. He

commented on the interconnectivity of social justice and the good efforts of Bloomington. He stated his opposition to the tax.

Resolution 20-13 (cont'd)

Volan commented on the excellent feedback from the public though he was astonished at the non-city community members' opinions on how Bloomington should behave. He explained that the notion of cutting taxes to help combat poverty was a talking point used by Republicans for years. Volan commented on the restrictions set forth by the state as well as the LIT process submitted by the mayor. He stated he felt that if this tax was proposed to fund public transit, it would have had more support. He provided additional details on his reasoning.

Rollo appealed to his colleagues who were in opposition to work on other forms of revenue to replace what will be lost due to the pandemic. He stressed the need to address climate change immediately.

Sandberg said she respected her colleagues. She thanked members of the public who reached out to her regarding the proposed tax. She was committed to looking for other solutions and explained her position.

Flaherty thanked his colleagues as well. He hoped that in the future the county and city governments would collaborate on climate crisis actions. He reiterated that all climate crisis actions should include substantial equity and social justice and be aimed at helping disproportionately affected groups. He provided additional details.

Smith followed up on his earlier comments, asserting his opposition to the tax and detailing his reasoning. He said that his concerns pertained primarily to the process.

The motion to adopt Resolution 20-13 received a roll call vote of Ayes: 4 (Flaherty, Rollo, Rosenbarger, Volan), Nays: 5, Abstain: 0. FAILED.

Vote to adopt Resolution 20-13 [11:07pm]

Flaherty moved and it was seconded to reconsider the motion to adopt Ordinance 20-16.

Motion to reconsider the motion to adopt Ordinance 20-16

Flaherty explained that the commission that was created by Ordinance 20-16 was not necessary because Resolution 20-13 was not adopted.

There were no council questions.

Council questions:

The motion to reconsider Ordinance 20-16 received a roll call vote of Ayes: 9, Nays: 0, Abstain: 0.

Vote to reconsider the motion to adopt Ordinance 20-16 (11:11pm)

There were no council questions.

Council questions:

David Keppel stated that if the commission was in place, it might encourage public engagement on the adoption of local income taxes in the future.

Public comment:

Alex Goodlad also spoke in favor of allowing the commission created by the ordinance to remain.

Flaherty responded to public comment saying that he felt that Ordinance 20-16 should be repealed and potentially revisited in the future.

Council comment:

Volan agreed with Flaherty’s statement. He stated his opposition to adopting Ordinance 20-16.

Reconsider the motion to adopt Ordinance 20-16 (cont’d)

Bolden received a Zoom chat message and Volan agreed to allow it.

Public comment:

Bolden read a Zoom chat message from Dave Askins from B Square Beacon that said given that the vote for Resolution 20-13 was not a simple majority, does the ordinance still go to the rest of the tax council for consideration or does the resolution putting the ordinance in front of the rest of the tax council not need to succeed on a basic majority in order to go forward.

The motion to adopt Ordinance 20-16 as amended received a roll call vote of Ayes: 0, Nays: 9, Abstain: 0. FAILED

Vote to adopt Ordinance 20-16 as amended [11:20pm]

There was no legislation for first reading.

LEGISLATION FOR FIRST READING [11:21pm]

David Keppel commented that he appreciated the work that went into the deliberation.

ADDITIONAL PUBLIC COMMENT [11:22pm]

Lucas reviewed the council schedule.

COUNCIL SCHEDULE [11:24pm]

Piedmont-Smith moved and it was seconded to hold a Special Session of the Council on Wednesday, September 23, 2020 at 6:30pm. The motion received a roll call vote of Ayes: 9, Nays: 0, Abstain: 0.

Vote to hold Special Session [11:25pm]

Piedmont-Smith moved and it was seconded to adjourn. The motion was approved by voice vote.

ADJOURNMENT [11:26pm]

APPROVED by the Common Council of the City of Bloomington, Monroe County, Indiana upon this ____ day of _____, 2023.

APPROVE:

ATTEST:

Sue Sgambelluri, PRESIDENT
Bloomington Common Council

Nicole Bolden, CLERK
City of Bloomington

Council Sidewalk Committee Report – 2023 Council Sidewalk Funding

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- **2023 Sidewalk Evaluation Matrix**
- **History of Common Council Transportation Committee / Sidewalk Committee Sidewalk Funds 2002-2021**

Note: The Report can be found at <https://bloomington.in.gov/council/sidewalks> once approved by the Committee.

Signatures for Sidewalk Committee Report - 2023 Council Sidewalk Funding (February 15, 2023)

Note: Your signature below indicates approval of the Report pursuant to BMC 2.04.230 Standing committees-Reports (a), which requires that reports be in writing and be signed by a majority of the membership.

Ron Smith (Chair), District III

Dave Rollo, District IV

Kate Rosenbarger, District I

Steve Volan, District VI

Report of the Common Council Sidewalk Committee - 2023 Council Sidewalk Funding (February 15, 2023)

Committee Members and Staff

The members of the Committee were appointed by the President of the Council and included:

- Ron Smith, District III (Chair)
- Kate Rosenbarger, District I
- Dave Rollo, District IV
- Steve Volan, District VI

The committee members were assisted by the following persons and departments:

Planning and Transportation

Ryan Robling, Planning Services Manager

Hank Duncan, Bicycle and Pedestrian Coordinator

Engineering

Neil Kopper, Senior Project Engineer

Roy Aten, Senior Project Manager

Utilities

Jane Fleig, Utilities Engineer

Parks and Recreation

Steve Cotter, Natural Resources Manager

Office of the City Clerk

Sofia McDowell, Chief Deputy Clerk

Council Office

Stephen Lucas, Council Administrator/Attorney

Ash Kulak, Deputy Administrator/Deputy Attorney

Schedule

The Committee met in person, with the meeting also accessible via Zoom on:

- Thursday, February 2, 2023 at 2:30pm

Highlight of Recommendations

This Report of the Sidewalk Committee (the Committee) outlines the Committee's recommendation to the Council on the use of \$336,000 of Alternative Transportation Fund (ATF) monies budgeted for 2023 for sidewalk and traffic-calming/pedestrian improvements projects. The Committee met on February 2, 2023 to review ongoing projects and allocations, discuss program criteria, consider new projects, and make recommendations regarding the allocation of these funds. As in the past, additional funds from various other sources – e.g. P & T (through ATF and other funds), HAND (through CDBG funding), and CBU (City of Bloomington Utilities - for storm water) may be necessary for some projects to move forward or be completed.

In brief, the Committee learned about or recommended funding for the following sidewalk and traffic-calming projects:

Update on 2022 Allocation:

Project	Allocation	Spent/Estimate	Difference	Description
Adams Street Sidewalk	\$120,000.00	\$101,110.00*	-\$18,890.00	Remainder right of way acquisition
Dunn Street Sidewalk	\$110,000.00	\$213,478.55	\$103,478.55	Construction
Overhill Drive Sidewalk	\$20,000.00	\$0	-\$20,000.00	Design
Liberty Drive Sidewalk	\$15,000.00	\$20,520.00	\$5,520.00	Design
Smith Avenue Sidewalk	\$21,000.00	\$0	-\$21,000.00	Conceptual Design
Resident Led Traffic Calming	\$50,000.00	\$0	-\$50,000	Construction
TOTAL	\$336,000.00	\$335,108.55	-\$891.45	

* An additional \$48,055 from an Engineering budget line was encumbered for this project.

Please note that P & T staff provide an annual Council Sidewalk Project Status Report, (a copy of the Report can be found in the February 2, 2023 Sidewalk Committee meeting materials [here](#)) that includes a summary of potential complementary initiatives.

Please note that other sidewalk and pedestrian projects are pursued by various other city departments and funded through various means.

Deliberation Materials and Minutes Available Online

Deliberation materials and meeting memoranda for the Sidewalk Committee’s meetings will be available online at <https://bloomington.in.gov/council/sidewalks> under Meetings and Documents.

Purpose of Committee and History of Funding

In the past, the Sidewalk Committee has made recommendations on the use of a portion of the Alternative Transportation Fund (ATF) monies appropriated for this purpose and, in the course of doing so, works in concert with City staff to identify funding priorities for sidewalk and traffic calming projects in the City. The ATF was established in 1992 with surplus revenues from the Neighborhood Parking Program and was dedicated to “reducing the community’s dependence upon the automobile.” (BMC 15.37.160). Over the years, the ATF has also received annual infusions from other City sources. This year, \$336,000 has been appropriated for use by the Committee, which is the same amount as the previous year.

The table on the following pages provides a rough historical view of funding for Committee projects which is divided into annual Council Sidewalk Budgets, contributions from CBU, and contributions from other sources. Please know that the maintenance of sidewalks is the responsibility of the property owner and that the construction of new sidewalks in the City is mostly done by the owner when property is developed or redeveloped.

Council Sidewalk Committee Projects – Funding Sources

Year(s)	Council Sidewalk Budget		Estimate of Other Contributions	
	Per Year	Total	Other	CBU
2007	\$185,000	\$185,000	\$0	~ \$46,174
2008-2012	\$225,000	\$1,125,000	~\$1,425,000	~\$538,742
2013	\$275,000	\$275,000	~\$1,200,000	\$0
2014-2016	\$300,000	\$900,000	~\$43,000	~\$136,697
2017	\$306,000	\$306,000	~\$239,000	\$0
2018	\$312,000	\$312,000	~\$14,000	\$0
2019	\$318,000	\$318,000	~\$173,500	\$45,000
2020	\$324,000	\$324,000	~\$106,000	\$0
2021	\$330,000	\$330,000	~\$0	\$0
2022	\$336,000	\$336,000	~\$140,000	\$0
2023	\$336,000	\$336,000	~\$140,000	\$0
Total		\$4,411,000	~\$3,480,500	~\$766,613

Table Notes

1. The amounts in the “Per Year” and “Total” Council Sidewalk Budget columns are amounts budgeted at the beginning of the year. They include amounts dedicated for traffic calming (which, up until 2017, were typically under \$25,000 per year), but do not account for re-appropriation of unspent reverted funds in subsequent years.
2. The amounts in the “Other” column of the “Estimate of Other Contributions” portion of the table were amounts estimated at the time the Committee Reports were filed and do not account for changes after the actual amount was known. Funding sources include, but are not limited to: Greenways Funds (within the ATF); HAND Community Development Block Grant (CDBG) funds (targeting low-income neighborhoods); Cumulative Capital Development (CCD) fund; bond funds; General Fund appropriations to various departments; Metropolitan Planning Organization (MPO); and INDOT funds (like the former Safe Route to Schools program). A \$140,000 CDBG award from 2022 will support the Adams Street project in 2023.
3. The amounts in “CBU” column of the “Estimate of Other Contributions” portion of the table highlight that because sidewalk projects, and more particularly curbs, channel water, they are part of the City’s storm-water infrastructure. The Committee has, over the years, recognized that the storm-water component of a sidewalk project frequently comprises a significant and sometimes a majority of the project cost. The amounts in this column are either fiscal or in-kind contributions from CBU. They are derived from a detailed accounting provided by Jane Fleig, Utilities Engineer covering the years 2007 to 2015, and from Committee Reports thereafter.
4. In 2013, Committee recommended funding the design for a portion of Rockport Road sidewalk project that was part of a much larger road project.

Previous Program Criteria for Sidewalk Projects

For more than 20 years, the Committee used six core criteria to decide upon the funding of sidewalks. The criteria were refined over time, but continued to prioritize the construction (not maintenance) of sidewalks that fill in gaps in the City’s sidewalk network that will be used by, and improve the safety of, pedestrians. The following Evaluation Matrix explains the criteria, analytics and information used in funding cycles before 2022:

Criteria	Analytics and Information	
1) Safety Considerations	Pedestrian Level of Service (PLOS) - gauges the pedestrian experience based upon traffic volume and speed, lane width, presence and width of sidewalk, and presence, type, and width of the buffer.	
2) Roadway Classification		
3) Pedestrian Usage	Residential Density	Walkscore – an online score that gauges pedestrian demand based upon proximity to a mix of destinations. Score: 0 (car dependent) – 100 (walker’s paradise)
4) Proximity to Destinations	Transit routes and stops	
5) Linkages	Proximity to existing sidewalks as shown on Sidewalk Inventory (updated intermittently).	
6) Cost and Feasibility	Estimates provided by Engineering Dept.	

Prior to 2022’s funding cycle, the P & T department prepared a Project Prioritization list which scored projects based upon objective measures associated with some, but not all, of the criteria. However, the Project Prioritization list did not incorporate objective measures for evaluating connectivity or feasibility, which left the satisfaction and weighing of those criteria to the judgment of the Committee members.

During the 2021 funding cycle, the Committee discussed a Sidewalk Equity Audit and associated recommendations prepared by Bicycle and Pedestrian Safety Commission President Mark Stosberg and submitted to the Mayor, City Council, and various city staff members. The full audit is available online [here](#).

In addition to the suggestions contained in this audit, the 2021 Committee members also discussed census block maps that were created by P & T staff and submitted to the Committee for consideration. The 2021 Committee discussed potential revisions to the program criteria and related objective factors, and, while no formal changes were implemented in the 2021 funding cycle, the 2021 Committee indicated it would like P & T staff to consider and recommend what additional or different metrics are available and best suited to objectively measure the criteria the Committee values in new projects.

Current Program Criteria for Sidewalk Projects – starting in 2022

For the 2022 funding cycle, the P & T staff submitted a report to the Committee and subsequently to the full Council, which included revised metrics best suited to objectively guide the Committee’s evaluation of projects. These revised metrics took into consideration the analysis provided in the Sidewalk Equity Audit and include two new mechanisms to inform sidewalk project prioritization: an inventory of all missing sidewalks and weighted metrics to identify those

areas best-suited for improvement. The Committee voted to revise the criteria in accordance with the recommendations of the P & T Staff at its December 9, 2021 meeting.

In order to prioritize projects objectively, the scope of projects eligible for review was identified by creating a map of all City of Bloomington maintained streets with missing sidewalks. This map was created using data from the 2018 LiDAR scan, and it was updated to include sidewalk projects completed or in design/construction phase in subsequent years.

Next, weighted metrics were developed to identify those areas from the map of missing sidewalks best-suited for improvement. The data for the development of these weighted metrics was collected from the Census, the City GIS inventory, and formulas that indicate high areas of potential use and connectivity to transit.

The Committee reviewed these criteria and metrics and made no changes for the 2023 funding cycle. The following Evaluation Matrix explains the criteria, analytics and information used in this year’s funding cycle:

2023 Sidewalk Evaluation Matrix			
	Criteria	Analytics and Information	Criteria Weight
Demand and Density Data	Walk Potential	Based on 10-minute travel maps between residential areas and destinations (cafes, libraries, banks, grocery stores, hardware stores). The 10-minute walk distance is based on the actual street grid, not how a bird would travel. The more destinations that overlap and that can be reached within a 10-minute walk, the higher the score. This tool replaces the manually-applied walk score data included in years past prioritization methods.	25%
	Population Density	2019 American Community Survey Census Block Group data converted to a weighted score. Higher scores reflect areas with increased population density.	25%
	% Walk to Work	2019 American Community Survey Census Block Group data, converted to a weighted score ranging from 1 to 26. Areas where residents report higher rates of walking to work score higher than areas with less reported rates of walking to work.	7%
	% Transit to Work	2019 American Community Survey Data converted to a weighted score ranging from 1 to 100. Areas where residents report higher rates of utilizing transit to commute to work are higher than areas with less reported rates of utilizing transit to get to work	7%
	Vehicle Count	Derived from the 2019 American Community Survey Data which counts private registered vehicles per household. The variable scores and weigh each Census Block Group to reflect priority for residents in areas where average car ownership rates are lower.	6%

Safety and Harm Reduction Data	Adjacent Street Speed	Scores based on City-maintained Centerline data for speed limits. Streets with higher posted speed limits are weighted for greater point values/priority over streets with lower speed limits.	10%
	Adjacent Street Width	Scores based on City-maintained Centerline data for road width. Wider streets are scored for priority over streets that are narrower. Wider streets are prioritized because generally traffic travels faster on wider streets.	10%
Historically Excluded Groups Data	% Resident Renters	2019 American Community Survey Data which scores Census Block Groups with higher percentages of residents who are renters over areas with fewer renter households.	3%
	% BIPOC Renters	2019 American Community Survey Data which scores Census Block Groups with higher percentages of residents who are Black, Indigenous, and People of Color over Census Block Groups with lower percentages of residents who are Black, Indigenous, and People of Color.	3%
	Median Income	2019 American Community Survey Data, scored such that Census Block Groups with lower reported median income are prioritized over areas with higher median incomes.	4%
	Total		100%

Order of Priorities for Project Funding

As it has done in past years, given the uncertainty surrounding actual costs for design/ROW/construction, the Committee has ranked its recommended projects in order of priority (1 being the highest priority project, 2 being the next highest priority project, etc.). This ranking provides guidance to staff on which projects should be fully funded first.

Funding Recommendations for 2023

Along with reviewing and addressing funding for ongoing projects, the Committee reviewed the comprehensive map of missing sidewalks and the P & T staff's prioritization of high-ranking projects identified by utilizing the revised sidewalk evaluation metrics.

Funding for In-Progress Projects

- Sidewalk Construction – Adams Street – Kirkwood to Fountain (Rank #1)**
 During the 2021 funding cycle, the Committee allocated \$63,955.00 for the remainder of design and right of way services. In 2022, \$120,000 was allocated (with \$101,110 spent) toward the remainder of right of way services. This year, the Committee recommends allocating \$125,000 to fund construction of the sidewalk. It is worth noting that the total cost of construction will be split between the funds allocated by the Committee and amounts received from a CDBG grant.
- Sidewalk Construction – Liberty Drive – 3rd to 360° south (northern entrance of Whitehall Plaza) (Rank #2)**
 Last year, the Committee allocated \$15,000 toward this project for design during the 2022 funding cycle. Design services came in at \$20,520, a difference that was made up by shifting funds from other 2022 projects. This year, the Committee recommends

allocating \$114,000 toward construction. As total construction costs are estimated at \$200,000, the Committee anticipates a 2024 allocation of ~\$86,000 to complete construction in 2024.

Funding for New Sidewalk Projects

The Committee utilized the metrics revised in 2022 for this funding cycle and recommends the following high-ranking projects, which were all funded in some manner last year.

- **Design – S. Overhill Drive – 3rd St to 5th St (Rank #4)**

Using a 2022 construction estimate that may have increased, the estimated total cost of this project is \$185,000. The Committee recommends allocating \$35,000 toward this project for design during the 2023 funding cycle. The 2022 Committee previously allocated \$20,000 for design, which was shifted to fund higher-priority projects in 2022.

- **Conceptual Design – Smith – College to Walnut (Rank #5)**

Using 2022 estimates that may have increased, the total cost of this project is \$325,000. The Committee recommends allocating \$12,000 toward this project for conceptual design during the 2023 funding cycle. It is worth noting that this project has a number of challenges and unknown variables owing to the narrowness of the street, the location of buildings in proximity to the street, and parking lots along both sides of the street. The 2022 Committee previously allocated \$21,000 for conceptual design, which was shifted to fund higher-priority projects in 2022.

Funding for Traffic Calming Projects

In 2020, the City implemented a new Traffic Calming and Greenways Program (TCGP). Information about the TCGP can be found here: <https://bloomington.in.gov/tcgp>.

- **Resident-Led Traffic Calming Projects (Rank #3)**

Based on the analysis and recommendation of P & T staff, the Committee recommends allocating \$50,000 toward resident-led traffic calming projects in order to provide funding for construction of a project or projects prioritized in that program's funding cycle. The Resident-Led Traffic Calming Program is accepting applications until March 10, 2023. Following the close of the application process, P & T staff will identify and prioritize the project for this funding cycle.

Summary of Actions

In summary, during the course of its deliberations, the Committee:

- Provided an opportunity for Committee members or staff members to disclose any potential conflicts of interest for those who might own or reside in homes along sidewalk projects recommended for funding by the Committee;
- Heard a progress report regarding on-going projects;
- Reviewed the list of projects recommended by staff for funding and provided an opportunity for public comment;
- Recommended the allocation of \$336,000 in ATF monies as described below – *See Funding Recommendations (attached)*;
- Authorized the Committee chair to adjust the allocation scheme in consultation with city staff to fund priorities on the current list of allocations.

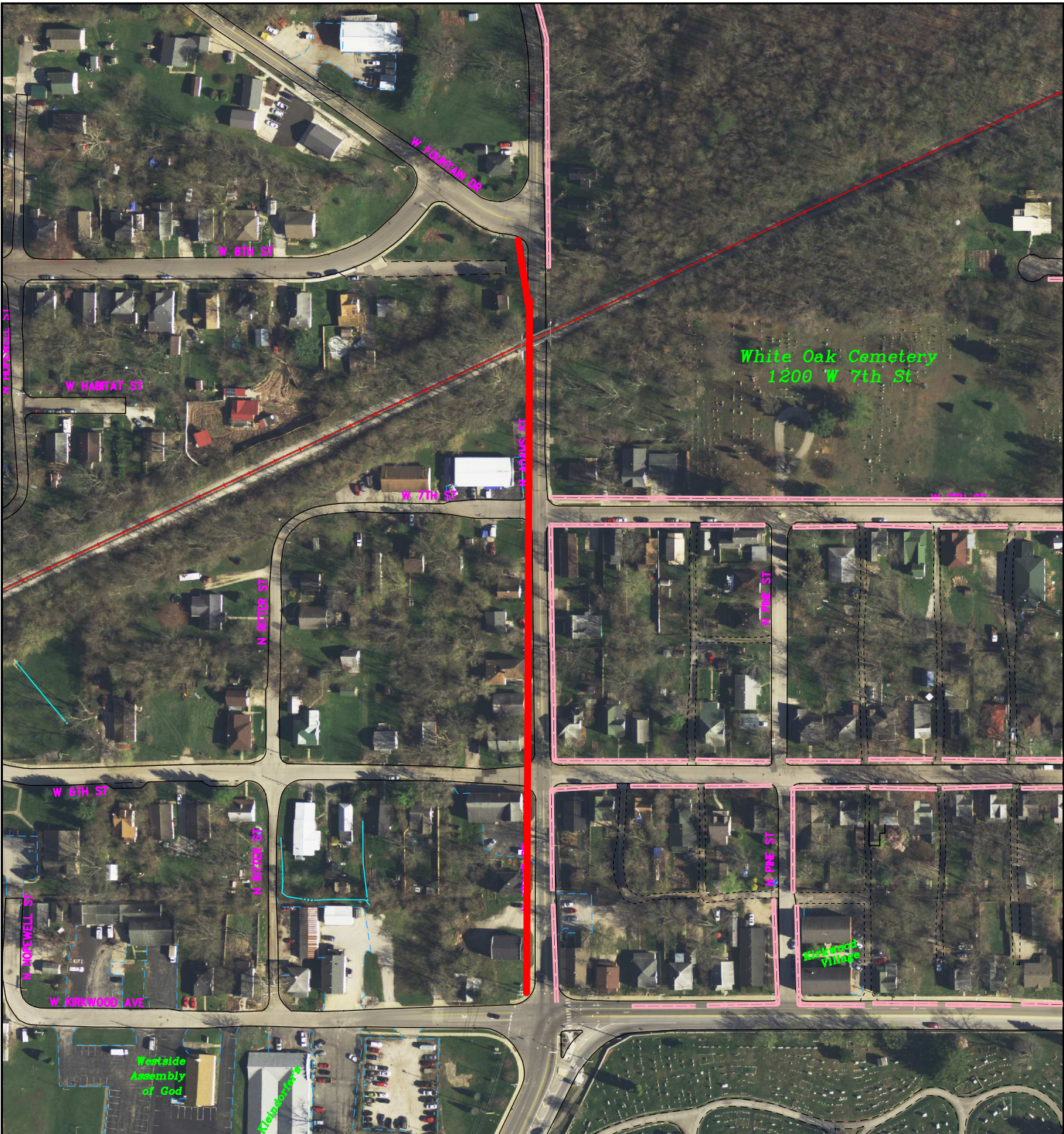
**COMMON COUNCIL TRANSPORTATION COMMITTEE (COMMITTEE) SIDEWALK
ALLOCATION RECOMMENDATIONS FOR 2023
- FUNDS AVAILABLE: \$336,000**

Project	ATF	ATF (Additional Amounts – Should They be Appropriated)	CBU	OTHER FUNDS	Priority
<u>Sidewalk Projects</u>					
Construction: Adams St. – from W. Kirkwood Ave. to Fountain Dr.	\$125,000		\$0	\$140,000	1
Estimated Costs					
Design: \$63,955					
Right-of-Way: \$149,155					
Construction: \$265,000					
Design: Liberty Dr. – from 3rd St. to 360° south (northern entrance of Whitehall Plaza)	\$114,000		\$0	\$0	2
Estimated Costs					
Design: \$20,520					
Right-of-Way: \$0					
Construction: \$200,000					
Design: S. Overhill Dr. – from 3rd St. to 5th St.	\$35,000		\$0	\$0	4
Estimated Costs					
Design: \$35,000					
Right-of-Way: \$0					
Construction: \$150,000 (2022 est.)					
Design: Smith Ave. – from College Ave. to Walnut St.	\$12,000		\$0	\$0	5
Estimated Costs					
Conceptual Design: \$12,000					
Design: \$50,000 (2022 est.)					
Right-of-Way: \$75,000 (2022 est.)					
Construction: \$200,000 (2022 est.)					
<u>Traffic Calming</u>					
General Traffic Calming and Greenways Program Resident-led Projects	\$50,000		\$0	\$0	3
Estimated Costs					
\$50,000					
2023 ALLOCATION	\$336,000	\$0	\$0	\$140,000	

**COMMON COUNCIL TRANSPORTATION COMMITTEE (COMMITTEE) SIDEWALK
ALLOCATION RECOMMENDATIONS FOR 2023**

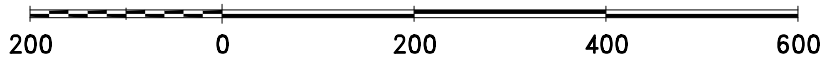
CHART NOTES

1. **Project.** This column identifies the location and details about the project.
2. **Alternative Transportation Fund (ATF).** This column represents ATF funds appropriated in 2023 for sidewalk and traffic-calming initiatives recommended by the Committee.
3. **ATF (Additional Amounts – Should they be Appropriated).** This column is available to capture unused funds from prior years should the Committee wish to make recommendations about the use of the remaining funds and any necessary additional appropriation proposals. No funds were identified for additional appropriation and, therefore the shaded column remains empty.
4. **CBU.** This column represents CBU assistance with the storm-water component of projects. The CBU evaluates the storm-water component of projects and, when able, offers some in-kind contributions when these projects align with CBU storm-water priorities. There were no CBU in-kind contributions identified for sidewalk construction projects recommended by the Committee for 2023.
5. **OTHER FUNDS.** This column represents project funding from other sources. The Adams Street project was recognized as eligible for CDBG funds and was awarded \$140,000 in CDBG funding last year. The Committee has recommended an allocation of \$125,000 toward the project to compliment the CDBG award and to complete the project.
6. **PRIORITY.** This column represents the Committee’s prioritized funding for the projects in order to provide guidance to staff in the event that funding shortages prevented the completion of all recommendations.

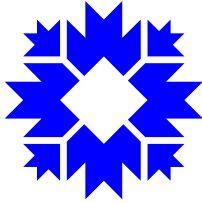


Adams Street - Kirkwood to Fountain

By: lucass
24 Jan 20

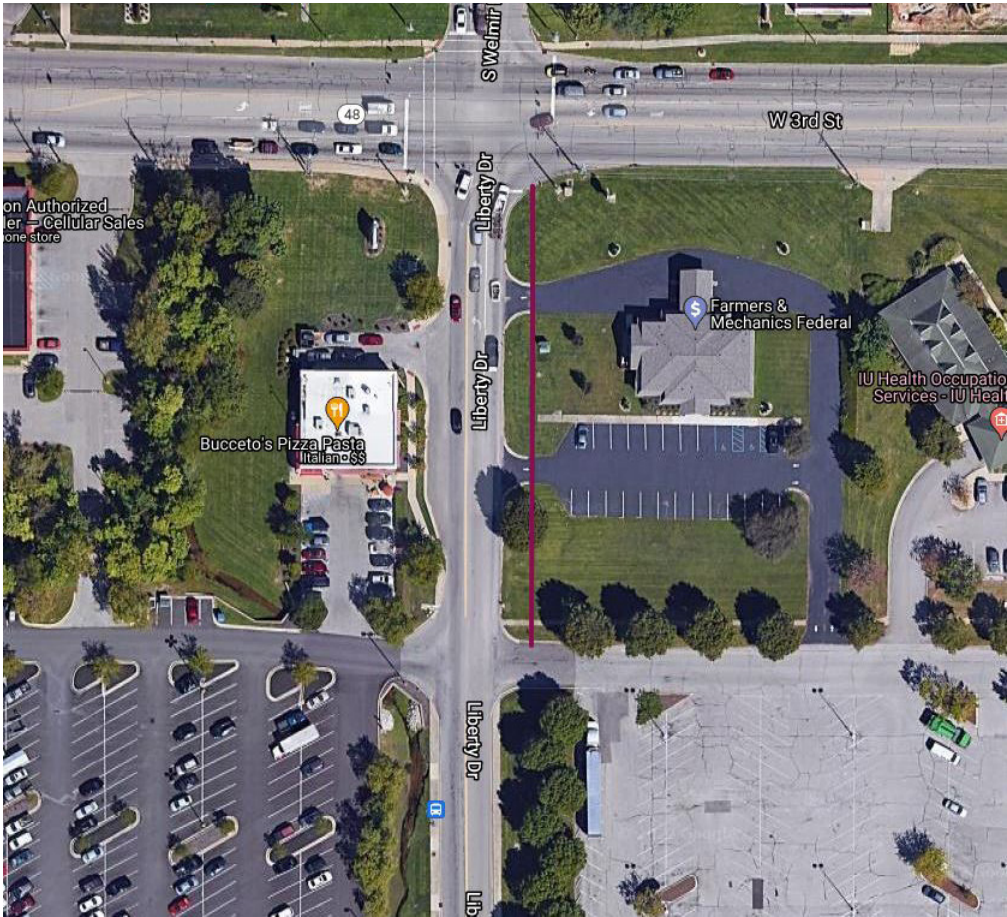


City of Bloomington
Council Office

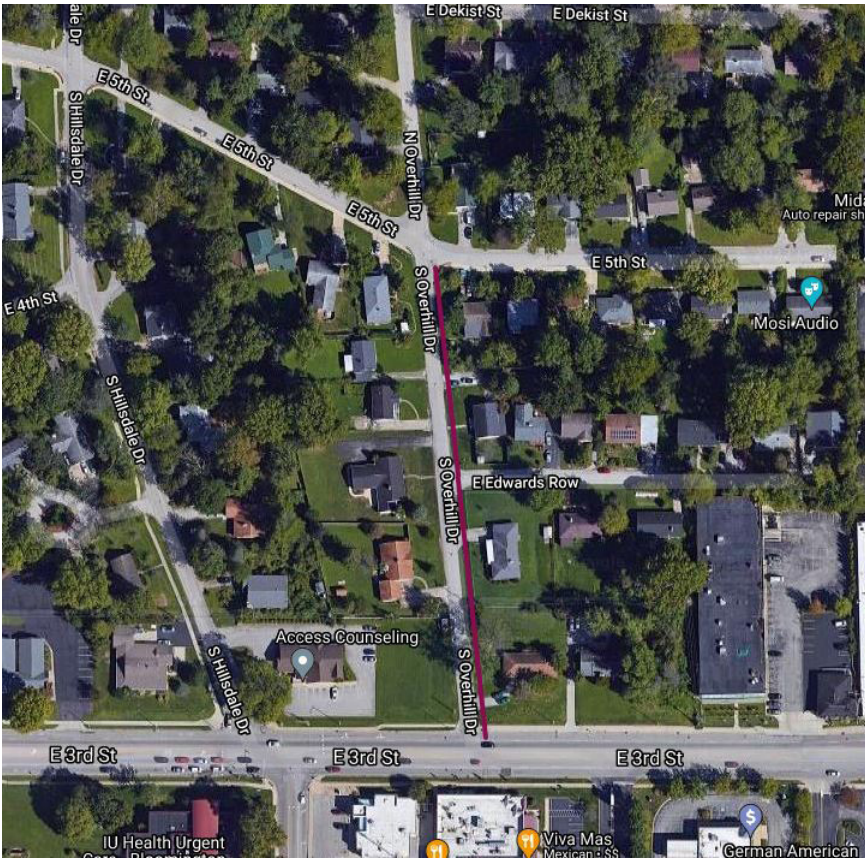


Scale: 1" = 200'

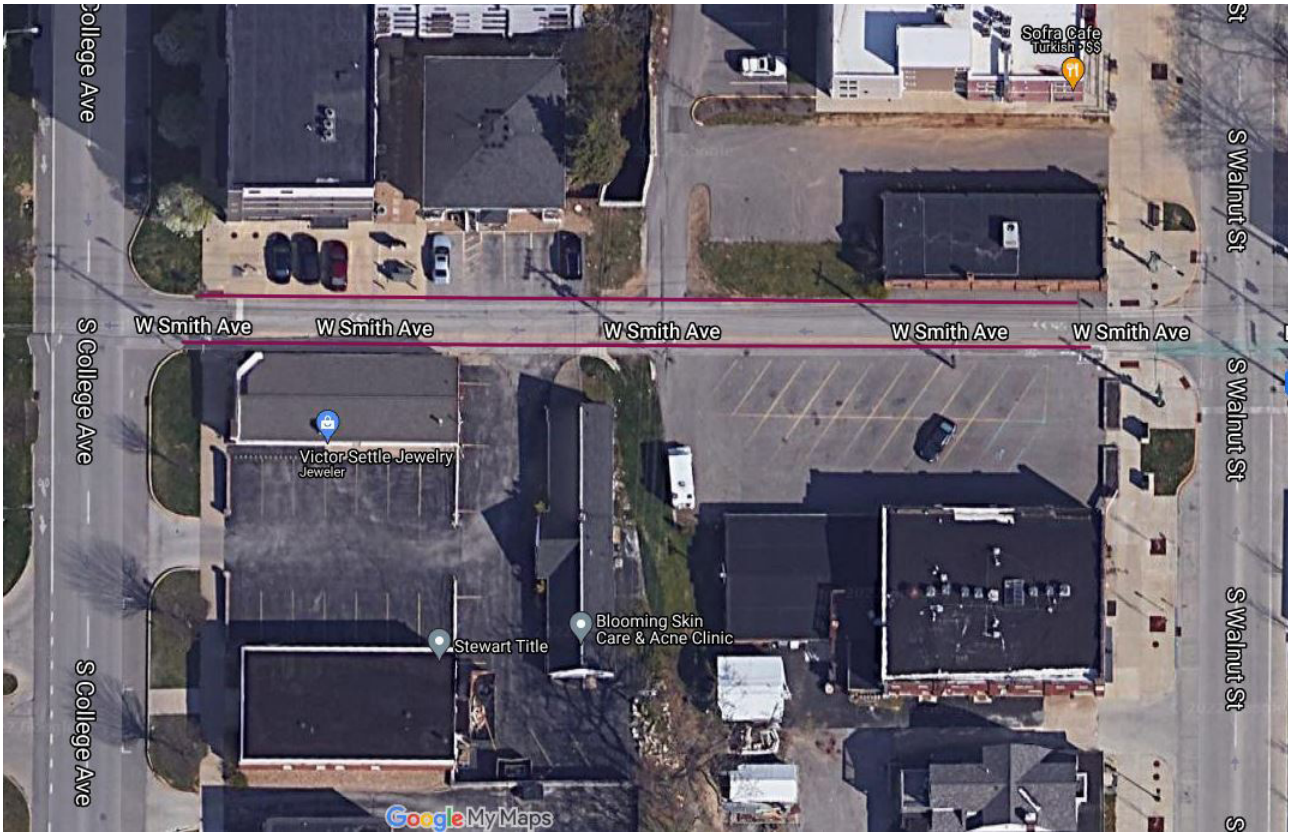
For reference only; map information NOT warranted.



Liberty Drive (South of W. 3rd Street intersection)



S. Overhill Drive (between E. 3rd Street and E. 5th Street)



W. Smith Avenue (Between S. College Ave and S. Walnut Street)

Common Council Transportation Committee/Sidewalk Committee Criteria, History, and Other Policies for Sidewalk Allocation

History of Criteria - The criteria for selecting sidewalk projects first appeared in a memo entitled the *1995 Linkages Plan – Criteria for Project Selection/Prioritization* and have been affirmed and revised over the years. These criteria for consideration initially included the following:

- Safety Consideration – A particular corridor could be made significantly safer by the addition of a sidewalk.
- Roadway Classification – The amount of vehicular traffic will increase the likelihood of pedestrian/automobile conflicts, which a sidewalk could prevent. Therefore, arterial and collector streets should be a priority for linkages over residential/subdivision streets.
- Pedestrian Usage – Cost-effectiveness should be based on existing and projected usage.
- Proximity to Destination Points – Prioritization of linkages should be based on proximity to destination such as elementary school, Indiana University, employment centers, shopping opportunities, parks/playgrounds, etc.
- Linkages – Projects should entail the construction of new sidewalks that connect with existing pedestrian facilities.
- Costs/Feasibility – Availability of right-of-way and other construction costs must be evaluated to determine whether linkages are financially feasible.

Over the years the Committee has revised these criteria as follows:

- On October 16, 2006, the Committee added “Indiana University” as another “destination point” under the fourth criteria (Proximity to Destination Points). At that time, it decided not to explicitly recognize “synergy” as another criteria, because it was already being considered as a factor under the fifth criteria (Costs/Feasibility).
- On January 4, 2008, the Committee added the fifth criteria defining “Linkages.”
- On November 12, 2009, the Committee revised “Proximity to Destination Points” to clarify that the list was illustrative and included “employment centers” among other destinations.

Current Criteria - On December 9, 2021, the Committee voted to revise the criteria in accordance with the recommendations of the P & T Staff taking into consideration the information gleaned from a Sidewalk Equity Audit and associated recommendations prepared by Bicycle and Pedestrian Safety Commission President Mark Stosberg. The revisions consist of the identification of three broad categories: Demand and Density, Safety and Harm Reduction, and Historically Excluded Groups. Each broad category contains weighted criteria, which will be used to prioritize and select sidewalk projects. The criteria, assigned weight, and analytic information are described on the 2023 Sidewalk Evaluation Matrix in this Report.

Other Policies – Overage Policy – Each year the Committee Report uses estimates submitted by City Engineering to allocate funds between projects. Even with a 10% contingency, these estimates are sometimes far-off the bid for, or actual cost of, the project. At its February 2, 2023 meeting, the Committee approved of a motion to allow the allocation scheme to be amended by the Sidewalk Committee Chairperson in consultation with city staff to fund priorities on the current list of allocations.

2023 Sidewalk Evaluation Matrix			
	Criteria	Analytics and Information	Criteria Weight
Demand and Density Data	Walk Potential	Based on 10-minute travel maps between residential areas and destinations (cafes, libraries, banks, grocery stores, hardware stores). The 10-minute walk distance is based on the actual street grid, not how a bird would travel. The more destinations that overlap and that can be reached within a 10-minute walk, the higher the score. This tool replaces the manually-applied walk score data included in years past prioritization methods.	25%
	Population Density	2019 American Community Survey Census Block Group data converted to a weighted score. Higher scores reflect areas with increased population density.	25%
	% Walk to Work	2019 American Community Survey Census Block Group data, converted to a weighted score ranging from 1 to 26. Areas where residents report higher rates of walking to work score higher than areas with less reported rates of walking to work.	7%
	% Transit to Work	2019 American Community Survey Data converted to a weighted score ranging from 1 to 100. Areas where residents report higher rates of utilizing transit to commute to work are higher than areas with less reported rates of utilizing transit to get to work	7%
	Vehicle Count	Derived from the 2019 American Community Survey Data which counts private registered vehicles per household. The variable scores and weigh each Census Block Group to reflect priority for residents in areas where average car ownership rates are lower.	6%
Safety and Harm Reduction Data	Adjacent Street Speed	Scores based on City-maintained Centerline data for speed limits. Streets with higher posted speed limits are weighted for greater point values/priority over streets with lower speed limits.	10%
	Adjacent Street Width	Scores based on City-maintained Centerline data for road width. Wider streets are scored for priority over streets that are narrower. Wider streets are prioritized because generally traffic travels faster on wider streets.	10%
Historically Excluded Groups Data	% Resident Renters	2019 American Community Survey Data which scores Census Block Groups with higher percentages of residents who are renters over areas with fewer renter households.	3%
	% BIPOC Renters	2019 American Community Survey Data which scores Census Block Groups with higher percentages of residents who are Black, Indigenous, and People of Color over Census Block Groups with lower percentages of residents who are Black, Indigenous, and People of Color.	3%
	Median Income	2019 American Community Survey Data, scored such that Census Block Groups with lower reported median income are prioritized over areas with higher median incomes.	4%
	Total		100%

A HISTORY OF COUNCIL SIDEWALK COMMITTEE FUNDS, 2002-2021

2021

Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
Sidewalk - Adams Street - from Kirkwood to Fountain (West Side)	\$323,955.00	\$120,000	\$140,000	This project received \$63,955 in design and right-of-way funding in 2020. The cost estimate for the sidewalk totaled \$323,955 (\$63,955 for design, \$70,000 for right-of-way services and \$190,000 for construction). This project was identified as a good fit for Community Development Block Grant (CDBG) funding. An application was submitted and awarded funding will be known in the coming weeks. It was the Committee's hope that the project would receive a least partial funding from CDBG in order to allow the project to move forward with an allocation of \$120,000 from the Committee.
Sidewalk - Dunn - from 15th to 16th	\$150,085	\$110,000		This project received \$41,000 in design and right-of-way funding in 2020. In April 2021, the City awarded a design contract to engineering firm Bledsoe Riggert Cooper James for \$40,085. Design for this project is ongoing. The cost estimate for this project is \$150,085 (\$40,085 for design and \$110,000 for construction) but these numbers may change based on refinements in the design process.
Sidewalk - S. Overhill Drive - 3rd to 5th	\$170,000	\$20,000		This sidewalk request was identified utilizing the revised metrics for prioritization of sidewalk projects. The project scored high based on the weighted criteria in the revised metrics - it is within a 5-minute walk of a transit stop and is located adjacent to high-ranking streets in the safety/and harm reduction criteria. The total cost of the project is estimated to be \$170,000 (\$20,000 for design, and \$150,000 for construction). The Committee recommends allocating \$20,000 for design to get this project started.
Sidewalk - Liberty Drive - 3rd to 360 degrees south	\$65,000	\$15,000		This sidewalk request was identified utilizing the revised metrics for prioritization of sidewalk projects. The project scored high based on the weighted criteria in the revised metrics - it is within a 5-minute walk of a transit stop and is located adjacent to high-ranking streets in the safety/harm reduction criteria. The total cost of this project is estimated to be \$65,000 (\$15,000 for design and \$50,000 for construction). As with all projects, these cost estimates are subject to change as more information becomes available based on design and right-of-way acquisition refinements. The Committee recommends allocating \$15,000 to begin the design process on this project.
Smith - College to Walnut	\$325,000	\$21,000		This sidewalk request was identified utilizing the revised metrics for prioritization of sidewalk projects. The project scored high based on the weighted criteria in the revised metrics - it is within a 5-minute walk of a transit stop and is located adjacent to high-ranking streets in the safety/harm reduction criteria. However, this project has a number of unknowns at this time owing to the proximity of the buildings and parking lots on either side of the street. The Committee recommends allocating \$21,000 to the conceptual design process in order to assess the feasibility of this project.

Resident-led traffic calming	\$50,000	\$50,000		The Committee recommends allocating \$50,000 to resident-led traffic calming projects. This recommendation was based on P & T staff's recommendation and would provide funding for the resident-led traffic calming project that was prioritized in 2021 and a project that is prioritized during the 2022 funding cycle. The resident-led traffic calming program is currently accepting applications. The application process will close March 11, 2022.
Total	\$1,084,040	\$336,000	\$140,000	The Committee should learn status of the CDBG funding allocations in the coming weeks. The project rankings may be utilized to provide guidance for staff in the event that the CDBG funding allocation is less than needed to complete the Adams Street sidewalk project.
2020				
Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
Sidewalk - S. Maxwell Street - from E. Miller Dr. to north of Short Street (West Side)	\$123,000.00	\$123,000.00		This project received \$13,000 in design funding in 2018. At that time, a Planned Unit Development for a co-housing project in the area raised concerns about increased vehicular traffic. The Committee believed a sidewalk on the west side of Maxwell Street might help mitigate any adverse impacts. The cost estimate for the sidewalk totaled \$123,000 (\$8,000 for right of way services and acquisition and \$115,000 for construction).
Sidewalk - 14th Street - from Madison to Woodburn (North Side)	\$156,000.00	\$50,000.00	\$106,000.00	This one-block sidewalk project is ranked #4 on the Committee's Evaluation Sheet. It would provide a missing link in sidewalks which currently connect with South College to the east and North Madison on the west. HAND staff indicated that the project might be a good fit for Community Development Block Grant (CDBG) funding, and an application for funding was submitted in 2019. Awarded funding from CDBG will not be known until Feb 2020. It was the Committee's hope that the project would received at least partial funding from CDBG that would allow the project to move forward with an allocation of \$50,000 from the Sidewalk Committee (with a total estimated construction cost of \$156,000).
Sidewalk - Walnut Street - from Winslow to Ridgeview (East Side)	\$239,000.00	\$32,000.00		This sidewalk request was new to the Committee in 2020. A resident submitted a request for a sidewalk along the east side of the street, given the difficulty in crossing a busy road to get to the sidewalk on the west side of the street. Staff also identified this project as worthy of consideration for funding. The project was estimated to cost approximately \$239,000 (Design: \$32,000, Right-of-way: \$0; Construction: \$207,000).
Sidewalk - Adams Street - from Kirkwood to Fountain (West Side)	\$240,000.00	\$31,000.00		This sidewalk request was also new to the Committee in 2020. Staff identified this project as one they would prioritize, though staff also cautioned the committee against expanding the scope of the project farther north, which could complicate the project given the nearby railroad crossing. The project was estimated to cost approximately \$240,000 (Design: \$45,000; Right-of-way: \$49,000; and Construction: \$146,000). The Committee's recommendation of \$31,000 was meant to provide staff with enough funding to begin design work on the project.

Traffic Calming -Graham Drive/Broadview Neighborhood	\$60,000.00	\$60,000.00		This allocation follows from previous committee allocations for temporary traffic-calming devices used in the Broadview Neighborhood. Assuming nearby residents support installation of permanent traffic calming devices, this allocation provides funding to install such devices.
Traffic Calming/Pedestrian Crosswalk improvements - Intersection of E. Moores Pike/Smith Road	\$28,000.00	\$28,000.00		This intersection was brought to the Committee's attention by Committee member Rollo, who pointed out that the sidewalk on Moores Pike west of Smith Rd (north side) does not align with the sidewalk east of Smith Rd. (south side). He suggested a crosswalk with signage or other improvements to permit safe crossing for pedestrians to cross at that intersection to continue on the existing sidewalks. This allocation is intended to accomplish this purpose. Staff estimated that design for the project would cost \$8,000 and construction would cost \$20,000.
Total	\$846,000.00	\$324,000.00	\$106,000.00	The Committee should learn the CDBG funding allocations in February 2020. The Committee also indicated its order of priorities for the 2020 projects to provide guidance to staff in the event CDBG funding comes back lower than needed to complete the 14th Street project.

2019				
Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
Sidewalk - 14th Street - from Madison to Woodburn (North Side)	\$186,000.00	\$30,000.00	\$156,000.00	This one-block sidewalk project is ranked #4 on the Committee's Evaluation Sheet. It would provide a missing link in sidewalks which currently connect with South College to the east and North Madison on the west. Staff noted that there would be stormwater issues on the north and an impact on parking and properties on the south. HAND staff indicated that the project might be a good fit for Community Development Block Grant (CDBG) funding. At suggestion of staff, the Committee agreed to recommend \$30,000 to fund design this year, with the prospect of CDBG funding construction in 2020. The estimated cost of construction is \$156,000.
Sidewalk - Moores Pike - from College Mall Road to Woodruff (South Side)	\$136,880.00	\$195,000.00	\$0.00	Moores Pike east of College Mall Road is a busy road with neighborhoods to the south without a sidewalk to the intersection with College Mall Road. In 2009, the Committee funded a sidewalk from Andrews Circle to an existing sidewalk to the east, but was stymied by the estimated cost for widening the roadway at its approach to the intersection. In 2017, the Committee requested new estimates which, with use of the existing roadway, brought down the costs. After allocating \$41,880 for design in 2016, the Committee allocated \$195,000 in 2019 for construction.
Sidewalk - Mitchell Street – from Maxwell Lane to Circle Drive (East Side)	\$285,503.00	\$73,200.00	\$45,000.00	The bids for construction of this project were higher than expected (see 2018) and the Committee recommended allocation of an additional \$73,200 (over the \$136,808 previously allocated for this project) toward this project in 2019, with P & T funding the shortfalls in the other projects. It is anticipated that construction will be completed in 2019. Note that CBU has agreed to contribute \$45,000 toward the stormwater component of this project. Based upon work done so far, the phases/components of the project, have or will cost as follows: Design (\$35,828), Right-of-Way (\$0), Sidewalk Construction (\$249,675), and stormwater improvements (\$45,000 – via CBU).
Traffic Calming - W. Allen Street - from Patterson Drive to Adams Street	\$35,000.00	\$17,500.00	\$17,500.00	This segment of W. Allen came forward as a request for a sidewalk in 2018. P&T staff studied the area and observed high speeds, poor visibility in certain areas, and transit stops. The traffic calming should make crossing the street safer for transit users and would likely take the form of speed humps. Staff assured the Committee that public outreach would be part of this project. After hearing from P&T staff, the Committee agreed to allocate \$17,500 to join the \$17,500 to be contributed by P & T.
Crosswalk - Intersection of S. Mitchell Street and E. Maxwell Lane	\$2,300.00	\$2,300.00	\$0.00	In the past, and in conjunction with its discussion of pedestrian facilities on S. Mitchell Street south of E. Maxwell Lane, the Committee has explored the installation of a crosswalk at this intersection. The rise of the hill and intersection with Jordan Avenue and Maxwell Lane to the west create potential vehicular/pedestrian conflicts at this intersection. For this reason, the Committee allocated \$2,300 for a crosswalk at this location.
Total	\$931,186.00	\$318,000.00	\$173,500.00	

2018				
Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
Moore's Pike and Clarizz Boulevard - Pedestrian Crossing	\$95,000.00	\$81,000.00		In 2016, when discussing the request for sidewalk on the south side of Moore's Pike at the intersection of College Mall Road, the Committee also looked further east to Clarizz Boulevard and beyond, where there are sidewalks on the north but none on the south. The Committee thought a pedestrian crossing at Clarizz Boulevard would provide some connectivity, but the costs would only be known after an evaluation of what might be done at that intersection. That evaluation was completed in 2017 and proposed the removal of a left-turn lane for eastbound traffic and a possible rectangular rapid flashing beacon, if needed, after experience with usage of the crossing. Costs for the project are expected to include: Evaluation (\$10,710), Engineering (\$20,000), Possible Temporary Right-of-Way (\$ unknown), and Construction (\$75,000). After learning that P & T would contribute up to \$14,000 toward the project, the Committee recommended funding \$81,000 of the \$95,000 needed to complete the project this year.
Walnut Street - Winston/Thomas to National Guard Armory - Sidewalk (West Side)	\$63,000.00	\$63,000.00		In 2003, the Committee began funding missing sidewalks on the west side of South Walnut between Country Club and Rhorer roads, where commercial centers with grocery stores are located at each intersection. It started on the north end, progressed as far as Pinewood, and the Committee has continued to discuss filling in the gaps to the south. In 2016, the Committee reviewed those gaps in sidewalks and sought an estimate for this segment. Total cost of the project would be about \$95,750 – Design (\$32,750), Right-of-Way (\$0) and, Construction (\$63,000). The Committee recommended funding construction this year (\$63,000).
Mitchell Street - Maxwell Lane to Circle Drive - Sidewalk (East Side)	\$198,000.00	\$153,000.00		This sidewalk would serve pedestrians who, due to previous Committee recommendations, have sidewalks on the south at Circle Drive and sidewalks on the north along Maxwell Lane. In 2012, with a modest investment of ~\$1,100, the Committee was able to fund lane-markings for that block (after the Council restricted parking on the east side of the street). In 2016, the Committee recommended funding the design for this project which will be completed in 2018. Based upon work done so far, the phases/components of the project, have or will cost as follows: Design (\$27,250), Right-of-Way (\$0), Sidewalk Construction (\$153,000), and Storm Water Improvements (\$45,000). After learning that CBU would be willing to pay for the storm water improvements, the Committee recommended funding \$153,000 for construction of the sidewalk.
Maxwell Street- Miller Drive to Short Street (West Side)	To be Determined	\$13,000.00		This project was suggested to the Committee by Cm. Rollo after the other new requests had been compiled and ordered on the Evaluation Sheet. It addressed a Planned Unit Development for a Co-Housing project at the corner of Short Street and South Maxwell Street. He heard concerns from residents about the additional vehicular traffic that they anticipate with the additional units and the proposed connection of Short Street to Highland Avenue. A sidewalk along the west side of South Maxwell from Miller Drive would help mitigate the problem. The Committee discussed whether splitting the project into a northern portion to be paid by the City and a southern portion to be paid by the developer might bring about these changes quickly and at a lower cost to the City. To start the process, the Committee recommended allocating \$13,000 toward design of the northern portion of this sidewalk.
Traffic-Calming	\$50,000.00	\$2,000.00	037	Rather than fund a particular traffic-calming project, the Committee recommended funding \$2,000 toward the acquisition of temporary traffic-calming (speed slowing) devices.

Total	\$406,000.00	\$312,000.00		
2017				
Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
E. 10th from Smith Road to Deckard /Tamarron Drive (South Side) - Sidewalk, Pedestrian Crossing, and Other Safety Improvements	\$274,650.00	\$58,000.00		In 2016, after previous interest and investment in the proximate area, the Committee recommended allocating \$50,000 for the design of the sidewalk from Smith Road to the Deckard / Tamarron Drive intersection and a crossing of 10th at that intersection. The design, which cost \$24,460, entailed a meeting with residents and others, and led to a proposal that includes a 10' wide sidewalk from Deckard to Tamarron and a crossing at Tamarron. The crossing should include a median island, school zone, pedestrian hybrid beacon, pavement markings, and advance signage. In 2017, the Committee recommends funding \$58,000 and staff is proposing to add another \$177,000 from other City funds. In addition, staff is working with MCCSC for contributions toward a school zone beacon (~\$15,000) and with INDOT for contributions towards a pedestrian HAWK signal (\$131,000). The City should complete its portion of this project in the summer or fall of 2017 and INDOT should complete its HAWK signal in the summer of 2018.
Rockport Road from Graham Drive to south of West Pinehurst Drive (West Side) - Sidewalk	\$224,460.00	\$200,000.00		Last year, the Committee recommended allocating \$22,000 for the design of this sidewalk project along Rockport Road. Once this sidewalk and sidewalks associated with the intersection improvement at Rockport Road and Tapp Road are completed, the entire boundary of the triangular Broadview Neighborhood will have had pedestrian facilities installed since its phased annexation spanning the late 1990's and early 2000s. This year, the Committee recommends funding \$200,000 for the construction of the second-to-last leg of this long-term pedestrian plan.
Sare Road at Buttonwood Lane and at Spicewood Lane -Traffic calming / Pedestrian Crossing	\$95,000.00	\$48,000.00		This pair of crossings was proposed by Cm. Rollo and was supported by a representative of the Spicewood neighborhood, who addressed the Committee. The crossings would complement a multiuse path from the Renwick development to Buttonwood Drive that the City has proposed for Metropolitan Planning Organization (MPO) funding. Together, these projects would provide a bicycle and pedestrian connection to similar facilities both north and south of this section of Sare Road. The Committee recommends allocation of \$48,000 for these crossings and P & T staff offered an additional \$47,000 needed to design and install these projects. The high cost for this pair of crossings and the discussion of other possible traffic-calming projects led the Committee to adopt a motion to separate consideration of traffic-calming from sidewalk projects in the future.
Total	\$594,110.00	\$306,000.00		
2016				
Site	Estimate	Recommendation	Possible Additional Appropriation	Comments
SR 45/46 Bypass and Tunnel to 7th Street (West Side) - Sidewalk	\$65,000.00	\$20,000.00	038	This project would connect the sidepath on the west side of the SR 45/46 Bypass and the bicycle/pedestrian tunnel at this site with 7th Street and, thereby, to the neighborhoods to the south and west. It would include installing a ramp from the Bypass to the tunnel and stairs to 7th Street. The cost has grown as the project moved from an in-house to a contracted one. Design was paid for previously. A contingent allocation last year was left unspent because other funds were not available. This year the P & T department has made \$35,000 available and the Sidewalk Committee recommends allocating the remaining \$20,000 to complete this project in 2016.

E. 10th from Smith Road to Tamarron Drive (South Side) - Sidewalk, Pedestrian Crossing, and Other Safety Improvements	\$249,000.00	\$50,000.00		In 2003 and 2004, the Committee funded a sidewalk east of Grandview to connect with existing sidewalks toward town. Over the years, various requests for pedestrian infrastructure from Grandview Drive to Russell Road have been made. The reasons for funding this project include the need to help children walk safely from neighborhoods south of East 10 th to University Elementary School and possibly help MCCSC reduce transportation costs associated with bussing the children to and from school. Staff has been in contact with Indiana Department of Transportation (INDOT), which has jurisdiction over this portion of the corridor, about use of the right-of-way and other cooperation with this project. The Committee recommends funding \$50,000 for design which would include a crossing of East 10 th . An additional \$12,000 would be needed for right-of-way and \$187,000 for construction to complete this project.
Morningside Drive from Sheffield Drive to Park Ridge Road - Sidewalk	\$110,000.00	\$110,000.00		This project would extend a Committee sidewalk project on Morningside Drive which ended at Sheffield to sidewalks and park on Park Ridge Road. The curve in Morningside raised safety issues for pedestrians who now walk in the road and may entail some storm water infrastructure. The Committee recommended funding design (\$15,000), right-of-way (\$4,000), and construction (\$110,000) this year (or bid this year for construction next year).
Moore's Pike from College Mall Road to Woodruff Lane (South Side) - Sidewalk	\$135,000.00	\$24,000.00		Moore's Pike east of College Mall Road is a busy road with neighborhoods to the south without a sidewalk to the intersection with College Mall Road. In 2009, the Committee funded a sidewalk from Andrews Circle to an existing sidewalk to the east, but was stymied by the estimated cost for widening the roadway for a sidewalk to the intersection with College Mall. This year, the Committee requested new estimates which, with use of the existing roadway, brought down the costs to \$135,000 - \$24,000 for design and \$111,000 for construction. The Committee recommended funding design this year.
Union Street from 4th to 7th Street (East Side) - Sidewalk	\$189,000.00	\$32,000.00		This project was first requested in 2008. Union can be a busy street, at times. There is a sidewalk on the west side from 3rd to 10th and on the east side from 3rd to 4th and from about a half block north of 7th to 10th. Over the years, the Committee has heard that pedestrians walk in the street on the east side. Total cost of this project would be \$189,000 with \$32,000 for design, \$34,000 for acquisition of right-of-way (which may be reduced by owner(s) willing to donate the land), and \$123,000 for construction. The Committee recommended allocation funds for design (\$32,000).
South Walnut Street from Winston Thomas to National Guard Armory (West Side) - Sidewalk	\$87,000.00	\$13,000.00		In 2003, the Committee began funding missing sidewalks on the west side of South Walnut between Country Club and Rhorer roads. It started on the north end and progressed as far as Pinewood, and the Committee has continued to discuss filling in the gaps to the south. This year, the Committee reviewed the missing sidewalk segments and sought an estimate for the Winston Thomas to National Guard Armory piece. Total cost of the project would be about \$123,000 – design (\$12,000), right-of-way (\$1,000) and construction (\$74,000). The Committee recommended funding design and right-of-way this year (\$13,000).
Mitchell Street from Maxwell Lane to Circle Drive (East Side) - Sidewalk	\$112,000.00	\$22,000.00		This sidewalk would serve pedestrians who, due to previous Committee recommendations, have sidewalks on the south at Circle Drive and sidewalks on the north along Maxwell Lane. In 2012, with a modest investment of \$1,100, the Committee was able to fund lane-markings for that block (after the Council restricted parking on the east side of the street). This year the Committee sought estimates for a sidewalk which totaled \$112,000 and recommended funding design (\$22,000). The remainder of the costs would be for construction (\$90,000) (with no funds needed for right-of-way).

Rockport Road from Graham Drive to south of West Pinehurst Drive (West Side) - Side Walk	\$137,000.00	\$22,000.00		For well over a decade, the City has invested in pedestrian infrastructure surrounding the triangular-shaped Broadview area. A ~\$1.2 million road & sidewalk project along Rockport Road near Countryside Lane was completed in 2015 (with a ~\$25,000 investment from the Committee for some preliminary costs). No sidewalks are in place on the west side of the street from Graham Drive to the intersection at Tapp Road. An intersection improvement at Tapp Road, primarily funded through the MPO (with federal money), will bring sidewalks to just south of West Pinehurst. The Committee sought an estimate for the missing segment north to Graham Drive and recommended funding for design. Total costs add up to \$137,000 and include \$22,000 for design, \$29,000 for right-of-way, and \$86,000 for construction.
Traffic calming	\$5,000.00	\$5,000.00		The Committee recommend an allocation of \$5,000 for some possible as yet unidentified traffic-calming projects.
Moore's Pike at Clarizz Boulevard (Pedestrian Crossing)	?	*(\$2000)	\$6,000.00	When discussing the south side of Moore's Pike at the intersection of College Mall Road, the Committee also looked further east to Clarizz Boulevard and beyond, where there are sidewalks on the north but none on the south. The Committee thought a pedestrian crossing at Clarizz Boulevard would provide some connectivity, but the costs would only be known after an investment in design (\$8,000). Given other priorities this year, the Committee recommended funding this project if funds reverted in 2015 could be reappropriated. In that event, the allocation would include \$2,000 from 2016 and \$6,000 for 2015.
College Avenue from 10th to 17th - Road Repaving and Curb and Sidewalk Replacement Project	?		\$12,885.00	In the event of an additional appropriation of unspent funds reverted to the ATF at the end of 2015, the Committee responded to a request from Public Works to help with this road repaving and curb and sidewalk replacement project.
TOTAL	\$1,089,000.00	\$298,000 *	\$18,885.00	Note: Another \$2,000 would be added to the \$298,000 to bring the total to the full budgeted amount of \$300,000 if an additional appropriation of unspent funds in 2015 (see column to the left) was approved and allowed, in part, monies for the Moore's Pike /Clarizz pedestrian crossing.

2015				
Site	Estimate	Recommendation	Additional Appropriation	Comments
Kinser Pike - 17th Street north to Apartments (East Side)	\$198,821.00	\$143,851.00		This highest ranking project has been on the list for over a decade due, in large part, to the cost of the right-of-way (which was estimated, at times, at over half of the total project cost. After obtaining estimates for both sides of the street, the Committee chose the east side, which was less expensive and more likely to be used. This recommendation follows expenditures for design and appraisals in 2014 and commits funds necessary to complete this project in 2015.
West 17th Street -- Four Parcels West of Maple to Madison (South Side)	\$600,000.00	\$70,000.00		* Installation of sidewalks on West 17th Street has been a high priority for the City. Given work on the east and west, the scope of the project now runs from four parcels west of Maple to Madison Street. This year's allocation will pay for acquisition of right-of-way between Maple and Madison and design of the sidewalk for the four parcels west of Maple.
Sheffield - Morningside Drive to Providence (West Side)	\$83,000.00	\$75,000.00		This project would complete missing sidewalk segments along Sheffield that would connect with existing sidewalks along Plymouth on the north and recently-completed Council Sidewalk Committee projects on the south along Morningside Drive. Speed of cars descending the curve to Morningside, in part, made this a priority for the Committee. The design was done last year by contract at a cost of \$8,010. The allocation this year will pay for acquisition of temporary right-of-way (\$20,000) and construction (\$55,000) and, if all goes well, should complete the project this year.
Traffic-Calming (Crosswalk at Maxwell and Mitchell Street)	\$5,000.00	\$5,000.00		The Committee initially set aside \$15,000 for a few possible traffic calming projects this year. These included a component of an old project by Fairview School, a crosswalk at Maxwell Lane and Mitchell Street, and traffic calming along Morningside Drive. Given other higher priorities and the likelihood of expenditures in 2015, the Committee allocated \$5,000 toward the crosswalk at Maxwell Lane and Mitchell Street.
SR 45/46 Bypass and Tunnel to 7th Street (West Side)	\$65,000.00	\$6,149.00	\$43,001.00	This project would connect the side path on the west side of the SR 45/46 Bypass and the bicycle/pedestrian tunnel at this site with 7th Street and, thereby, to the neighborhoods to the south and west. It would include installing a ramp from the Bypass to the tunnel and stairs to 7th Street, and may include landscaping provided through CDBG funds. The cost has grown as the project moved from an in-house to a contracted one. Given other higher priorities, the allocations included about \$6,150 from the \$300,000 ATF Budget and an estimated \$43,000 in inspect 2014 funds that might be additionally appropriated for this purpose. In effort to complete this project, the Committee also requested the Administration explore use of other funds to complete this project. That could include paying for traffic calming and allowing that money to go towards this project.
Total	\$951,821.00	\$300,000.00	\$43,001.00	* An additional appropriation may come forward to make unspent 2014 funds available for use in 2015. The amount is an estimate and may change.

2014				
Site	Estimate	Recommendation	Other Funds	Comments
Kinser Pike - 17th Street north to Apartments (East Side)	\$228,412.80	\$38,068.80		This highest ranking project has been on the list for over a decade due, in large part, to the cost of the right-of-way (which amounts to over half of the total project cost of \$228,412). After obtaining estimates for both sides of the street, the Committee chose the east side, which was less expensive and more-likely-to-be-used. This recommendation commits \$38,068.80 toward the design of this project in 2014 with construction considered a high priority in 2015.
West 17th Street -- Maple to Madison (South Side)	\$276,361.80	\$58,810.30		* Installation of sidewalks on West 17th Street has been a high priority for the City and will see progress to the east and west of this project in the near future. This year, the Committee learned it would cost \$276,361.80 for this project, which would include about 650 feet of sidewalk (with some sidewalk already in place), some steps here and there, and some storm water component (estimated at about \$59,000) that might be covered by City Utilities. The recommendation this year is to allot \$46,060.30 toward the design and \$12,750 toward appraisal work for this project and make construction a high priority next year. *CBU will explore in-kind contributions toward the storm water component of this project.
SR 45/46 Bypass and Tunnel to 7th Street (West Side)	\$20,000.00	\$20,000.00		This project would connect the side path on the west side of the SR 45/46 Bypass and the bicycle/pedestrian tunnel at this site with 7th Street and, thereby, to the neighborhoods to the south and west. It would include installing a ramp from the Bypass and stairs from the tunnel. The cost is estimated at \$20,000 and the stairs would have a "cheek wall" for bicyclists to use for their bikes after dismounting them. The Committee thought this may have the added benefit of encouraging more bicycle and pedestrian traffic between the neighborhoods to the east and the campus to the west.
Leonard Springs -- 300 feet South of Walmart Entrance to Tapp Road	Unknown	\$15,000.00	Unknown	Last year the Committee recommended contributing as much as \$15,000 to this Monroe County project over two years if it was going forward. The logic for contributing is two-fold: first, the roadway is owned by the City (but the adjacent land is within the County) and second, there are some pockets within the City to the south with residents that would use the sidewalk. The project would be about 1,200 feet long and cross 10 parcels of land. <i>It is conditioned on adequate assurances that the project will go forward and the contribution will be spend in 2014.</i>
Sheffield - Morningside Drive to Providence (West Side)	\$63,414.45	\$55,143.00		This project would complete missing sidewalk segments along Sheffield that would connect with existing sidewalks along Plymouth on the north and recently-completed Council Sidewalk Committee projects on the south along Morningside Drive. Speed of cars descending the curve to Morningside, in part, made this a priority for the Committee. The Engineering Department will design the project which reduced the outlay by \$8,271.45.
Maxwell Lane -- Jordan Avenue to Sheridan (North Side)	\$96,279.38	\$96,279.38		This follows on the project in 2013 that brought a sidewalk to the north side of Maxwell from the bottom of the hill at Highland to mid-way up the hill at Jordan. It will continue the project over the crest of the hill to Sheridan. Once this block is done - with the help of previous Committee-recommended projects - there will be a continuous run of sidewalks all the way from High Street on the east to Henderson on the west. Funds for the design of this project were provided in 2006.
Traffic-Calming (Unspecified)		\$15,000.00		The Committee set aside \$15,000 for unspecified traffic-calming projects in the event one is ready for installation this year.
Total	\$621,053.98	\$298,301.48		* Note: This history reflects Annual Committee Reports and not Interim Reports. An Interim Report was approved for both 2013 and 2014 that reallocated these funds.

2013				
Site	Estimate	Recommendation	Other Funds	Comments
West 17th Street -- Madison Street to College Avenue (South Side)	\$268,199.00	\$147,351.16	\$107,199.00	Following an investment in the design of this project in 2011 and an offer from City of Bloomington Utilities to cover the storm water costs associated with it, the Committee recommended funding construction of a sidewalk in 2013. The offer from CBU reduced the allocation for this project from \$268,111 to \$161,000, but with the understanding that some of the estimated \$8,500 in remaining funds for the year might be needed to cover any overage. <i>Note that, on December 18, 2013, the Council amended the recommendations to reflect the lower than expected bid for this project.</i>
Maxwell Lane -- Highland Avenue to Jordan Avenue (North Side)	\$87,000.00	\$95,543.62		This is one block of a two-block project that would be constructed on the north side of the street from the bottom of a hill (at Highland) to the other side of the crest at Sheridan. Once these two blocks are complete - with the help of previous Committee-recommended projects - there will be a continuous run of sidewalks all the way from High Street on the east to Henderson on the west. Funds for the design of this project were provided in 2006. <i>Note that, on December 18, 2013, the Council amended the recommendations to reflect an increase in cost of the project from \$87,000 to \$95,543.62, due to the removal of rock.</i>
Moore's Pike and Olcott Boulevard -- Pedestrian Crossing	\$18,500.00	\$7,959.90		This is a pedestrian crossing with a raised island and lane markings to narrow the roadway. It follows a denial of a stop sign request at the Traffic Commission in January and <i>does not</i> include the installation of a stop sign. The crossing will provide residents in Hyde Park and points south access to a continuous sidewalk that runs along the north side of Moore's Pike from Smith Road to Sare Road and further west. <i>Note that, on December 18, 2013, the Council amended its recommendations to reflect an altered project (now with no island, but with a solar-operated speed indicator) and a drop in cost from \$18,500 to \$7,959.90, largely due to the labor having been provided by the Public Works Department..</i>
Rockport Road -- Countryside Lane south 2,000 feet to just past Graham Drive (West Side)	\$1,200,000 +	\$24,145.32	\$1,200,000.00	<i>Note that, on December 18, 2013, the Council amended its recommendations to include this allotment toward a large multi-phased road-improvement/storm water project along Rockport Road. This contribution of \$24,145 can be committed in 2013 toward appraisal work necessary for the project and follows through on a recommendation in 2012 to use any remaining funds that year for this purpose.</i>
Leonard Springs -- 300 feet South of Walmart Entrance to Tapp Road	Unknown	\$0.00	Unknown	This is a County project to be constructed on land in the county that lies along a city-owned roadway. The design and total cost of the project are unknown at this time. The County sees the need for the project (which is evident with the path worn by pedestrians) and is interested in a contribution from the City. After learning that City residents to the south would probably use the sidewalk, the Committee agreed to contribute any funds remaining this year once there were adequate assurances that the project will be completed in the short term. The Committee also declared intent to contribute as much as \$15,000 toward this project over two years. <i>Note that, on December 18, 2013, the Council amended the recommendations to defer any contribution to this project until 2014 when the project moved forward to the point the money could be used.</i>
Total	\$373,699.00	\$275,000.00	\$1,307,199.00	

2012				
Site	Estimate	Recommendation		Comments
		ATF	Other Funds	
Third Street -- Overhill Drive to Travel Lodge Driveway (North S	\$154,474.00	\$154,474.00		See the 2011 and 2010 descriptions below for the details of the larger project, which will result in the construction of sidewalks on the north side of East Third Street from Union to the SR 45/46 Bypass. Contributions from other sources include: \$100, 00 from Greenways; \$75,000 from HAND; and the installation of sidewalks by INDOT as part of the SR 45/46 Bypass project.
Mitchell Street -- Maxwell Lane to Circle Drive	\$1,100.00	\$1,100.00		This project proposes the use of lane markings to designate a portion of the west side of the roadway of this one-block segment as a pedestrian corridor. It would provide a pedestrian facility that connects a City-created pedestrian corridor on the south, which runs from Bryan Park to sidewalks at Marilyn Drive and High Street, to City-installed sidewalks along Maxwell Lane. <i>Note: This recommendation was conditioned upon approval of the associated removal of parking on that side of the street. Please also note that the lane was eventually approved for the east side.</i>
Morningside Drive -- Saratoga to Sheffield (West Side)	\$19,866.00	\$19,866.00		This recommendation continues upon the completed 2011 recommendation to install a sidewalk from Smith Road to Saratoga. Please see the 2011 description below for more information about this project
Rockport Road -- Coolidge to 310 feet North of the Intersection (West Side)	\$80,440.00	\$34,560.00	*	This recommendation would partially fund the sidewalk project by contributing funds toward the cost of acquiring the right-of-way. It is intended to leverage other resources to fill-in one of three missing sidewalk links along Rockport Road from Tapp Road to Rogers Street in 2012. The other missing links include a long section north of Tapp Road which will be constructed as part of the roundabout at that intersection and a segment north of Ralston, which remains unfunded. <i>Note: This recommendation would allow any remaining funds to be applied towards the cost of right-of-way and is conditioned upon Committee acceptance of assurances that the sidewalk will be completed in 2012.</i> <i>* CBU staff have inspected the site and offered suggestions on handling the storm water.</i>
Total	\$255,880.00	\$210,000.00	\$0.00	

2011				
Site	Estimate	Recommendation		Comments
		ATF	Other Funds	
Third Street -- Segments 1-4: Bryan to Hillsdale	\$387,405.00	\$129,811.00	\$175,000.00	See 2010 description below for project details. The 2010 Committee dedicated the bulk of its funds to the E. Third Street project. After applying 2010 funds to this project, \$129,811 was need for the completion of Segments 1-4. The other funds include \$100, 00 from Greenways & \$75,000 from HAND.
Third Street -- Segment 5: Hillsdale to Travel Lodge	\$300,893.00			Design for this project will be completed with 2010 funds.
Southdowns -- Jordan to Mitchell (with exploring the possibility of CBU making an in-kind contribution toward stormwater improvement)	\$53,153.00	\$50,622.00		With the completion of Marilyn Drive sidewalk in 2011, this segment would culimate a multi year effort to create a continuous pedestrian corridor running from Bryan Park to the sidewalks at High and Covenancter. he stormwater component of this project is \$16,000. The Committee requested that CBU make a good-faith effort to explore whether they would be able to make an in-kind contribution re: the stormwater component of this project.
Morningside Drive - Smith to Saratoga (side of road to be TBD)	\$13,929.00	\$13,929.00		This project is intended to provide a pedestrian route to compensate for the loss of a Bloomington Transit bus stop on Morningside Drive. The closest stop is now on Smith Road and many people walk down Morningside to get to the stop at Smith. The walk is precarious and uncomfortable. This is a neighborhood with many children and a neighborhood that sees many pedestrians. A sidewalk would really help pedestrians get safely to the Smith stop
West 17th -- Woodburn to Madison (southside)	\$282,878.00	\$15,638.00		This is a highly rated, but expensive, project that has been under consideration by the Committee for many years. Funds remaining after the other projects are covered will be used the design of this project (\$25,000). The design should lower the cost of the project.
Total:	\$1,038,258.00	\$210,000.00	\$175,000.00	

2010				
Site	Estimate	Recommendation		Comments
		ATF	CBU Stormwater	
Marilyn -- Nancy to High (south side)	\$189,937.45	\$98,373.43	\$91,564.00	See 2009 description below for project details. As federal funds requested from the Mayor were not available for 2010, the Committee agreed to dedicate ATF funds to complete this project.
Third Street -- Bryan to Jefferson (north side)	\$95,408.78		\$22,638.00	Connection is needed from Roosevelt to the SR 46 Overpass to link up with the existing sidewalk. The 2009 Committee forwarded a recommendation to the 2010 Committee encouraging the latter to fund as much of this project as possible. The 2010 Committee agreed that, after funding the above previously-committed Marilyn project, it should devote all remaing funds to the Third Street project. The Committee voted to fully fund the first two stretches of this project (Bryan to Jefferson and Jefferson to Roosevelt) and to fund as much of
Third Street -- Jefferson to Roosevelt (north side)	\$63,507.68	\$31,912.23	\$4,366.00	
Third Street -- Roosevelt to Clark (north side)	\$118,387.50	\$114,252.60	\$4,135.00	
Southdowns -- Jordan to Mitchell (w/Stormwater on Jordan and Sheridan) (south side)	\$124,405.05		\$54,562.20	This is part of larger area in need of stormwater improvement and has been on the Sidewalk Committee's list of requested projects since 2002. The Committee agreed to address the stormwater issue on Southdowns first and then the sidewalk later. The amount of stormwater dedicated to this project is not to exceed the original estimated cost -- \$54,562.20
Total:		244,538.26	\$177,265.20	

2009				
Site	Estimate	Recommendation		Comments
		ATF	CBU Sidewalk	
Marilyn -- Nancy to High (south side)	\$189,937.45	\$0.00	\$91,564.00	This is one of the last segments of a route on the Bicycle and Pedestrian Transportation and Greenways System (Greenways) Plan that would connect Bryan Park with sidewalks at High and Covenanter. Prior ATF funds were used to install sidewalks on Mitchell, Circle, Ruby and Nancy Street. Last year the Committee requested and expected that the Greenways monies would be used to cover the sidewalk and the CBU Set Aside would cover the storm water component of this project. However, an amendment to the Greenways Plan and other projects left this one unfunded in 2008. As noted above, the Committee recommended that the Council respectfully request that the Mayor consider appropriating \$98,937.45 of federal reimbursement of matching funds to complete this project.
Henderson -- Moody to Thornton (east side)	\$99,319.17	\$71,877.77	\$27,441.40	This project was scheduled for funding in 2008. It was requested by the Planning Department, MCCSC, and a property owner and would complete the last segment of unfinished sidewalk on the east side of Henderson between Hillside and Miller Drive as well as much further north and south. The HAND department may help fund some of this project.
Kinser Pike -- Marathon Stn. to 45/46 (west side)	\$54,751.14	\$40,280.74	\$14,470.40	This is a heavily-travelled stretch. Many residents living in multi-family housing walk here to the grocery store and other amenities.
Moore's Pike -- Segment A - Woodruff to existing walk (south side)	\$22,758.00	\$22,758.00	\$0.00	This stretch provides connectivity with an existing walk and was requested by area residents. This project will provide residents with a safer crossing of Moore's Pike. Some residents indicated that they would be willing to make a contribution.
S. Madison -- 3 rd to Prospect (east side)	\$49,773.00	\$26,989.00	\$16,784.00	This project is in a highly-urban area and would link to the B-Line trail at the W. 3 rd Street overpass. Public Works will commit \$6,000 for concrete.
3 rd Street -- Roosevelt to Clark & Clark to Hillsdale (north side)	\$231,564.07	\$50,000 *	\$0.00	Connection to link up to the existing sidewalk network. A worn pedestrian path demonstrates the heavy use of this area. The 2009 Committee agreed that if the funds remaining for the above projects are not needed to complete said projects, up to \$50,000 of the remaining 2009 ATF balance shall be dedicated to right-of-way acquisition for this project.
Total:		211,905.51	\$150,259.80	

2008				
Site	Estimate	Recommendation		Comments
		ATF	CBU Sidewalk/ Stormwater	
5th Street -- Hillsdale to Deadend (south side)	\$535,088.97	\$70,485.63	\$0.00	This two-block long, multi-departmental project provides an east-west connection through the Greenacres Neighborhood and needed stormwater infrastructure for the area. Total funding includes: \$112,934.36 (2007 ATF), \$10,453.98 (2007 CBU Sidewalk/Stormwater Setaside); \$216,215 (CBU Capital Project), and \$125,000 (HAND Neighborhood Improvement Grant). Note: This project was completed in 2008.
Henderson -- Allen to Hillside (west side)	\$669,090.00	* \$3,667.21	\$0.00	This improvement is aimed at alleviating pedestrian/vehicular conflict in this elementary school area. The Committee funded design in 2007 at the request of Public Works. Public Works received a \$250,000 Safe Routes to School grant for this project and wanted an additional sign of support from the Council in order to garner funds from other sources (including CDBG). * Note: The Committee recommended that any funds remaining in 2008 may be applied to this project.
Marilyn -- Nancy to High (south side)	*\$167,578.63	\$0.00	* \$62,480	This is one of the last segments of a route on the Alternative Transportation and Greenways Plan that would connect Bryan Park with sidewalks at High and Covenant. Prior ATF funds were used to install sidewalks on Mitchell, Circle, Ruby and Nancy Street. The Committee requests that Alternative Transportation and Greenways monies fund the \$105,098.63 needed for the sidewalk portion of this project. *Note: The Committee also realized that the stormwater component will be more expensive than indicated and authorized that any remaining funds be used for this purpose.
E. 2nd Street -- Woodcrest to 300' east (north side)	\$34,300.00	\$32,319.00	\$1,981.00	This small project would fill-in the last missing stretch of sidewalk on both sides of East 2nd from College Mall Road to High Street, which sees high levels of vehicular and pedestrian traffic. Note: ATF funded design of this project in 2006. Note: This project was completed in 2008.
Henderson -- Thornton to Moody (east side)	\$71,735.90	\$49,405.90	\$22,330.00	This will complete a missing link on the east side of Henderson and provide uninterrupted sidewalks and crossings on that side of the street for at least a mile.
High Street - Across from Childs School (west side)	\$22,362.55	\$21,078.05	\$577.50	This project would create a continuous sidewalk on the west side of High Street across from Childs School, which has the highest walk-in rates in the community. The sidewalk may also allow the City to eliminate one crossing guard. Note: This project was completed in 2008.
West 17th Street -- Lindberg to Arlington Park Drive (south side)	\$52,077.21	\$27,337.21	\$0.00	A new development at the corner of W. 17th and Crescent Road led to this request. The total project should cost about \$52,077.21, but the possible donation of right-of-way by abutting property owners and contribution of materials by the developer would lower the cost to the amount as listed here. Note: This project was completed in 2008.
Total:		204,293.00	\$87,368.50	* Note: Any remaining ATF monies may be applied to the Henderson - Allen to Hillside project and any remaining CBU sidewalk/stormwater funds may be applied to Marilyn - Nancy to High Street. Also, using the estimates for CBU Sidewalk/Stormwater projects as presented in this chart and the carryover of \$22,834.79 from 2007, there would be approximately \$60,466.29 available for future CBU Sidewalk/Stormwater projects.

2007				
Site	Estimate	Recommendation		Comments
		ATF	USB Stormwater	
5th Street -- Overhill to Deadend (south side)	\$262,685.80	\$92,646.50	\$29,344.60	This provides an east-west connection through the Greenacres Neighborhood. * Note: The Committee committed to dedicate 2008 ATF monies to complete this project if the sum allotted is insufficient. This is part of a larger initiative to improve the stretch on 5th Street from Hillsdale to the deadend. CBU has dedicated
Henderson -- Allen to Hillside (west side)	unknown	\$45,000.00		
Arden -- Windsor to High (south side)	\$100,452.00	\$47,353.50	\$53,098.00	The neighbors met with Councilmember Rollo and wanted a sidewalk to help their kids get to High Street and Southeast Park. Note: This project was completed in 2007.
Total:		185,000.00	\$82,442.60	

2006				
Site	Estimate	Recommendation		Comments
Queens Way, Sussex to High (south side)	\$25,969.68	\$25,969.68		This is the missing link, connecting High to Renwick.
Roosevelt, Fourth to Fifth (east side)	\$127,269.79 with curbs	\$127,269.79		This ties in with the recent improvements made by Doug McCoy which made Roosevelt a through-street.
Arden -- From High to Windsor (south side)	\$59,486.72	\$5,000 (design only)		This project provides a safe walk way for the neighborhood's many children to travel to a near-by school & park.
E. 2nd -- Woodcrest to 300' east (north side)	\$31,574.66	\$5,000 (design only)		This project is the missing link on the north side of the street from College Mall to the west. Justin suggested that in future years, the Committee might provide material and ask CBU to install.
11th Street-- Washington to Lincoln (north side)	\$60,151.41	\$10,000 (design only)		
Maxwell -- Highland to Jordan (north side)	\$65,658.98 with tree plot & piping	\$5,000 (design only)		This 2-block project completes the missing link on Maxwell between Henderson & High.
Maxwell -- Jordan to Sheridan (north side)	\$72,479.88 with tree plot & piping	\$5,000 (design only)		This 2-block project completes the missing link on Maxwell between Henderson & High.
Total:		\$183,239.47		

2005			
Site	Estimate	Recommendation	Comments
Maxwell Lane from Clifton Sidepath to High Street (north side)	\$65,175.00	\$65,175.00	Since 1999, the Committee has funded sidewalks on Maxwell Lane between Henderson and High Street. The first project was north of Bryan Park and ran from Henderson Street to Manor Road and connected to an existing sidewalk that runs to Jordan Avenue. The second project connected a sidewalk on Sheridan with the Clifton sidepath. This project would connect the latter sidewalk to High Street. The Committee recommended that a cross walk be placed on High (to connect with an existing sidewalk) and that sidewalk be placed to preserve trees, if that isn't possible, include a tree plot. Note: The project was rebid and completed in 2007 and was funded, in part, with the reappropriation of \$34,000 in reverted funds .
Queens Way from Chelsea to Sussex (south side)	\$35,729.00	\$35,729.00	The Renwick developer will install a sidewalk on the south side of Queens Way from the new development to Monclair Avenue. The Committee received estimates for installing sidewalks the rest of the way to High Street (\$83,700), funded the first leg between Montclair and Sussex in 2004.
Marilyn from Nancy to High Street (south side)	\$155,216 (one block only)	\$11,497.54 (design only)	This project begins completion of the western end of what's known as the Southeast Neighborhood Initiative. This initiative will eventually connect the walking/biking lane on Southdowns / Jordan with sidewalks at Covenanter / High Street. The City has already completed a sidewalk from Mitchell / Southdowns to Ruby / Nancy Street, and Nancy Street from Ruby to Marilyn Drive. This allocation funds design costs and gives staff an opportunity to determine whether there are storm water costs that might be borne by CBU. One more leg on Southdowns from Jordan to Mitchell would complete this initiative. Note: This project was completed in 2007.
Roosevelt from 4th to 5th (east side)	\$86,340.00	\$6,395.62 (design only)	This is a new project that would complement new private development on Roosevelt that will make it a through-street and include a sidepath on 4 th Street. The estimate for the project is \$86,340 and this recommendation funds the design costs.
Total:	\$187,244.00		
2004			
Site	Estimate	Recommendation	Comments
Sidewalk Project - 10th Street for 350 feet West of Grandview (south side)		\$45,000.00	The Council funded this proejct in 2003 and approximately \$6,344 was spent that year on designing the sidewalk and acquiring right-of-way, but the remaining funds were not encumbered for its construction. The Committee recommends using unspent and unencumbered funds from previous years to fund this project.
Sidewalk Project - Nancy Street from Ruby Lane to Marilyn Drive (west side)	\$45,628.00	\$45,628.00	The Committee recommended funding this segment of the larger South East Neighborhood Initiative. That initiative first received funding in 2002 (see below).
Sidewalk Project - Jefferson Street between 7th and 8th (east side)	\$114,000.00	\$114,000.00	The Committee recommended funding this first segment of the larger Jefferson Street project, which has been designed as a result of previous funding in 2002 (see below). This segment, unlike the others, does not require a large complement of storm water funds.
Sidewalk Project - Winfield Road from Fair Oaks to existing sidewalk just south of Rechter (east side)	\$45,096.00	\$27,000 (+\$18,096 from Wininger/Stolberg)	The Committee recommended funding this project in concert with the developer of the Renwick PUD (Wininger / Stolberg) who has offered to pay for the cost of materials (approximately \$18,096).
Sidewalk Project - Queens Way from Montclair Avenue to Chelsea Court (south side)	\$22,139.00	\$22,139.00	The Committee recommended funding this and the previous project in order to have sidewalks in place before the Renwick PUD gets well under way.
Total:		\$253,767.00	This amount includes \$151,000 of funds appropriated for sidewalks this year and unspent monies from previous years. If there are not enough monies in the Alternative Transportation Fund in 2004, then the Committee will need to decide whether to recommend use of 2005 funds for these purposes.

2003			
Site	Estimate	Recommendation	Comments
Sidewalk Project - East 5th Street from 1 block east of Overhill (deadend) to Overhill.	\$255,596.00	\$52,597.00	On 6/18/03, the Council approved the Committee recommendation to allocate \$52,597 contingent upon the availability of storm water funds.
Sidewalk Project - 10th Street for 350 feet west of Grandview Drive (south side)	\$43,975.00	\$43,975.00	
Sidewalk Project - Walnut Street from Bank One (Country Club/Winslow) to Hoosier Street (west side)	\$104,354.00	\$63,427.00	On 6/2/03 the Committee recommended allocating the remaining funds (\$63,427) to this project and discussed ways to reduce its cost.
Total:		\$159,999.00	
2002			
Site	Estimate	Recommendation	Comments
Sidewalk Project - Southdowns from Jordan and along the north side of Circle and Ruby lane to Nancy Street.	\$148,000.00	\$108,731 (+ \$39,000 from Greenways)	The original estimate was for a sidewalk on the north side of the street, but the Engineering staff and neighborhood preferred south side at estimated cost of \$129,000 (and an additional \$19,000 for the leg from Jordan to Mitchel). On 6/19/02 the Council allocated \$59,547 for this project and, as noted below, on 12/18/02, the Council voted to shift \$49,184 from the East 2nd Street project to this one as well. On May 8, 2003 the Greenways group agreed to fund the remaining \$39,000.
Design for sidewalk and storm water project - Jefferson Street from East 3rd to East 10th Street.	\$27,840.00	\$27,840.00	
Design for sidewalk and stormwater project - East 5th Street from 1 block east of Overhill to Union.	\$28,832.00	\$28,832.00	
Streetscape Plan - East 2nd from High Street to College Mall Road.	\$49,184.00	\$0.00	On 12/18/02 the Common Council voted to shift these funds (\$49,184) to the Ruby Lane project (above)
Sidewalk design - East Allen from Lincoln to Henderson Street	\$4,000 - \$8,000	\$7,400.00	
Total:	about \$160,000	\$172,803.00	

MEMO FROM COUNCIL OFFICE ON:

Resolution 23-04 – A Resolution Authorizing the 2023 Expanded Outdoor Dining Program in the Downtown Corridor

Synopsis

Resolution 23-04 authorizes renewal of the Expanded Outdoor Dining Program in the Downtown Corridor. The Program as renewed in this resolution provides for the continued use of parklets and Kirkwood Avenue for additional seating space from April 3, 2023, through October 1, 2023. As required by Ordinance 22-01, passage of this resolution also explicitly adopts fees for the 2023 Program.

Relevant Materials

- Resolution 23-04
- Exhibit A: Program Guidelines
- Staff Memo from Chaz Mottinger, Special Projects Manager, Economic & Sustainable Development
 - Program Guidelines
 - Beautification and Parklet Art Guidelines
 - Data Collected
 - Kirkwood Parklet Feedback Survey
 - Compilation of additional program feedback

Summary

In June of 2020, the Council approved Ordinance 20-11, which suspended portions of the municipal code in order to support the operation of businesses in the downtown Bloomington area and assist in economic recovery from the COVID-19 public health emergency (background materials for Ordinance 20-11 can be found in the June 10, 2020 [legislative packet](#)). The Council extended provisions of Ordinance 20-11 several times with Resolution 20-15, Resolution 20-19, and Resolution 21-18. In early 2022, City staff recommended that the temporary closures continue to be implemented in future seasons as an Expanded Outdoor Dining Program (Program) irrespective of the COVID-19 public health emergency.

On January 19, 2022, the Council passed Ordinance 22-01, which established and approved the expanded Program, including the use of parking spaces by restaurants and the temporary conversion of certain blocks of Kirkwood Avenue into pedestrian-only spaces (background materials for Ordinance 22-01 can be found in the January 19, 2022 [legislative packet](#)). The ordinance temporarily suspended sections of Bloomington Municipal Code, including standards for encroachment (BMC 12.06), use of right-of-way (BMC 12.08), intoxicating beverages (BMC 14.36.090), loading zones (15.32.100), parking meter changes (BMC 15.40.010 Schedule U), and signs (BMC 20.04.100), to implement the Program for the 2022 season. Resolution 23-04 seeks to extend the Program for an additional one-year term, as provided for in Section 2 of Ordinance 22-01.

City staff recommends the Program be implemented during the 2023 season in accordance with the Program Guidelines, which are attached to Resolution 23-04 as Exhibit A. This recommendation is based on a number of factors, including economic benefits to the local business community still recovering from the impacts of the COVID-19 public health emergency, stakeholder and public input on past implementations of the Program, and increased pedestrian use and sustainability.

Based on the data from stakeholder and public responses, City staff recommends the 2023 Program be shortened. For more information on the timeline for the implementation of the Program, the data gathering process and results, and the differences between the 2022 and 2023 Programs, please consult the staff memo from Chaz Mottinger.

In brief, this resolution would:

1. Approve the Program as set forth in Exhibit A: Program Guidelines;
2. Set April 3, 2023 – October 1, 2023 as the timeframe for operation of the Program;
and
3. Approve the permitting fee and fee schedule for operation of the 2023 Program.

Contact

Chaz Mottinger, Special Projects Manager, Economic & Sustainable Development, 812-349-3418, chaz.mottinger@bloomington.in.gov

Larry Allen, Assistant City Attorney, Legal, 812-349-3426, allenl@bloomington.in.gov

RESOLUTION 23-04

A RESOLUTION AUTHORIZING THE 2023 EXPANDED OUTDOOR DINING PROGRAM IN THE DOWNTOWN CORRIDOR

WHEREAS, on June 17, 2020, the Common Council passed Ordinance 20-11, which suspended portions of the Bloomington Municipal Code during the public health emergency caused by the COVID-19 virus; and

WHEREAS, Ordinance 20-11 relaxed certain City regulations, including sign and seating encroachment regulations, which made it possible to assist local restaurants by extending the outdoor seating area into parklets; and

WHEREAS, the City also temporarily closed portions of Kirkwood Avenue to make it available for enhanced social distancing and outdoor seating; and

WHEREAS, the Common Council approved the Expanded Outdoor Dining Program in Ordinance 22-01, which extended the ability for local restaurants to utilize parklets and enhanced pedestrian and seating space on Kirkwood avenue (“Program”); and

WHEREAS, SECTION 2 of Ordinance 22-01 reserved the right of the Common Council to extend the term of the ordinance for additional one-year terms through an authorizing resolution; and

WHEREAS, the City desires to continue the Program in the downtown corridor through 2023 as detailed by the Program Guidelines that are attached to this resolution as Exhibit A; and

WHEREAS, the Common Council therefore wishes to extend the Program through proactive measures to assist the Bloomington business community and enhance the city’s vibrancy and economic growth;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY INDIANA, THAT:

SECTION 1. As authorized by SECTION 2 of Ordinance 22-01, the Common Council hereby extends the suspension of certain portions of the Bloomington Municipal Code described in SECTION 6 of Ordinance 22-01 through October 1, 2023, and approves the Expanded Outdoor Dining Program as set forth by the Program Guidelines attached to this resolution as Exhibit A.

SECTION 2. The Program shall operate from April 3, 2023, until October 1, 2023, unless earlier terminated under SECTION 7 of Ordinance 22-01.

SECTION 3. As required by SECTION 5 of Ordinance 22-01, the Common Council approves the following fees for the Program:

Program	Fee	Condition
All Programs	\$50	Permitting fee required of all applicants to either program.
Parklet	\$1,250 per space	Cost may be split between businesses sharing parklet space(s)
Kirkwood Seating	\$500	Business with seating capacity under 20 seats
	\$1,250	Business with seating capacity between 20 and 100 seats
	\$3,500	Business with seating capacity over 100 seats

SECTION 4. If any section, sentence, or provision of this resolution, Ordinance 22-01, or the application thereof to any person or circumstance, shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions, or applications of this ordinance which can be given effect without the invalid section, sentence, provision or application, and to this end the provisions of this ordinance are declared to be severable.

PASSED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2023.

SUE SGAMBELLURI, President
Bloomington Common Council

ATTEST:

NICOLE BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2023.

NICOLE BOLDEN, Clerk
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2023.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

Resolution 23-04 authorizes renewal of the Expanded Outdoor Dining Program in the Downtown Corridor. The Program as renewed in this resolution provides for the continued use of parklets and Kirkwood Avenue for additional seating space from April 3, 2023, through October 1, 2023. As required by Ordinance 22-01, passage of this resolution also explicitly adopts fees for the 2023 Program.

Exhibit A: Program Guidelines

Costs to Participating Businesses:

- All:
 - All extended outdoor dining covered under this program will be subject to the \$50 permitting fee to the Engineering Department.
 - All fees are due in full by March 13, 2023.
 - Businesses are responsible for any direct costs associated with utilizing a parklet or outdoor dining on Kirkwood, such as furniture or heaters.
 - Each participating business must submit a certificate of insurance to the Planning and Transportation and Economic and Sustainable Development Departments establishing proof of a comprehensive general liability policy naming the City of Bloomington as additional insured to the extent of at least \$500,000 bodily injury and \$100,000 property damage, which shall be in effect during the term of this authorization.
- Parklets:
 - Cost to businesses will be \$1,250 per parking space for the 2023 season, payable to Parking Services. A two-space parklet will cost \$2,500, plus the permitting fee(s).
 - Businesses which share a parklet may split the cost between them.
- Kirkwood conversion outdoor dining permit:
 - For businesses with a capacity of under 20, the cost will be \$500 for utilization of the extended outdoor space on Kirkwood during the 2023 season.
 - For businesses with a capacity between 20 and 100, the cost will be \$1,250 for utilization of the extended outdoor space on Kirkwood during the 2023 season.
 - For businesses with a capacity 100 and above, the cost will be \$3,500 for utilization of the extended outdoor space on Kirkwood during the 2023 season.

Parklet Logistics:

- Eligibility:
 - Eligibility is limited to any food service establishment adjacent to metered parking in downtown Bloomington.
 - All participating businesses must agree to cease alcohol sales in parklets by midnight.
 - Eligible businesses must complete the application and payment process as outlined in this memo.
- Application process:
 - Application form will go live on the City's webpage on February 16, 2023.
 - Applications are due by March 6, 2023. Businesses may submit applications in advance of the deadline.
 - Fees are payable by check or card to Parking Services by March 13, 2023.
- Implementation:
 - The Parking Services division in the Department of Public Works will install and remove the orange jersey barriers at the beginning and end of the 2023 season.
 - City staff will fill the jersey barriers with water on the day they are installed. Participating businesses will be required to top off the barriers with water as needed, and the barriers must remain sufficiently full for safety reasons.
 - If the weather in April of 2023 is not conducive to outdoor dining, City staff may exercise discretion on the exact dates that the parklets are installed
 - If a parklet is removed, either by request of the business or by determination of City staff, it may not be reimplemented in the same calendar year. Any fees paid by the business will not be refunded
- Requirements for participating businesses:
 - Participating businesses are required to provide their own furniture, decorations, etc.

- Participating businesses must meet all local, state, and national legal requirements for their extended outdoor seating, including those from the Indiana Alcohol Tobacco Commission (ATC), state and county health department, and all Americans with Disabilities Act (ADA), safety and insurance requirements.
- Participating businesses must comply with Indiana Fire Code regulations relating to the use of heaters and tents, specifically Chapter 31, Section 3107.4.
 - Tents are allowed to be used on a temporary basis, with a maximum cumulative number of 30 days of operation in a calendar year
 - Heaters are allowed to operate but must be 10 feet from any combustible material, including tents, buildings, seating, awnings, etc.

Kirkwood Conversion Logistics:

- Eligibility
 - Eligibility is limited to any business or organization located in the participating blocks of Kirkwood Ave. This program is focused on, but not limited to, food service establishments.
 - All participating businesses must agree to cease alcohol sales in parklets and on Kirkwood by midnight.
- Application process:
 - Application form will go live on the City’s webpage on February 16, 2023.
 - Applications are due by March 6, 2023. Businesses may submit applications in advance of the deadline.
 - Fees are payable by check or card to Parking Services by March 13, 2023.
- Implementation logistics:
 - City staff will temporarily reconvert the participating 2.5 blocks of Kirkwood Ave. during the 2023 season. No permanent physical changes will be made to the streetscape.
 - All cross streets will remain open.
 - A fire lane will be left in the middle of the street as necessary, as determined by the Bloomington Fire Department.
 - Where the street is closed with a contiguous barrier (i.e. orange jersey barriers as opposed to bollards), there will be a bike lane so that bikes and scooters may retain a path despite Kirkwood being a dismount zone.
 - City staff have analyzed the quantity and location of ADA parking spaces in the downtown area and are in the process of determining where additional ADA spaces can be added, taking into consideration which blocks of Kirkwood will be closed to vehicular traffic in 2023. These additional ADA spaces will be implemented in the spring of 2023, with exact dates determined by weather.
 - Areas not occupied by businesses or the fire lane will have greater arts programming, such as live music, mural and plein air painting, performances, or festivals. The nature and timing of this programming will be determined by City staff, in conjunction with community stakeholders.
- Requirements for participating businesses:
 - Participating businesses are required to provide their own furniture, decorations, etc.
 - Participating businesses must meet all local, state, and national legal requirements for their extended outdoor seating, including those from the ATC, state and county health department, and all ADA, safety, and insurance requirements.
 - All businesses utilizing the extended outdoor space on Kirkwood will be required to have a temporary ADA-compliant ramp from the sidewalk into the section of the street they are utilizing.
 - Participating businesses must comply with Indiana Fire Code regulations relating to the use of heaters and tents, specifically Chapter 31, Section 3107.4.
 - Tents are allowed to be used on a temporary basis, with a maximum cumulative number of 30 days of operation in a calendar year
 - Heaters are allowed to operate but must be 10 feet from any combustible material, including tents, buildings, seating, awnings, etc.



To: City Council

Cc: John Hamilton, Mayor
Mary Catherine Carmichael, Deputy Mayor
Adam Wason, Director, Public Works
Larry Allen, Assistant City Attorney, Legal Department
Alex Crowley, Director, Economic & Sustainable Development
Michelle Wahl, Parking Services Director, Public Works
Andrew Cibor, Director, Engineering

From: Chaz Mottinger, Special Projects Manager, Economic & Sustainable Development

Date: February 6, 2023

Re: Expanded Outdoor Dining Program: Parklets and the Continued Conversion of Kirkwood Avenue in 2023

Executive Summary

In response to the COVID-19 pandemic, the City of Bloomington Common Council approved Ordinance 20-11, which suspended portions of the municipal code to support the operation of businesses in downtown Bloomington. The Common Council extended this ordinance and approved the expansion of outdoor seating through the end of 2021. In Ordinance 22-01, the Common Council approved the Expanded Outdoor Dining Program (“Program”), which extended the ability for local restaurants to utilize parklets and enhanced pedestrian seating space on Kirkwood Avenue. Council reserved the right to extend the Program and terms of Ordinance 22-01 by authorizing resolution. Currently, the Indiana Alcohol and Tobacco Commission continues to allow operations in this capacity.

Due to increased economic benefits, sustainability, and vibrancy in our community, City Staff recommends asking City Council to pass a 2023 resolution to continue the Program as set out in Ordinance 22-01 with certain changes. The proposal below outlines how the City can continue to support this seasonal program in 2023 with modifications. Data and feedback from stakeholders is included in this memo.

Overview

Based on stakeholder input, we recommend asking the Council to allow for the reimplementation of the parklet and temporary Kirkwood conversion programs in a seasonal expanded program from April 3, 2023– October 1, 2023. The blocks of Kirkwood included in the Kirkwood conversion program for 2023 will stay the same with an optional change from 2022. These blocks we recommend to close are:

- Full block between Dunn St. and Grant St.
- West half of the block from Washington St. to Walnut St. (alley will remain open)
- Full block between Indiana Ave. and Dunn St. (OPTIONAL)

The proposed resolution would allow the continuation of the expanded outdoor dining program for purposes of continued economic and public health recovery from the COVID-19 pandemic in addition to creating more vibrancy in our pedestrian-friendly community. The 2023 expanded outdoor dining program would again require participating businesses to pay fees of \$500–\$3,500 depending on the specific program and size of business as outlined in Exhibit A: Program Guidelines - see exhibit for logistical details of the outdoor dining programs.

Adjustments:

- Shorten the season (eliminating March and October)
- (OPTIONAL) Remove full block between Indiana Ave and Dunn St

Timeline:

- Council vote on this ordinance: Feb. 15, 2023
- Applications available to businesses: Feb 16, 2023
- Deadline for submitting applications: March 6, 2023
- Final staff determination of number and location of parklets: March 13, 2023
- Implementation of outdoor dining program: April 3-5, 2023 (*weather permitting*)
- End of seasonal outdoor dining program: Oct. 1, 2023

Fiscal Impact Analysis

City staff modeled the financial impact of the Program based on last year's participation. We estimate that meters would ordinarily operate for 151 days excluding Sundays and holidays during the 2023 Program. For the 2022 parklet program, 8 businesses used 16 parking spaces and generated \$20,000.00 in revenue. For the 2022 Kirkwood conversion, which used 2.5 blocks consisting of 50 parking spaces, 6 businesses participated and generated \$18,750 in revenue. The total revenue from participating businesses was \$38,750.

Meters generate a maximum of \$13 a day from each parking space. Assuming maximum usage, the opportunity costs for parking meters would be \$31,408 from parklets and \$98,150 from Kirkwood conversion for a total of \$129,558.*

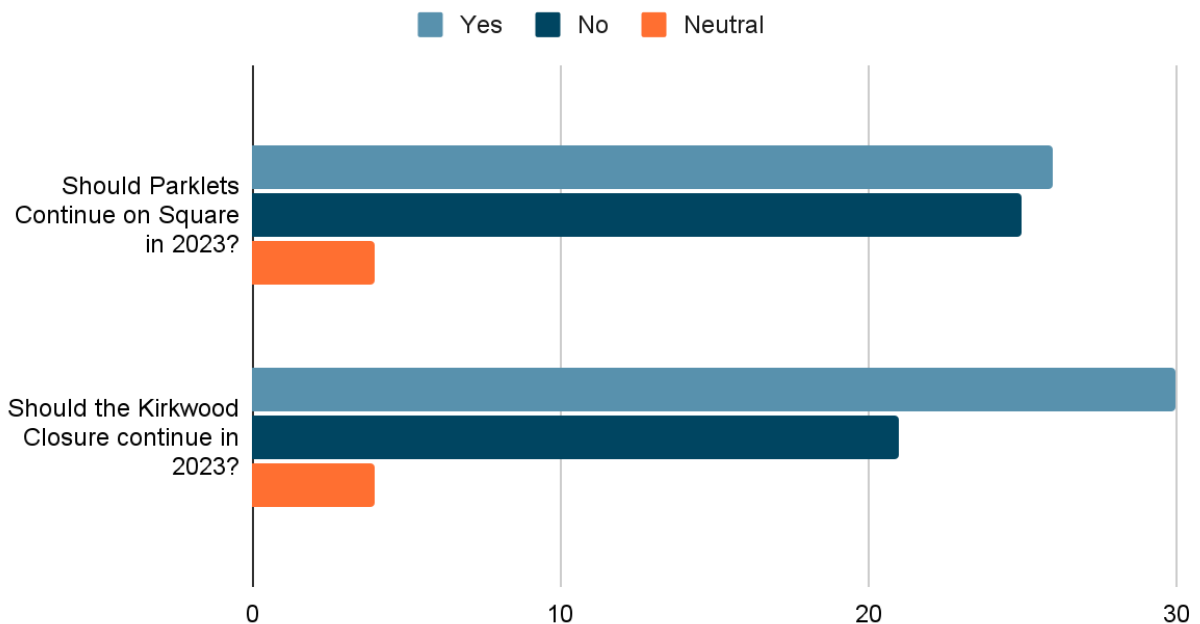
If participation and closure areas remained the same, the overall fiscal impact of the City's partnership with participating businesses would total \$90,808* for the 2023 Expanded Outdoor Dining Program. This amount represents the estimated public investment to promote sustainability, vibrancy and economic development in our downtown community.

Please note the 2022 Program ran for a longer timeframe than the proposed 2023 Program.

*At the time of this memo, we do not have specific data for metered parking revenue loss from Parkmobile or IPS yet.

Feedback and Data

DBI Survey



Feedback from DBI Members

- Suggests shortening the season
- Points out some parklet businesses using a parking space also have sidewalk dining
- “The Square is a big visible space with different types of customers and various reasons for coming downtown - offices, retailers, therapists, residents, restaurants - its use is a little different than Kirkwood. Sharing parking spaces for multiple purposes and turnovers

a day is an important aspect of downtown operations. Customers pop in and out for meetings with their accountant, attorney or therapist, they have destination errands to pick up one thing, it functions differently than Kirkwood. If we want to support all small businesses and want downtown to be welcoming, efficient and convenient for community residents, customers and employees -- shared parking for different reasons is important.”

- Trinity Episcopal Church issued a letter suggesting a half block closure to alleviate issues with the alley for their parishioners.

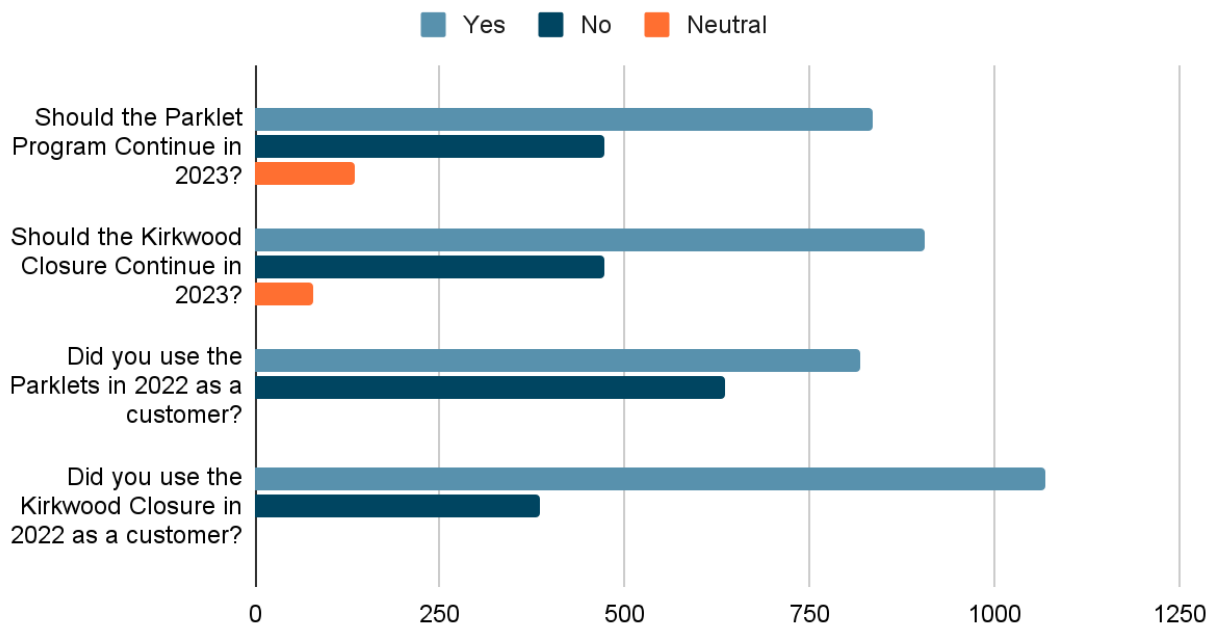
Positives:

- Helps brings sales in
- Enjoyable/adds to vibrancy of downtown
- Less traffic congestion on Kirkwood
- KCA wants the closure

Negatives:

- Parking becomes an issue both on Kirkwood and on Square
- Unfair to some businesses
- Dangerous with traffic
- Doesn't look pretty (parklets)
- Only beneficial during COVID

Polco Public Survey



Feedback from Public

Some wanted more permanence to programs and felt that would help better programs
Some folks did not understand the program guidelines - need better marketing

Positives:

- At least 87% liked the ability to be served in fresh air
- At least 72% said it adds vibrancy to the downtown experience
- At least 64% said they liked the expanded opportunities for customers
- Creates sense of community / feels more welcoming / festive / user-friendly
- Lots of comments about loving the street closure/wanting a Kirkwood pedestrian plaza
- Spent more money downtown
- Better for pedestrian foot traffic

Negatives:

- At least 45% said they dislike the loss of on-street parking
- At least 31% said the parklet appearance is not appealing
- Traffic and transit disruption / inconvenience
- ADA concerns
- Adjacent traffic to parklets is loud / unsafe
- Does not benefit all businesses impacted
- Homelessness issue

Feedback from 2023 Parklet Participants

Benefits:

- Increased sales and visibility / Attracted customers
- Gave outdoor seating options / Easy and affordable way to expand outdoor seating
- Kept business floating during continuing challenges of pandemic
- Allowed better competition with the restaurants on Kirkwood

Disadvantages

- Cleanliness
- Water barricades maintenance
- Graffiti
- Traffic

2022 Business	2023 Participation	Reason
Smokeworks	YES	Increased sales/visibility / easy and affordable
The Tap	YES	Increased sales/visibility / easy and affordable

Small Favors	NO	Biz too new and too small to utilize parklet / street uneven and not convenient to their set up
The Orbit Room	YES	Kept business afloat, expanded seating and ADA compliance
BuffaLouie's	YES	Expanded outdoor seating options / competitive
The Bishop	NO	Potential dangers on Walnut corner / cost and demand running contrary; 11pm limitation not great for a bar

Conclusion

We acknowledge there can be natural tensions between non-restaurant and restaurant businesses when closures/parklets are concerned and the loss of parking revenue to the City. On balance, the above recommendations attempt to promote vibrancy, sustainability, as well as economic benefits. Recalling that the program was initiated as a temporary measure, the recommendation strives to create predictability for the future, while continuing to gather feedback and to improve implementation through this seasonal program.

Exhibit A: Program Guidelines

Costs to Participating Businesses (note: no change to 2023 fee structures):

- All:
 - All extended outdoor dining will be subject to the \$50 permitting fee to the Engineering Department.
 - All fees are due in full by March 20, 2023.
 - Businesses are responsible for any direct costs associated with utilizing a parklet or outdoor dining on Kirkwood, such as furniture or heaters.
- Parklets:
 - Cost to businesses will be \$1,250 per parking space for the 2023 season, payable to Parking Services. A two-space parklet will cost \$2,500 plus the permitting fee(s).
 - Businesses which share a parklet may split the cost between them.
- Kirkwood conversion outdoor dining permit:
 - For businesses with a capacity of under 20, the cost will be \$500 for utilization of the extended outdoor space on Kirkwood during the 2023 season.
 - For businesses with a capacity between 20 and 100, the cost will be \$1,250 for utilization of the extended outdoor space on Kirkwood during the 2023 season.
 - For businesses with a capacity 100 and above, the cost will be \$3,500 for utilization of the extended outdoor space on Kirkwood during the 2023 season.

Parklets Logistics:

- Eligibility:
 - Eligibility is limited to any food service establishment adjacent to metered parking in downtown Bloomington.
 - All participating businesses must agree to cease alcohol sales in parklets by midnight.
 - Eligible businesses must complete the application and payment process as outlined in this memo.
- Application process:
 - Application form will go live on the City's webpage on February 16, 2023.
 - Applications are due by March 6, 2023. Businesses may submit applications in advance of the deadline.
 - Fees are payable by check or card to Parking Services by March 20, 2023.
- Implementation:
 - The Parking Services division in the Department of Public Works will install and remove the orange jersey barriers at the beginning and end of the 2023 season.
 - City staff will fill the jersey barriers with water on the day they are installed. Participating businesses will be required to top off the barriers with water as needed, and the barriers must remain sufficiently full for safety reasons.
 - If the weather in early April 2023 is not conducive to outdoor dining, City staff may exercise discretion on the exact dates that the parklets are installed.

- If a parklet is removed, either by request of the business or by determination of City staff, it may not be reimplemented in the same calendar year. Any fees paid by the business will not be refunded
- Requirements for participating businesses:
 - Participating businesses are required to provide their own furniture, decorations, etc.
 - Participating businesses are required to invest in beautification of parklets through art or decor that meets safety standards (see Exhibit B)
 - Participating businesses must meet all requirements for their extended outdoor seating, including from the Indiana Alcohol Tobacco Commission (ATC), Americans with Disabilities Act (ADA), Health Department, safety, and insurance requirements.
 - Participating businesses must comply with Indiana Fire Code regulations relating to the use of heaters and tents, specifically Chapter 31, Section 3107.4.
 - Tents are allowed to be used on a temporary basis, with a maximum cumulative number of 30 days of operation in a calendar year
 - Heaters are allowed to operate but must be 10 feet from any combustible material, including tents, buildings, seating, awnings, etc.

Kirkwood Conversion Logistics:

- Eligibility
 - Eligibility is limited to any business or organization located in the participating blocks of Kirkwood Ave. This program is focused on, but not limited to, food service establishments.
 - All participating businesses must agree to cease alcohol sales in parklets and on Kirkwood by midnight.
- Application process:
 - Application form will go live on the City's webpage on February 16, 2023.
 - Applications are due by March 6, 2023. Businesses may submit applications in advance of the deadline.
 - Fees are payable by check or card to Parking Services by March 20, 2023.
- Implementation logistics:
 - City staff will temporarily reconvert the participating 1.5 blocks of Kirkwood Ave. during the 2023 season. No permanent physical changes will be made to the streetscape.
 - All cross streets will remain open.
 - A fire lane will be left in the middle of the street as necessary, as determined by the Bloomington Fire Department.
 - Where the street is closed with a contiguous barrier (i.e. orange jersey barriers as opposed to bollards), there will be a bike lane so that bikes and scooters may retain a path despite Kirkwood being a dismount zone.

- City staff will make sure there are additional ADA parking spaces in the downtown area taking into consideration which blocks of Kirkwood will be closed to vehicular traffic in 2023.
- Areas not occupied by businesses or the fire lane will have greater arts programming, such as live music, mural and plein air painting, performances, or festivals. The nature and timing of this programming will be determined by City staff, in conjunction with community stakeholders.
- Requirements for participating businesses:
 - Participating businesses are required to provide their own furniture, decorations, etc.
 - Participating businesses must meet all requirements for their extended outdoor seating, including ATC, ADA, Health Department, safety, and insurance requirements.
 - All businesses utilizing the extended outdoor space on Kirkwood will be required to have an temporary ADA-compliant ramp from the sidewalk into the section of the street they are utilizing.
 - Participating businesses must comply with Indiana Fire Code regulations relating to the use of heaters and tents, specifically Chapter 31, Section 3107.4.
 - Tents are allowed to be used on a temporary basis, with a maximum cumulative number of 30 days of operation in a calendar year
 - Heaters are allowed to operate but must be 10 feet from any combustible material, including tents, buildings, seating, awnings, etc.

Exhibit B: Beautification and Parklet Art Guidelines

Due to parklets’ modular and malleable nature, there are several options available to parklet program participants for beautification. Examples of parklet beautification may include, but are not limited to the adornment of art, accessibility alterations, or cosmetic improvements, all per guidelines listed in “Exhibit B.” Given that some figures for professional parklet beautification may range in the thousands of dollars, it is crucial to employ the use of cost-friendly and easy-to-implement solutions as costs will be borne by businesses.

Beautification Options:

- Additional seating platforms
 - Outside of their uses associated with outdoor dining, parklet installations may include wooden or metal benches, platforms, ramps or additional forms of seating to compensate for increased customer capacity and general community interactions
- Adornment of art
 - Parklet program participants are encouraged to use a variety of art sources to beautify parklets
 - E.g., collaboration with local artists and the utilization of murals
 - Program participants must explicitly follow pertinent measures per “Exhibit B”

- Parklet greenery options
 - In keeping with the seasonal spirit of the parklet program, program participants are encouraged to employ a mix of temporary and permanent greenery installations
- Additional lighting
 - Both ensuring visibility and increasing parklet aesthetic, increasing lighting sources utilized within a parklet will ensure a welcoming aura for all
- Further cosmetic and logistical improvements
 - More options for cosmetic improvements include use of budget-friendly space dividers, use of outdoor rugs/flooring, temporary installations of menu/special boards, spaces dedicated for bike parking, and inclusion of interactive features
- Accessibility alterations (requirement if not ADA compliant)
 - To ensure inclusive accessibility, parklets can be made to include various features to increase overall accessibility
 - Possible additions include wooden ramps, accessible entry and flattened deck surface, while guaranteeing enough space for inclusive mobility

Parklet Art Guidelines:

Must get approval from ESD + Parking for art in parklet. To do so, provide a detailed description and mock up of art. Photos of the barrier before and after the art installation must be taken. If the City deems the barriers are damaged, the business must pay to replace.

To get approval, art must follow these guidelines:

- Art must not obscure the exterior orange of the barriers
- Barriers are City property and can't be damaged/drilled/nailed
- Art must not pose any visibility issues for stop lights/stop signs/pedestrians
- Art cannot be on top of barrier
- Adhesives, if approved for use, cannot be permanent or melt onto barrier or cause any other damage
- Art cannot impede/get in the way of filling barrier with water

Exhibit C: Data Collected



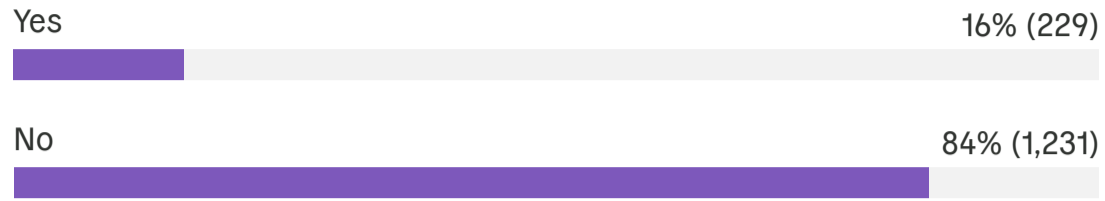


Kirkwood Parklet Feedback Survey

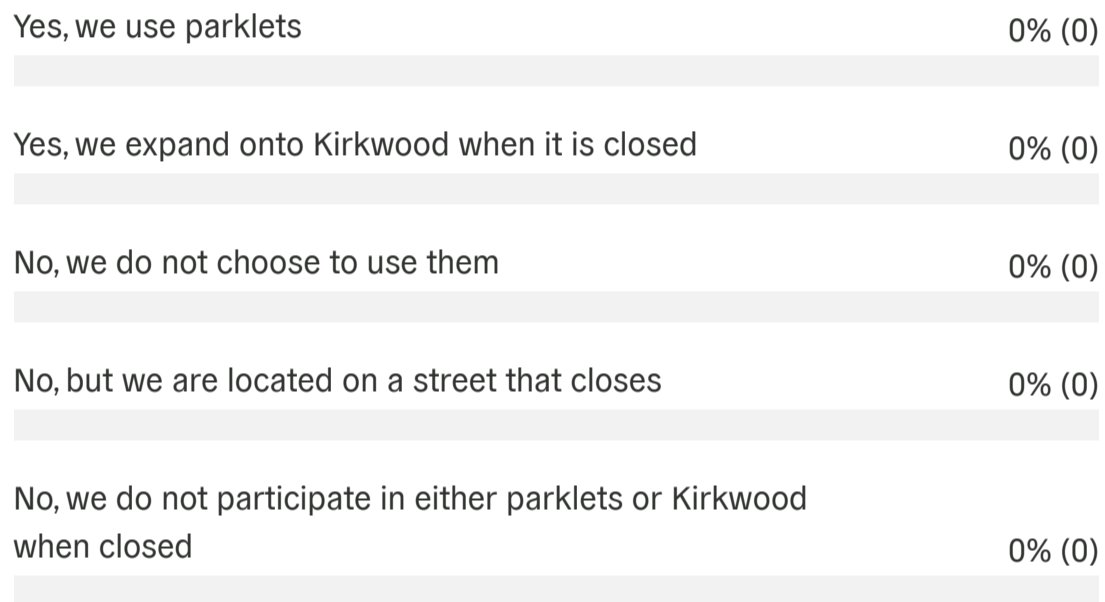
Survey Results
FINAL

01/13/2023

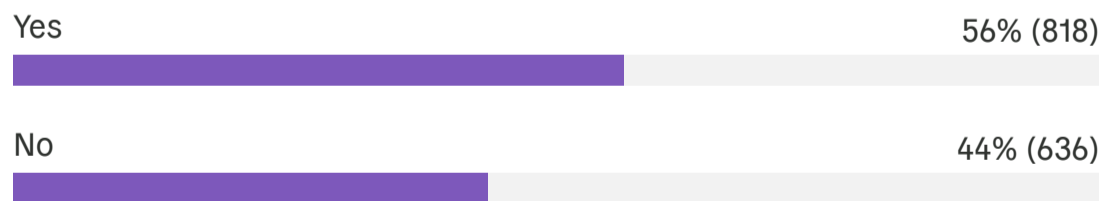
Are you a business operator or employee in the downtown area of Bloomington?



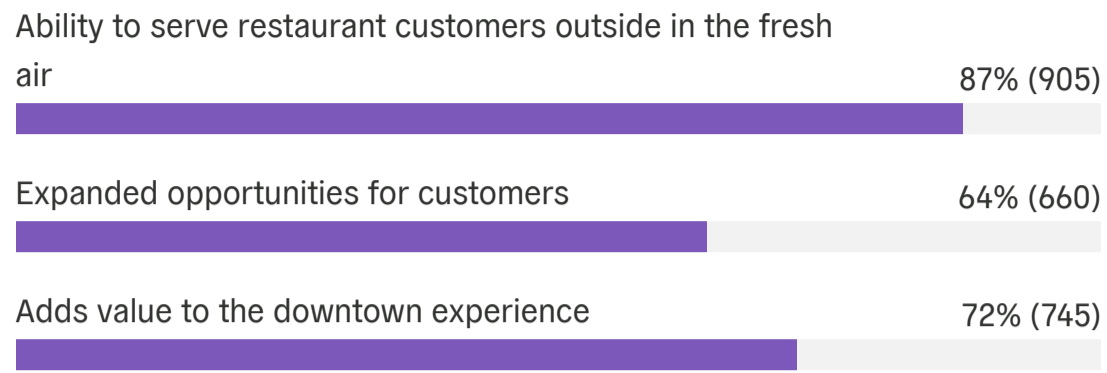
If yes, has your business participated in the seasonal parklets or street closures?



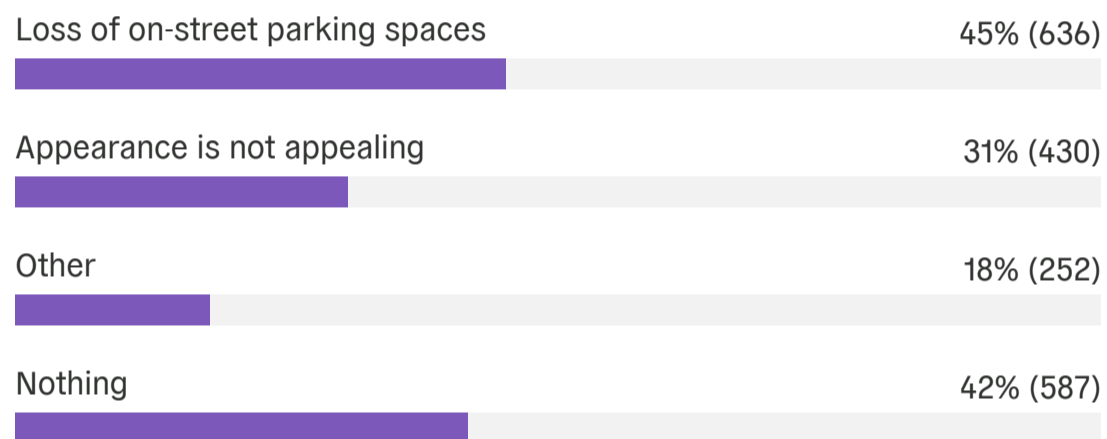
Did you use a parklet in 2022 as a customer?



What do you like about the parklet program?



What do you dislike about the parklet program?



If you answered "Other", please provide more insight here!

Harder for restaurants to serve outside - decline in service

Safety hazard

Driving access and access in general to businesses in the area

Eating a meal with cars, pedestrians, and 'unhoused' going by inches from me is not appealing in any way. Dirty!

Seems to favor restaurants at some possible costs to other retail.

It's unpleasant to eat next to fast moving traffic

No forethought was given to accessibility issues. The loss of parking means extra walking for elderly people and people with disabilities. Also the change in bus routes needed to accommodate the parklets affects autistic people or others who rely on a routine and familiarity of surroundings.

It was a little difficult this year just because of the Utilities projects that clocked soem cross streets for extended times making downtown travel less simple.

It has precluded any of the bus routes that used to use Kirkwood. While that's an inconvenience for me, I think the advantages for the restaurants are more important. Just wanted to note that that has been an issue (the lack of bus access).

Office those tables filled up quickly or required a reservation, so did it really add to the ability for more people to eat there?

Traffic weirdness

Perceived disparity in providing space for retail while sweeping Homeless off the street was painful

Traffic patterns are disrupted, which is particularly difficult with all of the construction in nearby streets.

It makes navigating around and through the center of town much harder. The function of streets particularly in the downtown area is to move traffic efficiently and thereby make the experience of shopping in that area (as opposed to other shopping areas) a more appealing option.

In the summer of 2021, the parklet program did not account for bicycles movement in front of Uptown and Farm forcing cyclists to ride on the sidewalk. This was remedied in 2022, but would like the note it so the problem can be avoided in the future.

Loss of close access along E.Kirkwood using transit

If we can make room for people to dine outside, we can make room for homeless people to exist outside! They are our neighbors and deserve to use public space as much as the rest of us do.

it reminds me of COVID

I almost selected "appearance is not appealing," but worried that would be misinterpreted. What I mean is that using orange jersey barriers as temporary measures *prevents* Bloomington restaurants from having truly lovely parklets like many cities around the U.S. have bought into in recent years. Providing stability and even semi-permanence to the program would allow restaurants to invest a few thousand dollars or more to make these spaces great (e.g., platforms built up to make the space level with the sidewalk; more enclosed, semi-walled structure to separate from the seat; pergola-type canopies; plants and flowers; charming lights; etc.). You don't need to look hard to find great examples all over that we've had a hard time replicating in Bloomington due to the temporary nature of the program.

Lack of HANDICAPPED parking. Also, ugly blue picnic tables are NOT a sidewalk cafe.

Makes it more difficult for delivery drivers to deliver product to businesses in those areas

Can't drive

I personally am not inconvenienced by the lost parking, but worry about folks with disabilities. Also not sure it's fair to downtown businesses NOT on Kirkwood, who don't get the opportunity for greater capacity. Huge fan of more outdoor dining/gathering space, though. Also, consider looking at Iowa City, IA downtown "ped mall" as an example of similar use of space. Theirs is built in, year round.

There's inherent danger eating next to moving traffic, and there's noise, air pollution, etc.

Their color made me assume construction was happening and not that it was a spot to park.

Traffic noises can be quite loud and fumeey

Make sure the streets are properly cleaned/swept, and washwater or other wastes aren't dumped into storm drains. The washwater is a concern - I've seen folks hosing down dirty tables and areas without any capture/treatment before entering drains (not the biggest concern compared to alleys and other discharges, but still...). Ensure parklets aren't blocking gutters/drainage.

Difficulty in accessing businesses for differently-abled persons. Even drop-off doesn't get close enough to help much.

more difficult for people with disabilities to get from car or BTaccess/Rural Transit to door of business.

Makes traffic more congested and confusing

It makes anyone with accessibility issues unable to access most restaurants in Kirkwood, also it makes carryout very difficult from the Kirkwood restaurants.

They are ugly and aren't safe from vehicles

Harder to get to other areas down and along kirkwood and in between. I hate having to go out of my way to get to these locations.

Limits accessibility

Road closures

Not a program that is fair to everyone

The parkLet at the Tap felt a little unsafe if your back was to the street. Cars flew by there if they had a green light

Inconvenience to pedestrian traffic.

Difficulty of driving through downtown (especially when 4th St was also closed)

Puts people in the "open air" while traffic and exhaust is going by

Closes off access east and west. When going all the way around (which is a pain because of 1-way streets), roads get clogged w/delivery trucks.

Uncomfortable to sit so close to traffic.

What do you plan to do about handicapped accessibility? I am mobility impaired, not wheelchair bound. I cannot walk an entire block to and from my car everytime I want to go to Kirkwood. Or, is it true that you figure that there are so few of us, we don't matter. That is what it looks like.

Lack of reasonable parking options and loss of on-street spaces makes it difficult for those with disabilities or mobility issues to access downtown attractions at the same rate as others.

Difficulty for those with mobility issues.

Space misused by the homeless. They sit on top of the tables. Makes me not want to sit there

Diners are too close to moving traffic; feels hazardous as a customer to sit in these spaces.

Problem for drivers limiting through streets. Problem For handicapped to be dropped off.

Lack of equity among downtown business owners - especially those of us who the need the seating more more than Kirkwood businesses.

Disruption of essential traffic patterns for vehicles.

I prefer to order my food online and pickup at the restaurant. Parklets serve the restaurants and people visiting downtown and/or Bloomington but is very inconvenient for drivers for hire, delivery services and their customers. Which operate beyond restaurants peek hours.

Difficulties for people with disabilities to access local establishments

Allows drunks to walk the streets

Disruption of buses, delivery, trash pickup, and commerce in general. It's hurt business.

I didn't know what streets would be closed and when.

Terrible! Handicap cannot even go downtown snd park close. Its really a nuisance and an accident waiting to happen. !!

Businesses should not have to pay for this, or tables or anything. The city needs to cover all parklet expenses including dining tables.

Bloomington streets are largely just not well-capable of supporting such a program.

it messes up the traffic flow, makes it harder to get to stores in areas where streets are closed off. You have to take detours. It caters to the people who have big bucks to spend at the Uptown or Farm Bloomington.

Parklets are the most ridiculous, unsafe, totally unnecessary, in my opinion ILLEGAL creations I have ever seen this city do! You can't sit customers on a street next to traffic that is still flowing! No barricade will protect anybody from tragedy in that circumstance! Get rid of it before everybody gets sued.

Closure of a major east west street though town

Loss of fast EMS and FIRE services

You take away access the city bus provides to the library. Blocking off the streets serves only people who have money and leaves the "poor people" to have limited access to downtown. Not to mention those with mobility issues who struggle MORE to get accessible parking close to were they need to go.

The one on College outside Inkwel feels slightly unsafe, cars just whiz by.

Emergency vehicles and traffic with forcing delivery trucks onto Dunn only is dangerous. Seriously, what would happen if an emergency vehicle needed to get to a patron? They park around the corner and run back and forth?

Safety concerns next to streets with high speed traffic

Liability outweighs any benefit, it's dangerous and unnecessary.

Fast driving cars within feet from where you're sitting. (The Tap/Smokeworks area in particular)

It benefits a small handful of restaurants and bars that take advantage of the seating at the expense of hurting all of the retail businesses both on

Kirkwood and the rest of the square. It disrupts traffic flow as well as parking.

I would not sit in parley, noisy and right on the road.

Road closures

Should be permanent so restaurants can invest in comfortable seating

City sponsored space for private business

Accessibility for handicapped employees and business patrons

Ñò

Hinders other local businesses who don't get to use Parklets. Seems to only service a few handpicked restaurants. Also creates even more parking and downtown flow problems.

It would be different if it weren't super costly for the business owner. It's tacky. It's semi dangerous. It's greedy.

Sometimes need more of a barrier between seating and the cartway

It was very difficult to get to other businesses on Kirkwood.

Makes travel downtown even harder than it already is.

Inconvenient when navigating downtown roads, makes those businesses less accessible to handicapped individuals

Business should not be charged for use

Not enough of them

Felt like I was going to get hit while dining in a parklet. Didn't feel safe.

I worry about ADA concerns and Emergency access.

Parklet spaces are often unused and wasted space for pedestrians and drivers.

Sometimes it feels unsafe as cars go whizzing by

Nasty exposure to people walking by and the tables aren't level.

If you have to do maintenance work on building absolutely no parking. To carry tools or a ladder is a pain. People park and block off alley ways because they are aggravated trying to deliver to places. Open it back up it helps alot of works and delivery people.

I pay road taxes so that all citizens can drive cars on roads. You are serving a very small population by blocking those roads and you have no right to do it. You're giving special treatment to very few businesses. That is not fair to us taxpayers paying for those roads and we expect to be able to drive on them.

Remembering that I can't turn right or left to through to cross streets.

Most not ADA accessible

Unsafe to sit so close to cars, what if one swerves? And so noisy to sit there, awful idea

Interfering with multiple businesses to for the benefit of a couple of restaurants. Access difficulties for the elderly. Loss of parking. I *stopped* shopping downtown because of this debacle.

Driving down Kirkwood is an enjoyable experience that has been taken away. Students just gather in the middle of the road making it difficult to even walk through.

My husband is handicapped we don't go to restaurants during the street closures. Causes massive traffic priblenms.

What is the transparency of taxing citizens of Bloomington for streets they are not allowed to use during parts of the year? How do the restaurants contribute to paying for the roads that they are using? Two restaurants have to pay to occupy these parts of kirkwood?

Parklets are one additional distraction for all road and sidewalk users

Buses no longer go down Kirkwood which limited the access to the library. This was very challenging for bus riders who are elderly or disabled.

Disruption of traffic flow.

As a handicapped senior, this makes accessing businesses much more difficult.

Dangerous, too close to traffic.

Hard to find parking with disability

Extremely dangerous as a patron. I tried and then once I sat I chose not.

Unsafe

I work as a delivery driver for several restaurants on Kirkwood. It makes my job nearly impossible to complete in a timely fashion with the parking restrictions removing access to the restaurants.

It was already hard enough to park reasonably close to a restaurant to do a quick pick up but now I don't even really patronize establishments on Kirkwood when it is closed to through traffic. This town was not made for this many people & this much traffic so with the "parklets" and all of the delivery services & Uber/Lyft, etc double parking & blocking driving lanes, it gets hard to drive downtown and even dangerous when people refuse to courteously wait and merge into the flowing traffic and they just dart into another lane whether it's really safe to do so or not. And NO, not everyone can ride a bike for their transportation needs.

It is very difficult if not impossible for someone with mobility issues. It is certainly not accessible.

Detours to get where I'm going

Makes businesses including BCT inaccessible to those who are disabled and who need to be dropped off at front entrance to business or venue. (My husband and I have not been able to access any businesses for Buskirk-Chumley Theater, ever since this went into affect.

Shutting down street. Limiting folks that can't walk far from getting to desired restaurants.

I'm disabled and use ADA parking. It's much harder for me to utilize restaurants on Kirkwood and much harder to get dropped off places.

Loss of being able to drive down the street

It is difficult to navigate the streets with all the one ways coupled with the road closures.

Loss of accessibility for those that are not able to walk the distance down the street and can't find nearby parking on crowded days downtown.

Restricts traffic flow

Access to businesses is tricky & difficult. Especially during times when there is road construction going on. It can be almost impossible to get to a place such as the public library. The library is such an important part of our community, it's a shame to reduce people's ability to get to it. What about folks with disabilities, how are they supposed to get to those businesses along Kirkwood when parts of the streets are closed.

Like to be able to drive down Kirkwood and not have road blocked! Might decide to stop if you can drive by and be enticed.

Make it permanent, nice looking structures - the construction come orange lewk isn't cute.

Over crowding on Kirkwood

It makes it difficult to get around downtown.

As a delivery driver year round, full time, it is WAY too difficult to find any parking, let alone decent parking. I've also talked to a few business and they've discussed how the closed street isn't conducive to disabled people.

Makes it too difficult for mobility impaired and elderly to access some of their favorite venues.

It is totally not accessible for the citizens who are disabled. The number of disabled spots are very small in number and this limits even more.

It's loud and very close to the road. Safety is questionable.

Inability for disabled individuals to park close and visit businesses

The streets need to be opened up for driving not for restaurants to use as their dining room.

You are catering to the worst drunk college students Locals do not use it Please go to Kirkwood on a Friday night at 1 am and see what you have done....

We try to avoid downtown during warmer months BECAUSE of these closures. So inconvenient!

There were some street closures for construction during the summer in addition to the kirkwood closure that made navigation in that area difficult.

There aren't enough east-west streets downtown that can comfortably handle traffic from Washington to Indiana. When Kirkwood is closed, it's just a waste of space.

Makes it hard to navigate to other restaurants. Also, dangerous. Also, air not that fresh by the road.

Dangerous

The number of times those spaces are used (usage is very low when looking at the time it's in use) is disappointing knowing these parklet's and closing of Kirkwood is a major mis-use of what the real need is. In addition is an eyesore for Bloomington and to us who own properties in the downtown. And the amount the is required for a permit is not appropriate should be in alignment with the loss of parking revenue. Get rid of them!

Did you give any thought to handicapped people? We wanted to go to the Raising Cain first day. We are 71 and 73. Many joints replaced. The closest we could park was 3 blocks away at a gas station. We were in extreme pain and lack of strength when we walked it. Had fun. Then I could not make it back to the car and gave out at 1 1/2 blocks. My husband used all of his strength to make it to the car and come back to pick me up. If you block off the much of an area, you should feel obligated to run a shuttle. Even a golf cart ride would have been great. We could not try this again.

Made it inaccessible if you are physically capable of walking/biking etc. also Dislike eating outside

Wrong to steal taxpayer funded portions of streets for private business!

Loss of parking means that people with mobility issues are shut out from restaurants and businesses

Streets closed. Very confusing and frustrating driving downtown. Makes it harder to access the library.

Removing the ability to drive to businesses on east Kirkwood.

It's a street and it needs to be opened up year round

As a service company it terrible for service. I can't park within two blocks of the business I'm to provide service to. And then the customer complains about the extra time charged looking a place to park and carrying everything in I need.

A street needs to be a street not an eatery!

Need downtown parking. Do away with bike lanes

With Kirkwood blocked off, I avoid shopping in this area. Too much walking and not ADA friendly.

LITERALLY MAKES MY LIFE A LIVING **** * CUT IT OUT

Causes traffic to be more congested at other intersections

I door dash and it's hard to get to many restaurants

Hard to navigate downtown in a vehicle.

Blocks off very important areas of access

Accessibility issues arise, plus making restaurants pay to be able to use the newly made space is extortion.

Adds to homeless look of our once beautiful city that our local government has ruined

Safety! I refuse to eat dinner 18 inches away from tourist drivers in downtown traffic. And I certainly hope that businesses fortunate enough to use community right-of-way in this manner are at least being charged the going rate for a downtown parking spot.

It has kept me from dining in those areas

I am handicapped and cannot walk far enough to eat any of the restaurants in that area due to lack of parking.

Limited access for vehicles

It only benefits a few and seems very unfair to other businesses

more shade would be great

Not all businesses, restaurants especially, have equal space or opportunity to use these spaces. Closure of Kirkwood and loss of parking is a distinct advantage to only a few businesses, which seems quite unfair.

The traffic is already terrible and no one knowing where there going downtown and this makes it worse

Older (Baby Boomers) can not enjoy the places they so happily frequented due to walking distances from parking.

An unnecessary nuisance when driving. Gas prices are high and you have to drive out of your way to get where you are going.

No driving access

IU employees coming from the west side have a hard time finding a route to campus bc all the streets are closed most of the time.

Not being able to drive easily around Kirkwood

Makes traffic in general more congested and inhibited those with physical disabilities from easily going to those restaurants and shops.

Hinders traffic in an already hard area to navigate

The streets that cross Kirkwood being closed made it difficult

Limits easy access to restaurants from a vehicle

Our tax dollars provided the sidewalks and pavement. Other businesses on the square do not reap from this proposal. I like the idea of outdoor seating but be fair.

The streets downtown are very busy, blocking them off makes it harder to get around.

Makes negotiating around downtown difficult.

I'm not sure exactly what "parklet" means. I'm presuming that it is the use of the closed street for outdoor restaurant service. As it was used this year it complicates motorized traffic. In a few cases it interfered with bicycle traffic.

Overly loud music at a couple of venues that undermines public's opportunity to enjoy dining at their selected venue. Please limit outside decibels.

It's too hard to navigate kirkwood

It needs to be open to traffic. PERIOD

The homeless cussing, threatening to kill people, throwing beer cans, laying on the tables, throwing food, drunk, vomiting. I had several meals ruined. Tables were pretty gross by the end of the warm weather. We stopped going to our favorite restaurant to sit outside as more and more homeless were on kirkwood.

The road should be driveable by car... at least in one direction.

Increased difficulty navigating downtown, especially with current construction

Hard enough to navigate without thus main thoroughfare being closed. How do first responders get thru. If restaurants want outside dining they should pay for the ground and not use public streets.

Can't drive on the street

Stops the flow of traffic

safety

I think there a road use it as a road

Makes driving downtown so much more confusing, makes me not want to eat downtown at all just to avoid it

Exposure to cigarette smoke

It takes up parking that could be used to go into several businesses downtown instead of just the restaurants

I do think it's a safety issue.

Dangerous being so close to traffic along Walnut and college

Felt dangerous when cars would fly down college or walnut

Lack of disability access and spaces

Emergency services are hindered access and response times

It's dangerous! I'm surprised no one has been hit by an impaired driver.

It creates a logistic problem for business suppliers and creates a safety problem for emergency vehicles to be able to get to their destinations in a timely manner.

The amount of heroin users in this town, I'm afraid one will drive right through there and hurt people. Had one total my parked car one block away while zoned out on drugs while driving.

Loss of commerce and negative impact on budget owners.

Congestion

It just made the whole downtown dining area to crowded and dangerous at times Please don't close the road next year

Unable to use the roadway for it's intended purpose. To DRIVE on.

This is a mess, and one of the reasons we have not visited that area for a couple years.

You libtards are idiots. Take you poison jabs and poison booters and go away.

Disrupted traffic flow.

It is inappropriate for the City to subsidize selected businesses. There is no comparable subsidy to hundreds of other businesses. The parklets and

Kirkwood closure benefit a few restaurants at the expense of other restaurants and businesses.

Its a huge struggle to get around downtown because of one-ways, you have to zigzag many different blocks to get to Kirkwood area businesses.

This gives private businesses advantages simply due to location and gives them public spaces to use to gain additional revenue. Public space should not be given to private entities.

Reduces overall parking in the downtown area that is available for customers' use related to all businesses; makes traffic more challenging; discourages downtown traffic except to maybe go to the places that have the privilege; if running the program charge a large amount of money comparable to inside square footage rent pricing.

Makes Kirkwood look like a construction zone. Not fair to all merchants who have business there. Not "quaint" looking at all.

Road accessibility

Not a good idea

Keeps larger families from being able to find adequate parking and utilizing businesses downtown.

It is harder to get around and discourages me from going to the businesses on Kirkwood

Cannot drive street and emergency vehicles have no access!!

It's a street with parking spaces that are lost we need more parking as it is.

Takes away from the downtown experience. I definitely have no desire to shop that end of Kirkwood which is now very congested and especially at night, unsafe to be. I personally feel like you are catering to a smaller group of people. I'm in favor of opening Kirkwood up and get the traffic flowing again.

Hard to carry any kind of material such as stuff we work with in construction an delivery

Conversion of public property to private gain, at the expense of the public.

I'm handicapped the street closing makes it difficult to easily get to dining and shopping locations

Makes driving around down town a lot harder

Being able to drive close to the businesses I wish to go to.

Coupled with the extensive, ongoing, and poorly thought-out street construction, it makes navigating the entire area virtually impossible.

They are only benefitting a few restaurants and are HURTING and TAKING AWAY business from all of the other restaurants and businesses in the area. The parklets are giving an unfair advantage to about 2 restaurants and taking away parking spaces from those who want to visit all of the other wonderful shops and restaurants in the Kirkwood/Walnut area. The space given to two restaurants is completely unfair and is costing everyone around them who is working equally hard to provide a stable business in the downtown area

Hurts other business

discriminates against business over benefiting other select business

Provides special access to some businesses and not others - would highly impact leasability and value of certain properties over others

People with disabilities can have a tougher time accessing storefronts.

travel space left for pedestrians and bicycles is too narrow on some blocks, especially between walnut and washington.

traffic backups

Loss of loading/unloading space on street front

safety...parklets are located on well trafficked roads

lack of offset accommodations for those with limited mobility

Safety - Did not like having people eating so close to the road.

Felt unsafe to be sitting so close to traffic on College Ave

I didn't like that Kirkwood was closed almost completely. The restaurants on Kirkwood should get a space or two but not the entire street.

Safety. I saw vehicles run into a couple barriers and would have injured anyone seated at a table. Also noise and depending on where the parklet is, vehicle emissions.

Makes it harder to get where I want to be downtown bc traffic is more gummed up

Lack of communication. The start and end dates need to be more accessible so that citizens are not surprised by street closures.

Disrupts traffic along with all the road work and building

As disabled it limits my access to other businesses on kirkwood especially cvs and library due to loss handicap parking

It takes up valuable parking space for non-restaurant businesses

Safety issues and liability cars crashing into customer while dining within parklets. Drunk college students jumping over them and hurting themselves.

Gives an unfair advantage to those restaurants who are able to participate.

Dangerous to have on busy streets

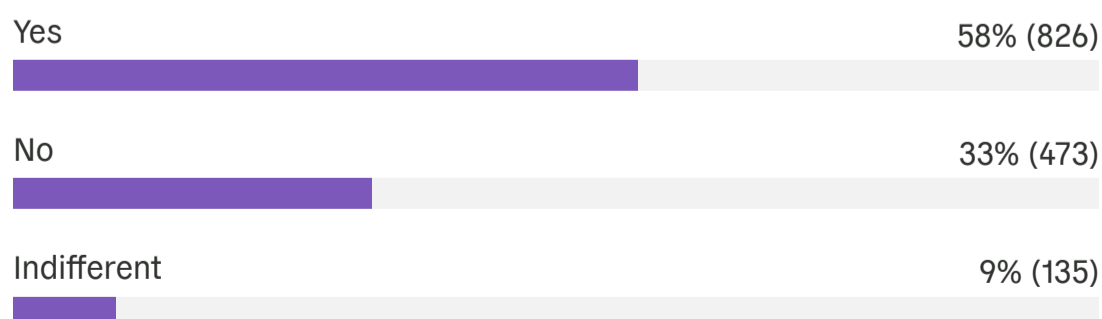
Limits access to Kirkwood businesses for elderly and handicapped individuals. And it's a nightmare for potential customers looking for parking. Not everyone in the world lives inside the Bloomington downtown area no matter how many mega apartments you allow and there just happens to be people that have to DRIVE into Bloomington and not everyone is a STUDENT. Get a clue.

Not always compliant with ADA

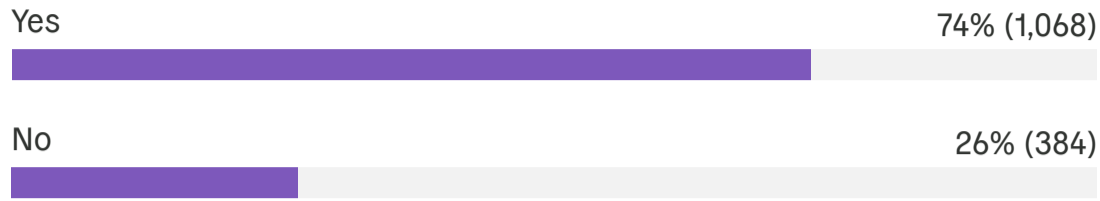
Space does not seem to be evenly applied to all restaurants

Does not benefit all the restaurants downtown, only a select few.

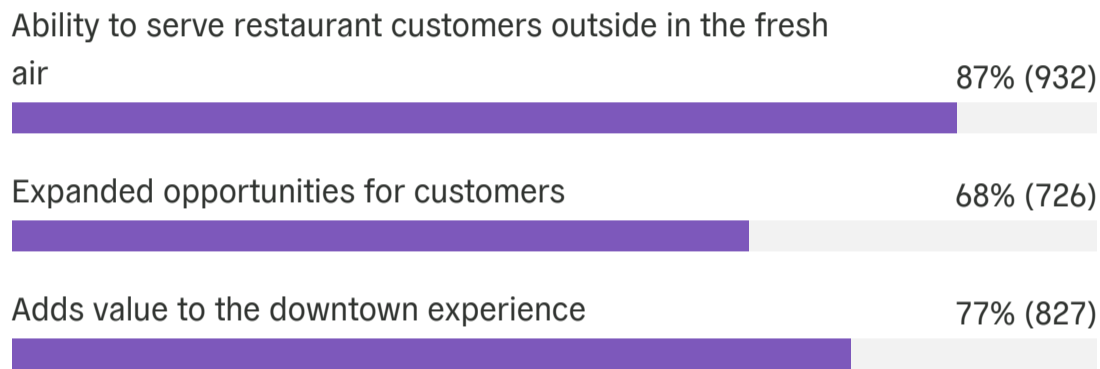
Would you like the parklet program to continue in 2023?



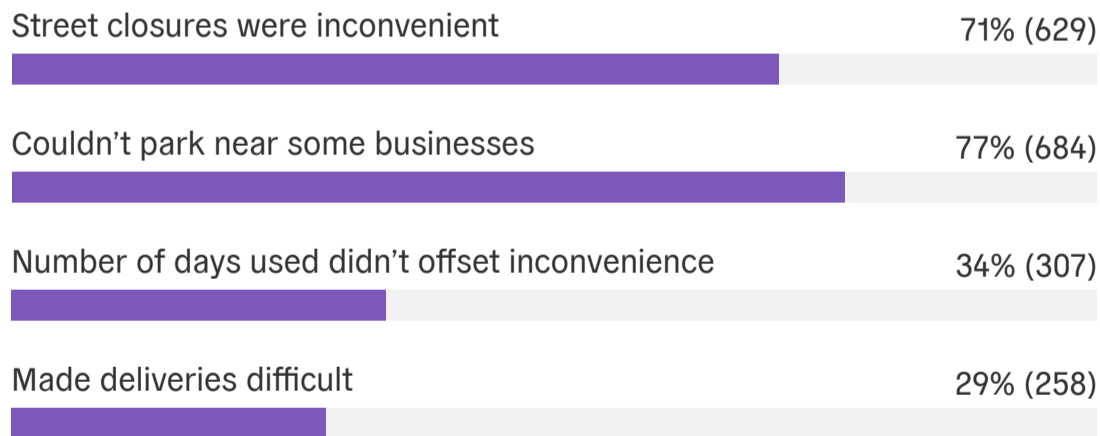
Did you take advantage of the Kirkwood closure as a customer or pedestrian in 2022?



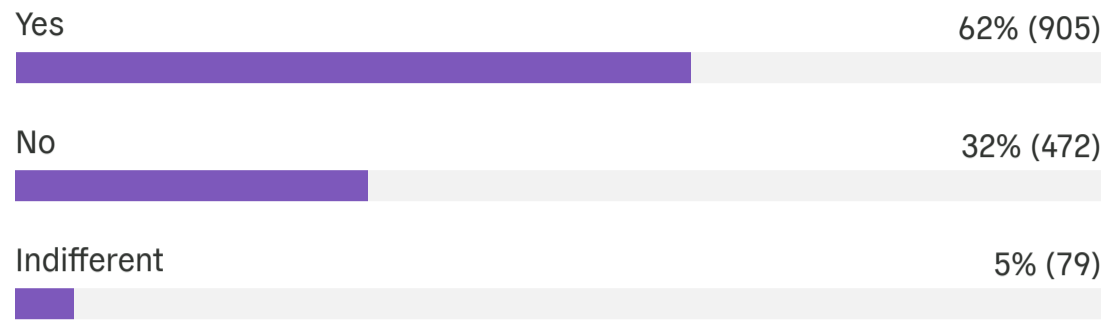
What did you like about the Kirkwood street closure?



What did you dislike about the Kirkwood street closure?



Would you like the Kirkwood street closure during the warm months to continue in 2023?



What else would you like to tell us about the parklet program or Kirkwood closures?

I love the outdoor dining on Kirkwood because it creates an even greater sense of community and togetherness during the warm months. It's such a great addition to the downtown dining scene and feels very European!

I'm very concerned about the number and aggression of the unhoused population in Bloomington. Crime and use of services - policing and healthcare are strained to breaking.

Bloomington Isn't big enough for that type of thing. Also it's uncomfortable and too hot and humid to eat outside most of the time.

Fantastic. Keep it up.

Just keep the road closed year-round!

It is inconvenient

Close Kirkwood permanently, brick over the street, BUT CLEAN IT REGULARLY.

If doing in 2023, shorten it to a couple months only

It was wonderful! Please don't let complaints from a few negative nancies take away this awesome change.

I appreciate that these setups allow spaces for some restaurants to have more casual/communal open-air dining spaces. They feel less formal and are opportunities to visit restaurants I might otherwise not visit (or not visit regularly) because the outdoor spaces feel more welcoming for an after-work or last minute drop-in for a meal.

I loved the festive community atmosphere that resulted most evenings and weekends on Kirkwood.

It's great that it adds outside seating

I work on Kirkwood every day of the week, and never experienced any significant increase in parking or traffic issues during the closure. It also increased my families patronage of Kirkwood on weekends. I have experienced traffic-related issues both as a pedestrian and a biker since the closure ended

My family purposefully visits restaurants where we can dine outside, both as a Covid precaution and also because we enjoy the ambience. We very rarely eat indoors during decent weather. The atmosphere on Kirkwood when it is pedestrian only is vastly more enjoyable, which leads us to visit more often.

It is a wonderful way to experience downtown as a pedestrian. There is plenty of parking in adjacent areas to access Kirkwood as a pedestrian way. This was one of the COVID silver linings that I would hate to see go away.

While the theory was good, one of the benefits of being so close to the IUB campus has been the parking issues. These closures only made that worse.

Close the street but I'm not sure if Park lets should continue. If the do, rethink traffic flow, and parking - include drop off areas- enforce delivery areas so the flow of traffic is not impeded and slow down traffic thru the area

I don't like the parklet program as much as the Kirkwood closures project, but I think out of fairness to the businesses not on Kirkwood who want to offer similar options for their guests they should be able to continue as well.

Kirkwood, from Walnut to Indiana Ave., needs to be closed permanently! There is absolutely no reason to let cars barge through that dense, destination-rich area. Kirkwood is Bloomington's "strip". It should be absolutely safe for pedestrians to walk around and enjoy without having to worry about cars potentially running them over.

I like the idea but in practice I avoided the area. Parking was difficult and simple carryout orders and shopping were far more complicated.

I don't go downtown anymore. Cannot take patients or clients with disabilities. No place to park. Keeps local people away.

My husband has cancer and we can only dine or meet friends outdoors. If Kirkwood is not closed, we will be unable to eat at restaurants or meet our friends. This will only increase our covid isolation.

I utilized the Kirkwood closures to dine at Uptown, Farm, and Lennie's. The outdoor space was attractive. I didn't use any parklet seating at other restaurants (even though I prefer outdoor dining and frequent many downtown restaurants). It didn't seem safe to sit there with traffic driving by so closely, and the big orange bumpers took away from the ambiance.

It is a fun idea, but with constant construction in downtown adjacent streets, and numerous one-way streets... the closures just added to frustration and traffic congestion.

Kirkwood closure was (perhaps) worthwhile during the pandemic. No longer needed for that purpose. Given the difficulty of east-west travel in Bloomington, including the problems on 7th street due to the 7-line, the street is needed for its primary purpose.

I really enjoy the Kirkwood closure! It creates a really fun atmosphere. It creates a destination to go eat and hang out.

As I see it, it's a no-brainer. Hundreds of people benefit from this: 1. It makes downtowns more vibrant because you SEE more people downtown. 2. It helps our businesses by bringing in more customers. 3. It's a trend that other cities we want to emulate have already fully adopted and validated empirically as a winning strategy.

Creates an energetic vibe for our downtown. The street closure makes pedestrian traffic much easier - more space.

Decision should be made by the owners/tenants whose businesses front Kirkwood whose livelihoods will be affected and the not general public

LOVE IT...spent more \$ downtown than would've otherwise. Felt safer to bring my family to enjoy downtown businesses because we often go by bike and like to sit outdoors.

I think it's great for customers and great for business! While it is somewhat inconvenient when driving, I think it adds value to the city. Like the walking mall in Boulder Colorado- I think the city should lean into this opportunity to make the positive outdoor vibes for pedestrians in Bloomington something we are known for

This could become an iconic pedestrian plaza. It's so much more lively when it is OPENED and free of cars!!

As I said in one of the above questions it makes navigating the downtown area much more inconvenient. Not only harder for people who know how it works, full time residents, but certainly confusing for any strangers or newcomers to town. As to Question 4: concerning what you like about the closures and parklets, the options offered: "expanded opportunities for customers" and "adds value to downtown experience" seem to be leading questions and there should be the option for the respondent to offer their own reflections

I would like to see Kirkwood closed to vehicle traffic permanently.

This is a great idea - it really makes this area feel more lively and community-oriented

I love the pedestrian-centric atmosphere. Being able to walk on Kirkwood stress-free, enjoy myself, and even listen to free concerts at People's Park every Thursday truly were the highlights of my summer. Please continue to close Kirkwood every summer from here on out.

LOVE having a street only for pedestrians/bikes and dining. We should have more of them in Bloomington! I would be very sorry if this program was discontinued.

The street closures made the street look more inviting and appealing creating a community atmosphere.

I would like to see permanent Kirkwood closure. It makes the downtown area much more "user-friendly".

I understand that businesses are important, but this city is made of PEOPLE, not businesses!

As a parent who frequents the downtown area, I love the sense of safety and space provided by the closures. They make the downtown area more accommodating to families. I feel like I belong and that my children get to actually be children. The space was created with parents in mind rather than centering on non-parents. Feels much more inclusive.

The Kirkwood closure creates a problem of reduced access for people with mobility limitations that the city and planning department in particular has openly dismissed and equated to laziness and people not wanting to walk.

The folks at Metalwork's did a brilliant job with their parklet, it was attractive and felt safer being raised above street level. Encouraging others to do something similar, if they can afford it, would be nice.

I think it's a nice balance between shutting Kirkwood down to make it a full pedestrian way and having it be just another downtown street. It was a real positive draw for me and my family, particularly once the 4th St garage reopened. Now that 4th St has also reopened, driving in the area will also be easier. We love going downtown and this made it feel so much more inviting.

Dig up the asphalt and make some green space!

Driving down/around Kirkwood is stressful anyway because of the high foot traffic and construction. It's so much more enjoyable of a space without worrying about vehicles.

It only benefited those who were rich enough to eat out so often. It make navigation, pleasure driving, and picking up from businesses very difficult as someone with mobility issues. I stopped frequenting some businesses because I wouldn't be able to walk to and from the business to where I had to park. It's awful for disabled people--even those with disabled parking permissions.

First, this is an economic no-brainer, providing a substantial stimulus and boost for our downtown businesses and great improvements to our public space and quality of life; it just takes leaning into the programs more to make them a sure thing (and allow/encourage investment by restaurants). Second, the idea of "lost revenue" from parking spaces utilized as public space is a myth; parking is simply displaced to somewhere else (the next street over, a parking garage, etc.). Consider this article from Strong Towns: "Toronto's Curbside Patios Made 49 Times More Money Than the Parking They Replaced" (<https://www.strongtowns.org/journal/2022/11/22/torontos-curbside-patios-made-49-times-more-money-than-the-parking-they-replaced>). "[R]esidents spent a total of \$181 million at curbside patios within 13 weeks of summer in 2021. If those spaces had remained dedicated to parking, only \$3.7 million would have been reaped during the same time period. In other words, curbside patios produced 49 times more revenue

than what would have been earned from parking fees." Note, even this article gets it wrong however; the \$3.7M that "would have been reaped" from those parking spaces didn't disappear, it was simply captured elsewhere in less utilized parking spaces. I.e., the 49x value proposition touted here is still *underselling* it.

Being able to freely move downtown as a pedestrian is a wonderful thing!

It made Bloomington feel like a more people centric town and reminded me of major cities with pedestrian zones.

There are a number of issues with the downtown parking. I understand reduced parking supports various sustainability initiatives, however Bloomington has a large commuting population from surrounding counties. These individuals are unable to find alternative transportation. In addition, Bloomington residents are not finding alternative transportation either, creating a large issue for parking. If downtown parking closures are to continue, there needs to be more attention towards the parking garages, in addition to a parking hub with a downtown shuttle. Loss of parking also affects individuals with disabilities and creates more barriers to them. The road closures should only occur on Saturdays and Sundays. Having loss of parking during the weekdays affects day-to-day business and creates a hassle that perhaps doesn't warrant the limited economic benefit during the weekdays.

Closing Kirkwood increases traffic congestion downtown. The city should stop treating transportation as a zero sum game and make improvements that reduce congestion and improve traffic flow. The automobile is not going to disappear. An example of a terrible change was reducing Walnut to two lanes south of 3rd St. This has had the effect of increasing congestion downtown due to drivers scrambling to move right out of the left lane.

Very inconvenient

I liked the ease in crossing as a PEDESTRIAN

Loss of parking means no business from many customers

I don't ever see a reason to drive on Kirkwood, it should stay optimized for pedestrian/bicycle use

Makes it hard to reroute, and not all restaurants benefit from this.

I think it has been a great addition to the city and would love to see it become a permanent fixture.

Closing Kirkwood is the best thing to happen downtown in decades. It'd be even better if it was closed entirely from Indiana to Walnut, no through streets.

Older patrons cannot walk the distances to go to restaurants and businesses and this adds to the parking spot pressure. Easier just to now go to other places surrounding Blgtn like Nashville, Bedford, Martinsville or Indy or outlying businesses away from downtown. Rarely go to downtown Blgtn if every for past 3-4 yrs.

The city should make the program permanent and build infrastructure to support Kirkwood being a pedestrian zone all year. Pedestrian areas of other cities are enormously popular and drive foot traffic and support businesses. Furthermore the Kirkwood street closure makes being on the street much safer. For example, my small children were able to bike and walk on Kirkwood without worrying about cars, which is impossible when the street is open. We should be making more safe and accessible areas for pedestrian use rather than prioritizing cars and parking.

I love all of it. It was confusing at first when driving but like all other changes I adjusted pretty quickly and now know where I can and can't drive on Kirkwood.

I hope there is not a fire or medical emergency in the closed down sections. I realize the poles can be removed, but they are extremely heavy, then you have the people to deal with to get an emergency vehicle to the area where needed.

Stop narrowing streets, someone is going to get hurt.

We enjoyed the outdoor dining many times and love the ambience of the seating on Kirkwood.

Driving down or through Kirkwood was always a HUGE pain due to pedestrians that don't care or are drunk. I feel it keeps drivers and pedestrians more safe and allows those pedestrians to properly enjoy the area.

I appreciate the expanded opportunities for downtown business. I did not enjoy having to re-route around Kirkwood closures.

Wish you would do something similar on the square. Close it to traffic. Make a park/walking street

All of Kirkwood should become a Pedestrian mall, from Walnut to Indiana.

Keep the closures all year!

The entire stretch of Kirkwood from Walnut east to Indiana Ave. should be permanently closed to vehicular traffic and turned into a pedestrian mall. Examples of this being done successfully include Miami Beach (Lincoln Rd. Mall), downtown Boulder, etc.

If it helps downtown businesses, particularly those owned locally, I'm all for it.

I avoided kirkwood often during that time because I was often unsure about how to navigate around without using that street. When I went as a pedestrian I enjoyed parts of the atmosphere, but the barricades remained me too much of construction.

Both promote a vibrant and exciting downtown.

Kirkwood closure was absolutely inconvenient. The city says they are "green" then closes the roads and makes so many one ways that you have to drive blocks to get one block over.

Having areas like this make our town seem friendlier, more accessible, and more pleasant. It's more calm and quiet to walk through these areas and enjoy the city. I also really appreciate having more options to eat outside as COVID and other illnesses continue, and I like eating outside in general.

It makes accessing businesses downtown difficult for people who are differently-abled.

I enjoyed the ability to walk on Kirkwood and I feel it is safer for bar goes to have that road closed. I do believe closure is a very positive change for the citizens of Bloomington.

Need for added parking in the area

Do. Not allow the homeless to lay along those areas, near customers who are eating

It was an eyesore

Na

The construction on Grant and 4th Street made the program much more challenging for both parking and navigation last year. Hopefully that will not be an added challenge this year.

Loved having the handmade market on Kirkwood in the summer

there's some minor conflict with restaurant operations crossing/blocking the sidewalk, especially at Village Deli

I don't even know what the parklet program is so apologies if my advice below would be solved by that program. i only go downtown to use the library and occasionally do carryout for food. would be nice if there was a way to park for 2 mins to pick up food. specifically cafe pizzeria was impossible to park near. i wasn't bothered by the closures for the most part. i think it makes kirkwood more inviting

This is a good way to encourage pedestrian traffic, and that's valuable.

I love being able to walk freely & dine outdoors on Kirkwood. Please keep the program!

All parking should be free! I would make more appointments with downtown business, but tend to avoid them because I don't know how long appointments will last and worry about how much \$ to put in. The first thing I look at is where business is located and only make appointment if I absolutely have no other choice! And I like the closer because of street vendors and you have more room to avoid vulgar comments from the homeless laying across the sidewalks!

I like how the lack of curbside parking encourages people to park in garages, walk or bike to Kirkwood, or take the bus (hopefully)

You exclude disabled people

I really dislike it when people park (temporarily, with their emergency flashers on) next to the barriers. Their vehicles stick out into the traffic lane and everyone has to go around them, which slows traffic down and makes some drivers very hostile.

I've wanted Bloomington to turn Kirkwood into a pedestrian mall for decades. This is the next best thing.

These programs enhance the downtown experience. They increase capacity at the restaurants allowing more to enjoy the food scene and are safer for those concerned about Covid or other airborne diseases. I understand the concerns of some, but the 4th street and other parking garages are cheap, accessible, and convenient.

It has definitely been a major pain to get to many restaurants with construction closing streets around kirkwood

It is discrimination against people with accessibility issues and creates too much seating for restaurants with small kitchens.

We need greater civic participation in Bloomington, and this extension into 2023 would help further that.

I would like for Kirkwood to be permanently closed

Actually found driving simpler downtown with Kirkwood closures ~ much less concern about drivers running through all way stop intersections. Definitely made visiting multiple businesses more appealing once parked~ walking around was much less stressful.

I'd love to see it expanded! Kirkwood could be entirely a parklet from April to October, allowing for street fairs and other visual arts. I'm no traffic expert, but it would seem that there are plenty of other ways to reach the businesses on Kirkwood.

I think the parklets themselves need to be improved aesthetically

Fabulous way for everyone to walk. I would suggest more Handicap parking close to Kirkwood during the closure of the street.

We really enjoy the opportunity to go downtown and eat outside and would love for it to continue.

More pedestrian focused downtowns are the future. I liked less focus on cars and more focus on pedestrians

I like having a safe gathering space for pedestrians. We have too many

spaces that are difficult to use because of high traffic, like cascades park

Absolutely made me not utilize businesses in area during closure.

You should consider doing it year round and making the whole street pedestrian/bike only.

It felt a bit strange and unsafe to sit so close to traffic speeding by (parklets). Would love a lower or enforced speed limit downtown—and not just because of the parklets. There are so many pedestrians, delivery trucks, scooters...slower traffic would be safer for all. Otherwise I think it is great, especially the Kirkwood closure.

I'll be the radical that says I wish we could shut down Kirkwood, down to the square, permanently. I work on Indiana Ave, and the traffic at the Sample Gates, both pedestrian and car, is hectic and dangerous all the time, but obviously esp. when IU is in session. I think it's wonderful to be able to just walk down Kirkwood to check out the restaurants and shops. I think the closure is great for events. I think there's so much that could be done with it. I know it makes traffic patterns different and not everyone is as big of a fan as I am, but I would certainly encourage keeping the program going.

Really causes traffic issues. I can't walk long distances

It's like 20° today. Even in the cold weather, I was reminiscing about other college towns i've been to that have been their main streets pedestrian only. I especially love it in the summer, but see value to it year round

Why are the businesses that benefit from this service not paying for it?

I think it should be utilized year-round, or only closed during the coldest months. Alternatively, making Kirkwood one-way would still allow some vehicle traffic, but allow businesses to continue the parklets. Perhaps some parking spaces can be permanently turned into rain gardens to add more visual appeal to the area.

Small motorcycles and mopeds should be allowed through, and to park within pedestrian areas.

Not all desirable shopping destinations and downtown attractions were restaurants. Church activity, library visitation, theater/music/comedy club venues, retail stores...there is much more to the downtown than restaurants, and with the closures there were some winners during the worst of COVID, but other downtown amenities did not benefit by restricting parking. Also, I am told by restaurants who struggle with getting and maintaining employees, downtown parking is getting too expensive for them to adequately recruit workers. Let's not limit parking and look to help our restaurants and retail stores by coming up with better, more affordable parking solutions for their low-wage employees.

I'm not sure what "parklet" means as opposed to street closures.

It's not handicap accessible in any convenient sense.

Do whatever you want, as long as it is a.) creative and clever, b.) truly in the best interest of citizens in the city, not just businesses and c.) you remember the handicapped and the Disabled Americans Act.

I think the Kirkwood closures are brilliant! It makes dining downtown very pleasant.

Kirkwood closures made the area a hectic mess. I have stopped going to the area entirely while the streets are closed. I originally thought that other than my concerns about access for older adults or persons with disabilities (myself included), it was a good idea to have more outdoor dining, since I really prefer to dine outside when weather permits. I did try to go in 2021, however it was difficult to access, confusing to try to get a table, and a homeless person was allowed to harass diners, so I did not try going back. Important: It also vastly increased the amount of delivery people/stopped

cars illegally blocking traffic and creating hazards for vehicles and pedestrians.

I haven't eaten in a restaurant since spring of 2020 and don't plan to do so in the foreseeable future. Having the option to eat outdoors allows me to continue supporting local Bloomington eating establishments.

More!

Love it. Otherwise I wouldn't go to downtown restaurants.

Make sections permanent; this would make it possible to make areas nicer vs dining on the street.

Keeps people from coming downtown since it limits parking to businesses

Its kind of a one-size-fits-all mandate since it applies to all businesses in the effected area. It may not be optimal for everyone.

Kirkwood closure made traveling with mobility impaired people more difficult.

The construction projects to the drainage system and the building of the apartments off third street are more of an inconvenience to traffic than closing Kirkwood.

There is no continuing emergency for Covid in Bloomington. Stop acting as though there is.

Allows more space for outdoor dining at many highly rated local restaurants.

I think Kirkwood should continue the closure year round

It is biased

Needs to be year round

Takes buses off Kirkwood, made downtown very confusing for visitors/customers, etc.

Close it permanently. Make kirkwood pedestrian only.

It should be expanded to year-round. Make it permanent!

It is terrible!!!

Consider permanently closing Kirkwood and creating permanent non-vehicle infrastructure.

Love the Kirkwood closure. Would maybe like to see it limited to weekday evenings and all day Sat-Sun (versus 24/7) but totally worth it either way.

If "parklets" are the parking spots like on College and Walnut transformed into outdoor seating separated by the orange barriers - I have used them and I like the idea in theory. But As a patron, sitting next to traffic isn't awesome. I prefer the street closures. All in all though, good idea and I'm proud of Bloomington for actually going through with it these options. I would not have dined out in 2021 without this outdoor seating - so win win win for me, the restaurant and its workers and for the city/county for less impact to retail and food/bev taxes during the pandemic. I think it is bringing more people downtown now in the nearly post-pandemic. How are the city and merchants working together to build upon the success of the program? What's the next level? Who is making plans to optimize opportunities? How can you make the atmosphere even more welcoming? How can you make the next-to-traffic spots seem less close to traffic?

It's wonderful for pedestrians and bikers to navigate Kirkwood with ease when cars are banned from that space. And it's better for our health to walk a couple of blocks.

I cannot stress you enough how important it is that you completely discontinue this Parklet program-that's not even a real word! Let's call it what it is - Dining death traps for stupid people who have no sense in their head! Is it really worth it? If you would like to be taken seriously as a city

government, then stop making ridiculous accommodations that violate basic common city ordinances and infrastructure. Stop allowing painting on ALL Streets while you're at it! Everybody had their fun, so can we go and correct all of the mess that you have allowed on the streets with paint, and turn those back into actual respectable safe roadways? That's not art-that's reckless endangerment! Why don't you go ahead and allow people to paint their messages, whatever they are, on your city or county buildings? That would be a more appropriate "canvas", don't you think?

Streets were closed.when.the restaurants were closed with no pedestrians. Inconvenience for no gain.

Access to the public library by public bus is wrecked by Kirkwood closure.

Make these changes permanent and year round

In question 10, you didn't provide the answer "nothing" like in question 5. I want the record to show there was nothing i disliked with the Kirkwood closure.

If businesses are going to receive services such as barricades they should pay for those items and services to fill.

Would like the closure to be made year round and allow for enclosed outdoor winter dining.

Really appreciate the opportunity to gather outdoors under umbrellas and awnings

I think it needs to be a permanent closure. That way we could build out the promenade like Burlington VT and so many other towns.

I wish this felt more like an organized on purpose program and less like a slap together solution for COVID. Also even filling this out I wasn't sure what parklet meant until reading the intro.

Kirkwood closure provides great connection from campus to edge of town

Close it year round, leave cross streets open.

I love listening to my fellow rural citizens complain about how the street closure is ruining their lives. Don't try to solve their problems- you will never succeed. They hate Bloomington and nothing will change that.

They make enjoying the businesses on Kirkwood much more enjoyable. Our family is much less likely to visit Kirkwood businesses when the street is closed.

Love the idea and hope to keep it

I absolutely love the parklets and closures. I believe Kirkwood should be closed to traffic permanently and, as many people in other cold areas do, we can learn to enjoy outdoor time in the winter, perhaps adding heaters if needed. Being outdoors improves both physical and mental health and we should prioritize that over gas-guzzling cars, idling with their blinkers on, blocking traffic as students run into places to grab take out. A closed-down Kirkwood invites the community to enjoy communal space. It give room for children to run and bikers to cruise and groups to leisurely stroll. So many other cities have turned their main 'drags' into pedestrian ways for the betterment of the community.

Q10 answer is confusing: "number of days used..." My answer there is that I found no inconvenience from closing Kirkwood, only benefits. It felt like a permanent festival all summer and I love it. The solutions to keep the north/south streets open is ingenious.

You didn't include this in you positive reasons, but the major reason our family uses outdoor seating is protection from Covid-19 which killed a million Americans and continues to do so.

Please, please, please start supporting local businesses in ways that actually benefit them! Kirkwood closed, 4th street closed for an art fair, and

construction all on the same weekend? Again, what if there had been an emergency??

With more intention, the Kirkwood closure could be made to look very nice! Restaurants and businesses like ourselves could invest in better infrastructure to make the dining nicer and easier for our staff.

Creates a great downtown dining experience.

0

It's essential to offer outdoor dining during these trying and contagious times. We can't all eat inside safely. People who won't walk to get to these restaurants because of road closure are lazy and inconsiderate to others. Period.

They are really unattractive!

Homeless folks hanging out sometimes get aggressive or cause a ruckus.

LOVE that Kirkwood is pedestrian! I wish it was that way all the time.

I liked it but not for that length of time.

The Kirkwood closure inconveniences everyone that drives/parks downtown to provide EXTRA seating for three businesses. Parking downtown is already challenging enough and closing one of the busiest streets only adds to traffic congestion!

Make it year round!

Kirkwood closure is SUCH A GREAT idea. Such a great experience, ties it all together and reduces traffic/pedestrian issues! LOVE IT. Parklets are fine, though in the evening feels unsafe with fast moving traffic near you

I feel the parklet program is unsafe. Having people sitting so close to moving traffic is going to end badly.

I would love it to be permanent like walking malls in downtown Boulder Colorado and Ithaca New York

I wish the City would close off Kirkwood sooner, when the weather starts to warm, in April and close later, maybe around Thanksgiving.

It benefits a small handful of restaurants and bars that take advantage of the seating at the expense of hurting all of the retail businesses both on Kirkwood and the rest of the square. It disrupts traffic flow as well as parking.

Makes it hard for elderly to visit the restaurants because of the walking distance it creates.

More vendors and restaurants ok kirkwood would be awesome! We love the closures and outdoor dining

I love not having traffic on Kirkwood!

Close Kirkwood the entire year. Make it like Burlington, VT. Pretty up the street - make it more of a destination.

Pretty it up. Check out Church Street in Burlington, VT or Pearl Street in Boulder, CO. We want that.

Need more parking btwn Indiana and walnut

Kirkwood should be a mall. Permanently closed.

If it was permanent, the businesses would make it prettier. We need that downtown.

I really enjoy it as an employee who works on Kirkwood. In the summer we open the patio on kirkwood and I love being able to work outside. I also rent on Kirkwood and having a pedestrian street in my front yard is why I chose to resign my lease.

The Kirkwood closures make the space feel more social. It also reduces unnecessary traffic and traffic emissions downtown. It also makes it safer

to be a pedestrian on Kirkwood.

This causes a great inconvenience to drivers in Bloomington and minimizes parking spots. It is also a waste of city resources (streets).

The barricades are UGLY. I think it works best when the entire street is blocked- like on Kirkwood. That way it feels like a pedestrian mall (a bit European).

This is a public subsidizing of a few private businesses that does not benefit everyone

It creates a park-like energy with shopping and dining that is unattainable in other area of Bloomington.

The benefits DO NOT outweigh the disadvantages!

It only benefits a few businesses

Please don't take this from me

Ñó

Makes driving downtown a nightmare. The closure coupled with delivery's, taxi services, etc. creates inadequate flow for the downtown area

Love it! Keep it! I work downtown and think it's great!

Outdoor dining adds to the positive vibe and culture of Bloomington!

It's a dis service to business owners to have to pay for outside seating. It's hard for emergency staff. It is too close for comfort to all of the drug addicted homeless people wandering around begging for money and drugs. It's a very unwelcoming scene to be sitting outside on a beautiful day with your family and possibly family or students visiting from out of town and be panhandled by mentally ill, drug addicted, homeless people. It's sad. It's not ok. It needs to be addressed. I used to love Bloomington. Now I can barely stomach even coming downtown.

Clogs up traffic even more. With all the development in Bloomington, we need more thruways not less. Might have worked 20 years ago, but too much growth at this point. Thank God Kirkwood wasn't filled with outdoor diners when the flash flood turned it into a river a few years ago! I just don't think it's safe for many reasons! But mostly overdevelopment and poor, aging infrastructure..

It makes it very difficult for anyone with mobility issues- to eat 'at the Kirkwood restaurants when Kirkwood is closed. It's also difficult for restaurants to serve outdoor and indoor customers. They are not set up and do not have enough staff to do it well.

Love it close, east kirkwood permanently and model Boulder Colorado!

I love the street closures. It provided a great opportunity for outdoor dining, play for kids and family, and connected well with the bike infrastructure.

I think Kirkwood should remain closed all year.

It seems the only business that benefit are the ones that can have dining outside. And those have to pay which for the privilege which doesn't seem fair

Sometimes it wasn't very nice eating on the street. It can be hot or cold and tables at a strange angles.

I love the closures, I think it should be permanent

I love the idea. It is helpful in the summer.

Love it.

continue these programs

N/A

Makes delivering anything around there an absolute disaster

It's absolutely worthless

I think it should be closed to cars all year round

Having a walkable downtown is amazing.

There are plenty of other streets close by to park in or drive down. The closures and out door dining are such a pleasure in the warm months. Please keep it up!

The parklets are trashy looking. We should build real street side dining sheds like they have in NYC!

We love dining Al fresco worth no traffic.

It's a great alternative

Having outdoor dining available makes coming downtown much more appealing. Limiting car traffic makes the experience of dining on Kirkwood more fun and more safe. The pedestrianized portion of Kirkwood promoted people walking around, shopping, getting together with friends. I would highly recommend that the city keep up this great program.

We are still not eating indoors in restaurants and this is a great opportunity for us to enjoy a meal out. Can't wait for warm weather again.

Kirkwood closure should be year round

Keep the roads open

it created a very pedestrian friendly and welcoming environment! great idea please keep it going!

Very inconvenient acces for those with disabilities.

I would be interested in knowing the cost incurred to the city and Kirkwood store fronts is. Other restaurants don't have the same opportunity to expand their business. What happens if one restaurant wants it but another doesn't? As a consumer, this is a great benefit. But there is much more to consider and I don't have enough information to make an informed decision. I said yes, but think there is more to consider.

Just close off the streets altogether. Fort Wayne has done something similar. Create walkable streets and neighborhoods that allow for more community

Many a visitor from larger cities and other countries commented on how great it was that we provided this options I think from spring break to thanksgiving should Kirkwood should be closed.

I would love to see a car-free Kirkwood year round. Fussganger zone.

At this point, you could level Kirkwood and it wouldn't make any difference to me. I have no reason to go there.

Please please permanently close Kirkwood to cars. See improvements below.

Encourages biking and walking, makes biking through downtown safer, makes people rethink our reliance on cars and how much space is reserved for cars instead of actual people and activities

It is stupid. Again you are serving only a select few. Why are they getting preferential treatment?

I would love to see the kirkwood closures be permanent and for restaurants to have the outdoor heaters to provide outdoor dinning in the colder months.

I like the street closures because it is pedestrian friendly. It makes me want to go downtown more.

The downtown area needs to become more pedestrian driven and this was a major step towards that. making college and walnut 2 way would be a huge to making the downtown thrive a as a pedestrian driven area.

The Kirkwood closure should be permanent and used as a promenade.

Creates a nice synergy

I enjoy eating outside and the parklet program makes it possible to eat at my favorite local places. It helps the vibe downtown and is fun to see all the good people.

I think kirkwood should be for bikes & pedestrians only all the time.

Eating out on the street makes it almost feel like a street party and created great community vibes and fellowship.

Should enforce accessibility requirements

Continue all months and add more streets to closure

I loved it! We don't eat inside at restaurants still, so it was a way for us to still go to restaurants. It also created more seating at restaurants that often have a wait.

I choose not to go downtown very often now because parking is so limited near where I want to go.

My husband and I like to dine at restaurants on Kirkwood, but had to park so far away. We are both disabled and probably won't go back if the street closure continues.

I love that it makes many restaurants per friendly.

Don't ever do it again.

It is a no brainer to continue it. Open 7th street back up all the way while you're at it. That is a mess.

The few times I have attempted to visit businesses there parking was impossible and metered spots I did find ha inoperative meters. Long walks for disabled patrons.

Open Kirkwood

Ridiculous! Not enough parking normally

Parking is premium and lot were taken away. Also I had to pick up food at chipotle for my daughters wedding. 22 boxes no easy way for pick up

Love being able to walk down Kirkwood without car exhaust and traffic on the street. I spent more time walking with friends and seeing friends in passing that I wouldn't have otherwise.

I have difficulty walking more than a block without assistance. These make accessing the restaurants on Kirkwood difficult for us.

Please reconsider the parklet program or work with Bloomington transit to get at least one bus route servicing the library. The closure greatly impacted many elderly and disabled riders who struggled to get to the Monroe county public library.

I couldn't get close enough to restaurants I like to frequent on Kirkwood and I don't feel safe with so many panhandlers in the area.

Love the non traffic

The closure causes parking problems plus utility companies have limited access to business for work purposes. Trying to navigate narrow alleys that parallel Kirkwood are difficult and often times not very safe and with Kirkwood being closed it increases vehicle traffic in those alleys.

Very difficult for people with disabilities to get downtown. I was in a wheelchair from June thru September and on crutches September to November. I was unable to access Kirkwood businesses.

Kirkwood should Be closed year round. See Charlottesville's pedestrian mall for a model. A wonderful community space. People who complain about loot parking don't come downtown often anyway. Please let downtown be vibrant and full of life, not cars.

close Kirkwood year round!

I don't use businesses on Kirkwood due to the lack of parking with parklet. No convenient parking and homeless aggressively asking for money.

At my age I can't walk several blocks to get to a restaurant so I don't go. My family and I used to enjoy eating at restaurants on Kirkwood but we won't as long as it's blocked!

I did not like the loss of handicapped or close proximity on Kirkwood but it was ok.. I do things warm months to continue and see the benefit. However parklet should not continue.

Unsafe

Not fare to other businesses

I love the outdoor dining and ease of walking around downtown.

I think closing Kirkwood permanently to vehicle traffic would be great and allow more people to enjoy the shops and restaurants downtown

Love this program and hope it continues. It adds to the downtown vibe.

Although we had to park away from the place we were going, it was nice to eat downtown on Kirkwood, and then walk around the area and not have to worry so much about crossing in traffic.

I love the pedestrian-centric Kirkwood in the nice weather.

End it please

Offering outdoor dining is great and I even like to take advantage of it when the weather is nice however impeding traffic progress is just not the way to do it. It's actually pretty unsafe for the diners and pedestrians and it certainly is inconvenient and even unsafe for some drivers. See my answer to the earlier question.

I would love for the closure to be permanent. Kirkwood should be a car free street!

I've eaten pizzeria since the 60s and I'm now 68 and having to walk to my favorite restaurant is sometimes difficult. Open street parking

I am a consumer and spend money in local businesses, not just restaurants. I feel for shop owners. It is certainly not convenient.

Thank you

Love this program and would love to see it happen every summer from here out

Dangerous to make whole city blocks inaccessible to emergency response vehicles.

If you keep the program going then some more available outdoor seating for walkers is a must. It's frustrating to need a bench or a short stop and all the empty seats are reserved for customers.

It's value is not worth the problems it causes.

I am a project manager by trade and have lived in Bloomington my entire life of 52 years. I must not have enough information because I am a bit confused. At the end of day, we need to ask ourselves, who are we trying to please? The residence of Bloomington or students that stay here for 9-10 months a year? Also where's the data? Have the downtown businesses provided any financial reports to inform us on whether the closures are stimulating the local economy or hurting it? I understand that other cities have done this, but those cities typically have a major sports venue or other destination. Since I don't usually visit the area like I used to, I'd like to know more data to make an educated decision about pros and cons.

I'd like the street to be closed all year!

While I understand the desire to encourage walking, not everyone who wants to visit downtown is able to do it on foot. I already have to drive to patronize those businesses, so the lack of parking and the inconvenience of driving around to find parking makes it less desirable.

More city/European vibe

More options and ability for enclosures and strictures in the parklet spaces

Having Kirkwood closed makes it impossible for people with disabilities to visit businesses. As someone with stage 4 asthma and not being able to walk far, and not being park right by the business it means I can no longer visit and support my favorite businesses.

I feel much safer downtown as a pedestrian when the road is closed and the barriers are up, particularly when those unfamiliar with Bloomington arrive in town in mass or during events such as a PrideFest that could be a target for a vehicle attack. Overall, I think the pros outweigh the cons on both issues.

During the day, when there were not as many pedestrians I felt a little unsafe. It felt deserted and people experiencing homelessness would sometimes be more aggressive and single you out.

I do doordash part time and having to park all the way on Washington or Lincoln to go down to some of the restaurants between indiana and grant is making it harder to do

Inconsistent opportunities for downtown businesses. City ignores some and favors others. Public right of way isnt for dining.

I love this idea and enjoyed having the space on Kirkwood especially. It felt safer and fun for pedestrians and restaurant patrons.

Walking down Kirkwood and shopping would be much nicer if the homeless were no there blocking sidewalks and using drugs in public view. That's more of the reason I do not often the Kirkwood area then lack of parking!

The Kirkwood closure was more of a community experience. I didn't enjoy so much the dining experience in some of the parklet areas because of the traffic/noise/exhaust.

Many big cities have done this. And it can be a great thing but either do it or don't. The ugly barriers are very unappealing. I do wonder how the city offsets the CV oat, and restaurants pay for the benefit?

Fun way to get outdoors and enjoy the downtown experience outside. Easier navigating as a pedestrian with less traffic along Kirkwood. Makes it feel like more of a cozy community.

As a customer and pedestrian, I really liked it. I understand others struggle with mobility issues and the closure creates problems for them. Also, I'm sure it adds challenges for delivery drivers. I think the opinions of the business owners should count extra since they know if there is reduced or increased traffic with the closure. Good luck! You can't make everyone happy, unfortunately.

I love these programs! It really makes our city feel like a walkable, welcoming, and lively downtown. Everyone we've had in to town to visit during the summer months we take downtown for the experience and they all love it! Great for night life as well!

Doesn't seem fully utilized, always dirty, uncomfortable, slow. And nowhere to park

Just leave it all year. That way it's not confusing for people and they'll just know it's closed.

Love it

None

Should only be closed for events . A street party etc.. lotus festival etc

I would like the city to consider making Kirkwood a pedestrian area permanently, and not limited to warm months. Additionally, it could be renovated to not look so unappealing to people if ornament infrastructure was put in place.

It provides immense value. Make it mandatory.

Close Kirkwood permanently.

Make it permanent not just during “warm months”

I hate coming downtown, I hate paying for parking

I cannot say how strongly I support increasing pedestrian infrastructure for the downtown area. It increases access, safety, and quality of life for kids, those who can't/don't drive, and those with some kinds of disabilities--populations whose needs are so often overlooked.

The only downside to me is trying to get takeout from businesses in the area that is closed off.

I was less likely to drive to downtown businesses and restaurants when Kirkwood was closed. I enjoy driving down Kirkwood and parking along there to visit businesses and restaurants but did not while it was closed off.

Love it! Really enhances the feel of downtown. There is plenty of room for parking in the garages, plus they're cheaper than on-street. We need pedestrian life in the downtown

I think it is important for the success of the restaurants who use them.

I am handicapped and it was very trying at times to get. Into the area

Thank you for asking for feedback!

I work at nicks English hut we have a patio outside and I have to say that it is a nice place for them

Might affect access issues for those with disabilities so this should be adjusted for

Restaurants could offer better outdoor dining experience, e.g. more shade from sun in hot weather

It's discrimination.

All of Kirkwood should be closed down for the whole year to allow for dining, center of road kiosks for small businesses, pedestrians, music, etc. Having vehicle traffic in a high-use pedestrian area is not pleasant. Many European cities are great examples of how closing roads and opening walkways leads to more beautiful pedestrian and business-friendly cities.

If these restaurants would like more dinning space then they need to find a bigger location

Traffic flow, looking for parking could be frustrating with 6th Street one way and no more parking on 7th.

Close 4th street. That's a no brainer. Like really. What I'm the actual ___k are you all thinking.

The way people drive in this town I wouldn't want to sit in a parklet say along College Ave or West Kirkwood (in front of Crazy Horse). And then you have the “unhoused” population that roams Kirkwood and harasses people who utilize the parklets. As a former server (for many years) unless you have a dedicated patio for a specific establishment the outdoor street dining is not profitable (for the server making \$2.13 an hour) and some establishments can't serve beer or wine at their designated “parklet.” (Osteria Rago).

Many people like this experience in other cities and I feel like it keeps Bloomington on the same playing field as other larger cities in that regard.

It would be nice if a more attractive barrier could be used for the Parklets.

The city bus doesn't serve the library during the closure, weekend closures not a big deal but weekday is a pain

Seems like a scheme to get students to party on Kirkwood.

If they want me to use them, more comfortable seating.

My family and I were dine outside enthusiasts before COVID and we continue to be along with what seems to be more people now. I feel that both these programs allow restaurants to provide more and better outside dining experiences for their customers. It's so nice to be able to dine outside during spring, summer and fall at a great local place downtown. The Kirkwood allowed us to choose food from different restaurants and eat outside at a picnic table while enjoying the sights and sounds of Btown.

Makes already confusing downtown one way streets even more difficult to maneuver for visitors and new students.

Make it permanent

The Kirkwood closure is fantastic and definitely needs to continue. It is so nice to be outside eating on Kirkwood in good weather!

No thought to older/handicapped people. Provide rides or shuttle.

I really like the quietness and pedestrian-friendliness of the Kirkwood program in particular. On Walnut/College the park let's feel awfully close to traffic, but still positive.

It made Kirkwood completely inaccessible to me physically.

Combined with other street closures and construction in 2022, the E. Kirkwood street closure made getting to the library almost impossible at times. I hope there would be better planning in 2023.

I really enjoy both of these programs. I've visited other college towns such as Iowa City and Charlottesville, VA and have long wish that Bloomington would move to a more pedestrian-centric downtown model. I think it enhances the city substantially.

N/A

I would like to see Kirkwood permanently designated as a pedestrian space with outdoor dining. It adds so much to downtown.

Please pay close attention to people with disability access needs to utilize these closed street areas. I like the Parklet offerings to promote less car use on streets. Better to use some selected streets for pedestrian, dining, canal interaction downtown.

Should consider making it a permanent closure

Downtown parking is difficult enough and the Kirkwood closure makes it even more difficult for seniors or those with difficulty walking.

Downtown feels more family friendly with the closures. We enjoy bringing our children to the area thanks to the extended spaces.

It's hard to explain how blatantly ableist it is to take away any parking in downtown Bloomington.

That section of Kirkwood has little utility for cars. It might as well be fully converted over in the long run. Even when open, its not reliably faster to get anywhere driving along that section of the road; anyone who actually knows their way around town takes 6th or 4th instead in that area and it is a totally inconsequential amount of parking.

There is usually not any spaces when Kirkwood is open so I don't think it really matters with the exception of handicap individuals. It should definitely be closed when the students are gone. But I feel like businesses should benefit if they don't then it is not necessary

Just do it all year round. Winter isn't too harsh here.

I enjoy driving down Kirkwood during day/night to see the crowds and energy of the district

The highway construction sign that is posted at the intersection of Kirkwood and Walnut is UGLY. Bloomington takes pride in its attractiveness and charm. Surely we could do better.

It has to make parking more difficult, even if you are not in that area to eat. And old crippled people like me don't feel like walking a long way. I love Trojan Horse, but I will go else where if I can't park with in a block of them. Since I don't walk well I really have no choice!

I feel like we should find a middle ground between permanent closure and seasonal closures. Permanent closures allow us to develop a very nice space, seasonal is rag tag, dining on asphalt ,etc...

It's confusing and frustrating enough for those of us who live here. I can't imagine what it's like for visitors. I'm afraid it's just a matter of time before there's a serious accident because someone got confused, or was just being stupid. We've never used those spaces. We don't know anyone who has, either. If people want a "fresh air" experience at a restaurant, they need to go to a restaurant that already has their own outdoor dining. Are these restaurants paying to take over our public spaces?

It adds much ambiance and neighborliness to our downtown, and helps to balance the chaos and craziness of the downtown during the university school year.

I like to eat outside, but only on a few occasions, more on the weekend was it ever really busy.

Closing the street is absolutely ridiculous and needs to stop. Myself, and others I know, avoid the area because of it being closed to traffic. I don't see how that helps any business at all. It's also extremely inconvenient and needs to be open to traffic all year long like it is designed for.

Hurts businesses. And keeps handicapped and elderly from visiting a restaurant or business cause they can't walk that far

City street should be used for transportation.

It's very inconvenient, how does closing Kirkwood help any of those businesses?

Get rid of it

Makes it impossible to pick up orders.

Failed idea. Return to a normal city street

In general I like it but I do think it gives those businesses an advantage over others not on Kirkwood because of increased foot traffic.

As a pedestrian and cyclist I felt much safer downtown due to reduced traffic speed/volume on Kirkwood and fewer cars pulling in/out of parking spaces.

I've been here for 5 years and lived in other college towns prior. There are several college towns that have pedestrian only streets for safety, and ultimately I think closing down the part of Kirkwood can benefit for the reasons listed above (downtown experience, dining, etc.) but also at the end of the day, I feel it provides a safer environment for pedestrians. Between crossing the road or trying to get in a Uber/Lyft, I think it's safer for those roads to be closed. I understand it's inconvenient for deliveries and for car traffic, but I think the safety component outweighs that.

Love sitting outside. Please keep.

It is ridiculous to inconvenience those who use the city streets and parking places. I stopped going downtown because of this.

My concern is for the safety of the parklets. I just worry they're an accident waiting to happen and they take away valuable parking spots.

It's hard to find parking

***** CUT IT OUT

The City closes the street, allowing the businesses on Kirkwood to have more space to serve customers. This is unfair to business not on Kirkwood, as their streets aren't/can't be closed.

Get rid of it. Period

This was a really bad idea. I don't see how it could have been seen as anything but that.

Very stupid idea from the beginning, it's street and should be used as such. These streets weren't designed or built to handle today's traffic and closing a street makes the situation even worse!

As a frequent customer of many businesses on Kirkwood, I loved this program. It created a fun, vibrant sense of community. I visited restaurants more often and spent more money than I normally would have because the atmosphere was so welcoming and inviting. Please continue this program, as it makes Bloomington feel alive!

These closures are inconvenient to those who live here year-round. Not mention there is not enough "convenient" parking in case you do actually want to shop or eat downtown.

Tax Payers cannot drive on roads designed to be driven on

It's makes it extremely hard for service providers to reach business and there's so little parking that large trucks and service vans end up blocking the alleys and side streets

The program makes Bloomington more inclusive and accessible for our neighbors with medical needs and disabilities. It also helps small business owners and employees. Please keep it in place.

It is super unappealing to those with mobility issues. I hate it honestly.

Don't know what a parklet is

It's a terrible idea. I'm sure the Fire department has a very hard time accessing those places if they had an emergency

The restaurants on the square seemed at a disadvantage. The orange street cubes were ugly

Needs to end with the removal of covid safety protocols

Love outdoor seating (even before Covid)! I hope it continues.

I am still not eating indoors because of covid concerns. Being able to eat at my favorite downtown restaurants and not having to only do carry out has been amazing.

The inconvenience of parking and driving around one way streets, and inequality of advantages to a few businesses does not warrant street closures.

Longer walking to meet with friends sets you up for more homeless/beggars to stop and confront you for money. I will not travel Kirkwood alone anymore.

I'd rather have walkable space than driveable space.

It's really inconvenient as an employee of a downtown business to have to PAY for parking everyday I work, and then that parking also becomes more limited when there are closures.

It makes it difficult to get to areas down town with the closures and the one ways. Made me go downtown a lot less

Don't do it!!

Dumbest thing ever, disadvantaged to those of use with disabilities who want to patronize Kirkwood businesses

I love it! It makes Kirkwood feel like an outdoor mall and it doesn't really cause traffic issues.

Ridiculous

It detours our family from visiting business in the downtown area

Not sure why businesses on Kirkwood should enjoy a parklet advantage when other businesses can not.

Easier for friends to go to dinner

I personally feel that Bloomington lacks outside dining options in comparison to other towns & cities and the parklets allow us to increase those options! Love them!

Connects downtown to campus for pedestrians

It kept people from coming downtown. Hard to find parking

Best thing ever! Should stay in place permanently.

Makes it difficult for mobility impaired individuals to access stores on Kirkwoof

Do they pay rent for using the street property?

Can't park to use the down town no place to park for carry out pick up

I rarely come downtown due to the hassle of finding parking. I have limited walking ability so many of the new enhancements are more like barriers for me.

I LOVE the atmosphere and ability to stroll down Kirkwood without the normal busy street environment!

Stupid idea but is typical of the current mayor's administration (Hamilton)

It's the most asinine program ever!!!

I would love for kirkwood to be pedestrian walkway all year. If permanent, it would be cool to turn it into a pretty pathway with seating and landscaping.

The street needs to be open as the street is designed. It is inconvenient to go to the restaurants due to less parking. I also don't want to eat in the street.

Make Kirkwood a year-round pedestrian-only street from the Sample Gates to the Square. We would come downtown 5x as often and it would transform the whole experience!

We pay the taxes for kirkwood. So they should be open. If you choose to close the streets then we should get a credit on our taxes

Makes getting to downtown business' very difficult, not including parking. Inconvenience all around.

Wish the block between Indiana Ave and Dunn St would remain open at all times. Dunn street frequently gets too backed up and is not setup to handle the increased traffic it is forced to carry with Kirkwood being closed at Indiana.

If it could be done without closing the street, I would have a more favorable view of it.

Love it.

People should get out and walk. It's good for them.

I work at Modish Hair Co. Several of our clients during the closure would show up late to appointments due to not being able to find parking. They ended up having to park multiple blocks away, most don't mind walking however it is inconvenient for any customers of any business to not be able to find parking to go into a store.

Overly loud music at a couple of venues that undermines public's opportunity to enjoy dining at their selected venue. Please limit outside decibels.

Please continue this program. I am a lifelong Bloomingtonian, that spent a couple of years in Boulder Colorado. I always thought that Kirkwood would shift beautifully to a pedestrian mall, similar to pearl street. The enhanced outdoor seating at restaurants, is so beneficial and appreciated.

I like to eat outside, but parklets are not the answer.

Keep it going! It's been great to have outdoor dining options.

Very difficult for handicap people/people with other issues, to enjoy the restaurants and other businesses

We pay taxes to use the street which is blacktopped for vehicles.

Parking is limited as it is. Closing an entire street down is extremely inconvenient.

All "cool" cities have areas like this and are a fun experience.

i'm in favor of parklets and the kirkwood closures in warm months when outdoor dining is possible, and appreciate when there is more parking/kirkwood is passable by car in the colder months. it also helps that students are often gone during warm months, so having the streets closed isn't as inconvenient as it would be when the town is more crowded.

The homeless ruined it, we stopped going because of the scary behavior, we felt like it was very unsafe situation.

Allowing parklets is fine. Shutting down kirkwood isn't. Let's not pretend that shutting it down adds value to the area inherently... allow certain restaurants and bars to operate parklet spaces that are permanent in either parking spaces or on one side of the street. Make traffic one way if that's a possible compromise.

I like the outside dining options, but I often skip eating downtown or getting carryout from those restaurants because it's too difficult to park anywhere near those businesses.

My family and I really enjoy the opportunities for outdoor seating at various restaurants that the Kirkwood closure provides. It creates a fun atmosphere too. One of the things that we'd like to see remain in place for 2023 and years following.

I frankly think they should continue year-round.

Kirkwood is not currently set up to allow closing it off and having businesses operate in the street. It's inconvenient, dangerous, and takes away parking.

It's very inconvenient to even get close to places you would like to visit just not possible.

Same as mentioned above: Hard enough to navigate without this main thoroughfare being closed. How do first responders get thru. If restaurants want outside dining they should pay for the ground and not use public streets.

N/A

Only frustrating the days/months other side streets also closed.

Terrible inconvenience

I felt safer as a pedestrian just because many roll through that stop sign on Washington and kirkwood, however it made car transient difficult.

I avoid the area now. Enjoyed mostly take out at the kirwood restaurants and it is not worth the inconvenience to find parking and walk far but believe it should be decided by business owners and what benefits them

most. I choose to avoid the area but if businesses get more customers then they should keep it for their sake.

As someone who is immune-compromised, eating outside is my (and my family's) only option for dining out. Since the parklets opened and Kirkwood closed, we eat out more frequently than we did prior to 2020. A secondary benefit is the ability to stroll, shop, and get dessert after a meal with a nice space cushion between us and others on Kirkwood.

Keep road open if you want people to eat outside to enjoy move to a bigger place or charge them more tax dollars to use the street make more money to serve more people food it's a road use it that way we all pay enough tax on food and drinks why let them use it make there place bigger to serve more think about that !!

Loss of parking spaces, especially while students are in town, makes it harder for disabled people to find close spots, or makes it impossible to park near some places at all. Also having less parking available during student move in week and while students are here in general makes zero sense, you know more people are in town why would you create less parking for everyone? Just so people can sit outside and clutter up the walking space?

I enjoyed being able to eat out and not have to worry about COVID as much.

I've always loved kirkwood/downtown but having the closure in the warm months has made me go downtown much more frequently than before! More seating and more business for the local restaurants.

I don't think businesses should be using city streets for their customers

I would like more Covid 19 safety precautions for immunocompromised, high risk, and disabled people. It is entirely too difficult to enjoy the town if we cannot safely go anywhere where masks aren't mandated.

Wonderful program! Outdoor seating is a huge addition to downtown restaurants.

I love being able to walk on the streety

I am new to the area and absolutely loved being able to eat outside and get a feel for the downtown area

Love it!

It's made it a mess and inconvenient to go to any restaurants or businesses near or on Kirkwood.

I have health issues and will be unable to go anywhere downtown due to lack of parking.

The street closures add value to the Bloomington social scene. It allows people to eat outside, walk more easily, and interact with other community members. I think it should continue in the warmer months.

Parking is already difficult. Closing more parking just makes no sense at all.

These are through streets and parking for cars. Not just the restaurants.

It really deters local from going downtown. It is very inconvenient. I already have to park for work downtown, at least I could be able to park closer to my work location without the closure pushing everyone to the lots instead of the street. 7th street bike lane was horrible as well, eliminating street parking.

I find that it liven ups Kirkwood and downtown. I love it!

I like it. Keep doing it.

Time to end it

It disrupts the intened use of the Kirkwood ave access to east side streets & businesses

Loved the increased safety and activity on the streets!

Pedestrians routinely walk in front of cars on the corner Indiana Ave & Kirkwood and especially the corner of Dunn & Kirkwood. Pedestrians seem to have a difficult time recognizing when the street is open for cars & pedestrians. All of Kirkwood should be closed or none of it.

The original intent was ok but there's no good reason for it to be continued whatsoever

It's fantastic and we'd love to have it year round

It stinks. A form of communism.

Terrible idea.

I questioned the safety of the park lets. That's why I never used one. I just didn't want to sit that close to traffic.

If the kirkwood closures continue it would be great if you considered door dash and delivery drivers, some sort of designated parking nearby would be amazing!

It makes it impossible to get into a store and out

I am still only eating outdoors, so this is the only way I can enjoy eating out at these restaurants.

Would love to see this be permanent and see the street space developed into a community pedestrian area—benches, greenery, etc.

It's really nice to have a place to just walk around without having to worry about cars.

Often had delivery drivers (Door Dash, delivery trucks, etc.) and Ubers parked in the driving lanes on streets such as Dunn, basically making Dunn a one lane street, especially near Kirkwood. Many pedestrians treat the cross streets at Kirkwood (Dunn, Grant, etc.) as part of the closure, thus the pedestrians walk across Dunn, Grant, etc. on Kirkwood, rather than at the crosswalks —VERY DANGEROUS! Very difficult for those with mobility issues to reach some favorite businesses and/or restaurants — unable to find parking nearby and drop-offs at the door are certainly not an option. Overflowing trash receptacles near restaurants most days/nights.

The closure got me out to eat at restaurants despite my health problems because I knew I could be outside and not crammed in with people. My doctor recommended outdoor dining due to health risks.

Parking downtown is difficult to find and closing part of Kirkwood makes it even more so.

Absolutely no reason to open it back up. Much better routes for cars other than Kirkwood. So much safer for students.

So many homeless people downtown in this area that I don't feel safe unless I can park reasonably close to where I'm going.

Not convenient for anybody but students

It makes down town feel festive

Please keep it going! It's such a nice benefit for residents.

Stop closing the streets and cutting off vehicular access to businesses. Its ableist and detrimental to those with disabilities.

I think it gives an opportunity for people to enjoy the outdoor experience while dining at their favorite venue. Gives a sort of European feeling to Bloomington

Unnecessary

Go WOKE GO BROKE

Honestly it would be so cool to have the whole square closed off in the warm months. it's a great space but all of the traffic makes it feel unusable

since it's so loud and inconvenient to cross the roads. I know that this isn't super feasible though

This survey is flawed. I have owned a downtown business for 8 years. This survey was forwarded to me by someone else. Who received tis survey? More importantly, who did NOT receive this survey.

Huge Inconvenience!

Restaurants should not be allowed to take up more than half the street. This limits pedestrian activity by requiring they walk through prep space. Uptown and Farm were especially bad in this regard.

Kirkwood should stay closed throughout the year and be developed into a public pedestrian space where people can sit and eat takeout from the various restaurants in they'd like. It is not appropriate to GIVE public space to people to are entities so that they can make additional revenue and exclude non-patrons from that public space. If restaurants want the additional space they need to purchase it or pay the city for that use.

Stop narrowing the streets!

Disproportionately benefits a very few businesses; the other businesses did not enter into their leases knowing this program would happen; a subsidy for a very few businesses that is entirely unfair and a burden to the rest of the downtown; no other business is able to expand in such a disproportionately preferential manner and street dining is no longer needed due to Covid.

I can't figure out why people would want to sit along the street with dirt and exhaust from traffic floating around. Also cars could run off into people sitting in the parklets at other locations than Kirkwood. Dangerous! Keeps other business from convenient pick ups for their customers for take out orders. Long way to walk for elderly people to park and eat. Looks ugly in general. Also has anyone considered what would happen if there was a fire or other emergency in the closed zone?

Make it permanent! It's more confusing when the street opens in the winter. So much safer for families, students, etc.

Almost as bad of an idea as the bike lanes. But I wouldn't expect much more from this administration

Enjoyed walking and dining outdoors on Kirkwood

It needs to go away

Keep it up but make it more visibly appealing. We love Pearl Street in Boulder at CU - PLEASE do something like that!!!!

I believe it's a great way for business to get more customers - only if that's what they're looking for

I quit doing business on Kirkwood

Closing the end of Kirkwood (library to Sample Gates , putting in public art, benches, etc...would greatly enhance downtown - many othe college towns have pedestrian malls/streets that add charm and draw people to the downtown! The parklets and closures are a great start!

Love it during summer months. Good to change during winter for holiday shopping and colder weather.

It would be nice if kirkwood could be open on oneside oneway going east.

It is ridiculous!!!

Maybe just close the street on weekends

I think you cannot close kirkwood like you did when there is much road construction everywhere in the vicinity. It was nearly impossible this summer to navigate the area let alone park. If side streets are open then then kirkwood closure is much more navigable.

Gives people the opportunity to patronize the local businesses and restaurants

I'm curious how it affected the businesses. I don't want them hurt by this.

Orange barricades unappealing.

Felt very unsafe on the open parklet with cars passing and my elderly parents were no longer able to visit Kirkwood with the extra walk. Homeless are always disturbing.

My only concern is that I have a disabled family member who we often drop off near restaurants (and then we go park elsewhere. I don't know that it's possible, but an accessible passenger loading zone, especially near the BCT, would be helpful.

What happens if there is a fire or any other emergency? Makes it hard for firefighters to get through tables full of people

The Kirkwood closure channeled vehicular traffic from a street designed for it to streets that weren't (4th, 6th, 7th and side streets) putting vehicular traffic in direct conflict with pedestrians and bicyclists. Also, all the extra carbon pollution from cars idling around looking for ways to get through or to find parking. Also, the business loss from people who decided to stay home rather than fight barricades. It's like the people who came up with this rarely get out of their cars themselves, or this would have been obvious from the getgo.

You're making it difficult for handicapped shoppers

Walking on Kirkwood when roads were closed helped improve our experience of stopping at shops. More room to move than being restricted to the sidewalks.

Please keep the expanded outdoor opportunities! Despite popular belief, there are still a lot of us who are concerned about Covid, and being able to eat outdoors expands access for the high-risk/immunocompromised/disabled and their families, while also offering more options to the general populace.

Too many homeless bothering people eating out

The whole "is it closed, is it not closed?" uncertainty is the biggest irritation. Either just make it a permanent pedestrian promenade, or keep it open all the time. This halfsy situation is absurd.

N/A

It is giving a very unfair advantage to two restaurants and hurting EVERYONE else in the area

Loved it!!!! Felt much more comfortable visiting restaurants that had (more) seating options outside. It also made Kirkwood seem more fun, open, and unique to visitors!

STOP closing it off for everyone to use. You know there are disabled people who would like to visit downtown not just your precious iu students and staff!

It makes downtown businesses inaccessible for residents with physical disabilities. People can't be dropped off or picked up near their destination. Makes getting to a restaurant in a wheelchair nearly impossible. Also not convenient for older residents who have difficulty walking short distances.

Na

More parking spaces

ill-advised and financially incompetent

I love it! I think it's so charming, especially the kirkwood closure, and a real draw to visit downtown. The road closure really doesn't impact my driving

and parking at all, I can just use a different street and we never tried to park on that block anyway. More pedestrian areas!

While it is a useful temporary option, it is unfair to businesses who do not have the option to use the space.

Closing Kirkwood may actually hurt business on days with inclement weather. On street parking would not be available near the businesses requiring people to park away and walk to the businesses. In inclement weather, patrons may choose to patronize businesses where they can park in close proximity to the business. It is especially difficult for some elderly or physically impaired people who may have difficulty walking to get to the businesses if parking near the businesses is eliminated.

Big Dig 2000 , I thought the city and its residents would benefit from the eclectics of a total pedestrian experience on Kirkwood . Once this came to fruition post covid the actual experiences was a positive social and economic impact in the Kirkwood Neighborhood. It's a definite benefit Bloomington. Critics will cite traffic and parking problems, but those factors were also affected by the multiple street closures in the area because of new apartment construction throughout the immediate area. The construction had the most adverse effect on the traffic/ parking issues, not the Kirkwood outdoor dining.

Most of customers wouldn't even come to our business until the street was opened. It was the number one complaint. We had the worst summer of business that we have had since the big dig.

Parklets I don't see as an issue but to shut down a major portion of a high traffic area in Bloomington is not a good idea bad for delivery drivers, and drivers in general. Please keep our streets open.

Homeless people are taking over Kirkwood

it greatly enhances the downtown experience. It reminds me of some of the most forward thinking brother-sister cities, such as Boulder and Fort Collins, CO and many other great college towns.

This program has made it more fun and interesting to travel through the downtown area.

This is a wonderful change to Bloomington! Seemed like Europe, eating outside in the street.

It would be nice to dress up the parklets better with paint / plants etc

Close Kirkwood permanently to cars

Move / Change Kirkwood dining to 4th & / or 6th streets or the alleys.

Outdoor dining in Kirkwood has been a really lovely experience. We participated several times during the season. We took family out to eat in Kirkwood when they visited, and they really enjoyed it, too.

Has any data been collected on how the closure of parts of Kirkwood have impacted the non-restaurant businesses? Ditto for the businesses on College and Walnut who are impacted by reduced parking due to the parklets. If data has been collected, please share that information with the public prior to making a decision for 2023.

Have not sat in the parklets due to the proximity to traffic. Feel very vulnerable to being struck by an inattentive driver. Kirkwood closure is a big success and enjoy the ability to sit outside in a safe space.

Please don't sacrifice loading zone space for parklets

I don't like doing anonymous surveys, so I willingly give you my name - The Rv. Matt Seddon, Trinity Episcopal Church

There needs to be a plan for accommodating those with limited mobility who need to be dropped off by car immediately in front of their destination.

At night; the street closure made Kirkwood a play pen for the Homeless; we completely stopped going downtown. You could not walk down the street without being harrassed.

The library sits in a bad spot for the elderly to use when all of the closures are going on. Worse than usual.

I like the space or two but not the Kirkwood closure. I felt like there was minimal use for the inconvenience and it only benefits a few restaurants and isn't worth the expansion of closed streets

What I didn't like is that there was a large electronic sign board up for a day before the Kirkwood closure, but it was blank. AFTER it was closed off did the sign board announce that Kirkwood 'would close'... that's putting the cart before the horse and closing the street before the sign could announce it was going to happen. Why not put the sign up and let it be up for a week or two prior to closing the street?

Closing Kirkwood is a brilliant idea and should be implemented year round. Affords a wonderful pedestrian and outdoor dining opportunity to enjoy the downtown free of loud and dangerous traffic.

I am hugely in favor of the Kirkwood closures and parklet program. It makes downtown feel much more social and alive, and I appreciate that it encourages a pedestrian experience.

Kinda sucks. Question 2 is not here

We should still be taking CPVID-19 seriously. The Kirkwood closures are a great way to allow those who are COVID-cautious to support local restaurants.

Need good security

The restaraunts on kirkwood are encroaching into the street too much. I just want it closed to cars and love walking in and biking in the street without them there. I dont like the tables taking up both lanes of kirkwood though. The parking spots should be enough. Just close the whole square to cars on weekends.

I feel like it made the restaurants and business on Kirkwood more accessible, especially as someone who is very COVID cautious and as a pedestrian! It was so much safer and easier to walk around downtown with the Kirkwood closure

This could be the start of an excellent program akin to what has been so successful with Pearl Street in Boulder, Colorado. The placemaking opportunities are endless if this program is to continue and grow!

It unfairly inhibits business for those who are not restaurants

Handicapped people who take the bus need to be able to get to the Library. Can't walk 5 blocks from bus station to Library !

While we love our restaurants, what about the other businesses, retail, office. So many parking spaces were eliminated on seventh Street. It was done because of the pandemic so more people would feel comfortable eating at restaurants. As we exit that era we need to preserve the parking for all businesses. Many of the Parklets were not used that much. I would say farm restaurant in The Uptown benefited the most. A couple of places that already had outside seating got parklets.

I wish Kirkwood would close permanently to traffic and you'd dig BIG tree plots so we could grow trees that meet in the middle of the street and create a shade corridor. A downtown park with shopping and restaurants!!! Part of my urban futurist dreamscape.

The Kirkwood closure made Kirkwood much nicer to bike on

A waist of parking spaces that we really need for clients and employees the closures are discriminating against non-restaurant businesses.

The idea of road closure is a good idea but not for the whole week. A Friday to Sunday would work better just like other big cities do. Keeping it closed all week is a loss of business and tips for the shops. Also, people/ business owners who vote to keep it closed for the week and year should have their business open all week not close 2 days during the week when many others struggle to make money every day of the week not just weekends. That is just unfair. Not everyone's business has the luxury of closing two days in a week and still make money. Have them keep it open and feel the financial burn with being open and not making money due to the week days road closure and still having to pay employees. Good look and do the right thing for downtown

This was implemented during Covid because restaurants couldn't operate at usual capacity. They are now able to operate as normal and there is no reason for this program to continue.

Thanks for City helping downtown small business

I think they served a purpose during the pandemic, but it is time to get back to normal.

Good addition to downtown

Too many bums

Totally enjoyable with opportunities far outweighing any inconvenience

Love the parklet program!

I would love to see all of Kirkwood Avenue become pedestrian-only year-round. Keeping the cross streets open at the same time, but no longer allowing cars to drive east west on Kirkwood ever again.

The closure increases traffic on nearby streets, including 4th, 6th, and Grant st.

Quit trying to fix things that ARE NOT BROKE!

Many of them are ugly and uninviting.

Maybe find more attractive parklets to use instead of the bright orange. Also, maybe focus on weekends for its use.

This puts an additional strain on parking near the busiest area of town and makes picking up takeout a hassle. This hurts businesses.

I think this is great but the bright orange barriers are very unsightly.

This is another example of the City of Bloomington administration failing to consider and hear opposing voices. The parklets are a terrible idea: 1. limits convenient parking 2. only benefits certain business (large restaurants) at the expense of other businesses (retail, small restaurants, other quick stops)

I own businesses on the square and felt there was enough sidewalk area to not need to take up parking spaces. However, overall, I don't think they hurt our businesses - I just wish they looked nicer. As a customer, I love the Kirkwood closures and feel, especially the one near Uptown which adds more of a European or city feel like that of Boulder (Pearl Street). It feels like they are able to look nicer than the orange barricades around the square. But I also know we have the big advantage of the parking garages at this end of town and other businesses did not fair so well further down Kirkwood.

You are taking up parking spaces for businesses downtown

Better safety for pedestrians

If given the right exposure/marketing, I feel the closure can add serious value to the downtown experience and bring pedestrians, diners and shoppers into the area on a more consistent basis. Perhaps allow food

trucks to park on side streets and add tables to kirkwood so that there are areas to sit and enjoy the downtown area.

Petty, but the bright orange barriers are eyesores. Do they have to be bright orange for safety, or can they be made more attractive?

I wish it was year round

more streets should be like this

Do you have suggestions for improvements?

More umbrellas for shade :)

No

Close Kirkwood permanently, brick over the street, BUT CLEAN IT REGULARLY. Allow the businesses to use the fire hydrants as a source of water to hose it down early every morning. And don't charge them for it. Have a few less bike lanes that aren't used and supply the water for free to actually make something nice.

All year long!

I like both programs, and I found the biggest inconvenience actually was with other drivers who, rather than finding a more distant parking space and walking, would throw on hazards to stop in whatever part of the street was closest to their destination. There is some reinforcing of "no parking zones" (like next to the Kirkwood street barriers) that would help keep flow of traffic smooth and safe for pedestrians.

If you choose to continue to close parking during the warm months, consider reducing the cost of parking on the street or in garages

Host a rotating featured food truck on the closed portion of Kirkwood

Invest more money in your athletic facilities, they are old and rundown.

The parklet closures could do better than the orange water-filled barriers that have been used on the west end of Kirkwood. They are not very attractive. I believe the road closures should be the first step in reducing parking/driving on Kirkwood and increasing pedestrian/bike areas.

Consult disabled and elderly individuals to ask what is needed for them to be able to equally access the businesses and library on Kirkwood

Street art in the summer. Encourage it as a programming space like Farmers Market stage/buskers and Granfalloon and Handmade fair. It is an awesome place to be.

Require better air filtration systems and more space between seats/tables inside restaurants to make people feel safer, more comfortable, and make it easier on servers, too. That wouldn't reduce parking and address each business equally around Bloomington.

Rethink downtown to close streets- forever. Close street, rethink one ways and deliveries. Outside areas to eat are so important

The parklet barriers aren't very attractive but I know safety is more important than aesthetics.

The temporary construction signage used and bright yellow poles are aesthetically unappealing. I feel there must be a more attractive way to do this.

Make the Kirkwood closure permanently. Free the streets.

Perhaps working with a one side closure would be more effective and still allow disabled individuals to find parking near their chosen venue.

Circulating busses.

I support opening spaces for restaurants to serve outdoors, but without full street closure as it seemed like only a handful of businesses used the space. I was also dismayed to learn that the businesses had to pay the city for the closure this year.

Something more aesthetic than the orange barriers would be an improvement.

This should be continued through Canopy of Lights. Perhaps the city could provide heat lamps to area restaurants around the square. Its just a

wonderful cheerful moment. Christkindlmarkt!

Maybe a bit more shade or potted plants? I really like the string Christmas lights, that makes the atmosphere really nice too.

"Parklet" isn't a term I'm familiar with. Something else might be more clear. Maybe "Kirkwood Outdoor Dining Program"

Either convert Kirkwood into a pedestrian mall between Walnut and Indiana or abandon current parklet patchwork plan.

Nicer looking barriers.

Can we close Kirkwood year round?!

Exclude cars from it permanently

Get rid of the program and make the downtown more accessible for everyone! I do hope the city in its bid for transparency will offer the results of this survey, pro and con.

I would like to Kirkwood closed to traffic permanently

Ideally, it would eventually be converted to a year-round area for people outside of cars. Having winter-themed events on this street would be fun!

1. Close Kirkwood year-round. Although pedestrian traffic is lower in the winter, Kirkwood is still bustling with people wanting have fun during the other three seasons. It would provide a consistent oasis for pedestrians to not worry about the dangers of cars. 2. Close surrounding streets to private vehicles, as well. In Paris, France, the city government began a pilot program of making the four most center neighborhoods pedestrian-only for the first Sunday of every month. After monumental success, they have now permanently closed many major streets to allow pedestrians full access to the city. I would love to see Bloomington implement something similar in which, the first Sunday of every month, we closed down 4th through 6th from Indiana to Morton. I have no doubt that we would see similar success in business activity and happiness of Bloomington residents.

Find more streets to do this on!

Don't do it again! Only close Kirkwood for big events like Lotus Festival and Pride.

Enhance the barriers to make them more attractive and give the business more opportunities to enhance, decorate, or substitute barriers. Invest in semi permanent barriers on Kirkwood for example (bars that lower into the street) would further improve appearance.

Although not an improvement per se, I think closing Kirkwood on a permeable basis could give the opportunity for kiosks in the closed area, thus more reasons for foot traffic and additional income for the city and the kiosk tenants/owners. And curate kiosk tenants to be complimentary to the dining experiences - it could be a win-win for everyone!

PLEASE focus on affordable housing options! Our city is in a state of crisis!

Require the planning department to provide an accessibility plan and solicit feedback from the Council for Community Accessibility for all proposed street and sidewalk closures

If closing Kirkwood is unpopular, it would be nice to allow businesses to purchase parklets. There are plenty of people with medical conditions that still preclude them from dining indoors in crowded situations (COVID is not over no matter how much we wish it was), but who enjoy outdoor dining and keeping our cherished local restaurants in business.

We think it works pretty well as it is.

See above.

I know there are individuals with mobility issues who find the change difficult. Are there specific access zones that they could use to make the

experience easier for them?

For festivals it would make sense to close Kirkwood for foot traffic-- everyone benefits then, and it's just for a limited time that would make it also more exciting for those participating. But regular weekdays? It just causes issues for everyone not dining.

As recommended above, greater certainty and permanence to allow investment in high-quality parklets like other progressive cities allow.

More events using the spaces, beautification, music, activities in the spaces

Having a downtown shuttle (that is clean and only available to those who parked further away downtown or in garages) that makes regular stops would be needed if downtown closures are to continue.

Cancel program

Close it for a shorter period, say April or May through September or October. Consider aesthetics---no picnic tables, please.

Do away with the street closure and loss of parking.

Honestly wouldn't mind the roads being closed to car traffic even more throughout the year

More obvious signage

Open the road

Close the through streets: Dunn, Grant, Lincoln, Washington.

Frankly, I would love it if all of Kirkwood from the Sample Gates to the square were a pedestrian mall closed to traffic. But I don't know that that would serve the best interests of drivers and/or businesses on that strip.

Don't close Kirkwood.

Long-term, I think converting other streets from one-way to bidirectional would make it easier to reach your destination when access is limited from street closures

Quit narrowing streets.

Improve the appearance of the barriers, especially on College. Can you find a way to use planters or something more aesthetically pleasing than the current orange barriers. Find a way to make level the dining areas on College.

Allow for the building of outside structures similar to what they do in new york

Add more varied seating on the closed streets (particularly the part by Sample Gates

Not a suggestion, but a comment...unless you are disabled, I'm not buying what anyone is selling about lack of parking downtown during the warm months when the street is closed. I guess if you aren't willing to get a bit of exercise and walk to one of the businesses, sure, you might not be able to park right next to the door of Farm/Uptown etc...but there is still PLENTY of downtown parking. This increases seating for all the restaurants and makes the outdoor dining experience fun and inviting. Everyone needs to stop complaining and get some exercise! Though, I do hope that the restaurants aren't having to pay a huge upcharge for this to happen, because that is ridiculous, in my opinion. The city is making enough money off the meters/parking lots that remain. This needs clarified.

Could the orange barricades be covered with something more visually appealing? Maybe this could be a public art project.

Signage displaying the date range of the closures would be helpful - even if it changes, some kind of explanation or warning of a change would be great. Otherwise, I love the utilization of the closures and parklets.

Close Kirkwood year round, but allow delivery, transit, and emergency services to pass

East Kirkwood should permanently become a pedestrian and diner friendly street.

Maybe make the parklet barricades a little more attractive?

Tax payers paved the road; they should get to use it not private businesses.

Let Trojan Horse have better outdoor space. Create designated bike travel lanes, at least near Uptown please.

Common-use tables in these areas would also be nice for takeout, etc., people enjoying being out in the community.

I would love to see a dedicated accessible downtown shuttle for park and ride!

Please allow patrons to consume alcohol when seated in the parklets. It is QUITE stupid that at 11pm so many people at the Bishop have to move inside. 11pm is exactly when it starts to get much busier and more seating becomes needed.

More free parking downtown, parking is already hard to come by and taking those spaces by some of the most trafficked businesses is very inconvenient.

Drop the concept

Make the closer signs more personal to Bloomington such as get local schools and clubs to design them. Would make the space one of a kind.

Signage on surrounding roads indicating closures.

please keep bike access open, especially at Uptown (2022 was a big improvement over 2021). please limit uber/doordash abuse of crosswalks near bollards.

just what i wrote above about being able to park very briefly to pick up an order

Remove the homeless to other areas, with perks for them for food, shelter, support!

Stop the parklet program

Have one large, connected space blocked off on Kirkwood... not 2-3 smaller spaces.

More street fairs, vendors, activities, etc. The more pedestrian activity on Kirkwood the better! Get as many people in the streets as possible.

If you insist on closing Kirkwood, okey 8 only do it in the weekends!

Close Kirkwood and make it a destination and a perfect connection from Downtown Bloomington to the Sample Gates

beautification

More handicap parking closer to kirkwood so those with mobility issues can enjoy.

Shutdown all of downtown to cars

No

Don't do it

Put in some plants and trees. Change the surfacing of the street to make it more attractive.

See above.

Other than making it a permanent change, no.

I can handle it as a partial year closing but would hate for it to be permanent

I would enjoy Kirkwood being closed year round and some "beautification" happening on Kirkwood (trees in the middle of the street, whole street being paved over with bricks, etc)

I think aesthetic requirements should be a focus on both parklets and Kirkwood in 2023.

I would support shutting down that section of Kirkwood permanently to vehicles and expand the community experience of the area.

Charge the businesses (restaurants) who benefit most a monthly fee for the space they use.

Closing Kirkwood creates such a more pleasant downtown experience, not having to deal with noisy traffic passing by as we are trying to enjoy being outside.

More low-cost attractions needed. Free music? Fairs? Markets?

Just open it back up the whole year round.

Make the walking street permanent and add another parking garage to compensate. I'm a big fan of Strøget in Copenhagen; it would be a good model (also a great biking city). Keep some of the cross streets open if they aren't already, maybe 1 or 2 in each direction.

Stop closing Kirkwood to vehicular traffic.

If the program does continue, the city needs to make it easy for those with mobility issues to have the same access to downtown/Kirkwood as others. And please, measures need to be put in place to prevent cars illegally parking and blocking off the streets!! This is also something that needs to be implemented in the other downtown areas. It creates a hazard for vehicles hitting other vehicles as well as increases the likelihood that a car could accidentally hit a pedestrian or cyclist while navigating around an illegally parked car.

I suggest a closure of that portion of the street permanently and add a permanent pergola and/or seating areas. The temporary use of the "parking spaces" with road dividers are tacky. Make the entire area a walking destination with cover from the sun and other elements.

I think the program stopped too early, we still have many warm days in the fall.

No, it's great.

Make sections permanent; this would make it possible to make areas nicer vs dining on the street.

Cobblestone or other paving will enhance the appearance of Kirkwood as a walking street. Find nicer appearing barriers for the parklets; consider redrawing the streets to move traffic a little further away from the diners (ie, add a bike lane).

Provide a smallish "jitney" that could transport people up and down Kirkwood (for free) in a "step on/step off" model.

Make the Kirkwood closure permanent on some future effective date, with a long advance notice to businesses on when it will happen.

Consider more focused times for Kirkwood. Maybe a "local food week" or something to connect with lotus or taste of bloomington. Consider giving up some of parking spaces permanently for outdoor dining. But not too many considering the joke that is the 7-line and the hundred spaces it destroyed. And the orange traffic divider things are damn ugly. The concrete ones aren't much better. Honestly, the entire street scape of the courthouse square should be redone to match Kirkwood. It is long overdue.

Expanding taste of Bloomington or other themed block parties might help offset costs of closure.

Stop micromanaging commerce in Bloomington. You are not good at it.

I would be interested in some kind of beautification of the parklets if they stay. I love the idea of outdoor eating and more space for pedestrians, but the parklet dining areas are kind of an eyesore.

Stop

Special event only Kirkwood closures (on weekends only). Consider allowing peoples park be used by Village Deli, Lennies, etc instead.

Close it permanently. Turn kirkwood into a pedestrian road year round.

Please add a stop sign at 6th and Washington for both directions. It is a very dangerous intersection and you can't see cars coming without driving into the intersection. It's the last intersection before Kirkwood and would increase safety when the road is closed in warm months.

Go a step farther and close the cross streets to make Kirkwood pedestrian-only from Indiana to Walnut St.

Stop doing it

Better announcement/posted schedules to make parking and visiting restaurants more predictable.

Higher quality tables could be provided, and the city could do a better job maintaining the cleanliness of these areas.

More general public seating and tables available, not just restaurant specific seating.

There can be sustainably managed flower beds that still look appealing. Put "pollinator gardens" somewhere else besides Kirkwood Ave. They looked like they were abandoned. That kind of an aesthetic on an iconic street - a gateway between town and gown that is enjoyed and used by tourists and panhandlers and residents and employees and students and theater goers and commuters and joggers — well, it sends a message that there is no attention being paid to the experience of such patrons, that maybe Bloomington is a city in decline. As for the things that I am guessing are the parklets, How about doing something other than orange traffic barriers? Let the restaurants/retailers Design their parklets; show some creativity, individuality, design-thought, awareness of use and user. Let's do this!

If there are going to be closures, the flow of traffic & parking needs to be managed better. Traveling & parking downtown is incredibly confusing and stressful without adding more un-explained closures & such.

I think it would be great to have some of the remaining parking spaces reserved for those 65 and up. Not a tag thing, just an honor system thing (like when you drive in the two person + far left lane during rush hour.). Older people often have legitimate concerns about walking too far but they don't always have a handicap accessible tag. And for them, street closures might prove challenging.

Businesses in all of these areas were doing fine before covid in the building spaces they chose. They will be fine again there as well. If you are looking to make a change to the way the community does business, then you should look at Lake Street in Oak Park illinois. It was once an open thoroughfare but they closed it off and spent the money to make permanent cobblestone walkway that do not allow for any misconception of what the purpose of the area is- pedestrian traffic only. Bloomington is too populated with drivers and pedestrians that are not permanently here and the temporary closures allow entirely too much room for confusion to a great portion of the driving community. It's dangerous. You need to contact some civil engineers and have them design a plan on how to make that a permanently closed thoroughfare and also accommodate some sort of emergency vehicle service lane at the same time. Good luck with that.

Your other choice is to leave it alone and let things go back to normal. Zero Street closures, except for well-publicized festivals.

No

It would probably be useful for the city to train someone in the fundamentals of survey design. This survey is not well designed. I would hate to think any policy decisions would get made based on this survey.

More bike lanes and bike boulevards.

The street feels much more pleasant without aggressive drivers blocking crosswalks on Kirkwood. Make it permanent.

Colorful sun shades stretched across Kirkwood would be festive and practical.

See above

Could we offer free parking downtown? I know there's an issue that customers can't park near the business they want to go to because of the street closures and parklets and thus they have to pay for longer parking because of this. Wouldn't it be nice if more free parking was offered closer to those businesses? Or more free parking at all... I think it would definitely help people feel like they could come downtown and actually park and walk around and enjoy themselves without constantly worrying about if their meter is running out or how much they'll have to pay for the garage.

Don't continue to underserve the lower income population by blocking off streets.

Could parklet barriers have an additional 2-3 ft layer, to reduce traffic noise, visual impact, etc?

I absolutely dislike parking meters. It discourages me from using and browsing downtown

Closing it during school year as well.

I believe Kirkwood between Walnut and Indiana should be. Loses permanently. It should be made a fully pedestrian. Look at State Street in Madison WI as a model. A very long pedestrian street between the Capitol building and the university, very similar to Kirkwood but on a larger scale. This is what Kirkwood should be.

Make the parklets more permanent. Perhaps w/ concrete bumpers and not the bright orange/white construction ones.

There were a few incidents with aggressive folks on the streets interacting with customers eating—perhaps we could have some well-trained police on foot in the area (trained NOT to escalate, as we don't really need a shooting in that environment.)

Yes. That parking lot next to CVS could be beautiful if there was a new, modern condominium building there, or the like. It would allow a demographic that isn't currently living that close to Kirkwood and many parts of downtown

Make Kirkwood closure permanent so quality of amenities, can be improved. Or at the very least, establish a permanent schedule of closures to encourage long term investment.

Yes, stop finding ways to "help" and actually do the work to HELP local businesses!

Invest in better barriers that look nicer and are safer for the parklets and also the bike paths.

Maybe a shuttle service to some areas if that doesn't increase traffic too much and helps people out. :-)

Close more road. Leave roads closed longer and offer outdoor dining in even more locations. Other cities offer much more outdoor dining options.

Bloomington is embarrassingly behind the curve on this.

I understand that they barriers need to be big and ugly in order to try and keep motorists from slamming into diners. So, I don't really have a suggestion, unless you were to make these parklets permanent. That way the restaurants could create safe, more attractive spaces. Larger cities have made really good use of these types of spaces for outdoor dining.

I heard he businesses were being charged for this closing. That doesn't seem right. Do they have a say in this

Some street grading would help the seating areas. May not be feasible for water drainage.

Keep East Kirkwood open year round. It made sense during Covid restrictions but I feel it's more of a deterrent to downtown than a benefit now.

More tables and picnic benches

Get rid of parallel parking altogether on the square!

Make it permanent and then distractors will just get over it, but doing and then stopping it gives them an argument for not doing it at all

Improve appearance of some outdoor restaurant areas

Could be more decorative.

N/A

Keep the street open.

More parking

Smaller section or none

Perhaps signage for alternate street navigation.

Increased consistency on when things are open or closed, dedicated vehicle space for deliveries or pick-up, and temporary (and rainwater non-blocking) leveling solutions for accessibility from sidewalk/curb onto parklet/dining space.

See question 12

Only in summer

Close off sections permanently and add more landscaping and brickwork

Keep Kirkwood closed to cars for the whole year. It improves the downtown quality as a whole.

Other places downtown to also convert. Add more trees to decrease urban heat island effect

Minimize the time allowed for this program.

Make prettier barricade for use on active streets.

Do not close the street!!!

Ñò

Don't close the street, also please explain how it's going to work with all the pending construction for next summer?

More handicapped accessible areas

See above

Traffic engineers, less downtown development

Find a way to make deliveries easier and offer shuttles to the entrance of a restaurant (e.g. using golf carts) for those with mobility issues.

I wholly support Kirkwood and any other street closures/parklets that make Bloomington more pedestrian-friendly and easy to traverse without a vehicle. Our towns should be built first and foremost for people, not cars.

Take more opportunities to program it with pop-up stores/shops, musical events or art installations.

The summer closure of Kirkwood has been a wonderful idea. In the long term I would be in favor of a permanently converting this section of Kirkwood to a car-free zone (while continuing to allow cross traffic at the intersections)

Increase number of handicapped parking spaces near outside dining. Parking in garages is not practical if you are of limited mobility.(and fix the atrocious uneven, broken sidewalks that make pushing a wheelchair a nightmare!!!!)

Either keep it open for parking 24/7 or turn Kirkwood into a pedestrian only street (no cars).

If it becomes permanent, I'd suggest better rents or awnings.

This is tricky to accommodate, but it would be nice to have better shade systems, especially for when the sun is setting and blasting every west-facing customer directly in the eyeballs.

Make it permanent year round. Consistency of nice tables all the way down, Permanent lighting. Make it a destination always.

N/A

Keep the road open

Probably would have made more use if more of the restaurants provided some shade.

Don't do it, open the street so we can actually drive on it

Keep it closed to cars!!

Ensure that downtown businesses are consulted

Deliveries around the courthouse square that block lanes should be better organized. Restricting deliveries to times outside of heavy traffic, alleyways and/or limiting lanes used for deliveries would be critical improvements.

Stop closing roads

More attractive parklet barriers

Free golf cart shuttles for those with disabilities

Allow for food truck businesses to participate.

Yes, change the streets on Kirkwood for good. Make them a presentable and completely walkable street. Include decorative concrete or pavers. More trees. Only one lane in the middle that a city crew could come down. Or a delivery in the am. Make it something that is unique and new. Don't just leave it as like I'm dining on the road specifically. Make it new.

If setting up for a festival put stages in best locations even if the businesses need compensated or additional street closures are necessary for short period of time.

Make Icarus-free Kirkwood permanent and year round!

Maybe a moat and gate around campus to keep students in their place?

Make Kirkwood (between campus and the square) a permanent proper pedestrian zone. Add art, benches, planters, sitting walls, a small playground for children (maybe an art installation playground), and bike lanes (the bike lanes could also serve as a fire-lane, which is presumably required). Add a natural gas fire pit & warming area for busy nights in the cooler months. Allow the restaurants and bars to continue to have outdoor seating space, but now they can make it nicer since it is permanent. We have a vibrant downtown, nevertheless, a proper pedestrian will make it only better. Visit them in other cities in the States or overseas. Pedestrian zones make a big difference and improve business for all, even the businesses which are not directly on Kirkwood. Get people out of their cars and walking. Make some

convenient drop offs at the cross streets for people with low-mobility (eg my senior parents). It will cost millions and be worth every penny. It becomes another linear park. Probably donors would be willing to cover the art-work (eg. sculptures) and a small artsy playground. Have a vision which is about pedestrians and not convenient parking. Business which want to cater to cars, rather than pedestrians, can locate somewhere else. There are plenty of vacant storefronts and buildings.

Keep the roads open for the automobiles that pay for the roads. Kirkwood was not built to be a pedestrian walkway. That is what sidewalks are for.

More outdoor dining opportunities during colder months?

concrete barricades so that the parklets can be improved on structurally for ADA accessibility and weather.

the street closer are unsightly. it needs to be beautified on the citys part. bright orange street barricades and flashing signs are not sophisticated and so ugly. im so sick of those orange barricades all over downtown. Some cities paint those a better color or design and turn them into planters or something more visually appealing and less trashy

Keep the strings of lights going!

Go back to the old way. With lack of employees in most restaurants, it would get them back to somewhat normal coverage inside.

Improve pedestrian crossings on the car travel lanes across Kirkwood. Pedestrians don't look for cars and make ot difficult to travel across by car.

See Answer #12

See #12

leave the street open

Open road back up

Close it off permanently and make Kirkwood a pedestrian only street.

More street parking is needed. Parking garages are not always convenient for those of us who have difficulty walking longer distances.

Don't close Kirkwood.

All of Mirkwood closed to autos

I understand the thought behind this but implementation seems a bit off. Plus businesses having to pay a fee for lost parking meter recenue seems unfair.

Less cars more life!

close Kirkwood year round!

You need more parking in the area! Either a ground lot or a parking garage on Kirkwood!

If it is to continue could there be a way to implement some sort of parking for delivery drivers?

Do not do as it gives unfair advantages to certain businesses

Improve downtown and city public transportation. Eliminate all cars in the dow town area.

Free parking everywhere.

Kirkwood closed longer

Tables or seating unaffiliated with a specific restaurant

Don't close the street

I wish I did have an actual suggestion for improvement because outdoor dining has its positive aspects however the downtown landscape was not built with this in mind at the time. Maybe with all of these huge buildings going up now instead of putting all the empty space at the bottom where

the retail is supposed to go but it's just sending empty in most of them, perhaps make some of those so they can open up and be dining. This might make it appealing to restaurants and actually fill the space instead of having it sit empty.

Make the closure permanent

People adjust to the situation. Leave parking on streets. That's what they're for

Find ways that individuals with mobility challenges can be dropped off near the restaurants

Make dates more public

Leave the streets open.

Don't do it. It's not Bloomington

I personally would be more interested in roof top dining than street dining. Or at least provide more of a destination. What can downtown provide that other parts of town can not?

Do not do it

Leave the streets open to vehicular traffic.

Make it permanent, set up donated pianos like Denver.

Don't do construction at the same time!

If you decide to close it, then think about having one or 2 days a week where it is open so people with disabilities can visit their businesses or find a solution for parking, golf cart or shuttle for people who can't walk far or any disability that prevents them getting to the businesses.

Possibly putting up additional barriers on side streets to limit confusion and unsafe driving around the barriers.

More foot patrol police for this area and keep the vagrants moving... I do not know how to stop people from using drugs but I do feel like I shouldn't have to see it on my public streets. I don't allow this behavior in my home so I'm sure these businesses don't want to this behavior where they work.

Either make it permanent and appealing or stop doing it. Do the restaurants actually want it, it's harder to staff tables outside I'm sure. And what about non-restaurant businesses, does it help or hurt them? Less cars traveling the road means less visibility for store fronts, ruining their location.

Nope

Keep Kirkwood closed year round!

Minimize parking spaces affected

None

Check out 4th st. in Columbus, IN. Open to traffic sometimes or easily closed to traffic for concerts, festivals etc.

See above .. nothing should be mandatory. Give restaurants options.

Adding the bike lanes are the nuisance. They took away parking and narrowed driving lanes to unsafe measurements.

Close it all year! Buy more lighting. Own the outdoors.

Safety barriers don't look safe enough along walnut

Make it look nice and not cheap.

Make parking free

Make a few spots closest to parklets for carryout customers/10-minute parking.

Maybe just close Kirkwood for certain events/weekends

I think the parking barriers could be more attractive. Perhaps concrete instead of the orange plastic.

Put back in delivery parking AND monitor it. Too many people took advantage of the "free" parking in the delivery spots and it seems that park8ng enforcement did not enforce this.

Put more security officers and different officers from the nicks all the way to Theater

Fewer construction projects in surrounding roads during this time period!

De. Answer to 12. Also better communication and signage.

Keep the road open. Who monitors outdoor alcohol consumption? Your signs don't work.

Love the summer street closure on Kirkwood! Make the whole street pedestrian only.

Yes, see above. Close Kirkwood to all vehicle traffic for the entire year and allow more small businesses to set up small kiosks. Make it a pedestrian-friendly shopping area.

I wish Bloomington could find a solution for the homeless population. We were waiting to sit outside at Farm and 3 men were being very loud and abusive. I had my 82 year old mother with me and I was not comfortable sitting in the area with the commotion they were making going on. The owner of Farm said there was nothing he could do as it was a public space. As more people came, the owner finally came out and asked the men to move along but it was very uncomfortable. My family and I have enjoyed sitting outside along Kirkwood at many locations. However, different outbursts etc from the homeless population take away from the experience.

Close 4th street cater to the actual restaurant row. Cater to diversity not corporate chain stores and bars!!! Actually go and look what an unsafe chaotic mess you've made the nights on Kirkwood

Don't worry about Kirkwood Ave., fix the **** show that is now 7th street!

Increase awareness of parking availability in IU lots on evenings and weekends; this is a convenient option, and it does not seem to be well-advertised by the city.

Yes! During Covid many businesses with carry out business had "10 minute pick up only" parking spaces near them which made puck ups MUCH easier than trying to find an open space. The Parklet program and Kirkwood closures make it even harder to find parking downtown for a quick pick up. Reinstating those pick up only spots would be great.

Weekend closures are preferred if keeping.

Barriers around the parklets that aren't ugly bright orange! Better signage announcing street closures. I turned down kirkwood once and had to turn around bc it wasn't clear that the other end of the street was closed. I like the idea overall just think it could be refined.

Don't close the street. If you do, only for special festivals. The 4th street festival would be great on Kirkwood. More room for vendors and shoppers!

Free parking around the area.

No...I feel both are great programs and would like to see them continue in 2023.

Do away with the program. It was brought on because of the pandemic (appropriate at the time) however, that is behind us. Put the parking back into to the use they were intended for.

Don't do it. At least not unless there is a lot more parking very close.

N/A

If this were permanent, the pedestrian space could really be beautiful with trees and plantings and be a wonderful connection between downtown and the Indiana University campus.

As above, pay attention to disability access as a priority.

Please extend closures to 4th street between S Grant and S Dunn!

Stop making it harder to drive and park downtown. Just stop.

Make it a more permanent thing; implement better aesthetic choices for barriers and so forth. This is an increasingly common thing to do around the country, even pre pandemic, and such areas end up growing high customer density commercial districts over the 10-15 years after implementing. I bring in interstate visitors for my business frequently and that little chunk of the city IS the attraction for the evening.

What ask question 14

I heard you're extorting business for this closure. If you choose to shut down the road you shouldn't penalize businesses. Also, parking meters should have reduced pricing within a certain radius of Kirkwood

Both the sign at Kirkwood and Walnut and the orange barricades look too industrial. If Bloomington is going to continue either of these programs, we should put some money into aesthetic improvements. There is so much artistic talent in this town perhaps some king of competition could generate good ideas.

Leave the streets open!g

I think you need to provide some sort of parking pass for residence that are not students so that we are not struggling to pay for parking but maybe pay it just an annual parking fee

I have no suggestion other than DON'T DO IT AGAIN. Everyone complains about parking in Bloomington. This just gives more weight to that complaint. Plus, with all the new apartments being built, traffic is going to get a lot worse. Bloomington roads weren't built for the amount of traffic we have. All the medians for bikes and pedestrians that have been added contribute to traffic congestion, confusion, and frustration. Taking away more road space is just adding fuel to the fire.

Trollies or slow electronic commuter rail from far western suburbs to far eastern, and northern to southern to aid transportation, decrease need for parking, and reduce carbon footprint.

What if you took out parking on north side temp. and kept the road open. Put up a nice temp barrier. Orange barriers are so ugly!! Lakeland, FL has put up parklets that's fill a parking spot and have wooden slats and metal - very attractive.

Open it up to traffic permanently

Keep Kirkwood open and all parking spaces open

Do away with paid parking to be more downtown friendly. Stop allowing student housing as developers don't provide enough parking & they take valuable parking around downtown. Making it unsafe for females having to walk so far in dark. Safety should be priority not money!

Open the road to vehicles

Open the street

Increase parking

Remove this failed concept.

Maybe limiting it to 6 months and look at maybe including other streets.

Quit building apartments ...making it worse

Na

Give us the streets back for what they are intended for. Out vehicles.

I like the street being open but for us senior citizens it makes it more difficult to find parking, especially handicapped spaces. If you're going to close the street, please consider more handicapped spaces around the square

Make parking cheaper

YES STOP GENTRIFYING THE ***** TOWN U *****

If a business wants to expand their serving area by paying for a full day's worth of parking fees, then they can do that at any location in front of a business. Closing the street causes a disruption in the traffic, which is already an issue in the downtown area with shrinking streets.

Don't do it

We need more convenient parking!!

Open the streets and eat inside the restaurant like a normal human being

Remove the closures.

Stop closing Kirkwood

For additional inspiration, take a look at what Midland, Michigan, has done with their street closure and programming. Super fun for all ages and aesthetically appealing.

Redo the downtown park into outdoor dining to be shared by various restaurants and still maintain open road. Or close only half the road and leave open half for one way traffic and handicapped ONLY parking on the other side. Not open for deliveries still. Or at least not during store hours.

Widen every road in Bloomington for cars not people and bikes.

I think it should close to traffic year round, add shade, lights, plants and art installations

Honestly I would like it to be a permanent thing.

Perhaps if people would like to dine in the fresh air they could do carry out and then find a spot on the overtly homeless populated sidewalks and alleys to enjoy their meal with air.

Do not close again in 2023.

Maybe just make it a Friday-Sunday thing and have the roads open during the week

If keeping the outside eating make more parking available close to the business

Open kirkwood back up!

Those establishments functioned just fine for years when Kirkwood was open.

I wish it was year round!

It should be free to park.

Keep the outdoor seating

Perhaps more large planters

Make kirkwood walkable year round!

Keep Mirkwood open

If the change were made permanent, restaurants could really make their outdoor space special with planters and maybe canopies and such.

If wanting to keep the parklets, maybe try implementing Kirkwood as a one way street instead or allowing parking on one lane with the parklets on the other lane.

Return to the way things were prior to this asinine parklet program being implemented.

Cease and desist the program immediately

Open the street

Have the businesses police their own trash and debris

Get a new Mayor first along with Deputy Mayor and put Bloomington on the map again as a safe small town.

Don't close the block between Indiana and Dunn

Don't close the street. Have smaller areas for each restaurant.

Overly loud music at a couple of venues that undermines public's opportunity to enjoy dining at their selected venue. Please limit outside decibels.

I would prefer that Kirkwood is closed to traffic Fridays-Sundays. Open it up for normal traffic the rest of the week.

It was built for vehicles, leave it that way! We pay road taxes, the restaurants don't

The Mayor shouldn't be able to tax County people that doesn't live in the city!

Open the road

Do not close down Kirkwood to street traffic. Keep businesses accessible.

Make it permanent and make it look better.

please get rid of those stupid balls

Provide musical entertainment, Keep the homeless out, and clean the tables.

Open it up take down the baracades & figure out something else Covid is done.

N/A

I liked it. Gives more relaxing feel to Kirkwood.

Make it free

Weekend closures open weekdays

Not doing it again

loss of street parking for people not in the apartments doesn't make sense

Maybe close half the street and have a one way leading towards the circle, that way both motorists and people dining downtown can enjoy themselves

Use the road as a road

Just stop closing the streets. If restaurants want outdoor seating they can deal with it themselves.

Would love to see scheduled live music

Burlington, VT turned their Main Street into a full pedestrian area permanently. For safety near college bars and restaurants, I think this is great and would love to see this in Bloomington.

Please include more gender options in your surveys

I would love to see it continue into cooler months as it can be pleasant being in the cool fresh air. Perhaps heaters could be used as it gets cooler

Open it back up

Don't close Kirkwood.

Yes...on this survey why does it matter my age and my gender...demographic shouldn't matter unless we are catering to students.

Nope. Open Kirkwood up. I agree with safety concerns.

Quit closing down the streets. Give this town access to these restaurants. It somewhat made sense for COVID restrictions but is not really practical now. It makes the most sense to close Kirkwood during specific weekends. Homecoming, March Madness if IU is Sweet 16 or better, little 500.

Live music on weekend nights

Open up the roads

Dont close down any streets

Make it year-round!!

The only improvement would be to stop doing this.

Please think of people's safety like I mentioned above.

Yes, let the business owners decide. Politicians have zero skin in the game.

Keep city streets open for vehicle travel. We pay taxes for streets and sidewalks for travel, not private businesses to conduct activities.

Make it year round, so there's no confusion about how the streets work. Turning Kirkwood into a pedestrian mall makes for a great downtown experience. I'm not sure which ones, and it probably shouldn't be all of them, but it might be worth closing two or three of the cross streets as well, so it's truly a pedestrian mall.

Only parking for delivery drivers!

Make parking for delivery drivers

I would like to have increased visibility of the announcement message from City Government regarding when the Kirkwood closures will start and end.

Permanently close to car traffic. Make Kirkwood more walkable/ bike able/ visually appealing.

Make it permanent. Brick over the road and make it a strictly pedestrian area.

Better handicap access. Better delivery access.

No because I like how it was done.

Would it be too difficult to only do this on the weekends? Fri-sun?

Don't do it

Remove all of it so other people can enjoy close parking and you fire safety is where it needs to be.

Yes, stop this.

My critiques are mostly aesthetic. I realize that safety is priority 1 and the water-filled barriers (and the big illuminated construction sign at the west end of Kirkwood) are necessary. If the program is going to continue, I would like to see more thought put into how the barriers and other safety mitigations look to fit them into the landscape. They always looked temporary, making the whole downtown seem like an unfinished construction zone. Big reflective adhesive "parklet art" decals designed by artists that go on the barriers? Incentives to biz owners who build out and beautify their parklets? (You may already do that - some businesses did a great job making their parklets look cool. Orbit Room comes to mind.)

Don't close main roads. It's hard, especially for those from out of town, to get around and know where they are going.

make the whole square pedestrian only! not super practical but it would be an amazing addition to the town

Don't do it.

More free parking times... MORE parking availability, which closing streets does not do. Also creates congestion on smaller side streets not made for flowing traffic.

I'd love to see kirkwood become a permanent pedestrian mall.

See above.

Stop narrowing the streets!

Get rid of the program other than for incidental festivals of only a couple/few days that are specifically event driven; charge nothing at city parking lots if continuing the program while the program is underway to encourage people to come downtown despite the massive inconvenience. If running this program, at no cost to owners and businesses, allow dining and sales on all sidewalks adjacent to buildings regardless of clearance for pedestrian traffic.

Don't bring it back!

Make it permanent

Stop letting college students vote worthless mayors

End the program

Same as above - love this and wish it was more like CU Boulder - we love the ability to walk and eat and shop with no concerns about cars. But... those stupid scooters...

It'd be great to allow other local vendors to post up in open sections

Leave the street open!

Announce far in advance so can make plans when coming downtown.

Live Music and more festivals.... Even local school or business entertainment

Keep the streets open so they can be driven on and so emergency vehicles can access.

Less apartments in city. Keep the old B'town intact. It's losing its charm.

Monitor road construction on side streets before deciding to close.

Ask the businesses

Open up Kirkwood!!!!

Open the road back up to be used for what its was made for. Vehicles

City needs a parking garage or two to serve Kirkwood, which was once a central business district of its own. The current surface lots on 4th and on 6th would be good locations. Also, restrict public accommodations to businesses (as well as ARPA or other funding) to those businesses which practice COVID protocols, including employee vaccine requirements. You may know that one of the rewarded businesses (Uptown Cafe) has a business principal who is a huge anti-vaxer. Why are we giving them public funding, parklets and street closures to subsidize their danger to the public? Prohibit the street "igloos." They're no safer than any other indoor space. Worse, actually, since they're tiny enclosed spaces, regardless of "ventilation." Charge a daily fee to any business which "parks" their tables on the sidewalk, and restrict the size of the usable space. Prohibit business fencing on the sidewalk. Prohibit any exclusive use of tables placed by businesses on the sidewalk.

No

N/A

Get rid of the parklet program and provide ample parking and exposure for all of the other area businesses

No

STOP CATERING TO IU

Make it permanent. That way you can make it more visually pleasing.

Leave the street open Year round!!!!

discontinue program

More police present on busy weekends and remove the homeless. I'll visit the downtown area only if I feel safe.

Do not make this permanent

If you wish to expand outdoor seating for restaurants then consider eliminating some curbside parking for expanded room to expand outdoor seating without closing streets. It is not an either/r proposition--just need to think creative how to permit more outdoor dining opportunities while balancing the need of all other downtown businesses.

Don't close the street!

Keep the street open

Allow parking again, clean up trash and appearance

more delineation between bars or eating areas and relax or travel zones, movable planters with durable evergreens, maybe boxwood. Make it more park and less a tailgate zone, especially between dunn and grant. Low-rise vegetation creating "courtyards", some beer gardens do this well.

take the east lane out of Walnut and the West lane out of College and widen the sidewalks for more restaurants to be able to serve outside.

Host the program again! I don't think it should cost so much for businesses. To say the parking is lost revenue when there are 2-10 people sitting and dining in the parking space is odd--how did those people arrive? Clearly, people are parking somewhere, and it is still barely in the 4th Street garage.

If parklets return, more substantial and more attractive barriers are preferred.

Close Kirkwood permanently which would allow the City and the businesses to make long term investments in outdoor furniture and landscaping

I suspect that in the cases where there is conflict between need for parking, or delivery access, etc., we could find site-specific solutions.

A golf cart shuttle going up and down Kirkwood to assist in getting those with limited mobility to their destinations. Perhaps this could be parked, staffed, and available by text or phone call as soon as it is needed. Maybe local businesses benefiting from the increased seating could offset that cost.

If the homeless are gone; it's a great idea.

STOP THE STREET CLOSURES AROUND THE LIBRARY AND OTHER BUSINESSES.

Give a space or two like on Indiana ave. Keep Kirkwood open. Also provides better parking for downtown keeping the road open and traffic less congested

Keep Kirkwood closed year round and build walkable environment similar to other cities.

From this prior response: Why not put the sign up and let it be up for a week or two prior to closing the street? What I didn't like is that there was a large electronic sign board up for a day before the Kirkwood closure, but it was blank. AFTER it was closed off did the sign board announce that Kirkwood 'would close'... that's putting the cart before the horse and closing the street before the sign could announce it was going to happen. Why not put the sign up and let it be up for a week or two prior to closing the street?

Don't close the street

It is wonderful seeing people dining outside enjoying the fresh air, but the orange barricades are atrocious. And ruin the ambiance. Is there something

else that can be put up to keep diners safe without turning the view into one that resembles a construction zone?

Close all of Kirkwood. Having a single block open is confusing for pedestrians and drivers and doesn't offer much benefit.

The noise level coming from private roof parties is excessive and damages the downtown experience.

The entirety of the East-West portion of E Kirkwood from Walnut to Indiana should be closed to vehicle traffic. This would allow increased pedestrian safety, increased business opportunities, and predictability for vehicle operators. This would allow for an amazing pedestrian mall to take shape that would link the Sample Gates to the heart of Downtown Bloomington.

Increase marketing well before and leading up to closures.

If it is deemed that you must go ahead., Is it possible to only close half of Kirkwood and make one lane one way?

See above

Keep them down

monitor the loitering homeless more

do not block kirkwood ave. Parklets would be fine if the street is not blocked. You can't have both

Close Friday to Sunday like every big city wanting to promote their main street and downtown.

Discontinue the program.

free parking in summer for everyone

Do it year round.

Get rid of bums

Continue the program

I'd love to see the entire downtown square become pedestrian only once or twice a year as well (in addition to already established events like Lotis and the lighting ceremony)

Start the closure the week before little 500 and no sooner. Consider temporarily opening parts of the street for IU move in week. Continue to reopen on 1st Monday after Halloween.

Take into consideration that there are actually real people that do not live inside the Bloomington city limits that have to drive into town from that mythical place called "Monroe County".

Make them more pleasant for dining

Maybe make them more physically attractive.

Better improvement of appearance for parklets. Focus on using them for weekends or special events as a possibility.

If continued in 2023, restaurants prioritize the parklets when staffing serving areas. Many times we went to eat outside on Kirkwood to find the street blocked off with tables and chairs, but no staff serving that area for the restaurant.

KEEP THE STREET OPEN!

If this is done to benefit local restaurants and businesses, than all interested groups should get the same amount of space at no to minimal cost.

Make outdoor dining with a more permanent solution

Next time you write a survey, try writing questions objectively. I can tell your agenda just by looking at the wording of the questions and answers. The survey was designed to produce what you want. What do you dislike about

the parklet program? includes options: nothing other Notice how there is no option in the question: What do you like about the parklet program?
excludes options: nothing other seem fair to you?

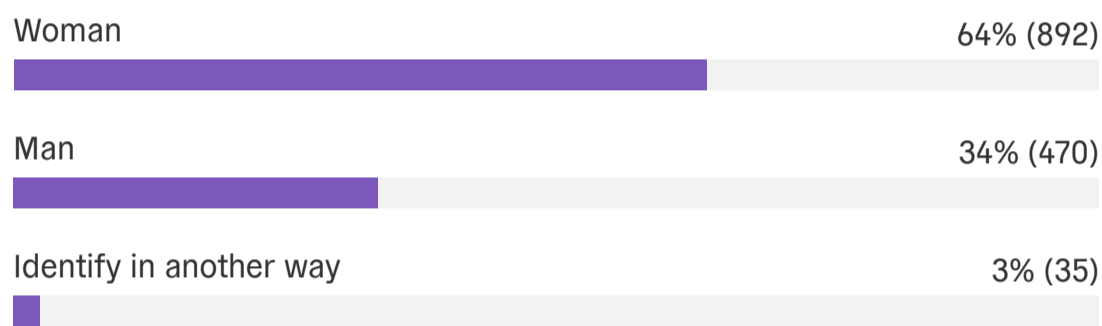
don't have a parklet

More lighting at night time

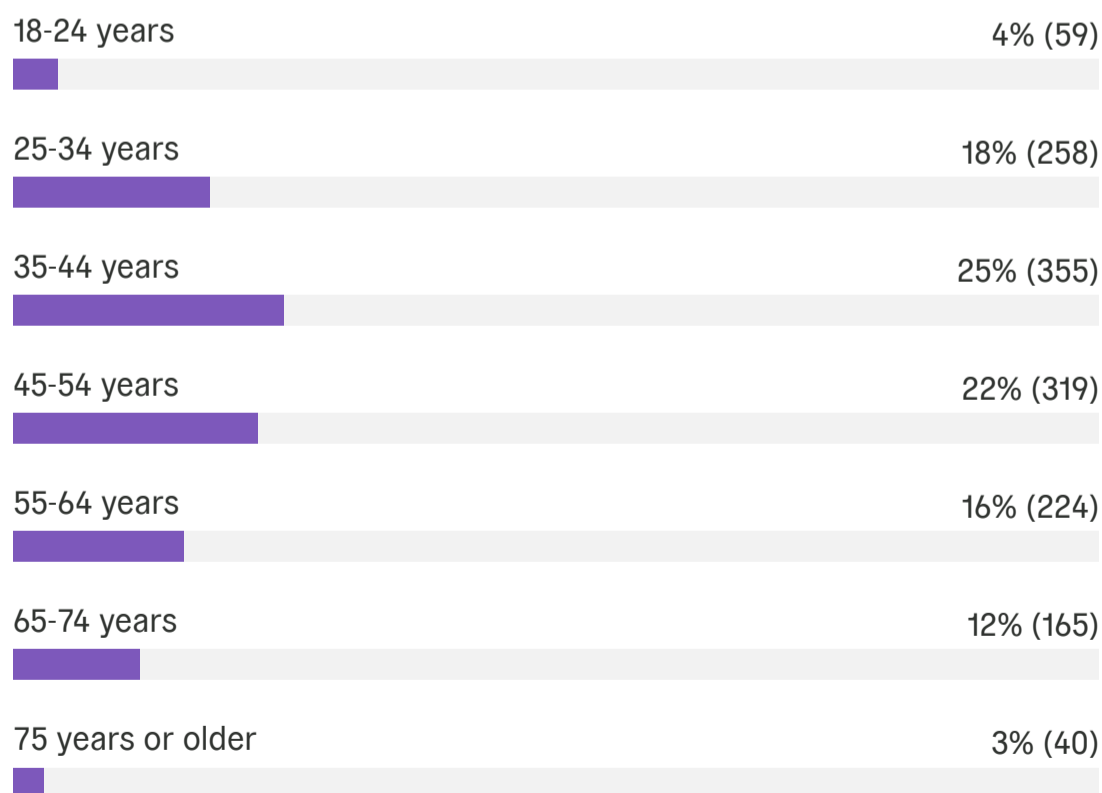
Love the string lights. Add additional benches/seating perhaps even schedule music so that there are areas to relax. Charlottesville downtown mall is a great example of an abundant use of a downtown area.

more semipermanent awning structures separate from that of individual businesses

What is your gender?



In which category is your age?



Parklet Program Feedback

The respondent's email (chris@finneyhospitality.com) was recorded on submission of this form.

I am a: *

Downtown business owner/operator

Downtown business employee

Other:

Are you a business operator in the downtown area of Bloomington? *

Yes

No

If yes, has your business participated in seasonal parklet usage or street closures? *

Yes, we use parklets

Yes, we expand onto Kirkwood when the street is closed

No, we do not choose to use parklets

No, but we are located on a street that closes

No, we do not utilize either

How many seasons has your business been a part of the parklet program? *

- 1 season
- 2+ seasons

If you have yet to participate in the parklet program, would you be interested in doing so? *

- Yes
- No
- Possibly, pending changes to the program
- Not applicable, already a part of the parklet program

Has your business benefitted from the implementation of the parklet program? *

- Yes
- No

If yes, please explain further:

Increased sales and visability

Would you like to see the parklet program continue in the 2023 season and beyond? *

Yes

No

What do you like most about the parklet program? *

Easy and affordable way to expand outdoor seating

What do you like least about the parklet program? *

Hard to maintain cleanliness and the orange barricades are ugly and charging for the concrete barriers was cost prohibitive

Please provide any suggestions you have related to the improvement and future development of the parklet program.

Regular cleaning, trash and leaves removed. Possibly create creative ways to use barricades for marketing.

Please provide any suggestions you have for the physical beautification of parklets.

Yes

This form was created inside of City of Bloomington, Indiana.

Google Forms

Parklet Program Feedback

The respondent's email (ed@buffalouies.com) was recorded on submission of this form.

I am a: *

Downtown business owner/operator

Downtown business employee

Other:

Are you a business operator in the downtown area of Bloomington? *

Yes

No

If yes, has your business participated in seasonal parklet usage or street closures? *

Yes, we use parklets

Yes, we expand onto Kirkwood when the street is closed

No, we do not choose to use parklets

No, but we are located on a street that closes

No, we do not utilize either

How many seasons has your business been a part of the parklet program? *

- 1 season
- 2+ seasons

If you have yet to participate in the parklet program, would you be interested in doing so? *

- Yes
- No
- Possibly, pending changes to the program
- Not applicable, already a part of the parklet program

Has your business benefitted from the implementation of the parklet program? *

- Yes
- No

If yes, please explain further:

it gives us more outdoor seating options

Would you like to see the parklet program continue in the 2023 season and beyond? *

Yes

No

What do you like most about the parklet program? *

Allows us to compete with the restaurants on kirkwood

What do you like least about the parklet program? *

Not all of the stanchions are filled correctly with water

Please provide any suggestions you have related to the improvement and future development of the parklet program.

Please provide any suggestions you have for the physical beautification of parklets.

This form was created inside of City of Bloomington, Indiana.

Google Forms

Parklet Program Feedback

The respondent's email (info@cafebaliusa.com) was recorded on submission of this form.

I am a: *

Downtown business owner/operator

Downtown business employee

Other:

Are you a business operator in the downtown area of Bloomington? *

Yes

No

If yes, has your business participated in seasonal parklet usage or street closures? *

Yes, we use parklets

Yes, we expand onto Kirkwood when the street is closed

No, we do not choose to use parklets

No, but we are located on a street that closes

No, we do not utilize either

How many seasons has your business been a part of the parklet program? *

- 1 season
- 2+ seasons

If you have yet to participate in the parklet program, would you be interested in doing so? *

- Yes
- No
- Possibly, pending changes to the program
- Not applicable, already a part of the parklet program

Has your business benefitted from the implementation of the parklet program? *

- Yes
- No

If yes, please explain further:

Attrack people to coming. Keep my business floating during hard time

Would you like to see the parklet program continue in the 2023 season and beyond? *

Yes

No

What do you like most about the parklet program? *

Attracted customer

What do you like least about the parklet program? *

Some jealous people hit the barrier on purpose

Please provide any suggestions you have related to the improvement and future development of the parklet program.

Please provide any suggestions you have for the physical beautification of parklets.

Just make sure that there is no calligraphy on barrier

This form was created inside of City of Bloomington, Indiana.

Google Forms

Comments from DBI Members throughout the downtown

Yes to Parklets and Closing Kirkwood

I am all for it. It can be permanent in my opinion. Look at Church Street in Burlington VT where UVM is located

I actually like being able to eat outside on nice days, but the lack of parking doesn't really affect me. I would presume that if I was a business on Kirkwood that wasn't a restaurant, the decrease in parking would be a problem

I am in full support of parklets and street closed to cars-for Kirkwood and/or 4th street. I only wish we could design them to be more interesting, plants, platforms, wooden enclosures as opposed to orange cones.

outdoor seating does help bring sales in:)

I personally really enjoy walking down the street without the traffic congestion. I'm sure the restaurants like having the extra experience to be offered. Touring other cities that have outdoor eating experiences is always a pleasure from the consumers point of view. I think it goes a long way in creating a more vibrant downtown, protects the pedestrians that don't have to dodge cars in an otherwise congested area and overall makes things look more appealing as you look down the street and view the table umbrellas and string lights.

If the city were to keep it closed permanently it would allow other stage events and entertainment venues to come to the area as well. Think Louisville. I've seen it in other places as well, this is just most prominent in my mind at the moment.

Thank you for asking

I'm not against it, but it was poorly advertised with the electronic street signs which went up *after* it was closed to read "street closing soon" ... it was already closed but with no warning.

My family enjoys outdoor dining downtown during the warm months. We park in the parking garage South of Fountain Square mall.

Yes, completely in support - this is a great program. we should narrow college and walnut and expand the sidewalks to accommodate the same thing there

I love the vibe that it gives us downtown retailers and other restaurants and I think that outweighs all...

Yes to both. It builds and demonstrates a sense of community. However, we are several blocks away so I don't know the particulars of the businesses in that area but I feel getting the community out and together is grand!!

We vote YES to Kirkwood closing Walnut to mid Block before Washington/ Grant to Indiana - with Dunn Street Open

I love having the outdoor street space for Uptown and The Farm, and I believe they maximize the street space.

I think they should open the streets up in front of Nick's, The Village Deli and Lennie's. I believe too many parking spaces are lost and sometimes it is a nightmare finding parking downtown.

I think the parklets and street closings are great! It gives us that hometown feel and allows our local restaurants to thrive!

Yes - to both but shortened time and other available parking

I think the Parklet situation served its purpose during a period of time when we needed them the most - Covid rules and regulations when restaurants were forced to have limited indoor seating. A continued parklet permits will hamper business and foot traffic when businesses need them the most. Perhaps I can see a situation where it's for the Summer months - Mid May to Early Aug.

If they had another parking lot/building, I'm all for the street being closed.

Neutral

I'm a hybrid. I don't know if that means I am **neutral**.

NO to Parklets and Closing Kirkwood

No, we are totally against closing any part of the street and taking away even more parking than the city has already taken away on 7th street. Regular customers stay away from area when street closed

No on both. Both were in response to COVID and are no longer needed for that purpose.

No. This initiative should go away because it unfairly benefits restaurants in certain locations and provide no benefit for others. It's misuse of public ROW, which instead should be used to benefit everyone, not just a few. The parklets are also often located right next to moving traffic and consequently underutilized because it's loud, uncomfortable, and somewhat unsafe.

NO!!!

Am no on both counts.

NO!!!

These were beneficial during covid;

- Allowed the restaurants to expand their dining area
- Made room for “social distancing”
- An attempt to help the restaurants be successful in a difficult time

The pandemic is behind us, no social distancing requirements, therefore the need for this no longer exists!

The parklets:

- Occupy parking spaces that should be used for their unintended use – parking
- The parklets occupy the space for an entire season, underutilized
- The parklet’s go mostly unused, based on a 24 hour or even an 18 hours schedule
- They are unsightly
- Empty space is not a positive reflection of downtown Bloomington
- The permit fee does not cover the cost
 - Loss parking revenue
 - Loss of business to downtown businesses

Kirkwood closing

Same benefits as above

- Allowed the restaurants to expand their dining area
- Made room for “social distancing”
- An attempt to help the restaurants be successful in a difficult time
 - again that time has passed
- Causes confusion and challenges for those driving from one location to another
- Unsightly, unsightly, unsightly
- More often than not, is underutilized
- Prohibits access to parking close to destination(s)
- Not the intended use
- The permit fee does not cover the cost
 - Loss parking revenue
 - Loss of business to downtown businesses

No on both. Both were in response to COVID and are no longer needed for that purpose.

I thought about this a lot. It was certainly something to do during the pandemic.

Now that we’re coming out of that, I believe it’s more important to preserve the on street parking to support all customers and businesses by multiple people throughout the day in parking spaces. We lost so many spaces on seventh Street. And so many spaces down Kirkwood. Oh, I know park in the parking Garages and walk two blocks. However, people don’t necessarily think that way.

They see lots and lots and lots of blocked parking spaces on the street, and it fosters the idea that it’s hard to park downtown. It is perception and perception becomes peoples truths.

Also, think of the retail store owners as well. If someone just wanted to go into JL Waters, To get one item for their camping trip, and they can maybe park a quarter or half block away, it makes a difference. Otherwise they'll just go to Dicks.

The meters were put in to create turnover parking for pop in stops to downtown. The parklets take away valuable parking spaces that support all downtown.

The bollards on Kirkwood prohibit parking near businesses which can be a deterrent to visiting the businesses in inclement weather and for the elderly and those who are mobility challenged. I've heard it said by those who support the closure of Kirkwood that it is nothing for people living in large cities to walk several blocks to restaurants, etc. People choose to live in small towns for many reasons including not having to walk long distances to businesses.

I am not convinced closing Kirkwood is a financial benefit to the restaurants and it doesn't seem fair provide restaurants, what they perceive as value, to the detriment of other businesses on Kirkwood.

Regarding the parklets, in addition to being ugly, the parklets take up valuable parking spots.

My response does not need to be kept confidential. I have been public with my opposition and will be again.

The parklets and the Kirkwood closure ordinance should not be repeated. It is wrong legally and is a disservice to many businesses.

The parklets and Kirkwood closure are effectively leases of public property and not done properly. More important, the parklets and the Kirkwood closure hurt other businesses. I am weary of the number of clients who complain how far they had to go to find a parking space when coming to our office. The City previously took parking from 7th St. with the ill-conceived/designed trail/protected bike path. The parklets take away more public parking spaces.

Several business owners on Kirkwood have told me they were damaged by the closure ordinance. Whether they will openly oppose the ordinance I do not know. Many were afraid to draw attention to themselves last year by opposing what was clearly a "done deal" by the council.

There is no justification for the City to subsidize a handful of restaurants generally, and certainly not at the expense of other restaurants and other businesses.

What subsidy or assistance has the City extended to other businesses whether downtown or elsewhere? I do mean the COVID assistance programs. Those were

available across the board and were a response to the pandemic. The parklets and the closure ordinance are by their nature limited and selective and discriminatory. Although justified last year as a response to the pandemic, they clearly were not. Not one term or provision of the parklet ordinance or the closure ordinance was a response to the pandemic. To claim the closure ordinance would kick start economic recovery downtown by providing a dining option outdoors for those who were reluctant to dine inside puts a spotlight on the City failure to address this issue for any restaurant not on Kirkwood. Where else did the City use public property to create dining space for a restaurant? What did the City do to mitigate the loss of parking and the restricted access to other restaurants on Kirkwood that resulted from the closure?

Not one person I have spoken to about the closure supported it because it was proper or appropriate for the City to do. The support for the closure was, sorry to say, entirely selfish. I was told "I like to sit outside to eat."

I am totally against the Kirkwood closing. It may be positive for 1, 2 or a few restaurants but it hurts all of the retail stores and other restaurants around the square and down Kirkwood. I am less concerned with the parklets because it doesn't effect an entire street or block but it does take up a few parking spaces. So to be fair they should go as well.

I am opposed to the parklets and closing Kirkwood. My office overlooked Kirkwood the past five years, and, after the road was closed, the homeless population hanging out in that area exploded. Daily, the area was used as their bathroom. While dining in the street at the various restaurants, we were frequently approached at the table and asked for money. The sidewalks were littered with sleeping bags and transients who were passed out. Let's keep Kirkwood open to traffic.

Yes Parklets

I originally didn't think it was a good idea but it sure seemed like the public liked it. I only worry that businesses struggle to get their goods to their stores. Is the city helping them with that situation?

Yes, to parklets- remains a nice outdoor option. I don't believe as necessary for Covid if I'm being honest just a nice vibe to the downtown area without disruption of traffic.

No Parklets

Don't like parklets.

Yes to Kirkwood Closing

Like Kirkwood closing on block with Uptown, FARM, Chubbies, Trojan Horse
Not with rest of street

No to Kirkwood Closing

We prefer an open street with parking.

No to Kirkwood closure- pedestrian traffic and dining don't justify the frustration of traffic flow, delivery issues, and parking spaces lost. I know many that stopped going downtown when streets were blocked off.

Also, some of us who have parklet spaces pay for the spaces for tables and I'm curious if the restaurants on Kirkwood have to pay to set up tables for using the blocked off street.

Deliveries a daily challenge with street closed.

Yes to Kirkwood Closing No to Parklets

I am a fan of the street closures on Kirkwood for outdoor dining. However, I have never sat in a parklet for dining. The proximity to the traffic, noise and exhaust is not appealing. I am concerned that one driver not paying attention will result in a terrible outcome if they careen into a parklet.

I like some of the street closures in certain areas. It creates a festive atmosphere. The various closures on Kirkwood this past year worked well. I thought it made for some fun outdoor dining. I did hear complaints that it creates issues with handicapped citizens. I think the parklets are a waste of parking spaces and I would never sit there due to the insane drivers in Bloomington. Especially on Walnut and College Ave.

No. This initiative should go away because it unfairly benefits restaurants in certain locations and provide no benefit for others. It's misuse of public ROW, which instead should be used to benefit everyone, not just a few. The parklets are also often located right next to moving traffic and consequently underutilized because it's loud, uncomfortable, and somewhat unsafe.



Chaz Mottinger <chaz.mottinger@bloomington.in.gov>

Reminder about Parklet removal: Oct 31

The Bishop Bar <info@thebishopbar.com>

Thu, Oct 27, 2022 at 4:12 PM

To: Chaz Mottinger <chaz.mottinger@bloomington.in.gov>

Np and thank you!

Feedback! I'm not sure we would participate in the program next year based on the general unpleasantries of sitting on Walnut. As you may know, last month someone plowed into one of our orange barriers fast enough to launch one picnic table 10 feet. We weren't open and no one was sitting there but even so that gives me some pause.

Aside from that incident, it's just loud on Walnut with cars whizzing by and trucks spewing fumes. At the same time, the rising price of the spaces is running contrary to the demand for outside seating as people are less covid-conscious. My tables were jam-packed during 2021 but not so much this year. Realistically, we probably did recoup the cost of the tables / spaces but the potential dangers I think more than negate that.

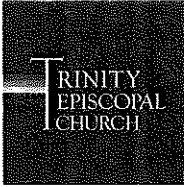
Lastly, the 10/11p limitation (I should probably know which it is) is also less than ideal for a bar.

Most of this is of course specific to the Bishop/Walnut St. I personally hope the program continues and if I were a restaurant on a quieter street I'd probably be more open to further participation. Still, it would probably only be worth it for places like Small Favors without existing outdoor seating.

Can we just make Walnut/College two way and slow down traffic? Should be easy haha!

Stephen

[Quoted text hidden]



111 South Grant Street
Bloomington, Indiana 47408
812.336.4466

January 25, 2023

Ms. Mary Catherine Carmichael
Deputy Mayor
City of Bloomington
401 N Morton St, Suite 210
Bloomington IN 47404

RECEIVED

JAN 31 2023

**OFFICE OF THE MAYOR
BLOOMINGTON, INDIANA**

Re: Consideration of closures on Kirkwood Street

Dear Deputy Mayor Carmichael:

Trinity Episcopal Church, at the corner of Grant and Kirkwood, has been challenged by many different obstacles to gaining entry to our building. The work in the alley and Grant St. effectively closed us off to traffic over the last two years creating a burden on our parishioners not experienced by other churches in the area. In addition, the summer closure of Kirkwood directly in front of the church means that parishioners cannot park there. Since we have no parking, other than 4 spaces in the alley, this parking is important to us. However, more importantly, our wheelchair ramp which provides direct access to the automatic door to the sanctuary cannot be accessed. Without the closure, parishioners can be let out of the car in or near the alley, use the ramp and automatic door to enter the church. While we do have wheelchair access through an elevator at the Grant Street entrance, it does not lead directly into the church.

Furthermore, the closure of Kirkwood creates a dangerous situation with the blockage of the alley between Trinity and Chipotle. The joint alley and the alley behind the church are often blocked with patrons picking up orders. This creates a fire hazard in that a fire truck could not get down the alley. This situation is entirely the result of the closure.

We would like the Council to consider beginning the closure on the other side of the alley between Trinity's east side and Chipotle. If this is not possible, then, at least, remove the closure on Sunday mornings from 7:00am-1:00pm. Thank you for considering our input.

Signed, Rector's Warden Susan Williams, for the Vestry of Trinity Episcopal Church

A handwritten signature in cursive script that reads 'Susan Williams'.



MEMO FROM COUNCIL OFFICE ON:

Ordinance 23-03 - To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles And Traffic" - Re: Amending Section 15.12.010 to remove seven stop intersections, to add six stop intersections, and to delete one 4-way stop intersection; Section 15.12.020 to add one yield intersection; Section 15.32.030 to delete angle parking on Fourth Street between College Avenue and Gentry Street; Section 15.32.080 to add no parking spaces on Duncan Drive, Nineteenth Street, and Strong Drive and to remove no parking spaces on Grant Street and Nineteenth Street; and Section 15.32.090 to add limited parking zones to Eighth Street

Synopsis

This ordinance amends Title 15, "Vehicles and Traffic," of the Bloomington Municipal Code and comes forth at the request of City staff, the Parking Commission, the Traffic Commission, and the Bicycle and Pedestrian Safety Commission. The ordinance makes the following changes:

- Removes seven stop intersections and adds six stop intersections.
- Removes one multistop intersection at Jefferson Street and Seventh Street.
- Adds one yield intersection.
- Removes angle parking on Fourth Street between College Avenue and Gentry Street.
- Adds no parking spaces on Duncan Drive, Nineteenth Street and Strong Drive; removes no parking spaces on Grant Street and Nineteenth Street.
- Adds limited parking zones on Eighth Street.

Relevant Materials

- [Ordinance 23-03](#)
- Memo from staff (including maps of affected locations)

Summary

Ordinance 23-02 proposes to amend various sections of Title 15 ("Vehicles and Traffic") of the Bloomington Municipal Code, accessible online here:

https://library.municode.com/in/bloomington/codes/code_of_ordinances?nodeId=TIT15_VETR. Title 15 contains codified traffic and parking controls for the city and is subject to frequent change and revision. Ordinance 23-03 represents the first Title 15 ordinance proposed in 2023. As the staff memo notes, the proposals are a result of public requests, feedback through commissions' recommendations, or changes stemming from temporary 180-day orders. The proposed changes would affect stop intersections, yield intersections, angle parking, no parking zones, and limited parking zones. The reasons for the proposed changes and maps of the affected locations are provided in the staff memo.

Contact

Amir Farshchi, Long Range Planner, 812-349-3423, farshchs@bloomington.in.gov (questions sent to Amir may be directed to other staff members as needed)

ORDINANCE 23-03

**TO AMEND TITLE 15 OF THE BLOOMINGTON MUNICIPAL CODE
ENTITLED "VEHICLES AND TRAFFIC" -**

Re: Amending Section 15.12.010 to remove seven stop intersections, to add six stop intersections, and to delete one 4-way stop intersection; Section 15.12.020 to add one yield intersection; Section 15.32.030 to delete angle parking on Fourth Street between College Avenue and Gentry Street; Section 15.32.080 to add no parking spaces on Duncan Drive, Nineteenth Street, and Strong Drive and to remove no parking spaces on Grant Street and Nineteenth Street; and Section 15.32.090 to add limited parking zones to Eighth Street

WHEREAS, the Traffic Commission, Parking Commission, Bicycle and Pedestrian Safety Commission, and City staff from the Engineering, Legal, Parking Services division of Public Works, Planning and Transportation, and Police departments recommend certain changes be made in Title 15 of the Bloomington Municipal Code entitled “Vehicles and Traffic”; and

NOW THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. Section 15.12.010, entitled “Stop Intersections” shall be amended by adding and deleting the following from Schedule A:

Delete

TRAFFIC ON	SHALL STOP FOR TRAFFIC ON
Abbey Lane	Winslow Road
Camby Court	Abbey Lane
Camby Lane	Abbey Lane
Durham Court	Abbey Lane
Durham Drive	Abbey Lane
Graham Drive	Bryan Street
Seventh Street	Hillsdale Drive

Add

TRAFFIC ON	SHALL STOP FOR TRAFFIC ON
Abby Lane	Winslow Road
Camby Court	Abby Lane
Camby Lane	Abby Lane
Durham Court	Abby Lane
Durham Drive	Abby Lane
Jefferson Street	Seventh Street

Section 15.12.010, entitled “Stop Intersections” shall be amended by deleting the following from Schedule B:

Delete:

Jefferson Street and Seventh Street	4-Way
-------------------------------------	-------

SECTION 2. Section 15.12.020, entitled “Yield Intersections,” shall be amended by adding the following to Schedule C:

Add

TRAFFIC ON	SHALL YIELD TO TRAFFIC ON
Graham Drive	Bryan Street

SECTION 3. Section 15.32.030, entitled “Angle parking,” shall be amended by deleting the following from Schedule L:

Delete

Street	From	To	Side of Street	Angle Type
Fourth Street	College Avenue	Gentry Street	North	Pull In

SECTION 4. Section 15.32.080, entitled “No Parking Zones,” shall be amended by adding and deleting the following from Schedule M:

Delete

Street	From	To	Side of Street	Time of Restriction
Grant Street	Eighteenth Street	Nineteenth Street	Both	Any Time
Nineteenth Street	Lincoln Street	Dunn Street	North	Any Time

Add

Street	From	To	Side of Street	Time of Restriction
Duncan Drive	Rockport Road	1000' west of South Rockport Road	Both	Any Time
Nineteenth Street	Lincoln Street	Grant Street	North	Any Time
Strong Drive	Allen St	1,140' south of Allen Street	West	Any Time
Strong Drive	1,100' south of Allen Street	2,330' south of Allen Street	East	Any Time

SECTION 5. Section 15.32.090, entitled “Limited Parking Zones,” shall be amended by adding the following to Schedule N:

Add

Street	From	To	Side of Street	Limit
Eighth Street	Walnut Street	First four spaces east of Walnut Street	South	9:00 p.m. to 4:00 a.m. only

SECTION 6. If any section, sentence or provision of this ordinance, or application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions or application of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

SECTION 7. This ordinance shall be in effect after its passage by the Common Council and approval of the Mayor, any required publication, and, as necessary, other promulgation in accordance with the law.

PASSED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2023.

SUE SGAMBELLURI, President
Bloomington Common Council

ATTEST:

NICOLE BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2023.

NICOLE BOLDEN, Clerk,
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2023.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

This ordinance amends Title 15, "Vehicles and Traffic," of the Bloomington Municipal Code and comes forth at the request of City staff, the Parking Commission, the Traffic Commission, and the Bicycle and Pedestrian Safety Commission. The ordinance makes the following changes:

- Removes seven stop intersections and adds six stop intersections.
- Removes one multistop intersection at Jefferson Street and Seventh Street.
- Adds one yield intersection.
- Removes angle parking on Fourth Street between College Avenue and Gentry Street.
- Adds no parking spaces on Duncan Drive, Nineteenth Street and Strong Drive; removes no parking spaces on Grant Street and Nineteenth Street.
- Adds limited parking zones on Eighth Street.

MEMORANDUM

To: Common Council

From: Andrew Cibor, City Engineer; Raye Ann Cox, Parking Enforcement Manager; Amir Farshchi, Long Range Planner; Gabriel Holbrow, Zoning Planner; Neil Kopper, Senior Project Engineer; Scott Oldham, Captain of Operations of Bloomington Police Department; Aleksandrina Pratt, Assistant City Attorney; Ryan Robling, Planning Services Manager; Beth Rosenbarger, Assistant Director of Planning and Transportation; Michelle Wahl, Parking Services Director

Date: February 7, 2023

Re: Proposed Ordinance # 23-03, to make various amendments to Title 15

City staff from the Engineering, Legal, Parking Services division of Public Works, Planning and Transportation, and Police departments have compiled a number of recommendations for changes to Title 15, the vehicle and traffic section of the Bloomington Municipal Code. Proposed ordinance # 23-03 would make these changes.

The current Title 15 of the Bloomington Municipal Code may be found at:
https://library.municode.com/in/bloomington/codes/code_of_ordinances?nodeId=TIT15VETR

The proposed changes will address issues that have come to staff's attention through public requests, commissions' recommendations, or 180-day orders. The maps attached to this memo indicate the proposed changes.

The ordinance includes the following changes:

- Section 1:
 - Removes one stop intersection on W. Graham Drive at S. Bryan Street and replaces it with one yield intersection. *Neil Kopper, Andrew Cibor*
 - Removes two stop intersections on E. 7th Street at N. Hillsdale Drive and replaces them with two yield intersections. 7th Street is offset and intersects Hillsdale Drive twice; updating both locations. Yield signage for 7th Street and Hillsdale Drive is already codified. *Neil Kopper, Andrew Cibor*
 - Removes one 4-Way stop intersection on N. Jefferson Street at E. 7th Street and adds a two-way intersection such that traffic on Jefferson Street stops for traffic on E. 7th Street. *Neil Kopper, Andrew Cibor*
 - The above requests originated from implementation of improved Neighborhood Greenway routes on both E. 7th Street and W. Graham Drive per the City's adopted Transportation Plan.
 - Neighborhood Greenways use traffic calming and other tools to create convenient and high comfort walking and bicycling connections along

shared-space streets. In order to increase the convenience and priority of these routes, existing traffic controls are analyzed and updated where appropriate.

- On these routes, there are two intersections where stop signs can be updated to yield signs and one intersection where an all-way stop can be converted to a two-way stop.
 - The Bicycle and Pedestrian Safety Commission reviewed and approved the design of the Neighborhood Greenway project on November 8, 2021.
 - The 180-Day Order 22-02 for this case was issued on August 15, 2022 for construction.
- Changing the wrong spelling of Abbey Lane to Abby Lane. *Ryan Robling*
 - Changing the wrong spelling, Abbey Lane, to the correct spelling, Abby Lane in Title 15.
- Section 2:
 - Adds one yield intersection on W. Graham Drive at S. Bryan Street: summary included in Section 1. *Neil Kopper, Andrew Cibor*
 - Adds one yield intersection on E. 7th Street at N. Hillsdale Drive: summary included in Section 1. *Neil Kopper, Andrew Cibor*
- Section 3:
 - Deletes angle parking on W. 4th Street from S. College Avenue to S. Gentry Street, north side of the street from Schedule L. *Amir Farshchi, Raye Ann Cox*
 - The change effectively codifies the existing condition and what people practiced for years.
 - There are parking meters but there are no angle parking spaces. Parking meters are already codified under section 15.40.010 as 100-400 W 4th St.
- Section 4:
 - Removes no parking spaces on both sides of N. Grant Street from E. 18th Street to E. 19th Street. *Gabriel Holbrow, Beth Rosenbarger, Ryan Robling*
 - The proposal is based on a redevelopment in this area. The redevelopment project included on-street parking.
 - It is related to the approved development at 420 East 19th Street.
 - The site plan was approved by the Plan Commission as SP-38-22 on September 12, 2022.
 - Removes no parking spaces on the north side of E. 19th Street from N. Lincoln Street to N. Dunn Street; adds no parking spaces on the north side of E. 19th Street from N. Lincoln Street to N. Grant Street. *Gabriel Holbrow, Beth Rosenbarger, Ryan Robling*
 - The north side of E. 19th Street from Grant Street to N. Dunn Street is currently listed as no-parking. The purpose is to change it to allow on-street parking on the north side so to have on-street parking on both sides of the street on this block.
 - The proposal is based on a redevelopment in this area. The redevelopment project included on-street parking.

- The proposal is related to the development on the north side of E. 19th and Dunn, approved by the Plan Commission as SP-40-21 on December 13, 2021. Grading permit C22-001 was approved on April 21, 2021.
 - Adds no parking spaces on both sides of W. Duncan Drive. *Neil Kopper*
 - The width of the existing street is 12 to 13 feet. The existing street has signs that restrict parking at certain times of the day, but those existing signs are not codified.
 - It is not possible to park a motor vehicle on the roadway without violating the following code: BMC 15.32.140 - Obstructing traffic. “No person shall park any vehicle upon a street, other than an alley, in such a manner or under such conditions as to have available less than twelve feet of the width of the roadway for free movement of vehicular traffic.”
 - The case 22-08 for this item was presented to the Traffic Commission on September 28, 2022, the Commission voted to forward the Title 15 changes to the Common Council with a positive recommendation.
 - Adds no parking spaces on Strong Drive south of W. Allen Street. *Andrew Cibor, Neil Kopper*
 - Engineering Department staff received requests from the public via uReport and from Catalent Biologics for on-street parking restrictions along Strong Drive.
 - The adjacent properties are occupied by Catalent and Organized Living, and the facility has significant truck activity.
 - There is significant on-street parking on Strong Drive which can limit maneuverability especially for the large trucks and the on-street parking limits sight distance at the driveway access points.
 - Currently on-street parking is permitted on both sides of Strong Drive except in locations that have yellow painted curb (some yellow curb exists at the southern terminus).
 - This proposal maximizes on-street parking capacity while providing sufficient space for truck maneuverability and improved sight lines at driveway access points.
 - The case 22-10 for this item was presented to the Traffic Commission on August 24, 2022, the Commission voted to forward the Title 15 changes to the Common Council with a positive recommendation.
- Section 5:
 - Adds limited parking zones on the south side of E. 8th Street, from N. Walnut Street to the first four spaces east of N. Walnut Street, 9:00 p.m. to 4:00 a.m. only. *Scott Oldham*
 - The Bloomington Police Department requested these changes.

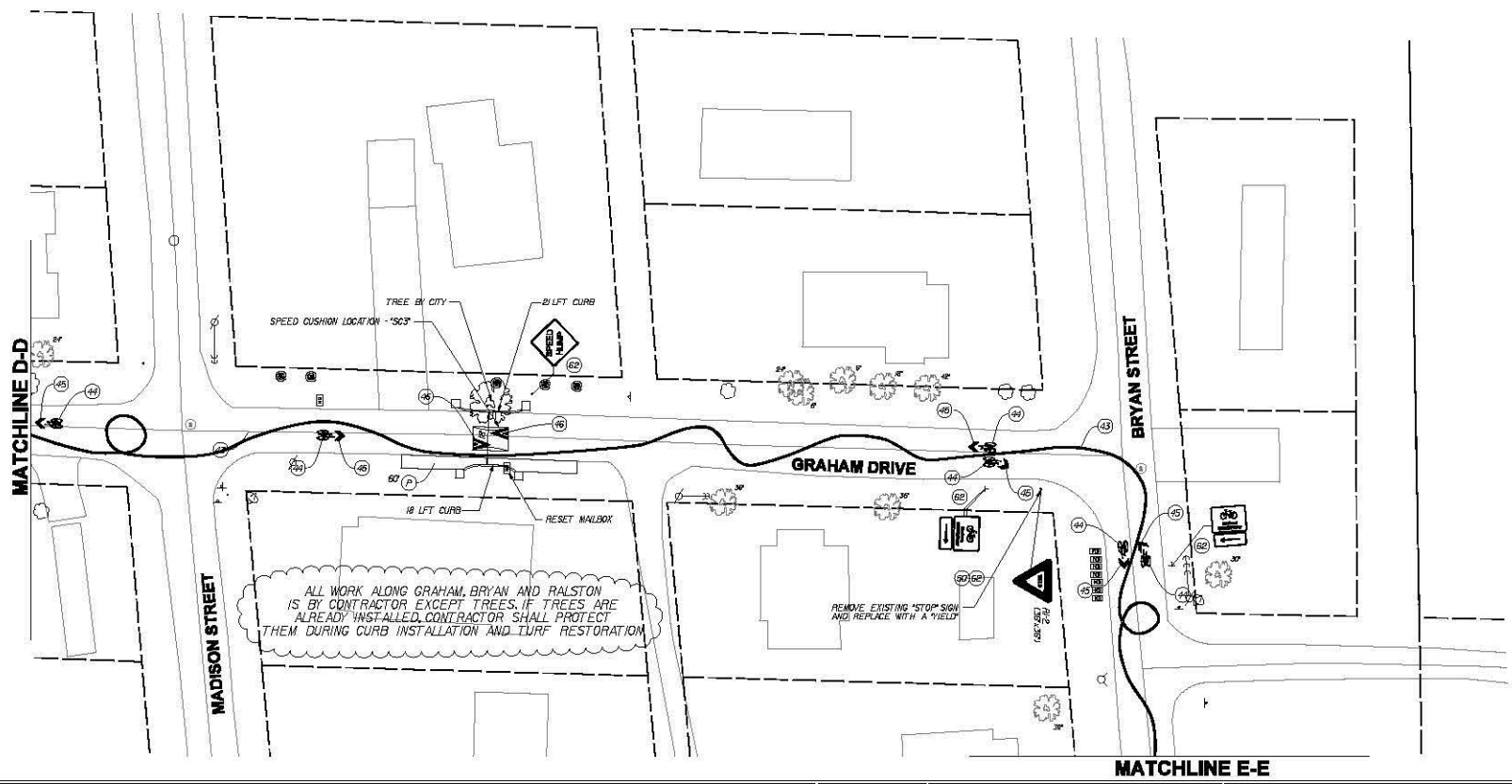
The anticipated fiscal impact of these proposed changes is minimal. Generally, anticipated expenditures to add or remove signs, poles, etc. related to the Title 15 changes proposed, are covered by the 2023 Budget.

Section 1

Removing one stop intersection on W. Graham Drive at S. Bryan Street and replacing it with one yield intersection



W Graham Dr & S Bryan St
Intersection



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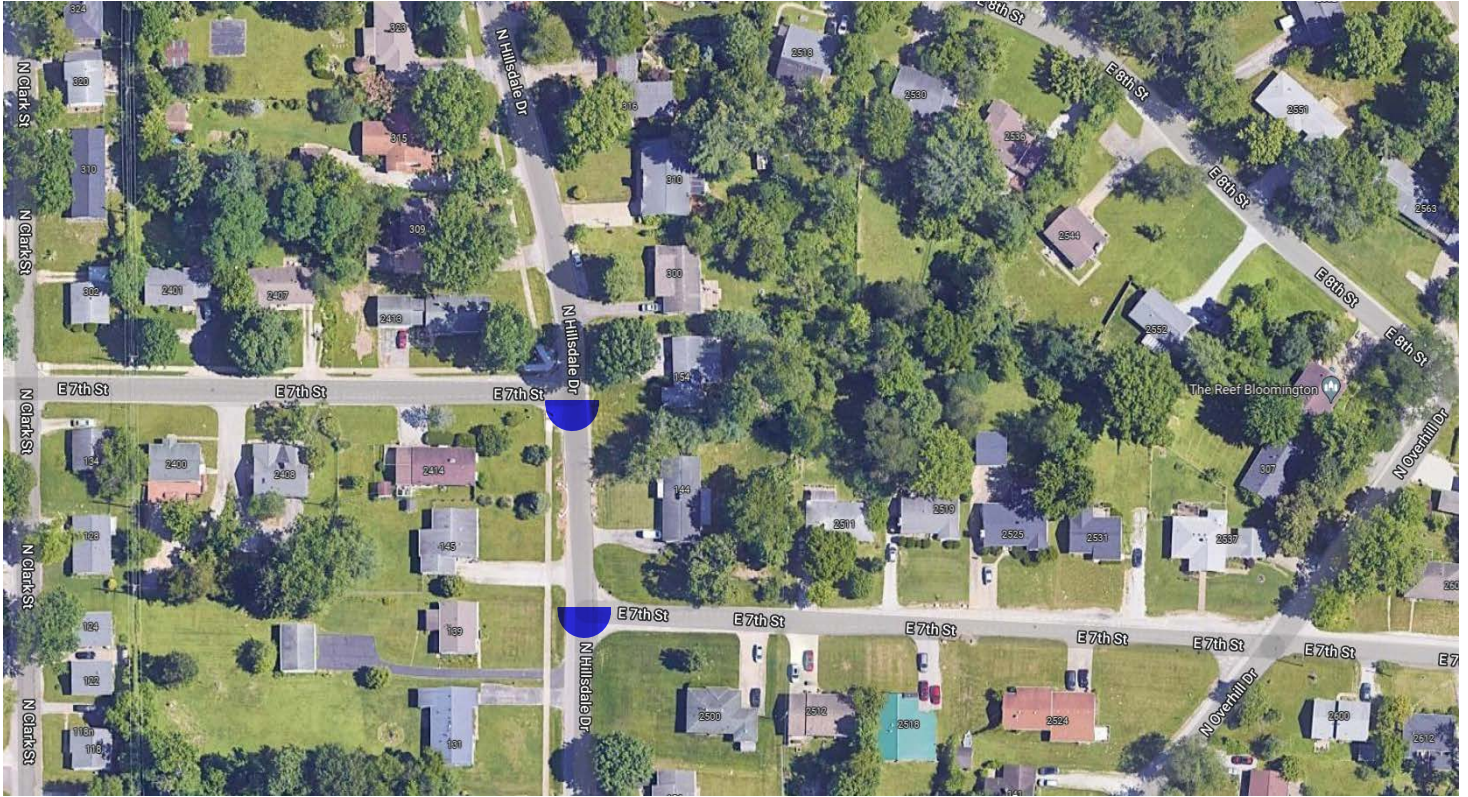
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CITY OF BLOOMINGTON
DEPARTMENT OF ENGINEERING
NEIGHBORHOOD GREENWAY PLANS
WEST GRAHAM DRIVE

HORIZONTAL SCALE	ERICSS PROJECT NUMBER
1" = 20'-0"	240
VERTICAL SCALE	
	SHEET NUMBER
	19

Section 1

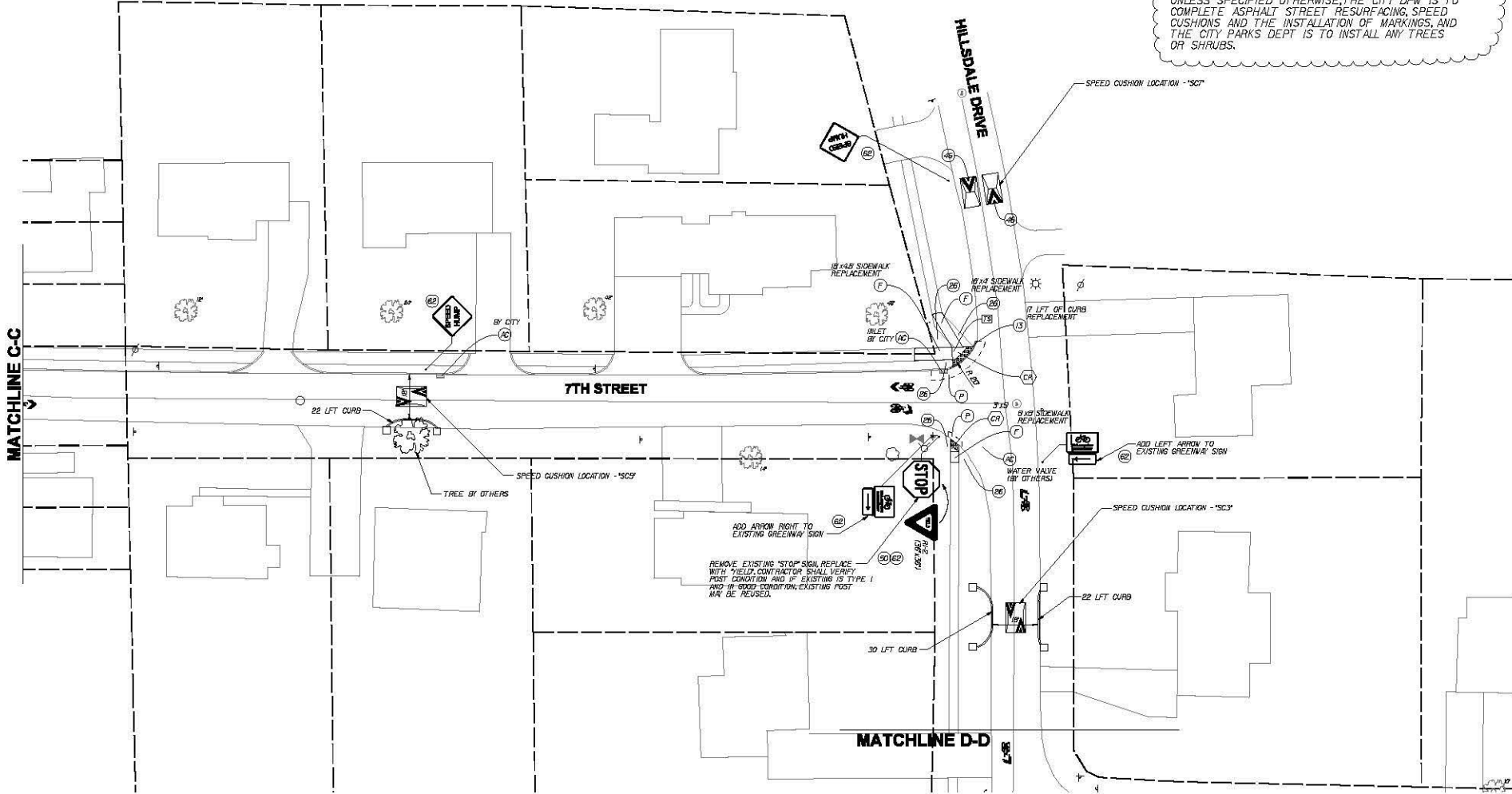
Removing two stop intersections on E. 7th Street at N. Hillsdale Drive and replacing them with two yield intersections. 7th Street is offset and intersects Hillsdale Drive twice; updating both locations.



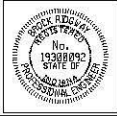
WORK ON 7TH STREET CORRIDOR IS TO BE PARTIALLY COMPLETED BY THE CITY.

UNLESS SPECIFIED OTHERWISE THE CONTRACTOR IS TO COMPLETE ANY REQUIRED CONCRETE WORK INCLUDING CURBS, SIDEWALKS, AND RAMPS; PATCHING ALONG CURBS, LIMESTONE BLOCK, TURF RESTORATION, AND SIGNAGE.

UNLESS SPECIFIED OTHERWISE, THE CITY DPW IS TO COMPLETE ASPHALT STREET RESURFACING, SPEED CUSHIONS AND THE INSTALLATION OF MARKINGS, AND THE CITY PARKS DEPT IS TO INSTALL ANY TREES OR SHRUBS.



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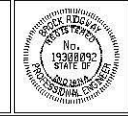
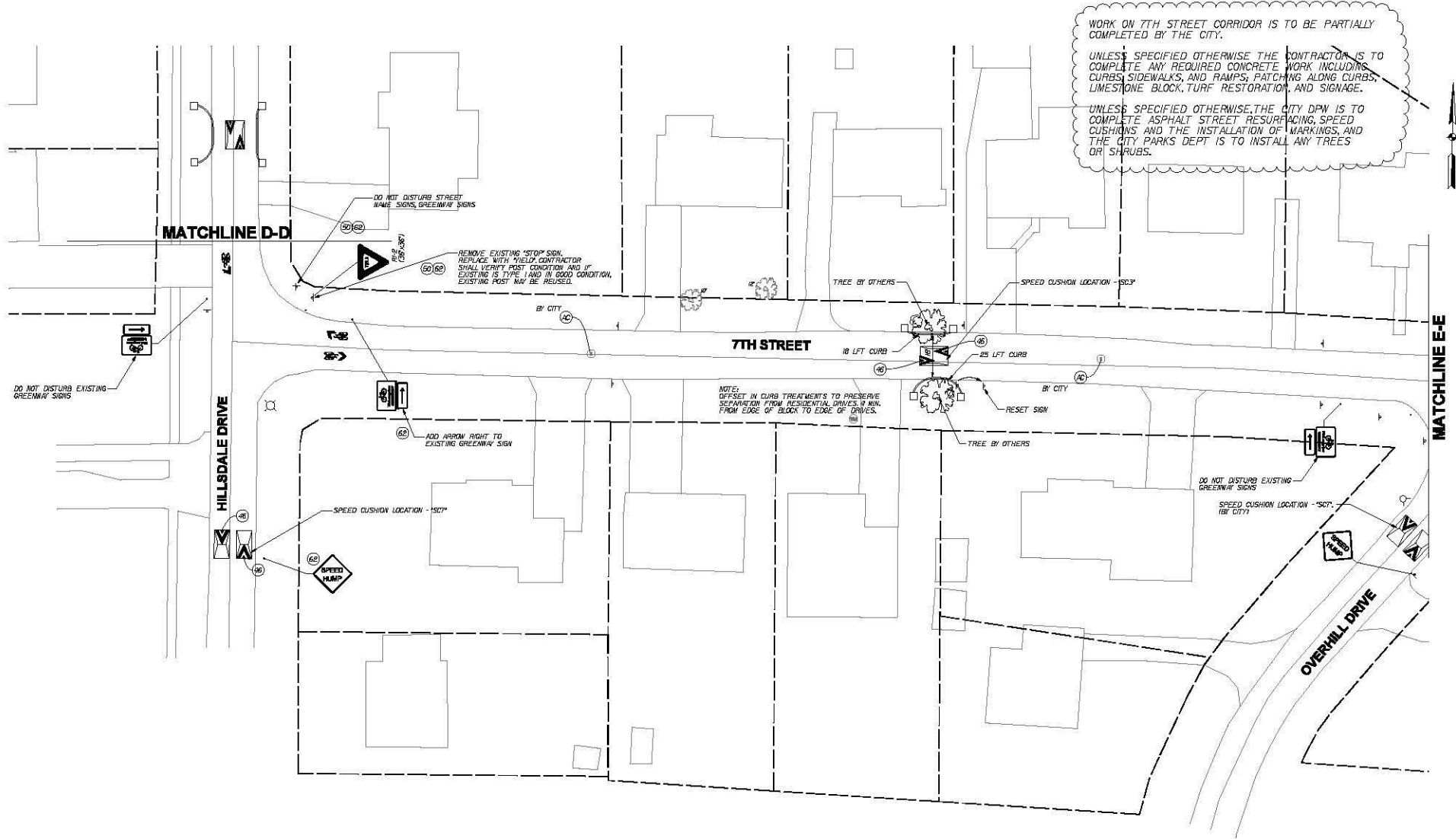
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**CITY OF BLOOMINGTON
DEPARTMENT OF ENGINEERING**

**NEIGHBORHOOD GREENWAY PLANS
EAST 7TH STREET**

HORIZONTAL SCALE	ERIC'S PROJECT NUMBER
1" = 20'-0"	240
VERTICAL SCALE	
	SHEET NUMBER
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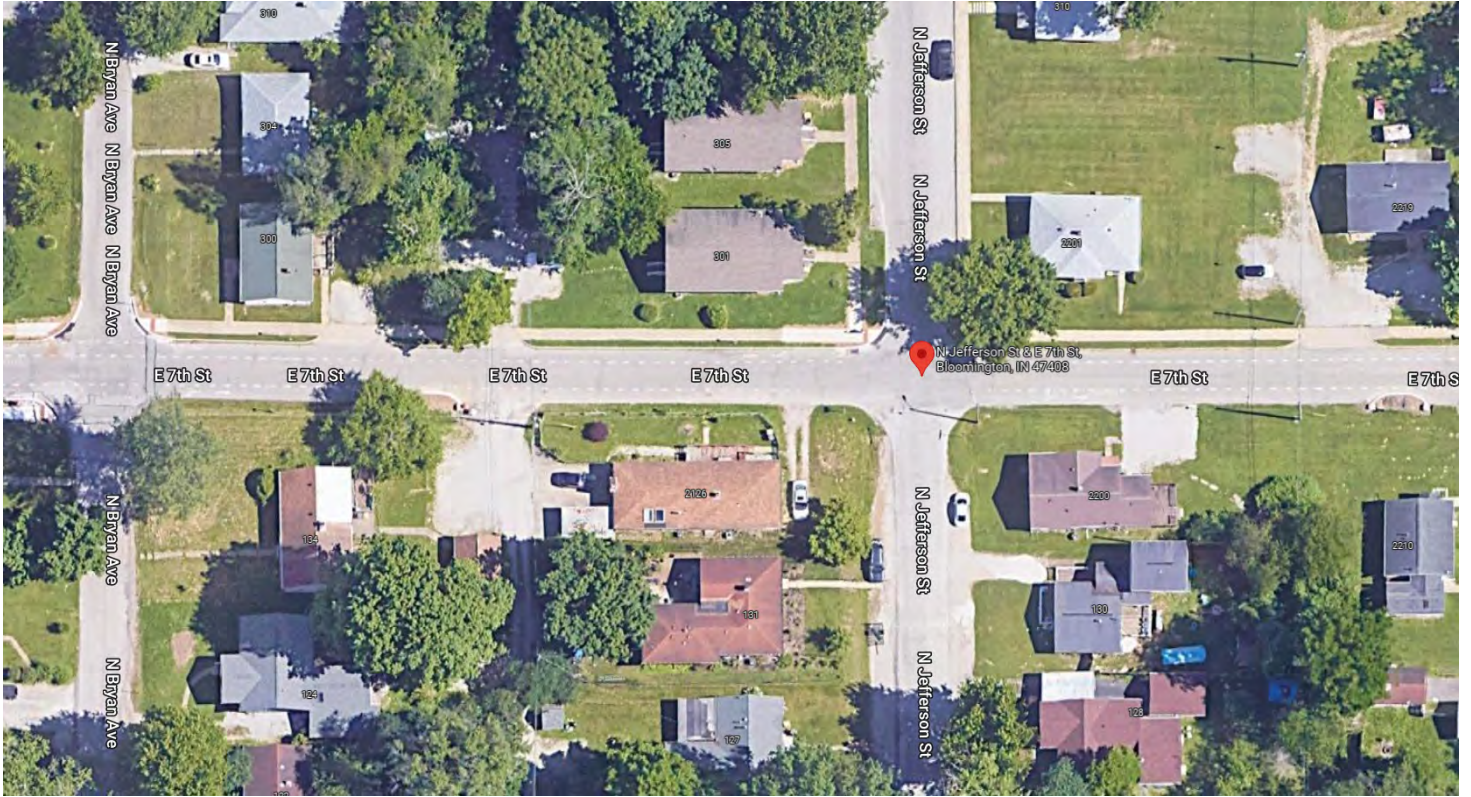
**CITY OF BLOOMINGTON
DEPARTMENT OF ENGINEERING**

**NEIGHBORHOOD GREENWAY PLANS
EAST 7TH STREET**

HORIZONTAL SCALE	ERIC'S PROJECT NUMBER
1" = 20'-0"	240
VERTICAL SCALE	
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Section 1

Removing one 4-Way stop intersection on N. Jefferson Street at E. 7th Street and adding a two-way intersection such that traffic on Jefferson Street stops for traffic on 7th Street

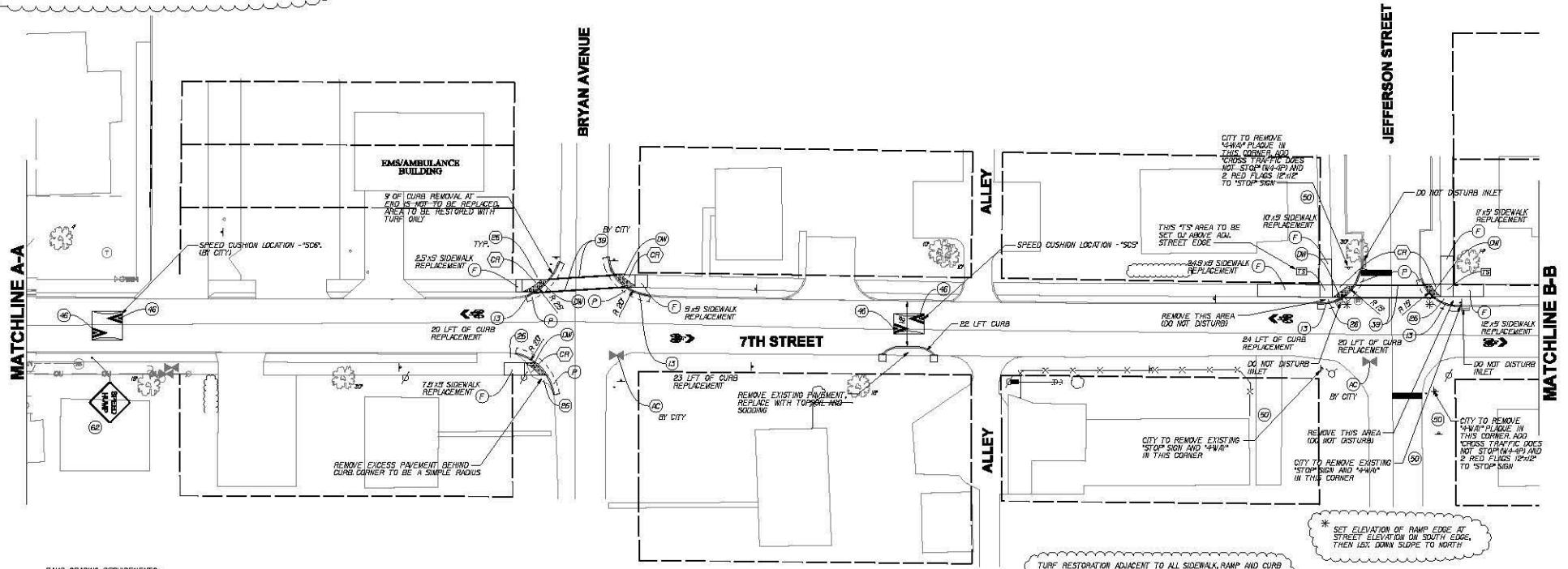


N Jefferson St & E 7th St
Intersection

WORK ON 7TH STREET CORRIDOR IS TO BE PARTIALLY COMPLETED BY THE CITY.

UNLESS SPECIFIED OTHERWISE THE CONTRACTOR IS TO COMPLETE ANY REQUIRED CONCRETE WORK INCLUDING CURBS, SIDEWALKS, AND RAMPS; PATCHING ALONG CURBS, LIMESTONE BLOCK, TURF RESTORATION, AND SIGNAGE.

UNLESS SPECIFIED OTHERWISE, THE CITY DPW IS TO COMPLETE ASPHALT STREET RESURFACING, SPEED CUSHIONS AND THE INSTALLATION OF MARKINGS, AND THE CITY PARKS DEPT IS TO INSTALL ANY TREES OR SHRUBS.

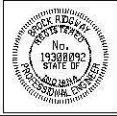


- RAMP GRADING REQUIREMENTS:**
1. CONTRACTOR IS TO FIELD VERIFY EXISTING ELEVATIONS AND CROSS SLOPES, THEN SET A PROPOSED STRIKELINE FOR RAMPS AND TURNING SPACE AND SIDEWALK GRADES.
 2. RAMP REPLACEMENTS WILL CONSIST OF A SECTION OF SIDEWALK TO BE REPLACED THEN A CURB RAMP WITH DETECTABLE WARNING PLATE.
 3. THE SECTION OF SIDEWALK WILL PROVIDE TRANSITION FROM EXISTING CROSS SLOPE TO A MAX 2% SLOPE AT THE BEGINNING OF RAMP.
 4. RAMPS WILL TRANSITION DOWN TO MEET EXISTING EDGE OF STREET AT ITS HIGHEST EXISTING ELEVATION (TYPICALLY SOUTH CORNER OF RAMP WHERE MEETS STREET). RAMP TO HAVE A MAX 2% CROSS SLOPE SO IT IS LEVEL TO THE OTHER CORNER WILL BE ABOVE EXISTING EDGE OF STREET.
 5. CONTRACTOR SHALL HAVE FORMS APPROVED BY CITY BEFORE POURING CURB RAMPS TO ENSURE COMPATIBILITY WITH FUTURE STREET RESURFACING.

* SET ELEVATION OF RAMP EDGE AT STREET ELEVATION ON SOUTH EDGE, THEN LET DOWN SLOPE TO NORTH

TURF RESTORATION ADJACENT TO ALL SIDEWALK, RAMP AND CURB WORK SHALL BE SOODING ON UP TO 6" OF TOPSOIL AS NEEDED TO RE-ESTABLISH SMOOTH FINISH GRADE. EXCESS AMOUNTS ARE TO BE REMOVED AND SHALL NOT BE USED TO TAKE THE PLACE OF TOPSOIL.

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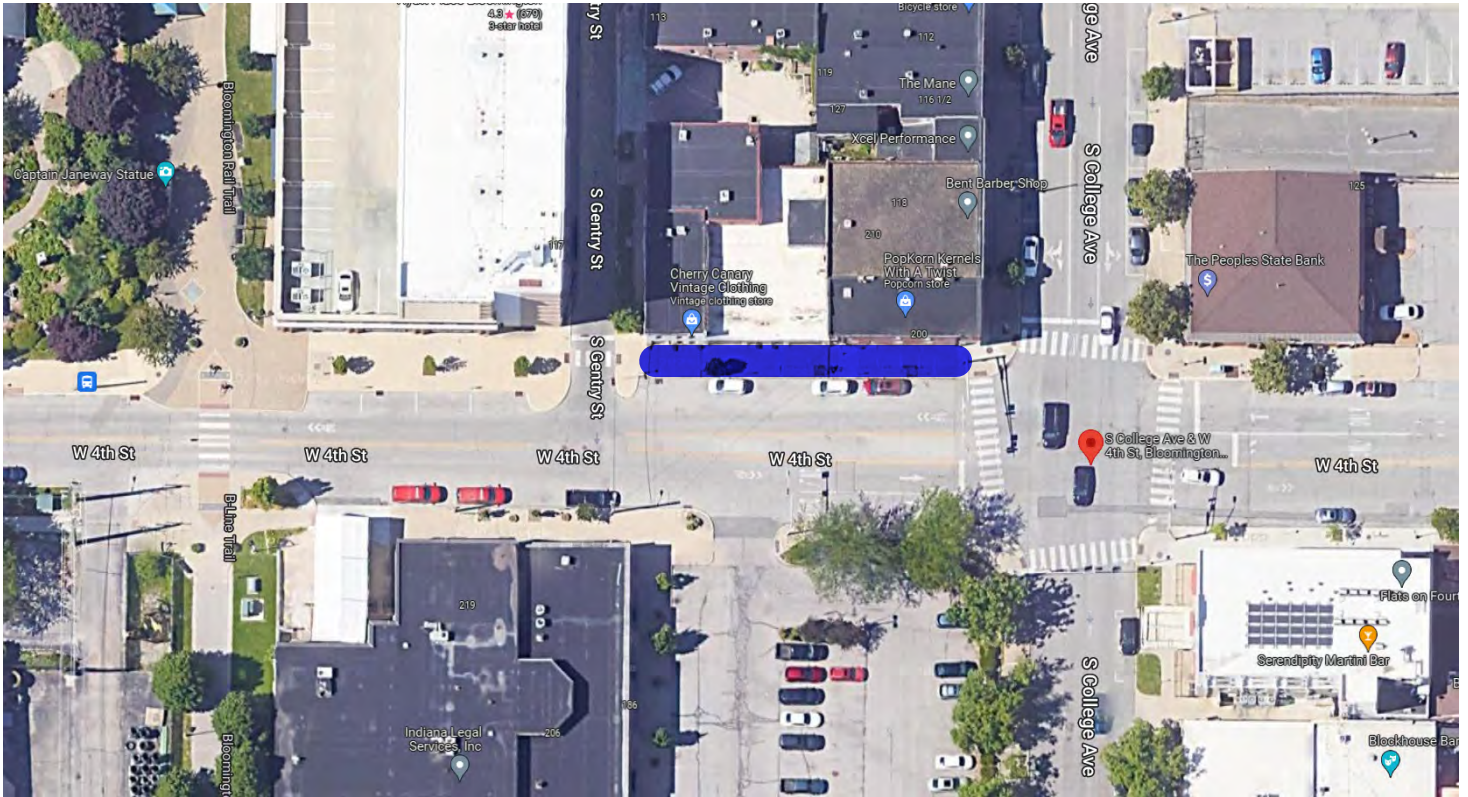
**CITY OF BLOOMINGTON
DEPARTMENT OF ENGINEERING**

**NEIGHBORHOOD GREENWAY PLANS
EAST 7TH STREET**

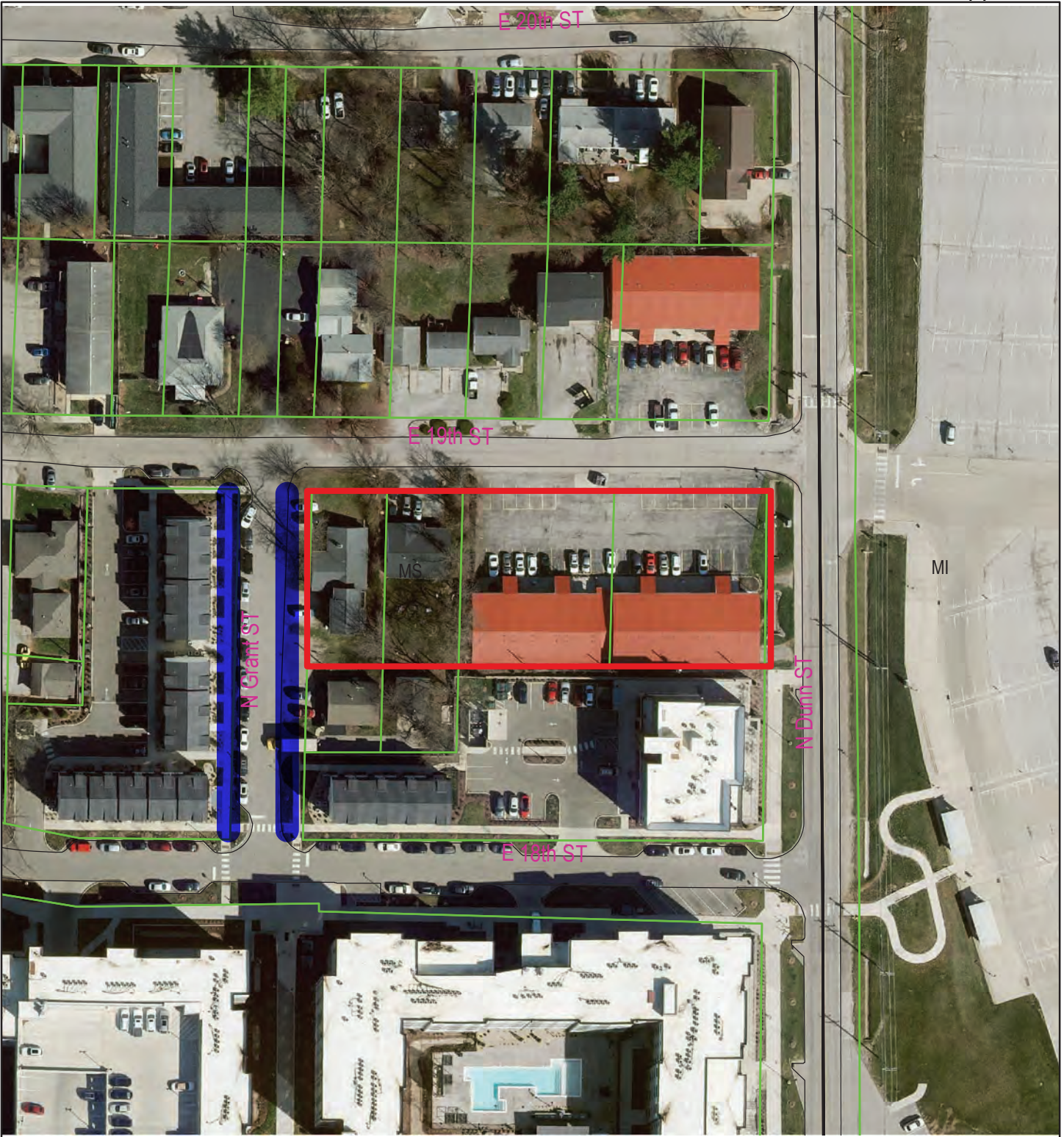
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VERTICAL SCALE		SHEET NUMBER	9

Section 3

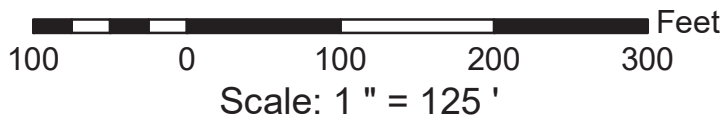
- Deleting angle parking on W. 4th Street from S. College Avenue to S. Gentry Street, north side of the street from Schedule L
- The change effectively codifies the existing condition and what people practiced for years.



S College Ave & W 4th St
Intersection

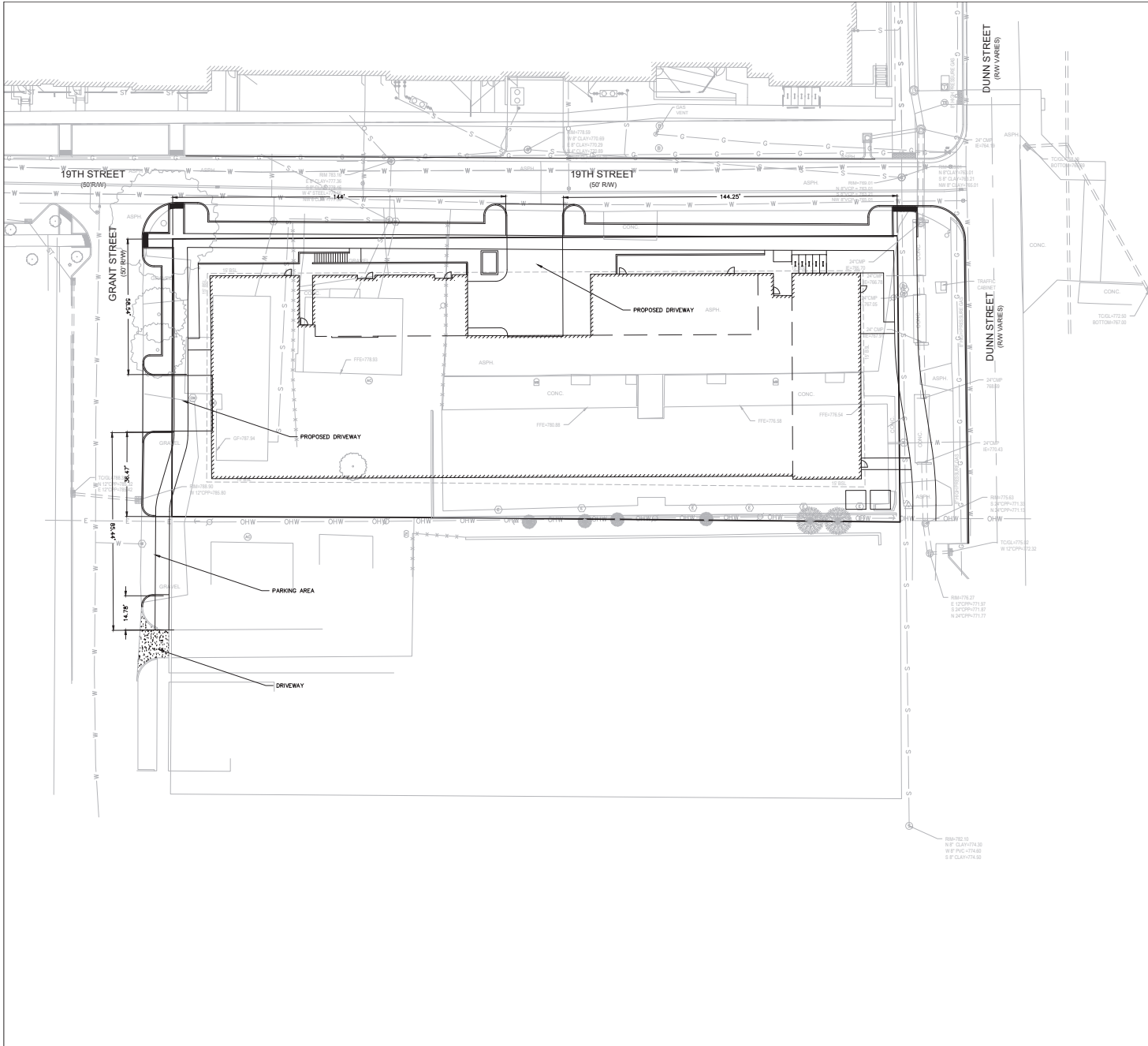


City of Bloomington
Planning & Transportation



By: Gabriel Holbrow
7/29/2022

For reference only. Map information NOT warranted.
165



SITE PLAN NOTES

1. THE 400 BLOCK OF E. 19TH ST IS CLASSIFIED AS "NEIGHBORHOOD RESIDENTIAL WITH NEIGHBORHOOD GREENWAY" IN THE TRANSPORTATION PLAN WHICH CALLS FOR A 6 FOOT (6'0" MIN WITH CURB) WIDE TREE PLOT AND A 6 FOOT WIDE SIDEWALK.
2. THE 1400 BLOCK OF N GRANT ST IS CLASSIFIED AS "NEIGHBORHOOD RESIDENTIAL" IN THE TRANSPORTATION PLAN WHICH CALLS FOR A 6 FOOT (6'0" MIN WITH CURB) WIDE TREE PLOT AND A 6 FOOT WIDE SIDEWALK.

SITE PLAN LEGEND

- CONCRETE
- CONCRETE DRIVE APRON
- CONCRETE PAVEMENT
- ASPHALT PAVEMENT



XX/XX/20XX

420 E. 19TH ST
MIXED USE BUILDING
BLOOMINGTON, INDIANA

REVISIONS	BY	DATE

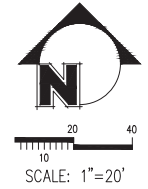
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DATE	

JOB NUMBER
6448

SHEET
C200

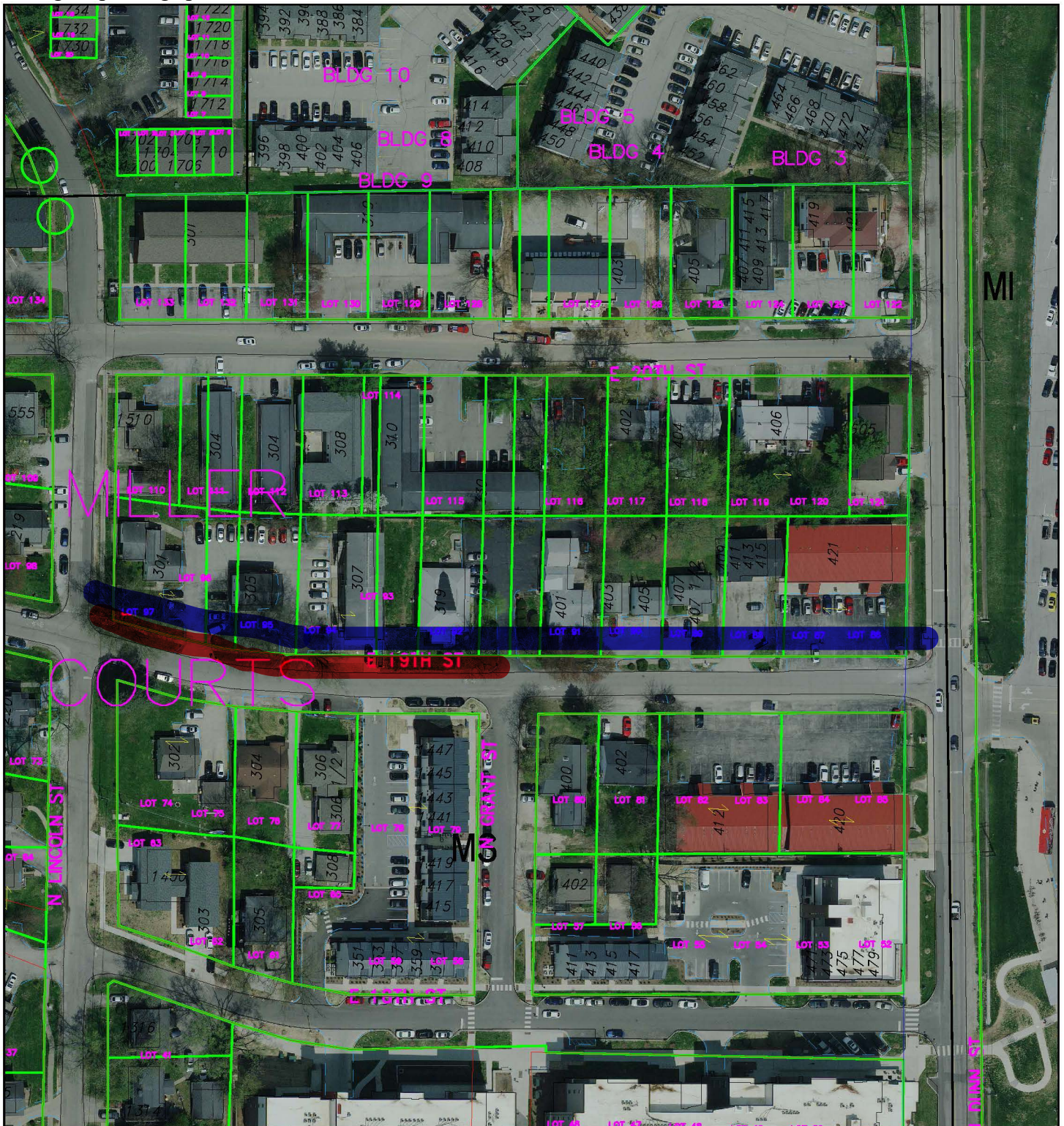
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08/29/2022

SITE PLAN



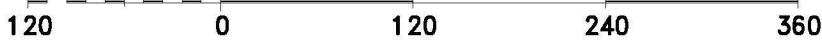
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Section 4--Removing no parking spaces on the north side of E. 19th Street from N. Lincoln Street to N. Dunn Street;
 Adding no parking spaces on the north side of E. 19th Street from N. Lincoln Street to N. Grant Street.



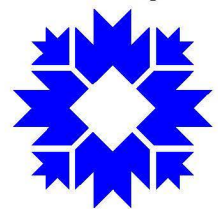
By: keegan.gulick

1 Nov 21



For reference only; map information NOT warranted.

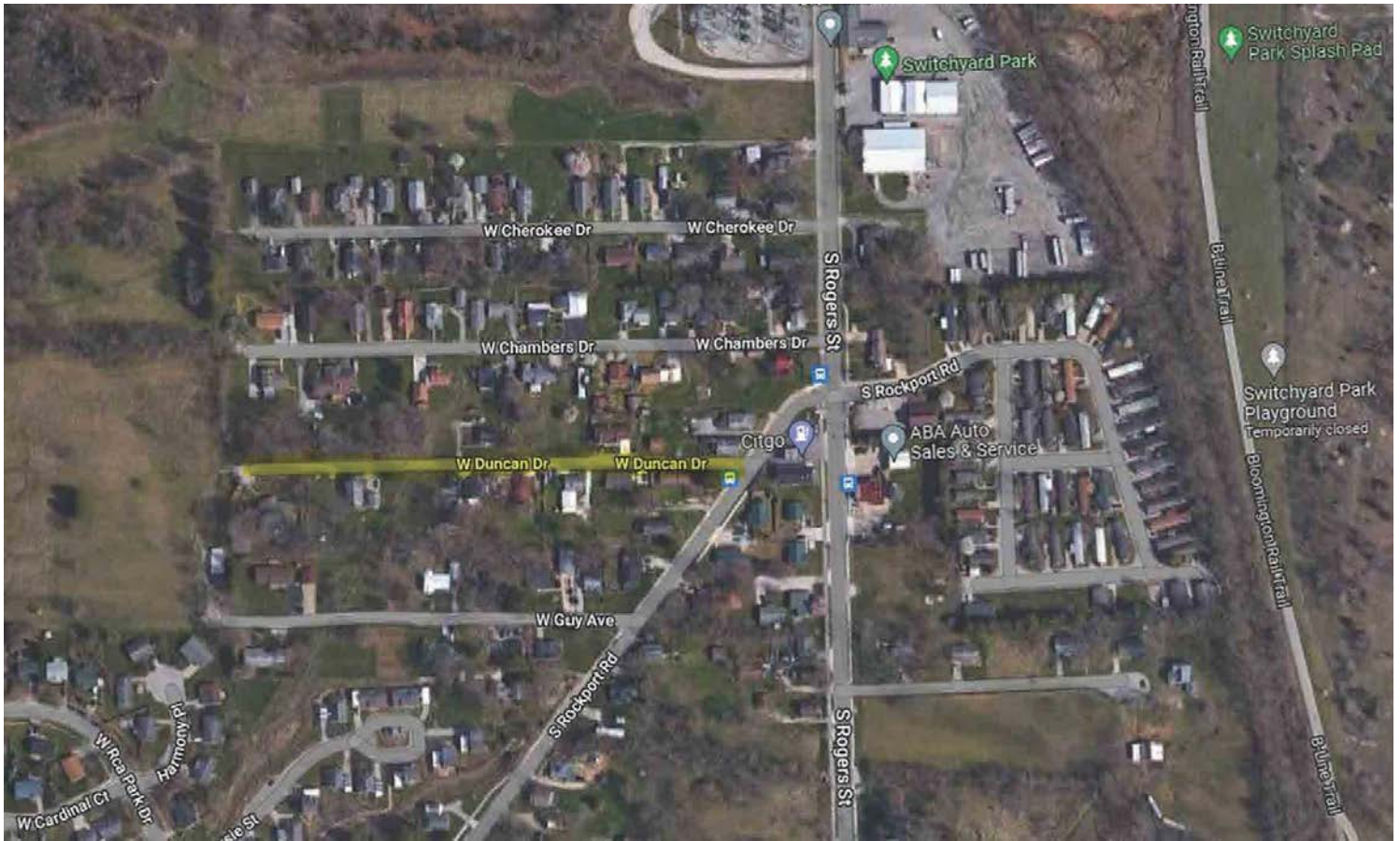
City of Bloomington
 Planning



Scale: 1" = 120'

Section 4

Adding no parking spaces on both sides of W. Duncan Drive.



Section 4

Adding no parking spaces on Strong Drive south of W. Allen Street.



-  **Yellow Curbs**
-  **Proposed restricted area**
-  **Parking area**



On-street parking on both sides of the road

Section 5

Adding limited parking zones on the south side of E. 8th Street, from N. Walnut Street to the first four spaces east of N. Walnut Street, 9:00 p.m. to 4:00 a.m. only.

