



City of Bloomington Common Council

Legislative Packet

Containing legislation and materials related to:

Wednesday, 03 August 2022

Regular Session at 6:30pm



CITY OF BLOOMINGTON COMMON COUNCIL

AGENDA:
REGULAR SESSION
WEDNESDAY | 6:30 PM
03 August 2022

*Council Chambers (#115), Showers Building, 401 N. Morton Street
The meeting may also be accessed at the following link:*

<https://bloomington.zoom.us/j/87844143464?pwd=QTZYNm9yM0ZhaU0vY2JhY0V2TFpqdz09>

- I. **ROLL CALL**
- II. **AGENDA SUMMATION**
- III. **APPROVAL OF MINUTES - None**
- IV. **REPORTS** (*A maximum of twenty minutes is set aside for each part of this section.*)
 - A. **Councilmembers**
 - B. **The Mayor and City Offices**
 - C. **Council Committees**
 - D. **Public***
- V. **APPOINTMENTS TO BOARDS AND COMMISSIONS**
- VI. **LEGISLATION FOR SECOND READINGS AND RESOLUTIONS**
 - a. Ordinance 22-20 – To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles and Traffic" - Re: Amending Section 15.12.030 to remove three signalized intersections and add three signalized intersections; Section 15.20.010 by deleting the reference to restricted turn intersections; Section 15.24.020 by changing speed limits at four locations; Section 15.32.030 to change parking on Lincoln Street between Smith and Third to back-in angle parking and to add pull-in parking on S. Morton Street from W. Patterson Drive and Grimes Lane to 190' north of W. Patterson Drive and Grimes Lane; Section 15.32.050 to allow for prohibiting parking in municipal parking lots to respond to weather and maintenance issues; Section 15.32.080, to remove no parking spaces on S. Morton Street and S. Rogers Street and to add no parking spaces on Lincoln Street and N. Park Avenue; Section 15.32.100 to add two loading zones on E. 4th Street; Section 15.37.020, to change the boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone; Section 15.040.010, to add paid parking spaces and the Trades District Garage; and Section 15.48.020 to add an administrative towing fee when vehicles are towed for reserved parking or outstanding citation violations

Committee recommendation:

N/A

* Members of the public may speak on matters of community concern not listed on the agenda at one of the two public comment opportunities. Citizens may speak at one of these periods, but not both. Speakers are allowed five minutes; this time allotment may be reduced by the presiding officer if numerous people wish to speak.

Auxiliary aids are available upon request with adequate notice. Please call (812) 349-3409 or email council@bloomington.in.gov.

VII. LEGISLATION FOR FIRST READINGS

- a. Ordinance 22-21 - To Amend Title 8 of the Bloomington Municipal Code, Entitled “Historic Preservation and Protection” To Establish A Historic District – Re: Bethel A.M.E. Church and Parsonage (Bethel A.M.E., Owner and Petitioner)

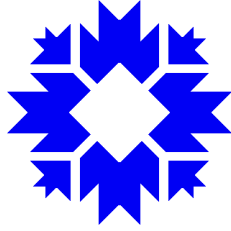
VIII. ADDITIONAL PUBLIC COMMENT* *(A maximum of twenty-five minutes is set aside for this section.)*

IX. COUNCIL SCHEDULE

X. ADJOURNMENT

* Members of the public may speak on matters of community concern not listed on the agenda at one of the two public comment opportunities. Citizens may speak at one of these periods, but not both. Speakers are allowed five minutes; this time allotment may be reduced by the presiding officer if numerous people wish to speak.

Auxiliary aids are available upon request with adequate notice. Please call (812) 349-3409 or email council@bloomington.in.gov.



**City of Bloomington
Office of the Common Council**

NOTICE

Wednesday, 03 August 2022

Regular Session

Starting at 6:30 pm

This meeting will be held in the Council Chambers (Suite #115, City Hall, 401 N. Morton St) and may also be accessed electronically via Zoom (see information below).

Join Zoom Meeting

<https://bloomington.zoom.us/j/87844143464?pwd=QTZYNm9yM0ZhaU0vY2JhY0V2TFpqc09>

Meeting ID: 878 4414 3464

Passcode: 589034

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Dial by your location

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+1 669 900 6833 US (San Jose)

+1 253 215 8782 US (Tacoma)

+1 346 248 7799 US (Houston)

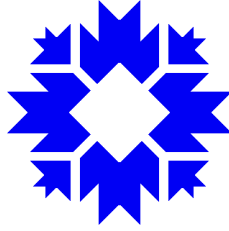
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Meeting ID: 878 4414 3464

Find your local number: <https://bloomington.zoom.us/j/kickjvgQj>

As a quorum of the Council or its committees may be present, this gathering constitutes a meeting under the Indiana Open Door Law (I.C. § 5-14-1.5). For that reason, this statement provides notice that this meeting will occur and is open for the public to attend, observe, and record what transpires.



**City of Bloomington
Office of the Common Council**

NOTICE

Friday, 05 August 2022

Common Council Work Session

Starting at 12 noon

This meeting will be held in the McCloskey Conference Room (Suite #135, City Hall, 401 N. Morton St) and may also be accessed electronically via Zoom (see information below).

Join Zoom Meeting

<https://bloomington.zoom.us/j/82443658481?pwd=K1lFMjNqbUFKTjVJT0VRRWRmOHpjQT09>

Meeting ID: 824 4365 8481

Passcode: 368459

One tap mobile

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+1 564 217 2000 US
+1 669 444 9171 US
+1 669 900 6833 US (San Jose)
+1 253 215 8782 US (Tacoma)*

Find your local number: <https://bloomington.zoom.us/j/kbeygeSSad>

As a quorum of the Council or its committees may be present, this gathering constitutes a meeting under the Indiana Open Door Law (I.C. § 5-14-1.5). For that reason, this statement provides notice that this meeting will occur and is open for the public to attend, observe, and record what transpires.



MEMO FROM COUNCIL OFFICE ON:

Ordinance 22-20 - To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles and Traffic"

Synopsis

This Ordinance amends Title 15, "Vehicles and Traffic," of the Bloomington Municipal Code and comes forth at the request of City staff, the Parking Commission, the Traffic Commission, and the Bicycle and Pedestrian Safety Commission. The ordinance makes the following changes:

- Removes three signalized intersections and adds three signalized intersections.
- Deletes BMC 15.20.010, "Restricted turn intersections."
- Changes speed limits at four locations.
- Provides for back-in parking on the west side of Lincoln Street between Smith and Third, and provides for pull-in parking on the west side of S. Morton Street from W. Patterson Drive and Grimes Lane to 190' north of W. Patterson Drive and Grimes Lane.
- Allows the City to prohibit parking in City parking lots in anticipation of snow emergencies or lot maintenance and repair.
- Removes no parking spaces on Morton Street and S. Rogers Street, and adds no parking spaces on Lincoln Street and N. Park Avenue.
- Adds two loading zones on E. 4th Street.
- Alters the boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone.
- Adds additional pay-by-phone parking on N. Walnut Grove, E. 13th Street, and S. Rogers Street, and adds the Trades District Garage to the list of municipal parking lots and garages.
- Adds an administrative fee for towing vehicles of \$25.00.

Relevant Materials

- [Ordinance 22-20](#)
- Memo from staff (including maps of affected locations)

Summary

[Ordinance 22-20](#) proposes to amend various sections of Title 15 ("Vehicles and Traffic") of the Bloomington Municipal Code ("BMC"), accessible online here:

https://library.municode.com/in/bloomington/codes/code_of_ordinances?nodeId=TIT15_VETR. BMC Title 15 contains codified traffic and parking controls for the city and is subject to frequent change and revision. [Ordinance 22-20](#) represents the first Title 15 ordinance proposed in 2022. As the staff memo notes, the proposed changes are a result of public requests, feedback through commissions' recommendations, or proposals stemming from temporary 180-day orders. The Council Office has requested a statement from city staff describing any anticipated fiscal impact from the proposed changes and will supplement these materials with any response.

Contact

Amir Farshchi, Long Range Planner, 812-349-3423, farshchs@bloomington.in.gov (questions sent to Amir may be directed to other staff members as needed)

ORDINANCE 22-20

**TO AMEND TITLE 15 OF THE BLOOMINGTON MUNICIPAL CODE
ENTITLED "VEHICLES AND TRAFFIC"**

- **Re: Amending Section 15.12.030 to remove three signalized intersections and add three signalized intersections; Section 15.20.010 by deleting the reference to restricted turn intersections; Section 15.24.020 by changing speed limits at four locations; Section 15.32.030 to change parking on Lincoln Street between Smith and Third to back-in angle parking and to add pull-in parking on S. Morton Street from W. Patterson Drive and Grimes Lane to 190' north of W. Patterson Drive and Grimes Lane; Section 15.32.050 to allow for prohibiting parking in municipal parking lots to respond to weather and maintenance issues; Section 15.32.080, to remove no parking spaces on S. Morton Street and S. Rogers Street and to add no parking spaces on Lincoln Street and N. Park Avenue; Section 15.32.100 to add two loading zones on E. 4th Street; Section 15.37.020, to change the boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone; Section 15.040.010, to add paid parking spaces and the Trades District Garage; and Section 15.48.020 to add an administrative towing fee when vehicles are towed for reserved parking or outstanding citation violations.**

WHEREAS, the Traffic Commission, Parking Commission, Bicycle and Pedestrian Safety Commission, and City staff from the Planning and Transportation, Engineering, Parking Enforcement, and Legal departments recommend certain changes be made in Title 15 of the Bloomington Municipal Code entitled "Vehicles and Traffic,"

NOW THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. Section 15.12.030 entitled "Signalized intersections," shall be amended by deleting and adding the following to Schedule D(1).

Delete:

CROSS STREET	CROSS STREET
Bloomfield Road	Basswood Drive
Leonard Springs	Tapp Road
Third Street	Franklin Road

Add:

CROSS STREET	CROSS STREET
Eleventh Street	Walnut Street
Fourteenth Street	Walnut Street
Tapp Road	Deborah Drive

SECTION 2. Section 15.20.010, entitled “Restricted turn intersections,” shall be deleted in its entirety.

SECTION 3. Section 15.24.020, entitled “Increased or decreased speed limits,” shall be amended by deleting and adding the following:

Delete:

Street	From	To	Posted Speed
Henderson Street	First Street	Atwater Avenue	30 MPH
Indiana Avenue	Atwater Avenue	Seventeenth Street	30 MPH
Madison Street	Eleventh Street	Seventeenth Street	30 MPH
Rogers Street	Country Club Drive	Eleventh Street	30 MPH

Add:

Street	From	To	Posted Speed
Rogers Street	Country Club Drive	Patterson Drive	30 MPH

SECTION 4. Section 15.32.030, entitled “Angle parking,” shall be amended by deleting and adding the following from Schedule L.

Delete:

Street	From	To	Side of Street	Angle Type
Lincoln Street	Smith Avenue	130' South of Third Street	West	Pull In

Add:

Street	From	To	Side of Street	Angle Type
Lincoln Street	Smith Avenue	Third Street	West	Back In
Morton Street	Patterson Drive and Grimes Lane	190' North of Patterson Drive and Grimes Lane	West	Pull In

SECTION 5. Section 15.32.050 (b) and (c), entitled “Parking during snow removal, street repair and street cleaning,” shall be amended by adding the term “municipal parking lots” to each reference to streets so that it reads as follows:

(b) In the event weather conditions or the condition of city streets or municipal parking lots make it necessary for parking on city streets or municipal parking lots to be prohibited or restricted for snow plowing, snow removal, street repair, street cleaning, municipal parking lot cleaning or repair, or other related purposes, the coordinator of public works shall implement a parking prohibition on parts or all of streets or municipal parking lots as deemed necessary. It shall be unlawful to park or allow to remain parked any vehicle on any street or municipal parking lot to which the prohibition applies during the hours of the prohibition when signs have been properly posted.

(c) The parking prohibition shall not go into effect until twelve hours after signs have been posted stating the hours in which the prohibition will be in effect. Such signs shall be posted at regular intervals along the streets and in the municipal parking lots on which parking shall be prohibited so that they are clearly visible along the street and in the municipal parking lot. The parking prohibition shall terminate at the time stated on the signs, or upon their removal by authorized persons if no time is specified.

SECTION 6. Section 15.32.080, entitled “No Parking Zones,” shall be amended by deleting and adding the following to Schedule M:

Delete:

Street	From	To	Side of Street	Time of Restriction
Morton Street	Grimes Lane	Dodds Street	East/West	Any Time
Rogers Street	Fourth Street	Kirkwood Avenue	East	Any Time
Rogers Street	Third Street	Tenth Street	East	Any Time

Add:

Street	From	To	Side of Street	Time of Restriction
Lincoln Street	Smith Avenue	140' south of Third Street	East	Any Time
Morton Street	Patterson Drive and Grimes Lane	Dodds Street	East	Any Time
Morton Street	190' North of Patterson Drive and Grimes Lane	Dodds Street	West	Any Time
Park Avenue	Thirteenth Street	Fourteenth Street	East/West	Any Time
Rogers Street	Fourth Street	Tenth Street	East	Any Time
Rogers Street	Third Street	150' North of Third Street	East	Any Time

SECTION 7. Section 15.32.100, entitled “Loading Zones,” shall be amended by adding the following to Schedule O:

Add:

LOADING ZONES	
200	Block of Fourth Street on the north side, two spaces

SECTION 8. Section 15.37.020, entitled “Applicability, Garden Hill Residential Neighborhood Zone (Zone 6),” shall be amended by deleting and adding the following:

Delete:

Street	From	To	Side of Street
East 13th	North Dunn	North Woodlawn	North/South
East 14th	North Walnut	North Woodlawn	North/South
East 15th	North Walnut	North Indiana	North/South
East 16th	North Walnut	North Dunn	North/South
East 17th	North Walnut	North Dunn	South
North Dunn	East 17th	East 13th	East/West
North Fess	East 17th	East 13th	East/West
North Grant	East 17th	East 15th	East/West
North Indiana	East 17th	East 13th	East/West
North Lincoln	East 17th	East 14th	East/West
North Washington	East 17th	East 14th	East/West

Add (bold type used to highlight the changes; the font will not be bolded in code):

Street	From	To	Side of Street
East 13th	North Dunn	North Fess	North/South
East 14th	North Walnut	North Fess	North/South
East 15th	North Walnut	North Indiana	North/South
East 16th	North Walnut	North Dunn	North/South
East 17th	North Walnut	North Fess	South
North Dunn	East 17th	East 13th	East/West
North Fess	East 17th	East 13th	East/West
North Grant	East 17th	East 15th	East/West
North Indiana	East 17th	East 13th	East/West
North Lincoln	East 17th	East 14th	East/West
North Walnut	East 17th	Indiana Railroad Company property	East
North Washington	East 17th	East 14th	East/West
Indiana Railroad Company property	North Walnut	North Dunn	North

SECTION 9. Section 15.40.010, entitled “Locations,” Schedule U, shall be amended by adding the following:

Add:

Block Face	Maximum Rate
700 E 13th	\$1.00
800 E 13th	\$1.00
200 S Rogers	\$1.00
1100 N Walnut Grove	\$1.00

(24) Lot 25: Trades District Garage, located at 489 W. 10th Street.

SECTION 10. Section 15.48.020, entitled “Renewal and impoundment procedure,” shall be amended by deleting the current 15.48.020(b)(1) and replacing it with the following:

(1) Payments of fees, including an administrative towing fee of \$25 to the City of Bloomington for towing of vehicles for reserved parking violations and outstanding citation violations.

SECTION 11. If any section, sentence or provision of this ordinance, or application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions or application of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

SECTION 12. This ordinance shall be in effect after its passage by the Common Council and approval of the Mayor, any required publication, and, as necessary, other promulgation in accordance with the law.

PASSED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2022.

SUSAN SANDBERG, President
Bloomington Common Council

ATTEST:

NICOLE BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2022.

NICOLE BOLDEN, Clerk,
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2022.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

This Ordinance amends Title 15, "Vehicles and Traffic," of the Bloomington Municipal Code and comes forth at the request of City staff, the Parking Commission, the Traffic Commission, and the Bicycle and Pedestrian Safety Commission. The ordinance makes the following changes:

- Removes three signalized intersections and adds three signalized intersections.
- Deletes BMC 15.20.010, "Restricted turn intersections."
- Changes speed limits at four locations.
- Provides for back-in parking on the west side of Lincoln Street between Smith and Third, and provides for pull-in parking on the west side of S. Morton Street from W. Patterson Drive and Grimes Lane to 190' north of W. Patterson Drive and Grimes Lane.
- Allows the City to prohibit parking in City parking lots in anticipation of snow emergencies or lot maintenance and repair.
- Removes no parking spaces on Morton Street and S. Rogers Street, and adds no parking spaces on Lincoln Street and N. Park Avenue.
- Adds two loading zones on E. 4th Street.
- Alters the boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone.
- Adds additional pay-by-phone parking on N. Walnut Grove, E. 13th Street, and S. Rogers Street, and adds the Trades District Garage to the list of municipal parking lots and garages.
- Adds an administrative fee for towing vehicles of \$25.00.

To: Common Council
MEMO

Ordinance #: 22-20
Date: 07/08/22

From: Andrew Cibor, City Engineer; Raye Ann Cox, Parking Enforcement Manager; Patrick Dierkes, Project Engineer; Amir Farshchi, Long Range Planner; Neil Kopper, Senior Project Engineer; Barbara McKinney, Assistant City Attorney; Beth Rosenbarger, Assistant Director of Planning and Transportation; Michelle Wahl, Parking Services Director

Request: Title 15 Amendments

REPORT

The City staff from the Planning and Transportation, Engineering, Parking Services division of Public Works, and Legal departments have compiled a number of recommendations for changes to Title 15, the vehicle and traffic section of the Bloomington Municipal Code. Link to the current Title 15 of the Bloomington Municipal Code:

https://library.municode.com/in/bloomington/codes/code_of_ordinances?nodeId=TIT15VETR

The purpose of these recommendations is to address issues that have come to staff's attention through public requests, commissions' recommendations, or 180-day orders. The ordinance amending Title 15 and respective maps are included in the meeting packet. Each section of the ordinance was presented either to the Traffic Commission, Parking Commission, or Bicycle and Pedestrian Safety Commission. The Commissions voted to forward each section of the Title 15 ordinance with a positive recommendation to the Common Council.

The ordinance includes the following changes:

- Section 1 removes three signalized intersections and adds three signalized intersections.
Neil Kopper
 - Removing one signalized intersection at Bloomfield Road and Basswood Drive.
 - This signal was previously transferred to Indiana Department of Transportation (INDOT) maintenance to improve coordination with the interchange.
 - Removing one signalized intersection at Leonard Springs Road and Tapp Road.
 - This was a temporary signal during I-69 construction. It is no longer in place.
 - Removing one signalized intersection at 3rd Street and Franklin Road.
 - This signal was previously transferred to Indiana Department of Transportation (INDOT) maintenance to improve coordination with the interchange.
 - Adding two signalized intersections in Walnut Street at 11th Street and 14th Street.
 - These projects were prioritized to mitigate crash patterns and to enhance pedestrian crossings. Construction began in April of 2022 and is scheduled to be completed by July 29, 2022.
 - The case 22-03 for this item was presented to the Traffic Commission on March 23, 2022, the Commission voted to forward the Title 15 changes to the Common Council with a positive recommendation.
 - Adding one signalized intersection at Tapp Road and Deborah Drive.
 - This new traffic signal was funded by the Indiana Department of Transportation (INDOT) via past agreements associated with the I-69 project. Construction began in March of 2022 and is scheduled to be completed by June 29, 2022.
 - The case 22-05 for this item was presented to the Traffic Commission on March 23, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 2 deletes BMC 15.20.010, "Restricted turn intersections." *Neil Kopper*
 - This section and schedule are blank, staff recommends removing this from the

code. If a turn is restricted at an intersection, the restriction is posted or the turn is not possible due to physical objects (medians, curbs, off-set, etc.).

- The case 22-04 for this item was presented to the Traffic Commission on March 23, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 3 provides for changing speed limits at four locations. *Neil Kopper*
 - It changes the speed limit at four locations from 30 MPH to the default 25 MPH on Henderson Street (1st to Atwater), Indiana Avenue (Atwater to 17th), Madison Street (11th to 17th), and Rogers Street (Patterson to 11th).
 - A Traffic Commission member requested a review of the existing posted speed limits in the area.
 - The cases 22-06 and 22-07 for this item were presented to the Traffic Commission on May 25, 2022, and the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 4 changes parking on S. Lincoln Street to back-in angle parking, adds pull-in parking on S. Morton Street.
 - Providing back-in angle parking on the west side of S. Lincoln Street between E. Smith Avenue and E. 3rd Street. *Beth Rosenbarger*
 - The street parking was changed in order to improve overall safety on the street and better align the street design with community goals as described in the Comprehensive Plan.
 - Summary of changes in this block:
 - There were 37 public parking spaces in this block before the changes. After the changes, there are 34 public parking spaces in this block. While this is a reduction in 3 parking spaces, the changes are focused on improving safety for all street users.
 - The sidewalk on the west side of the street, along Waldron Hill Buskirk Park is slightly wider.
 - Angled parking changes from back-out angled parking to back-in angled parking, also called head-out. To park, drivers back into the space, similar to parallel parking, but much simpler. When leaving, drivers have improved visibility as they pull out into traffic.
 - The car travel lane remains mostly the same although it may be wider.
 - For the portion of the block south of the Boys and Girls Club, there is no on street parking on the east side of the street. Near the intersection with 3rd Street, the on-street parallel parking will remain, and it will accommodate at least 4 vehicles. The bicycle lane on the east side of the street will include a buffer between the bicycle lane and motor vehicle traffic. These changes are to reduce the potential for conflicts. Parallel parking next to bicycle lanes poses hazards to bicyclists and drivers. Most commonly, a driver will open their car door into the bicycle lane (called dooring), which can harm or kill the person bicycling and which can harm the driver. Removing most of the parallel parking will reduce the risk of dooring in this block.
 - The sidewalk on the east side of the street remains the same.
 - The case 21-09 for this item was presented to the Traffic Commission on November 17, 2021, the Commission voted to forward the changes to the Council with a positive recommendation.
 - The 180-Day Order 21-03 for this case was issued on December 15, 2021.
 - Providing pull-in parking on the west side of S. Morton Street from W. Patterson Drive and W. Grimes Lane to 190' north of W. Patterson Drive and W. Grimes Lane. *Beth Rosenbarger*
 - The parking spaces are in front of the New Hope for Families site.
 - The area is currently codified as a No Parking Zone in Title 15. This update would allow pull-in angled parking in the designated areas.
- Section 5 allows the City to prohibit parking in City parking lots in anticipation of snow emergencies or lot maintenance and repair. *Michelle Wahl*

- The term “municipal parking lots” should be added to each reference to streets.
- Section 6 adds no parking spaces on Lincoln Street and N. Park Avenue; it changes no parking spaces on S. Morton Street and S. Rogers Street.
 - Removing no parking spaces on S. Morton Street: summary included in Section 4 for S. Morton Street. *Beth Rosenbarger*
 - Removing no parking spaces on S. Rogers Street: summary included in Section 9 for S. Rogers Street. *Patrick Dierkes*
 - Adding no parking spaces on Lincoln Street: summary included in Section 3. *Beth Rosenbarger*
 - Adding no parking spaces on both sides of N. Park Avenue between E. 13th Street and E. 14th Street. *Amir Farshchi*
 - The changes effectively codify what people practiced for years.
 - The area cannot support parking on any sides. This narrow area does not meet the minimum street width requirements and there is not enough width for drivers to park on the asphalt and maintain travel.
 - The Parking Resolution 22-03 for this case was presented to the Parking Commission on May 26, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 7 adds two loading zones on 200 E. 4th Street. *Michelle Wahl and Raye Ann Cox*
 - Converting the two metered parking spaces into two 30 minute loading zones that will be 24/7.
 - The Staff at the First United Methodist Church requested two loading zones for drop-off of congregants with disabilities for church services and children for the SCCAP Head Start Program.
 - The Parking Resolution 22-04 for this case was presented to the Parking Commission on April 22, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 8 alters the boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone. *Amir Farshchi, Beth Rosenbarger*
 - The Parking Services, Planning and Transportation Department, and Parking Commission have received parking complaints and requests from the residents of the Garden Hill Neighborhood regarding resident and visitor permit eligibility for the Garden Hill Residential Neighborhood Parking Zone (Zone 6).
 - Based on the streets included in Zone 6, several properties that are part of the Garden Hill Neighborhood are excluded from being able to purchase neighborhood parking permits. Staff is recommending to update the description of the applicable streets in order to include those areas, to include the east side of Walnut Street and the south side of 17th Street.
 - Staff recommends that the boundaries of Zone 6 more closely align with the boundaries of the Garden Hill Neighborhood. The changes allow any eligible resident who lives in Zone 6 to receive residential and visitor permits for Zone 6. The requests for permits and enforcement will be more easily implemented by staff.
 - All residential neighborhood parking zones are in effect from 8:00 a.m. to 5:00 p.m. Monday through Friday. Zone 6 has additional hours, compared to all other neighborhood parking zones, which include from 8:00 a.m. Thursday until 8:00 a.m. Sunday (Bloomington Municipal Code, Section 15.37.030).
 - Current eligibility requirements are: Each resident of a single household detached dwelling in a neighborhood zone shall be entitled to apply for one residential neighborhood zone parking permit and one visitor permit per household. "Single household detached dwelling" means a building designed for the occupancy of no more than five adults unrelated by blood or marriage. No changes to eligibility are proposed at this time.
 - The Parking Resolution 22-03 for this case was presented to the Parking Commission on May 26, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Section 9 adds additional pay-by-phone parking on E. 13th Street, S. Rogers Street, N. Walnut Grove Street, and adds the location of the Trades District Garage to 15.40.010.
 - Adding pay-by-phone parking spaces on E. 13th Street. *Amir Farshchi, Beth Rosenbarger*

- On two blocks on the northside of E. 13th Street between N. Fess Avenue and Woodlawn Avenue, Monday through Saturday, 8:00 AM to 9:00 PM. The rate would be \$1 per hour payable through ParkMobile for these parking spaces. This would allow visitors an option to pay during peak times or an option for unpaid parking during non-peak hours. There are approximately 15 on-street parking spaces.
 - There are no residences along these two blocks, and the IU Arts Annex is the only building, which is located at the southwest corner of E. 13th Street and N. Woodlawn Avenue. Activities in the Fine Arts Studio Annex building occur late into the evening hours, and unloading of often large and bulky materials is needed for the building.
 - In Section 7, staff recommends removing the two blocks of E. 13th Street and E. 14th Street between Woodlawn Avenue and N. Fess Avenue from the residential neighborhood parking Zone 6. This area does not contain any residential uses, is zoned Mixed-Use Institutional, and the adjacent property includes an IU academic building. The area does not closely align with the Garden Hill Neighborhood Association boundaries.
 - Parking is restricted on the south side of E. 13th Street between N. Fess Avenue and Woodlawn Avenue.
 - The Parking Resolution 22-03 for this case was presented to the Parking Commission on May 26, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.
- Adding pay-by-phone parking spaces on the east side of 200 S. Rogers Street between W. 3rd Street and W. 4th Street. *Patrick Dierkes, Neil Kopper*
 - Per City Code, parking is not allowed in right-of-way in this area, however, parking has existed in right-of-way at 207 S. Rogers Street (Hoosier Workwear) since 1961 per the aerial photograph taken that year.
 - The City received complaints related to the parking as it blocks the clear pedestrian path along the street.
 - Recognizing the history of parking at this location, staff recommends modifying the existing parking area from straight-in parking to parallel parking per the provided diagram. This will allow for a clear pedestrian route and satisfy the parking demands of the adjacent businesses.
 - The City has consulted with the impacted property owner and businesses to inform them of the pending change.
 - The area is currently codified as a No Parking Zone in Title 15. No Parking Zones section of the Title 15 needs to be edited to allow for parking in this area.
 - Locations section of the Title 15 will need to be edited to meter the on-street parking. The rate would be \$1 per hour payable through ParkMobile for these parking spaces Monday through Saturday, 8:00 AM to 9:00 PM.
 - The staff report for this case was presented to the Bicycle and Pedestrian Safety Commission on May 4, 2022, the Commission forwarded the changes to the Council with a positive recommendation.
- Adding pay-by-phone parking spaces on 1100 N. Walnut Grove Street. *Michelle Wahl, Raye Ann Cox*
 - On 1100 N. Walnut Grove Street along with the Indiana University McNutt Quad additions, 4 parking spaces, 1 of which is an ADA parking space, were added at this location in approximately August 2021.
 - Currently, IU Parking Services has converted the 4 spaces to Pay by ParkMobile only spaces. Indiana University Parking Services does not wish to continue to monitor or enforce these spaces, per Indiana University Parking Services.
 - The City of Bloomington Parking Services will assume the financial and enforcement responsibility of these 4 City owned parking spaces. The rate would be \$1 per hour for these parking spaces Monday through Saturday, 8:00 AM to 9:00 PM.
 - The Parking Resolution 22-02 for this case was presented to the Parking Commission on January 19, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.

- Adding the location of the Trades District garage to the list of locations of municipal lots and garages in 15.40.010.
- Section 10 adds an administrative fee for towing vehicles of \$25.00. *Michelle Wahl*
 - The ordinance charging an administrative release fee for a towed vehicle by Parking Enforcement through the Bloomington Police was removed from Title 15 in 2021.
 - The Parking Staff recommends the administrative fee that was once charged by Bloomington Police Department to release towed vehicles by Parking Enforcement, to become a \$25.00 non-escalation tow citation.
 - The Parking Resolution 22-05 for this case was presented to the Parking Commission on April 22, 2022, the Commission voted to forward the changes to the Council with a positive recommendation.

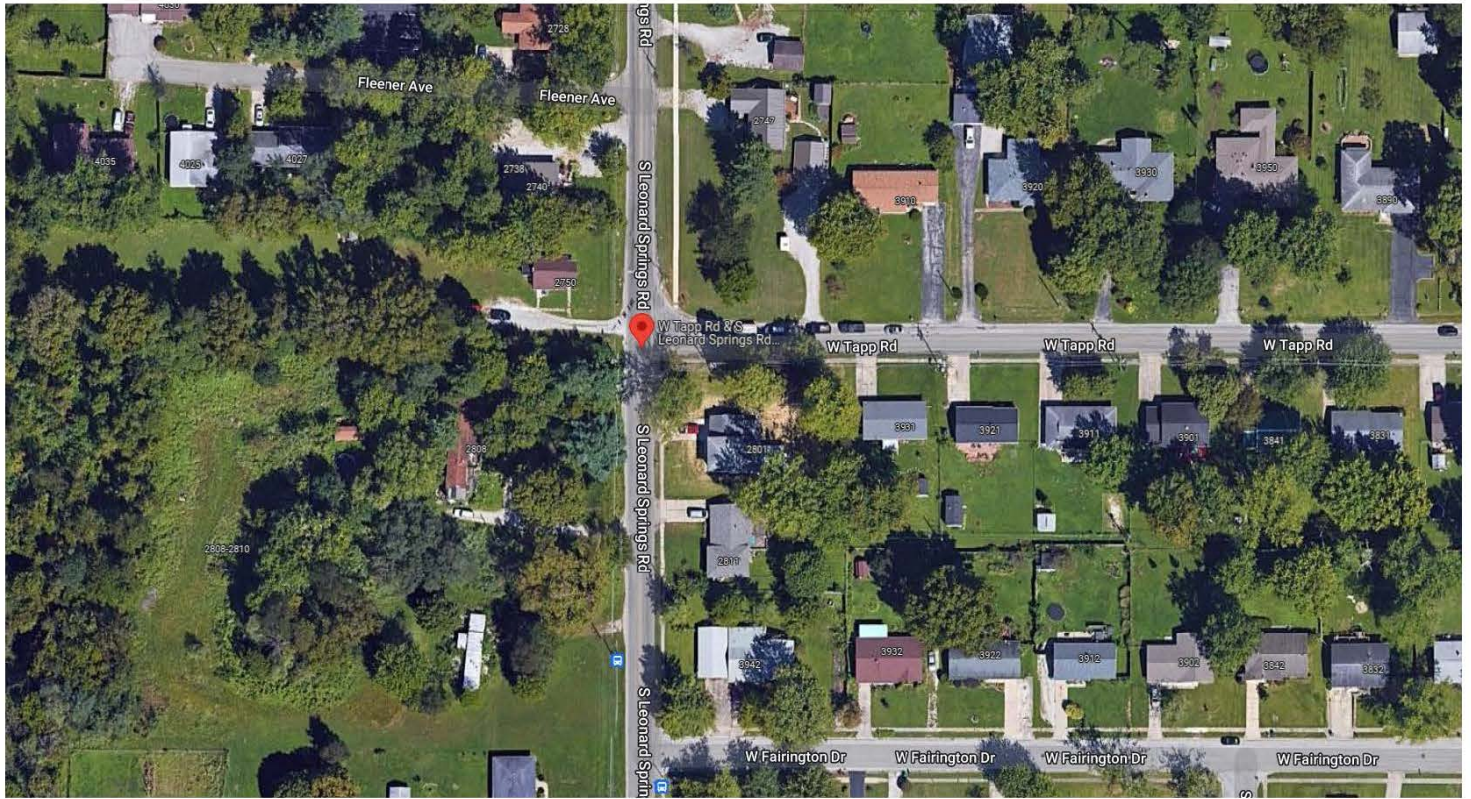
Section 1

Removing One Signalized intersection at Bloomfield Road and Basswood Drive



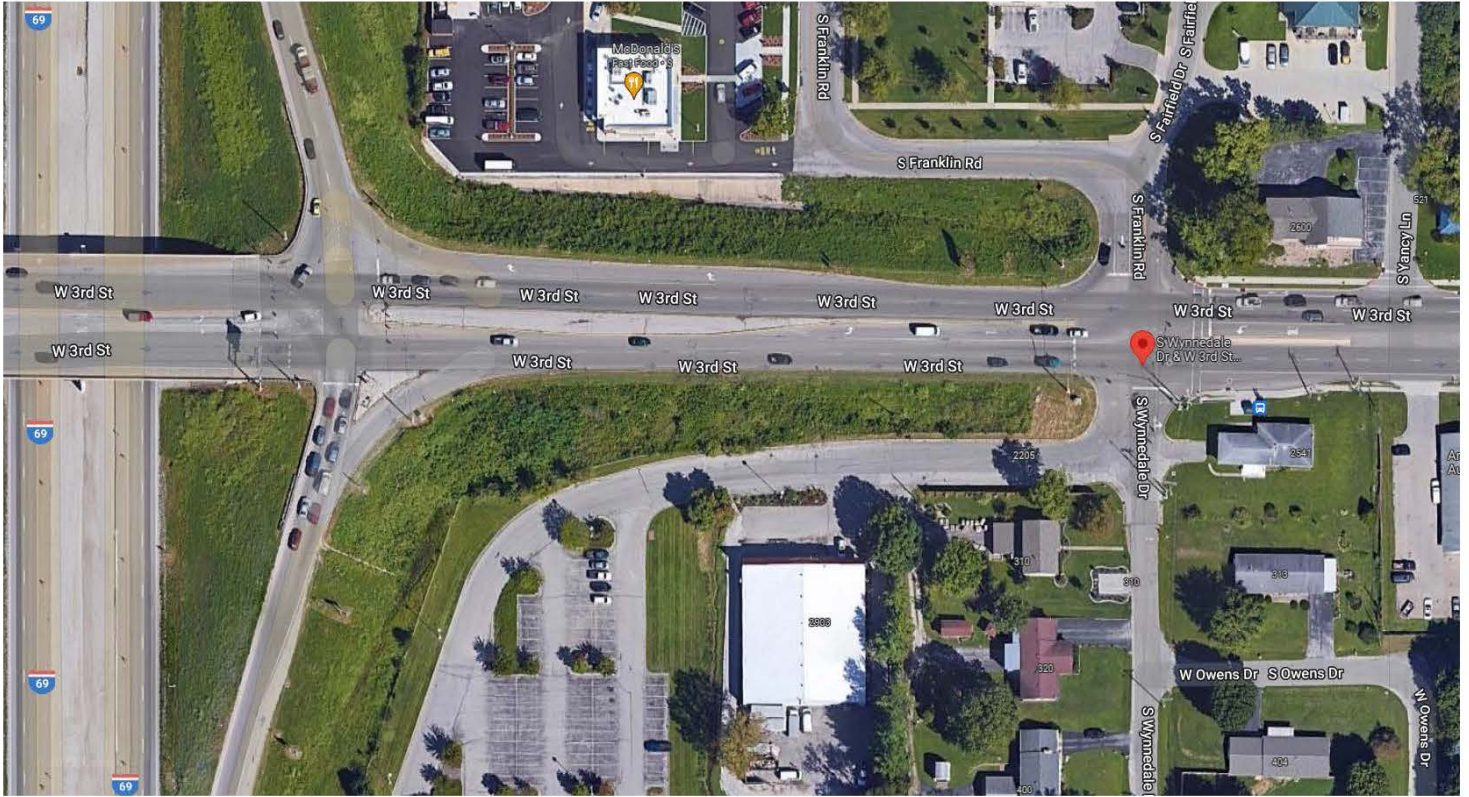
Section 1

Removing One Signalized intersection at Leonard Springs Road and Tapp Road



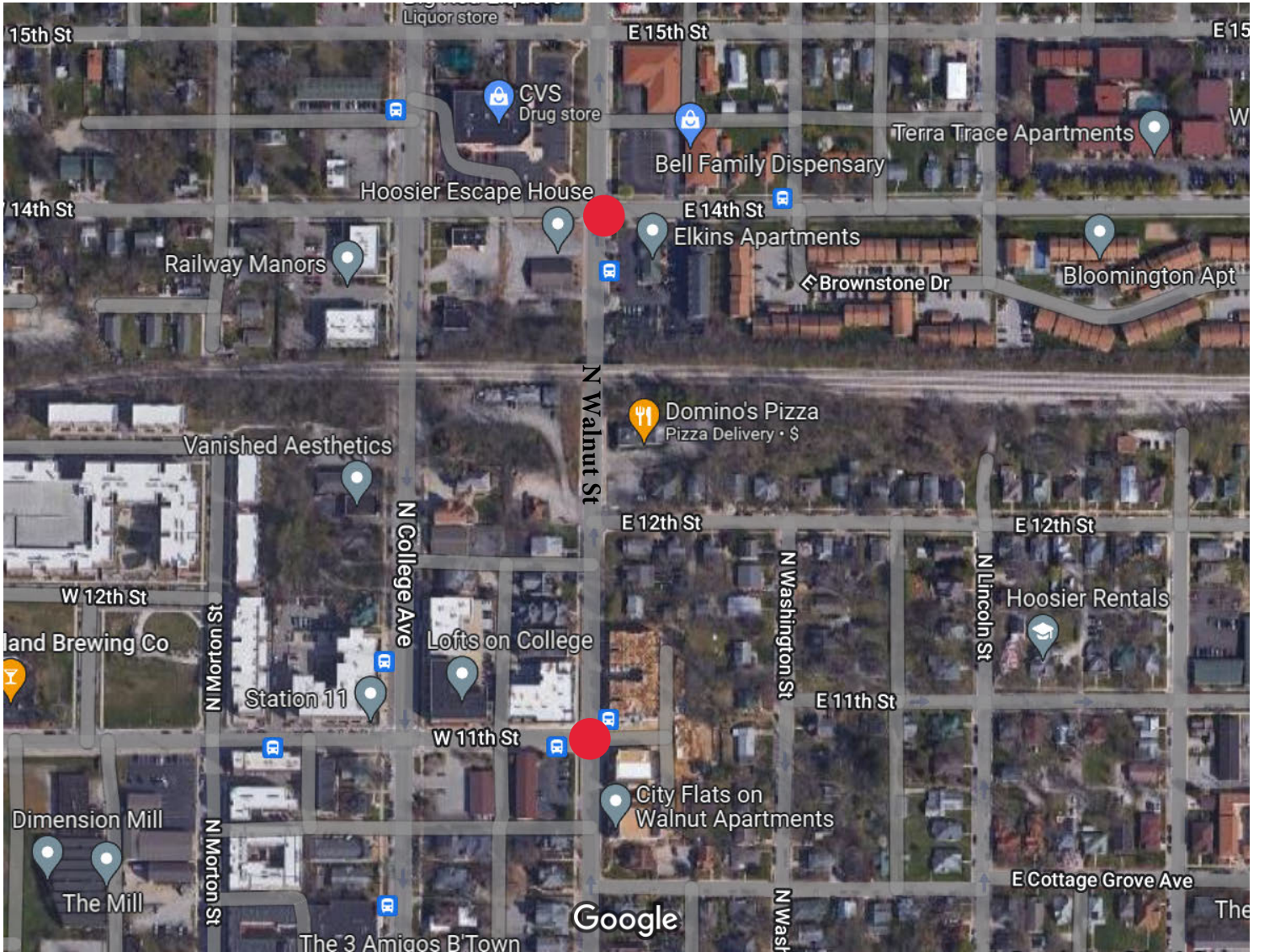
Section 1

Removing One Signalized intersection at 3rd Street and Franklin Road

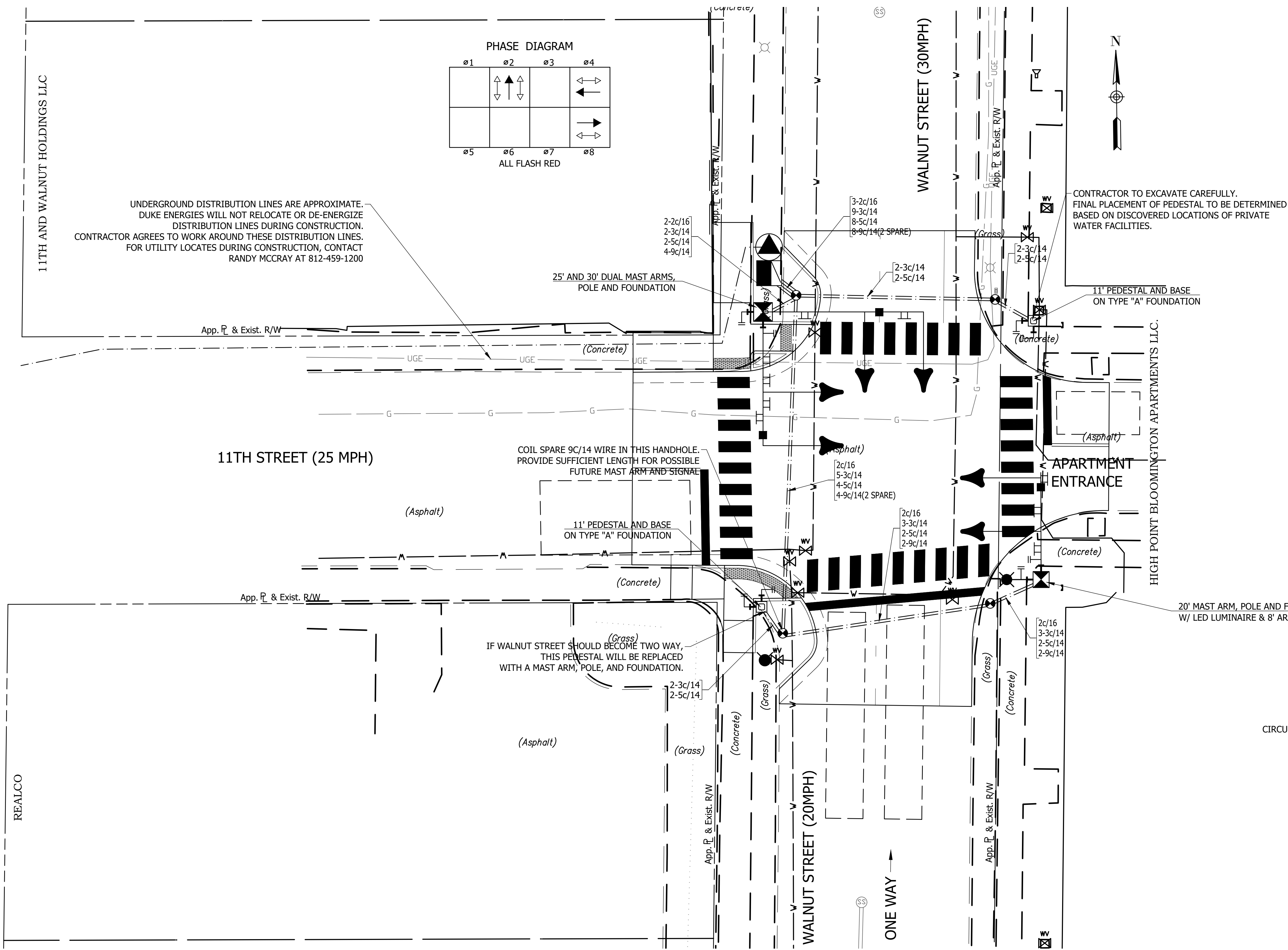


Section 1

Adding Two Signalized Intersections in Walnut Street at 11th Street and 14th Street



Adding One Signalized Intersections in Walnut Street at 11th Street

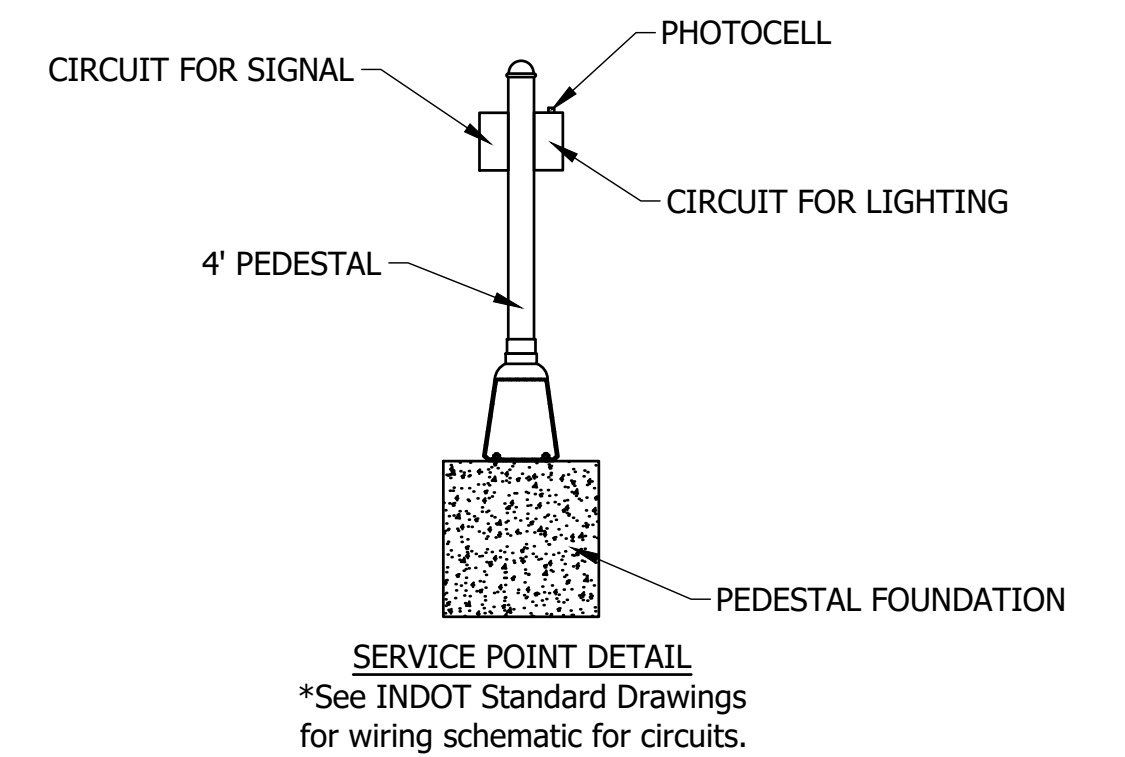


LEGEND

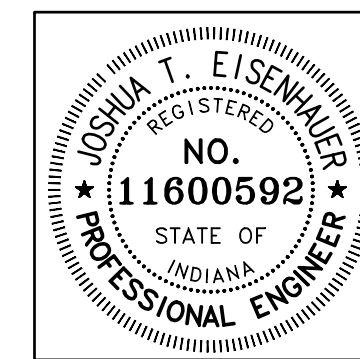
- 1-WAY, 3-SECTION (12-INCH RED, 12-INCH AMBER, 12-INCH GREEN) SIGNAL INDICATION
- CONTROLLER, 8-PHASE, MENU DRIVEN, IN CABINET ON BASE
- MAST ARM POLE AND FOUNDATION
- 11-FOOT PEDESTAL AND FOUNDATION
- HANDHOLE
- PEDESTRIAN INDICATION
- 2-INCH CONDUIT
- AUDIBLE PEDESTRIAN PUSH BUTTON AND SIGN
- VIDEO DETECTION UNIT
- SHEET SIGN (SEE SIGNING & MARKING PLAN)
- SERVICE POINT (FOR SIGNAL & LIGHTING)
- VIDEO DETECTION ZONE
- LED LUMINAIRE ON 8' ARM

NOTES:

- ADDITIONAL 9C/14 WIRE LABELED AS "SPARE" IS INSTALLED FOR THE POTENTIAL OF WALNUT STREET TO BE A 2-WAY ROAD IN THE FUTURE.
- ONE 3C/14 WIRE IS TO BE USED FOR LIGHTING POWER.
- PUSH BUTTONS TO BE WITHIN 10" REACH AND MOUNTED PERPENDICULAR TO CROSSINGS. USE OF EXTENSIONS IS ALLOWED TO ACHIEVE 10" REACH, WITH CITY APPROVAL.
- PEDESTRIAN INDICATION UNITS TO BE INSTALLED SUCH THAT ACCESS FOR MAINTENANCE CAN BE ACHIEVED WITHOUT REMOVING OR REPOSITIONING THE UNIT.
- FINAL LOCATION OF HANDHOLES AND CONDUIT TO BE FIELD PLACED TO AVOID UNDERGROUND UTILITY CONFLICTS.



DIRECTORY...F:\2019\19-4230 bloomington signals walnut & 11th and walnut & 14th\30 sheetdrawings\70 Signal
 FILE...4230 Signal Plans.dwg
 DIMSCALE...1 LTSCALE...1
 PLOTTED BY...J.Eisenbauer
 DATE...Jul 27, 2021 - 2:55pm



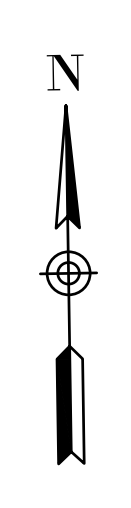
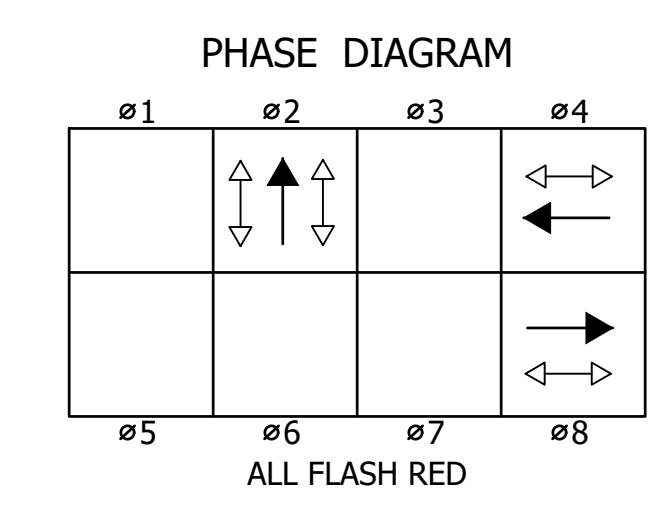
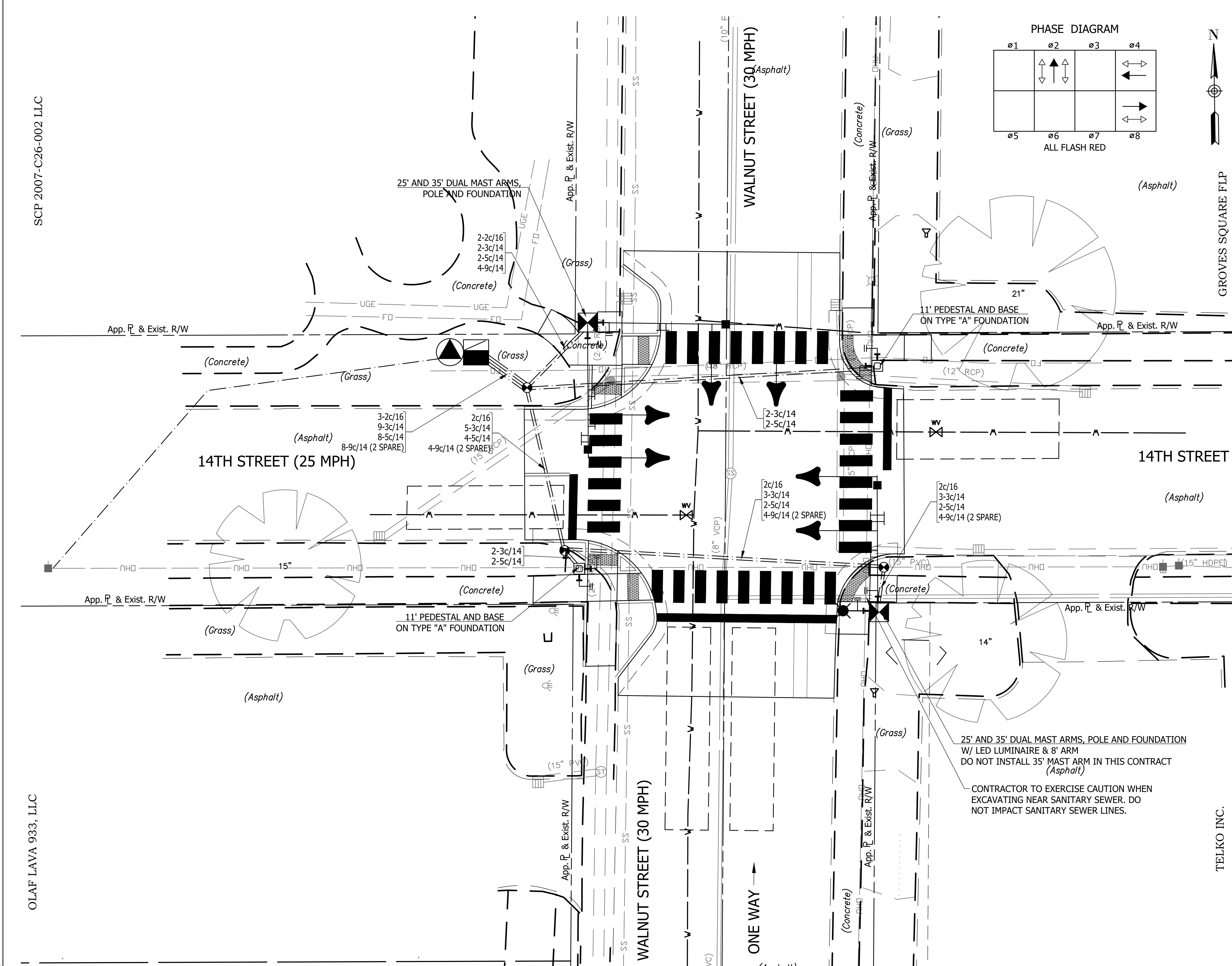
RECOMMENDED FOR APPROVAL	<i>Joshua T. Eisenbauer</i>	6/23/2021
DESIGNED:	JTE	DRAWN:
CHECKED:	NRJ	CHECKED:
		VSENG
		JTE

CITY OF BLOOMINGTON

WALNUT ST. & 11TH ST.
SIGNAL LAYOUT

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	N/A
SURVEY BOOK	DESIGNATION
	N/A
	SHEETS
CONTRACT	14 of 19
	PROJECT
	19-4230

Adding One Signalized Intersections in Walnut Street at 14th Street



- ### LEGEND
- 1-WAY, 3-SECTION (12-INCH RED, 12-INCH AMBER, 12-INCH GREEN) SIGNAL INDICATION
 - CONTROLLER, 8-PHASE, MENU DRIVEN, IN CABINET ON BASE
 - MAST ARM POLE AND FOUNDATION
 - 11-FOOT PEDESTAL AND FOUNDATION
 - HANDHOLE
 - PEDESTRIAN INDICATION
 - 2-INCH CONDUIT
 - AUDIBLE PEDESTRIAN PUSH BUTTON AND SIGN
 - VIDEO DETECTION UNIT
 - SHEET SIGN (SEE SIGNING & MARKING PLAN)
 - SERVICE POINT (FOR SIGNAL & LIGHTING)
 - VIDEO DETECTION ZONE
 - LED LUMINAIRE ON 8' ARM

NOTES:

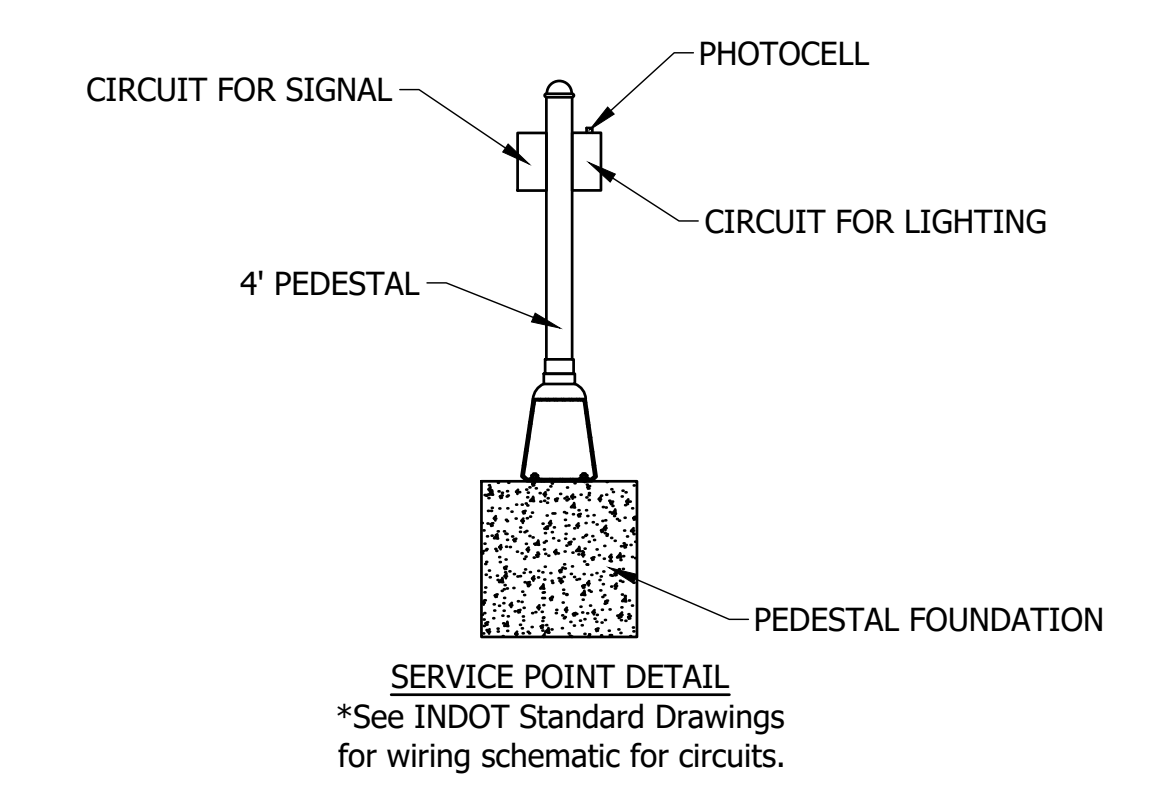
ADDITIONAL 9C/14 WIRE LABELED AS "SPARE" IS INSTALLED FOR THE POTENTIAL OF WALNUT STREET TO BE A 2-WAY ROAD IN THE FUTURE.

ONE 3C/14 WIRE IS TO BE USED FOR LIGHTING POWER.

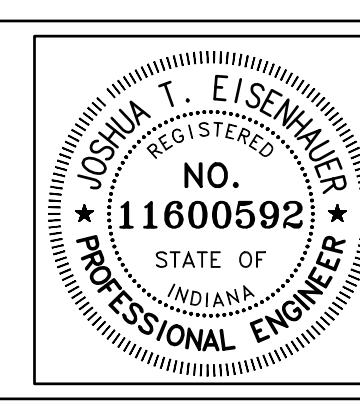
PUSH BUTTONS TO BE WITHIN 10" REACH AND MOUNTED PERPENDICULAR TO CROSSINGS. USE OF EXTENSIONS IS ALLOWED TO ACHIEVE 10" REACH, WITH CITY APPROVAL.

PEDESTRIAN INDICATION UNITS TO BE INSTALLED SUCH THAT ACCESS FOR MAINTENANCE CAN BE ACHIEVED WITHOUT REMOVING OR REPOSITIONING THE UNIT.

FINAL LOCATION OF HANDHOLES AND CONDUIT TO BE FIELD PLACED TO AVOID UNDERGROUND UTILITY CONFLICTS.



DIRECTORY...F:\2019\19-4230 Bloomington signals walnut & 14th and walnut & 14th\30 sheetdrawings\70 Signal
 FILE...4230 Signal Plans.dwg
 DIMSCALE...1 LTSCALE...1
 PLOTTED BY...J.Eisenhauer
 DATE...Jul 27/2021 - 2:55pm



RECOMMENDED FOR APPROVAL	<i>Joshua T. Eisenhauer</i>	6/23/2021
DESIGNED:	JTE	DRAWN:
CHECKED:	NRJ	CHECKED:
		DATE
		DESIGN ENGINEER
		STATE OF INDIANA
		REGISTERED PROFESSIONAL ENGINEER
		NO. 11600592
		DATE

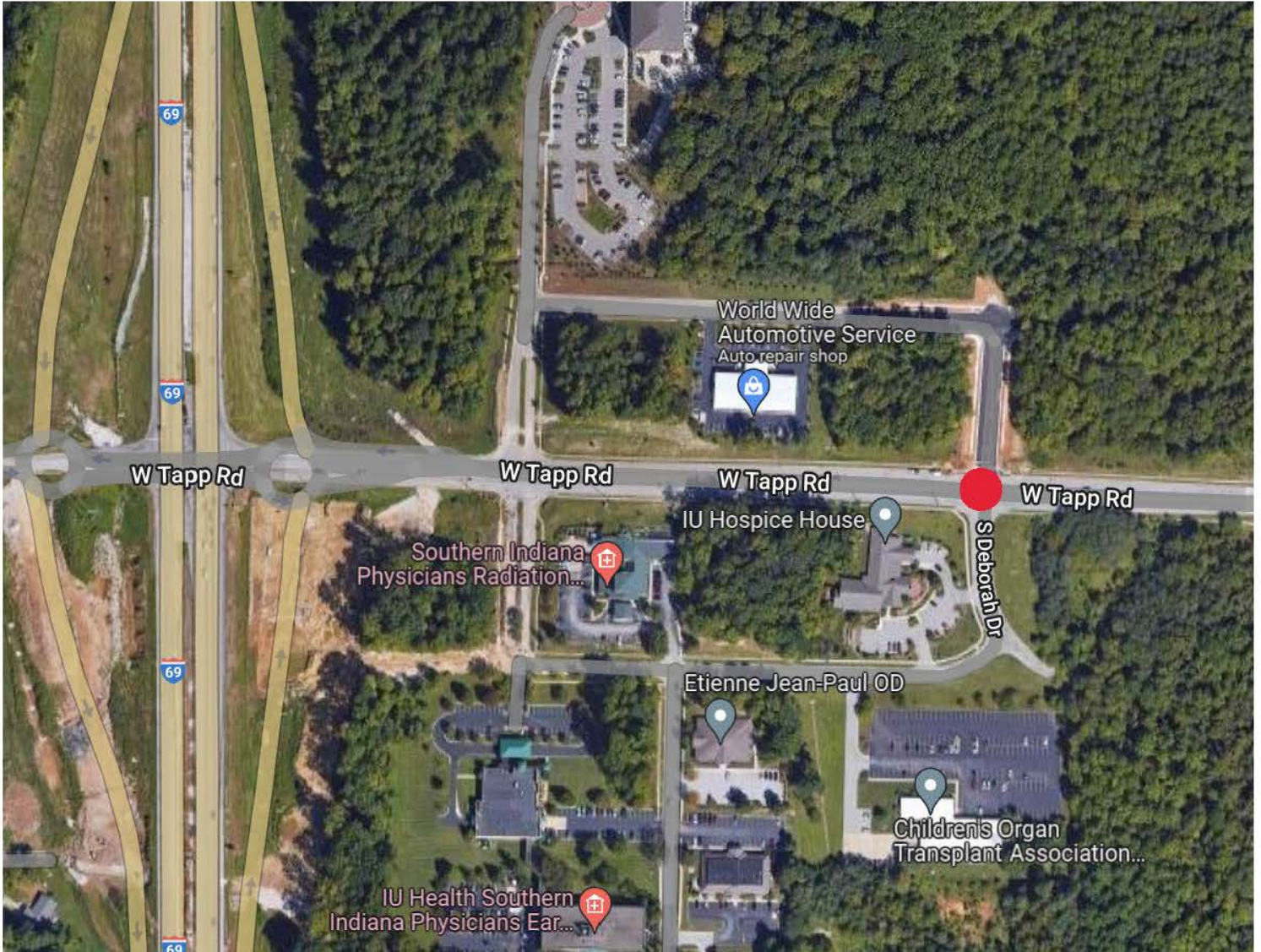
CITY OF BLOOMINGTON

WALNUT ST. & 14TH ST.
SIGNAL LAYOUT

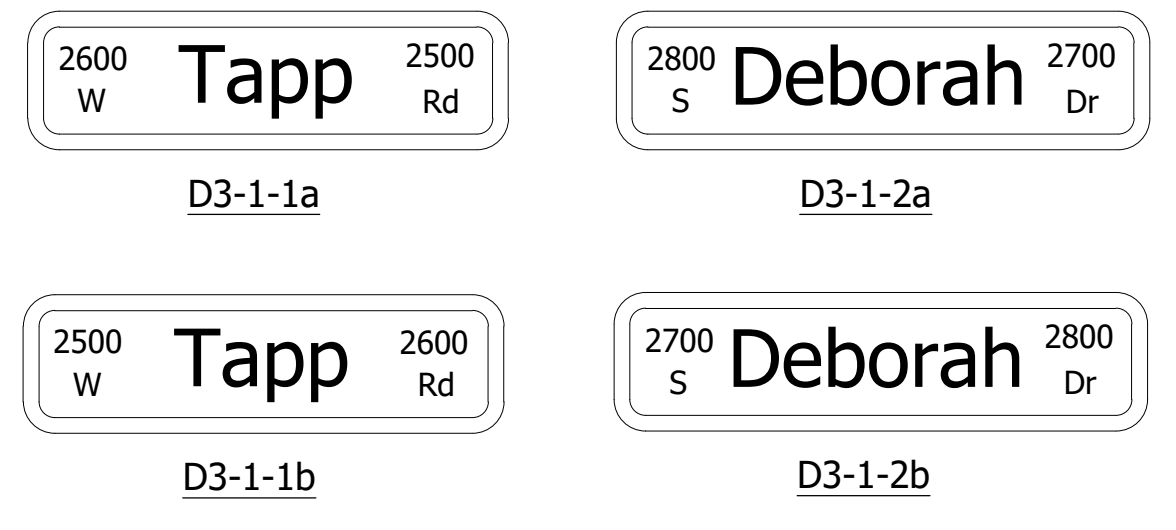
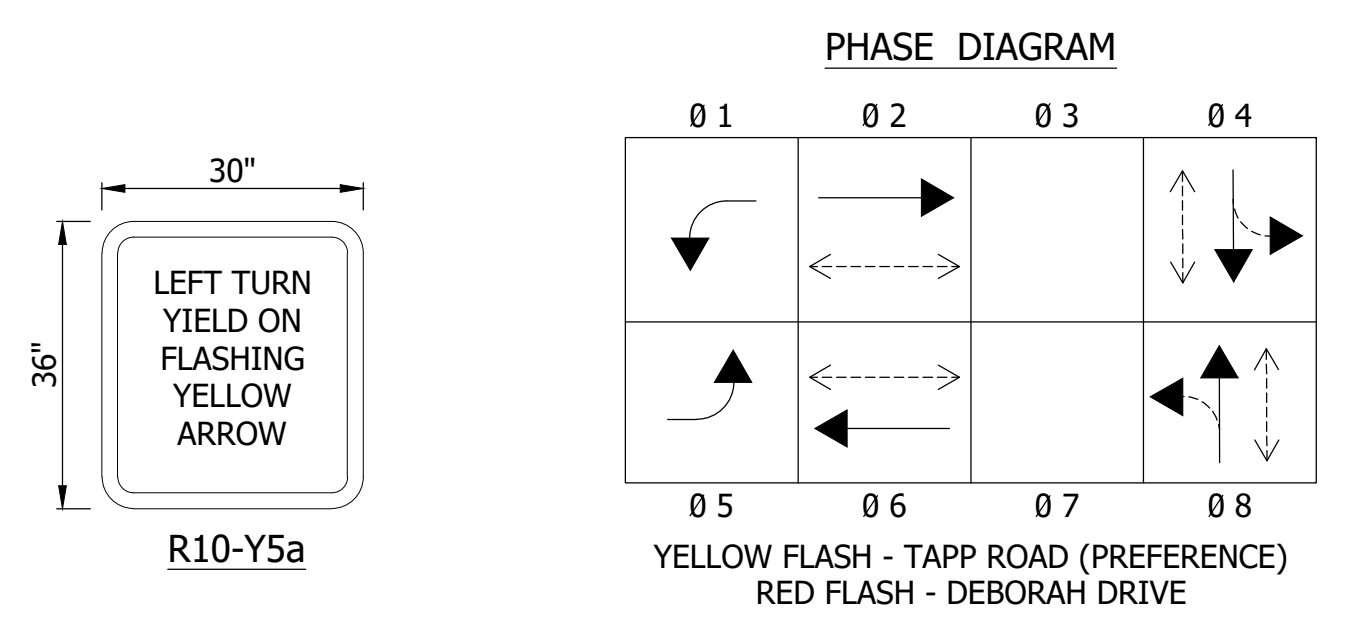
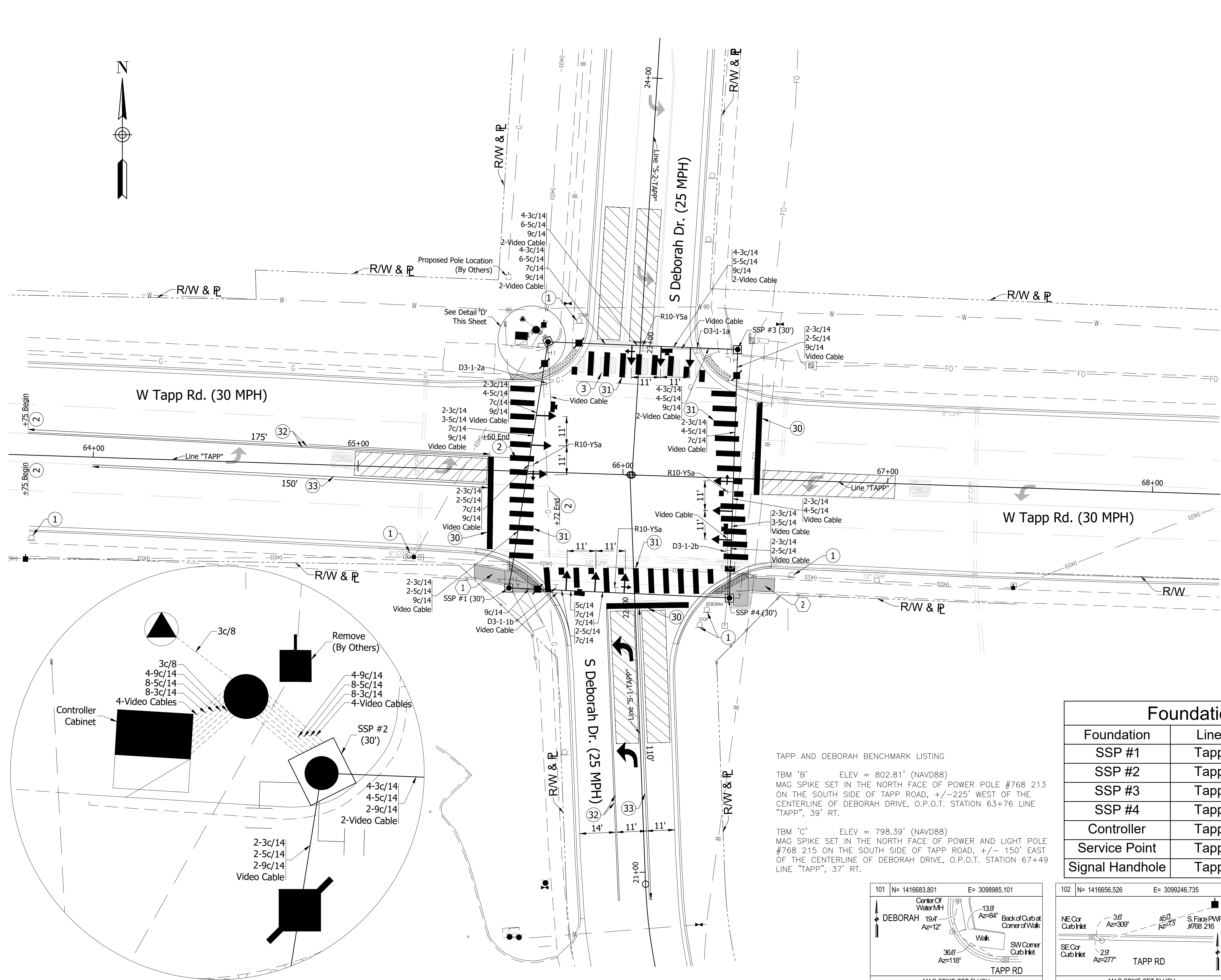
HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	N/A
SURVEY BOOK	DESIGNATION
CONTRACT	N/A
	SHEETS
	15 of 19
	PROJECT
	19-4230

Section 1

Adding One signalized intersection at Tapp Road and Deborah Drive



Adding One signalized intersection at Tapp Road and Deborah Drive



See City of Bloomington Standard Drawing SS-1, Street Name Sign Details

- ### LEGEND
- ① Sign, Remove
 - ② Line, Remove
 - ③ Transverse Marking, Remove
 - ③① Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
 - ③② Transverse Marking, Thermoplastic, Crosswalk Line, 24 in. See Detail, Sheet 9
 - ③③ Line, Thermoplastic, Solid, Yellow, 4 in.
 - ③④ Line, Thermoplastic, Solid, White, 4 in.
 - ⊕ Curb Ramp No. __, Concrete, Sheet No. 9
 - Span, Catenary, and Tether
 - 2" Galvanized Steel Conduit
 - Silt Fence
 - Signal Handhole, Type 1
 - ⊠ Steel Strain Pole (30')
 - ▲ Signal Service Point, H-Frame Structure Mounted See Duke Energy 'Gold Book' Figure 117
 - Disconnect Hanger
 - ⊕ 18" LED Pedestrian Signal Indication with Countdown Timer and APS Push Button (Speech Walk Message) and R10-3e Sign
 - 1-Way, 3-Section (12" Red LED, 12" Yellow LED, 12" Green LED) Signal Indication With Backplate
 - ↔ 1-Way, 4-Section (12" Red LED, 12" Yellow Arrow LED, 12" Flashing Yellow Arrow LED, 12" Green Arrow LED) Signal Indication With Backplate
 - ⊕ 8-Phase Fully Actuated Econolite Cobalt TS-2 Type 1 Controller, Applied Information unit 500-085-02, and Cabinet on 'P' Foundation
 - ⊕ Video Detection Camera With 25' Mast Arm (Autoscope RackVision Terra)
 - ▨ Video Detection Zone
 - ⊕ Curb Ramp, Concrete
 - ↔ Proposed Message Marking, Thermoplastic, Lane Indication Arrow

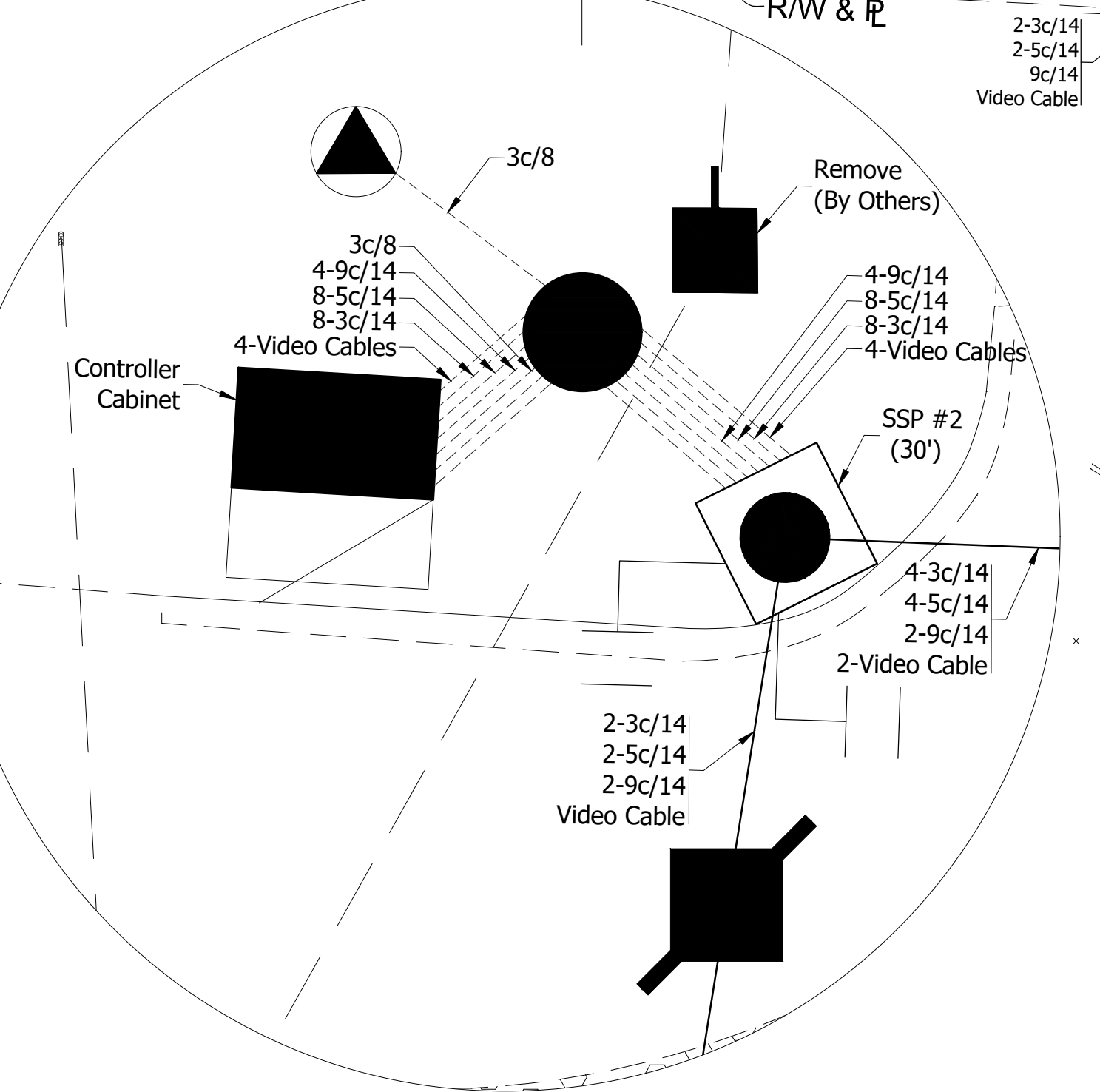
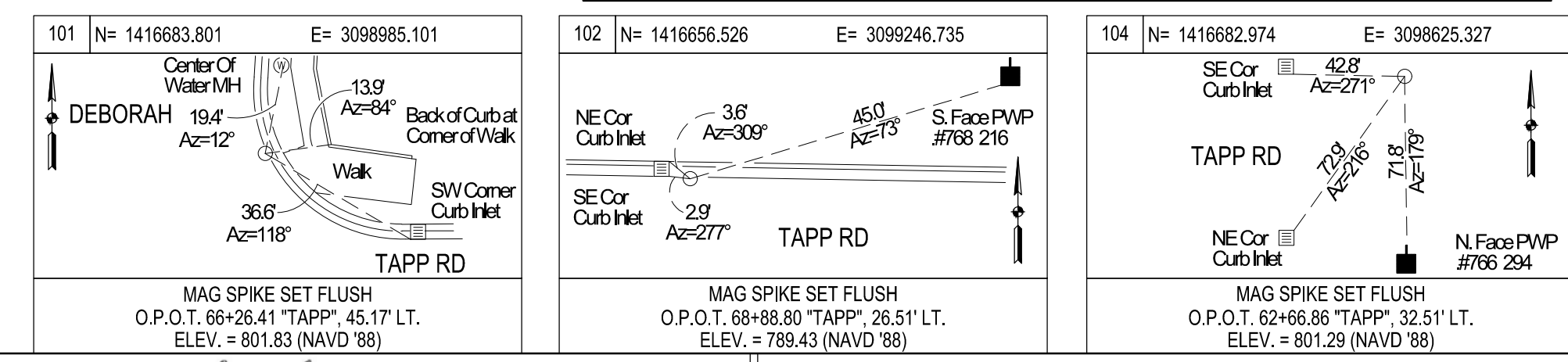
Foundation Schedule

Foundation	Line	Station	Offset
SSP #1	Tapp	65+58	44.1' Rt.
SSP #2	Tapp	65+70	49.7' Lt.
SSP #3	Tapp	66+42	48.6' Lt.
SSP #4	Tapp	66+42	45.2' Rt.
Controller	Tapp	65+60	47.8' Lt.
Service Point	Tapp	65+60	57.4' Lt.
Signal Handhole	Tapp	65+65	53.6' Lt.

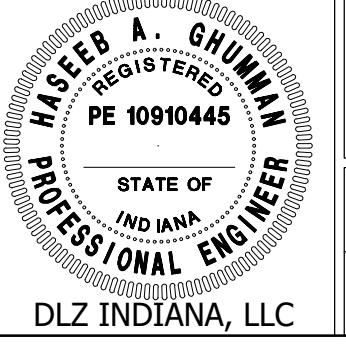
TAPP AND DEBORAH BENCHMARK LISTING

TBM 'B' ELEV = 802.81' (NAVD88)
MAG SPIKE SET IN THE NORTH FACE OF POWER POLE #768 213 ON THE SOUTH SIDE OF TAPP ROAD, +/- 225' WEST OF THE CENTERLINE OF DEBORAH DRIVE, O.P.O.T. STATION 63+76 LINE "TAPP", 39' RT.

TBM 'C' ELEV = 798.39' (NAVD88)
MAG SPIKE SET IN THE NORTH FACE OF POWER AND LIGHT POLE #768 215 ON THE SOUTH SIDE OF TAPP ROAD, +/- 150' EAST OF THE CENTERLINE OF DEBORAH DRIVE, O.P.O.T. STATION 67+49 LINE "TAPP", 37' RT.



- ### NOTES:
- All Signal Heads Shall Be Black With Black Backplates And Yellow Retroreflective Strips.
 - Video Cables Shall Be Installed Per Manufactures Recommendations.
 - Video Cables Denote A Coaxial Cable And A 3c/14 Cable for Video Detection Camera.
 - Add 6 in. Pushbutton Assembly Extensions At Each Location, As Directed By The Engineer.



RECOMMENDED FOR APPROVAL: *Hasep Ghuman* DESIGN ENGINEER DATE: 05/14/2024

DESIGNED: TAB DRAWN: TAB

CHECKED: HAG CHECKED: JRS

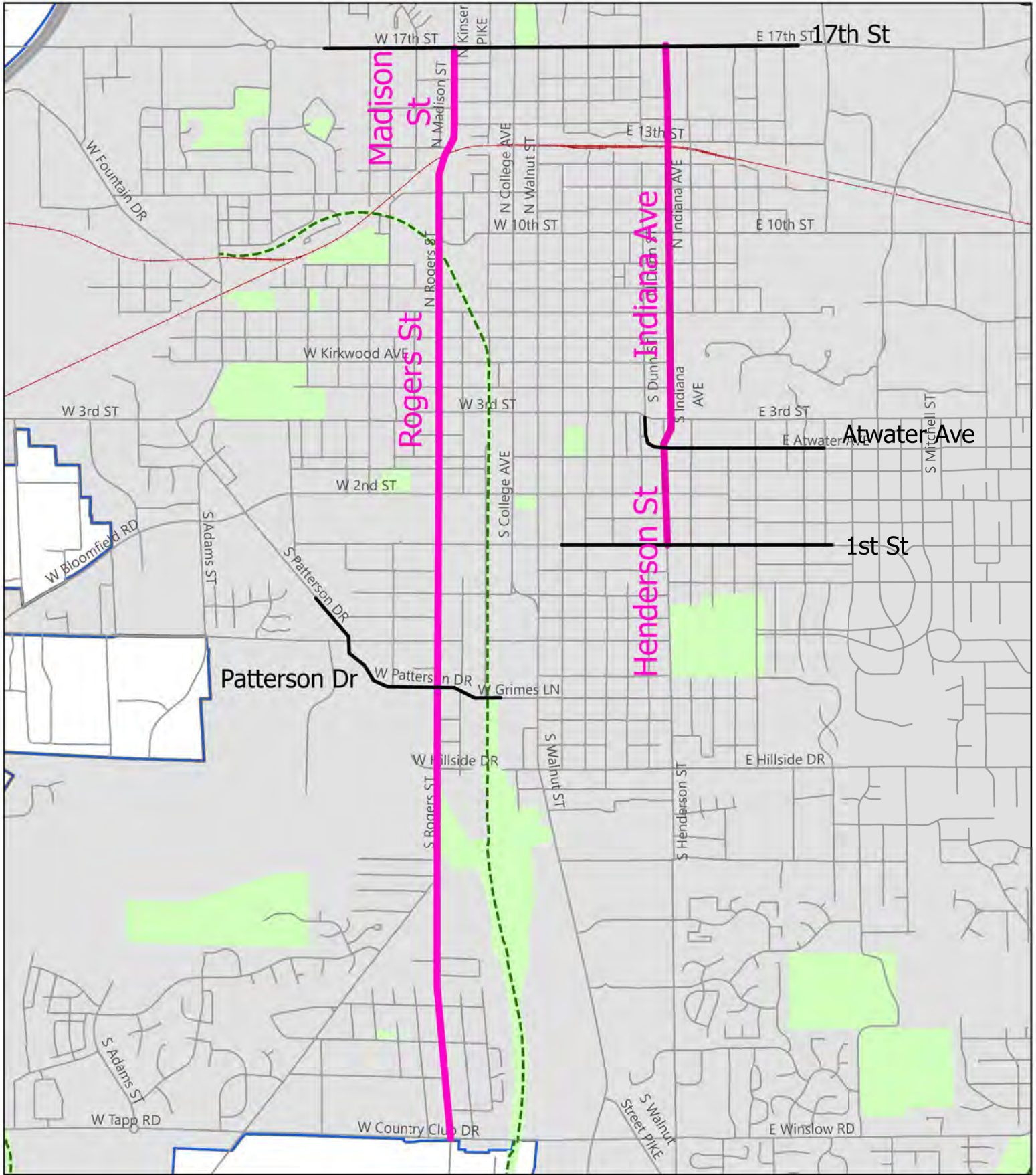
INDIANA DEPARTMENT OF TRANSPORTATION

SIGNAL PLAN
TAPP ROAD AND DEBORAH DRIVE

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	---
VERTICAL SCALE	DESIGNATION
---	---
SURVEY BOOK	SHEETS
---	8 of 9
CONTRACT	PROJECT
---	---

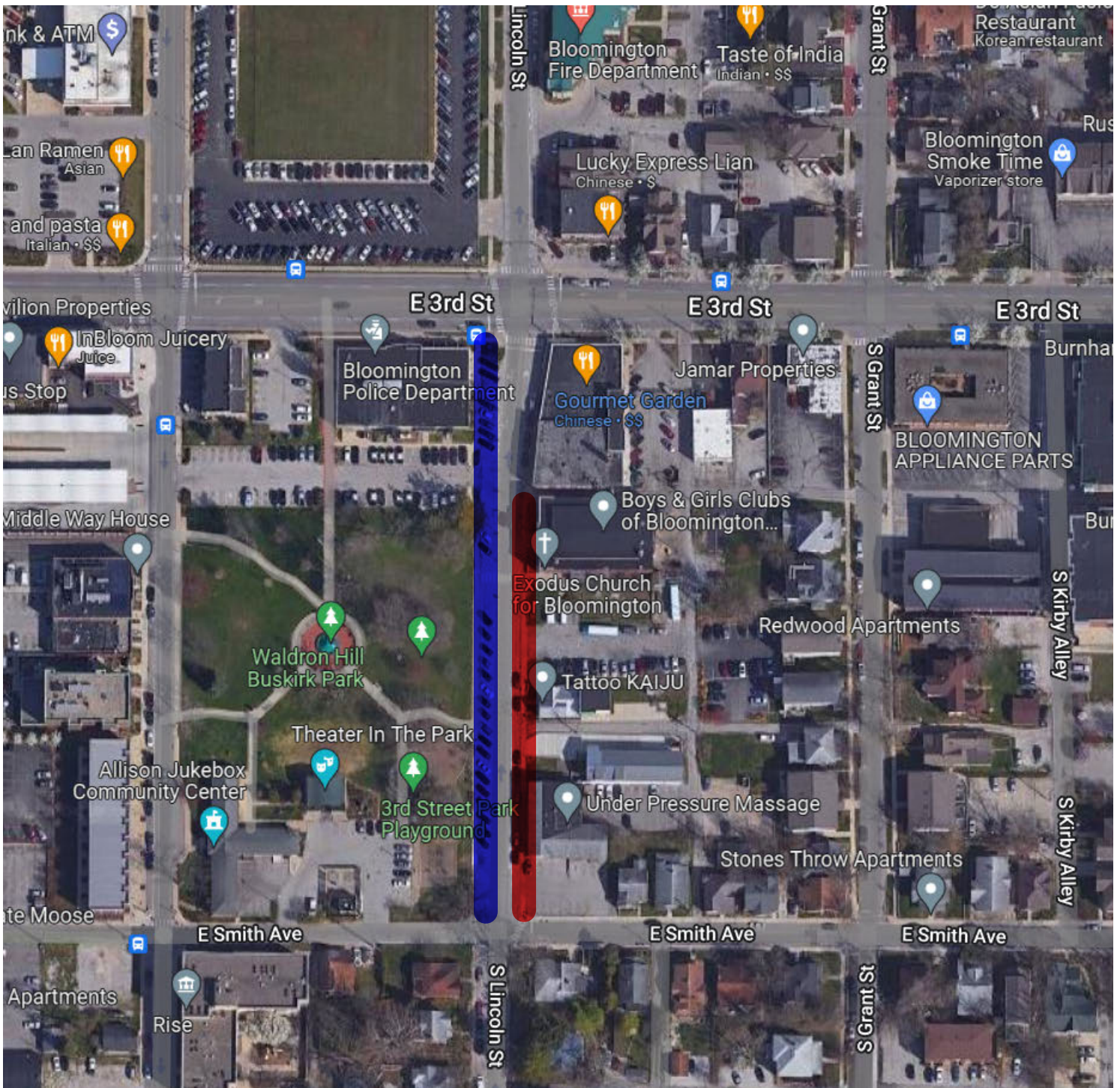
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Section 3 - Changing Speed Limit at 4 Locations

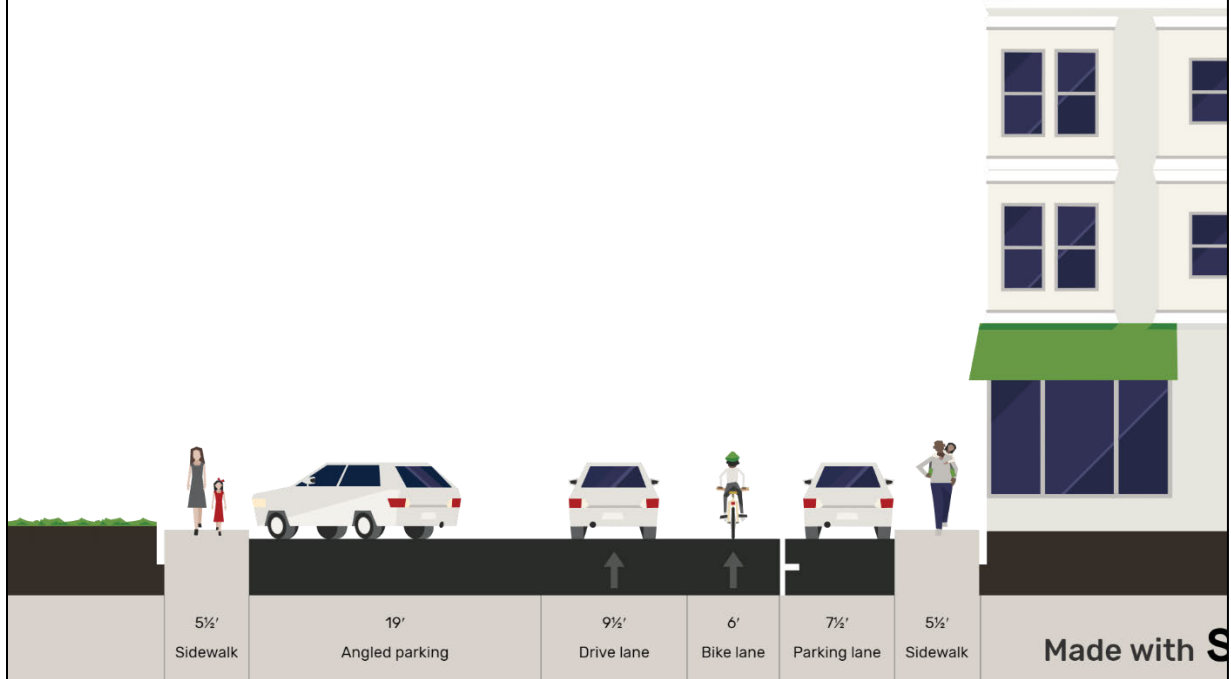


Section 4

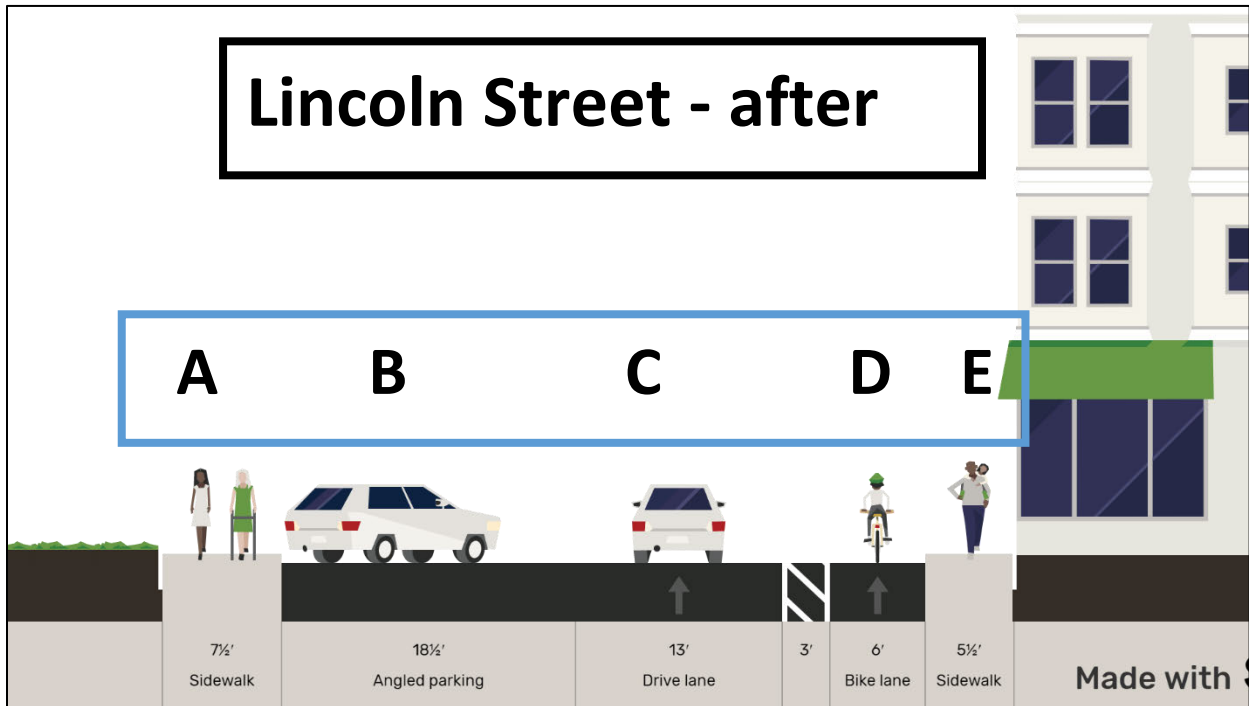
- Providing Back-In Angle Parking on the West Side of Lincoln Street between E. Smith Avenue and E. 3rd Street (Blue Area on Map)
- Adding No Parking Spaces on the East Side of Lincoln Street from E. Smith Avenue to 140' south of E. 3rd Street (Red Area on Map)

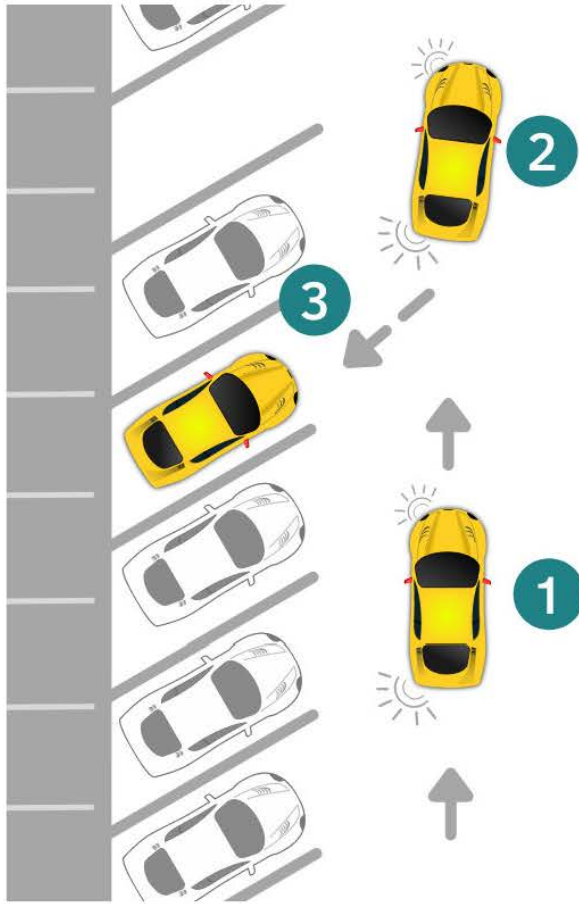


Lincoln - before



Lincoln Street - after





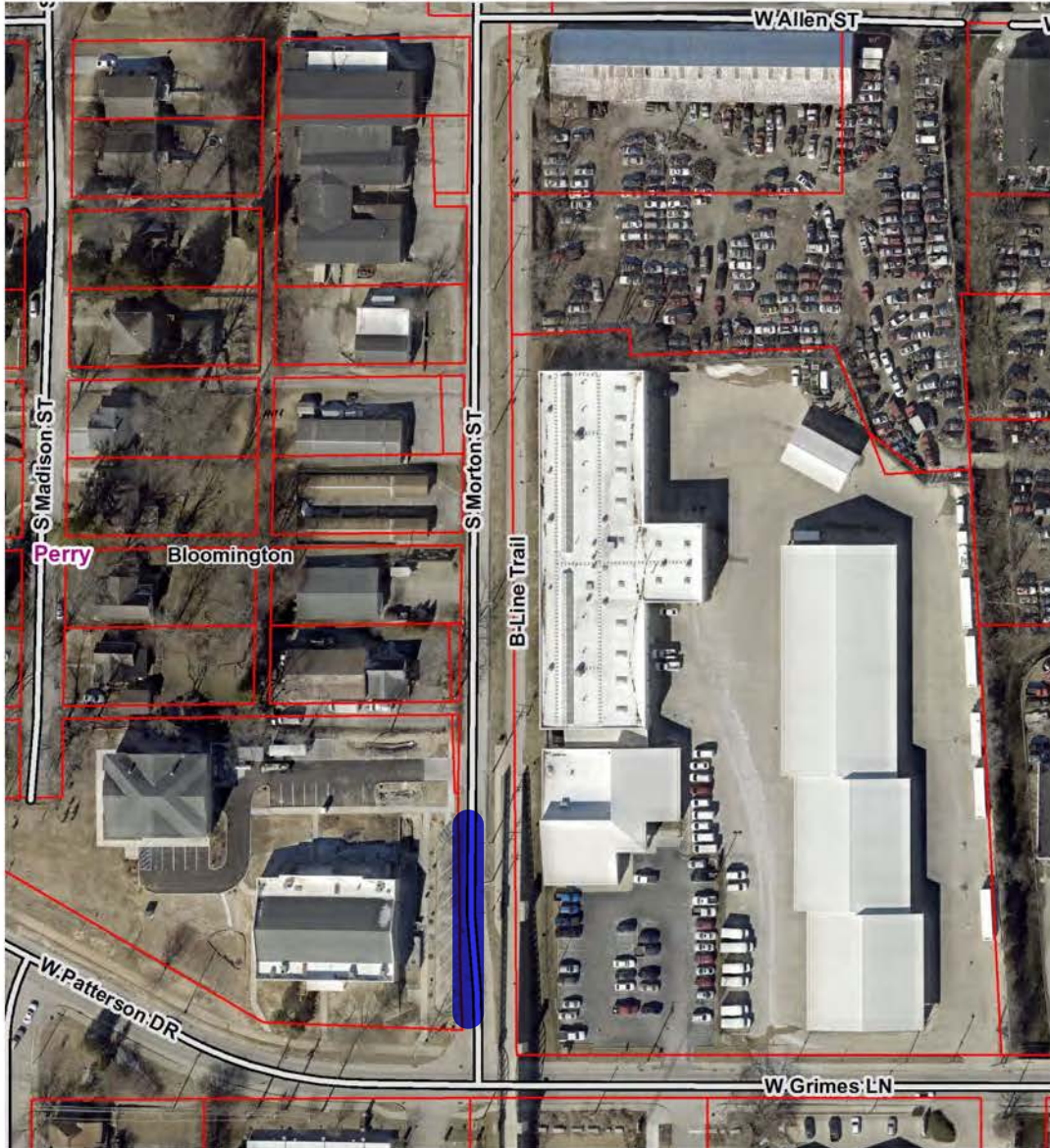
How to back-in angle park:

- 1 Signal and pull forward.
- 2 Stop just past the available parking space.
- 3 Reverse into the parking space.



Section 4

Adding Pull-In Angle Parking on the West Side of S. Morton Street from W. Patterson Drive and W. Grimes Lane to 190' north of W. Patterson Drive and W. Grimes Lane (Blue Area on Map)



The above map and photo show 1200 S. Morton Street
029

BID ALTERNATE NOTES

BID ALTERNATE NO. 1: CONTRACTOR TO PROVIDE ALL SITE PLANTINGS AND INSTALLATION.

BID ALTERNATE NO. 2: CONTRACTOR TO PROVIDE AND INSTALL SITE FENCING.

PLAN REVISIONS

12.2.20 - CHANGED OFFSTREET PARKING ALONG MORTON ST. TO 45° ANGLED PARKING, PAVEMENT C/O "D", ADDED AN ADA VAN PARKING SPACE W/ RAMP, ADJUSTED ADJACENT CURBS AND GRADES AND CHANGED PARKING SPACE MARKINGS TO THERMOPLASTIC. ELIMINATED BOTH ADA CURB RAMPS AT DRIVEWAY ENTRANCE, CHANGED DRIVEWAY ENTRANCE TO CONCRETE AND ADJUSTED GRADES. ON-SITE PARKING SPACES REMAIN AS PAINTED LINES. ADDED MORTON ST. REPAIRS NOTE. ADDED NEW RAMP AT THE NW CORNER OF W. PATTERSON AND S. MORTON STREETS. Jt 12.10.20 - ADDED TRANSFORM PAD SOUTH OF THE "ROOF". Jt

2.5.21 - ADDED RECONSTRUCT NOTE TO BOTH SIDEWALK CONNECTIONS TO THE EX. PATTERSON ASPHALT WALK, CHANGED CURB STYLE TO 13B AT CORNER OF PATTERSON & MORTON, CHANGED SHAPE OF SOUTHERNMOST ON-STREET MORTON PARKING STALL. Jt

3.30.21 - ADDED 7 PARKING SPACES CONSTRUCTED OF PERVIOUS PAVERS. Jt

MORTON STREET REPAIRS

ANY DAMAGE TO MORTON STREET PAVEMENT ADJACENT TO THE PROJECT DETERMINED TO BE CAUSED BY ANY CONTRACTOR OR SUBCONTRACTOR ASSOCIATED WITH THIS PROJECT SHALL BE REPAIRED TO ORIGINAL CONDITION AS REQUIRED BY THE CITY OF BLOOMINGTON.

EXISTING LEGEND

- PROPERTY LINE
- x- FENCE
- XW- WATER LINE PIPE
- XXX- CONTOUR & ELEVATION
- XSS- SANITARY SEWER PIPE
- XST- STORM SEWER PIPE
- XOHE- OVERHEAD ELECTRIC LINES
- XUGE- UNDERGROUND ELECTRIC LINES
- XOHT- OVERHEAD TELEPHONE LINES
- XUGT- UNDERGROUND TELEPHONE LINE
- XGAS- GAS LINE PIPE

SITE LEGEND

- (A) BITUMINOUS PAVEMENT
- (B) BITUMINOUS PARKING PAVEMENT WITHIN R.O.W.
- (AP) ADA ACCESSIBLE PARKING SPACE, 35" INTERNATIONAL ADA SYMBOL, STRIPING, AISLES, SIGNAGE AND SLOPES SHALL CONFORM WITH THE MOST STRINGENT OF FEDERAL, STATE AND LOCAL ACCESSIBLE GUIDELINES.
- (APR) ADA ACCESSIBLE PARKING SIGN
- (C) REINFORCED CONCRETE PAVEMENT
- (CL) OR - CL - CONSTRUCTION LIMITS
- (CT) CONCRETE CURB TURNOUT
- (EC) END CONCRETE CURB CONSTRUCTION
- (F) CONCRETE SIDEWALK
- (FI) MONOLITHIC CONCRETE CURB AND SIDEWALK
- (GR) 42" GUARDRAIL - SEE ARCHITECTURAL PLANS
- (HR) 36" HANDRAIL - SEE ARCHITECTURAL PLANS
- (L) LAWN OR LANDSCAPED AREA
- (M) MATCH EXISTING CURB, SIDEWALK, PAVEMENT ELEVATIONS
- (MP) MULTI-PURPOSE SIDEWALK PAVEMENT SECTION
- (P) PERVIOUS BRICK PAVEMENT
- (PR) PRESSURE TREATED WOOD SHADOWBOX PRIVACY FENCE, HEIGHT AS SHOWN ON SITE PLAN
- (PL) NEW PARKING LOT POLE LIGHT FIXTURE - SEE SITE LIGHTING NOTE THIS SHEET
- (PSD) STANDARD ROAD PAVEMENT PATCH
- (3) 3" WIDE CONCRETE PAVED SIDE DITCH
- (R) SIDEWALK ADA ACCESSIBLE RAMP, # FOR STYLES 1-3
- (RW) REINFORCED CONCRETE RETAINING WALL - SEE ARCHITECTURAL PLANS
- (VA) VAN ACCESSIBLE SUPPLEMENTAL SIGN
- (13) 6-IN WIDE CONCRETE CURB
- (13A) 4-IN CONCRETE CURB, SAME AS 13 BUT 3-IN CURB HEIGHT
- (13B) 18-IN CONCRETE CURB & GUTTER
- (13C) PERVIOUS PAVEMENT CONCRETE CURB/CAP FLUSH W/ PAVEMENT
- (20) 4-IN WIDE SOLID WHITE THERMOPLASTIC PAVEMENT MARKING
- (21) 4-IN WIDE SOLID WHITE PAINTED PAVEMENT MARKING
- (22) 4-IN WIDE SOLID BLUE PAINTED ADA PAVEMENT MARKING
- (XX) NUMBER OF PARKING SPACES PER LOT

UTILITY LEGEND

- FSL 4" POLYETHYLENE ENCASED DIP FIRE SERVICE LINE (PRIVATELY OWNED)
- DSL 2" SDR-21 PVC DOMESTIC WATER SERVICE LINE (PRIVATELY OWNED)
- 4W 4" POLYETHYLENE ENCASED DIP WATERLINE (PRIVATELY OWNED)
- 6W 6" POLYETHYLENE ENCASED DIP WATERLINE (OWNED BY CBU)
- W WATER LINE GATE VALVE
- F.D.C. FIRE DEPARTMENT "STORZ" CONNECTION
- P.I.V. POST INDICATING VALVE
- 6SL 6" SDR-35 PVC SANITARY SEWER LATERAL (PRIVATELY OWNED)
- 12ST 12" ADS BRAND SERIES N-12 HDPE STORM SEWER PIPE (PRIVATELY OWNED)
- 15ST 15" ADS BRAND SERIES N-12 HDPE STORM SEWER PIPE (PRIVATELY OWNED)
- 4UD 4" PERFORATED SDR-35 PVC UNDERDRAIN PIPE (PRIVATELY OWNED)
- 6UD 6" PERFORATED SDR-35 PVC UNDERDRAIN PIPE (PRIVATELY OWNED)
- 8UD 6" PERFORATED SDR-35 PVC UNDERDRAIN PIPE (PRIVATELY OWNED)
- 4UDS 4" SDR-35 PVC PIPE (PRIVATELY OWNED)
- 6UDS 6" SDR-35 PVC PIPE (PRIVATELY OWNED)
- 6BD 6" SDR-35 PVC BUILDING STORM DRAIN PIPE (PRIVATELY OWNED)
- 8BD 8" SDR-35 PVC BUILDING STORM DRAIN PIPE (PRIVATELY OWNED)
- 12BD 12" SDR-35 PVC BUILDING STORM DRAIN PIPE (PRIVATELY OWNED)
- GAS GAS SERVICE LINE - CONTACT VECTREN ENERGY
- ES 3" SCH. 40 PVC ELECTRICAL GRADE CONDUIT WITH PULL WIRE FOR COMMUNICATION LINES
- ELEC SCH. 40 PVC ELECTRICAL GRADE CONDUIT - SIZE AS REQUIRED BY THE NATIONAL ELECTRICAL CODE - SEE NOTE THIS SHEET

ARCHITECTURE
CIVIL ENGINEERING
PLANNING
BYNUM FAYO & ASSOCIATES, INC.
528 north street - Bloomington - Indiana
(812) 339-2996 (fax)
(812) 332-8050

CERTIFIED

No. 60018283
STATE OF INDIANA
PROFESSIONAL ENGINEER
9.11.2020

MATHEU ARCHITECTS, P.C.

205 N. College Ave.
Suite 010
Bloomington IN 47404
Tel: 812.339.1235
Fax: 812.339.1238

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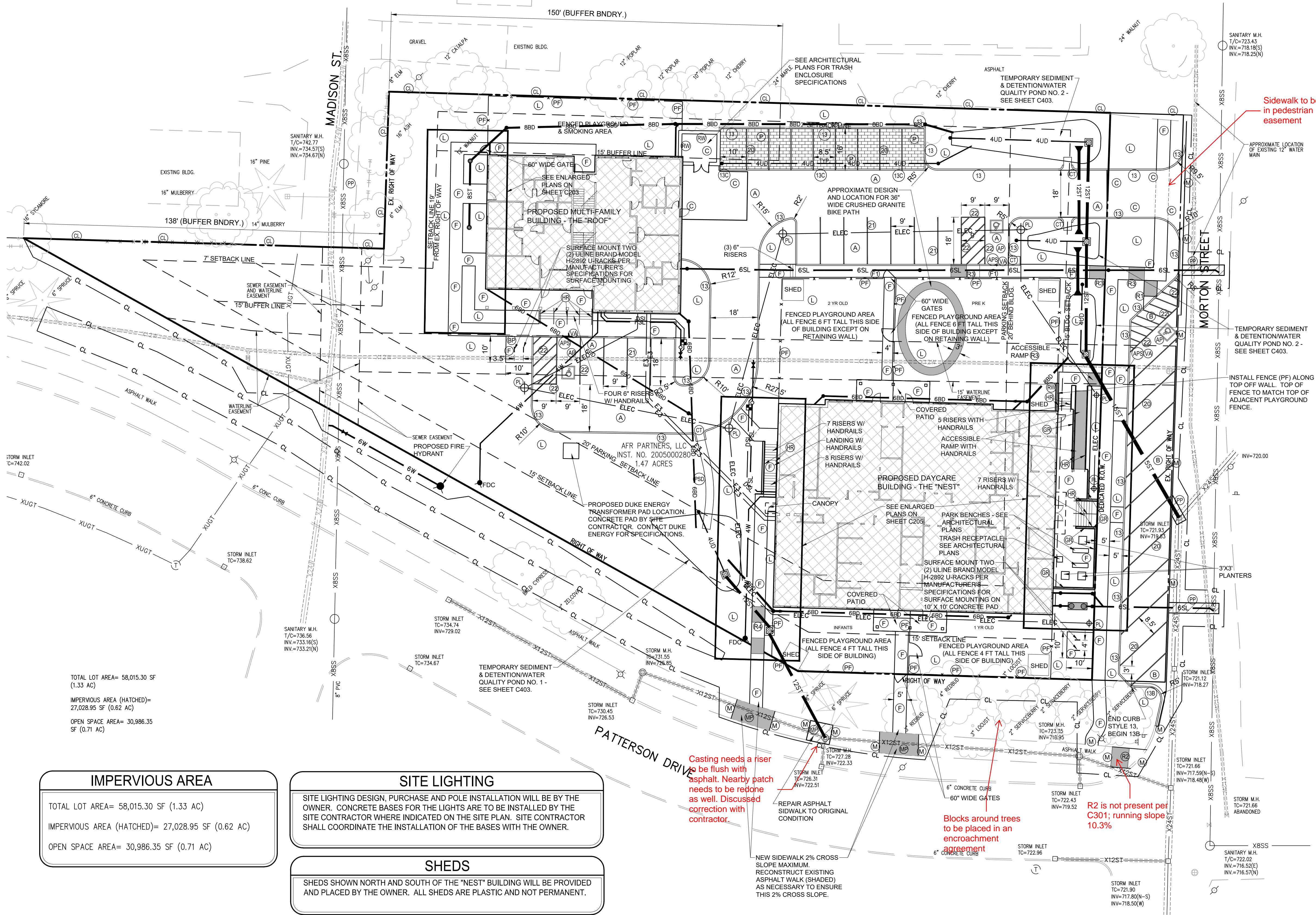
**NEW HOPE FOR FAMILIES
'ROOF' FAMILY SHELTER &
'NEST' CHILD CARE CENTER**

BLOOMINGTON - INDIANA

ARCHITECT'S PROJECT NO.: 1905
DATE: SEPTEMBER 11, 2020

SITE PLAN

SHEET NO.
C202
100% FINAL CD'S



IMPERVIOUS AREA

TOTAL LOT AREA= 58,015.30 SF (1.33 AC)
IMPERVIOUS AREA (HATCHED)= 27,028.95 SF (0.62 AC)
OPEN SPACE AREA= 30,986.35 SF (0.71 AC)

SITE LIGHTING

SITE LIGHTING DESIGN, PURCHASE AND POLE INSTALLATION WILL BE BY THE OWNER. CONCRETE BASES FOR THE LIGHTS ARE TO BE INSTALLED BY THE SITE CONTRACTOR WHERE INDICATED ON THE SITE PLAN. SITE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF THE BASES WITH THE OWNER.

SHEDS

SHEDS SHOWN NORTH AND SOUTH OF THE "NEST" BUILDING WILL BE PROVIDED AND PLACED BY THE OWNER. ALL SHEDS ARE PLASTIC AND NOT PERMANENT.

SIGNAGE

ALL PROJECT SIGNAGE TO BE PROVIDED AND INSTALLED BY THE OWNER. THIS SHALL BE COORDINATED WITH THE CONTRACTOR TO ENSURE THAT ANY CODE REQUIRED SIGNS ARE IN PLACE PRIOR TO THE INSPECTION FOR THE CERTIFICATE OF OCCUPANCY.

NOTE TO CONTRACTOR

CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS & DEPTHS AND NOTIFY ENGINEER OF ANY INACCURACIES IN LOCATION OR ELEVATION OR ANY CONFLICTS PRIOR TO & AFTER ANY EXCAVATION. NO PAYMENT SHALL BE MADE TO CONTRACTOR FOR UTILITY DESTRUCTION OR UNDERGROUND CHANGES REQUIRED DUE TO CONFLICTING ELEVATIONS.

Casting needs a riser to be flush with asphalt. Nearby patch needs to be redone as well. Discussed correction with contractor.

Blocks around trees to be placed in an encroachment agreement

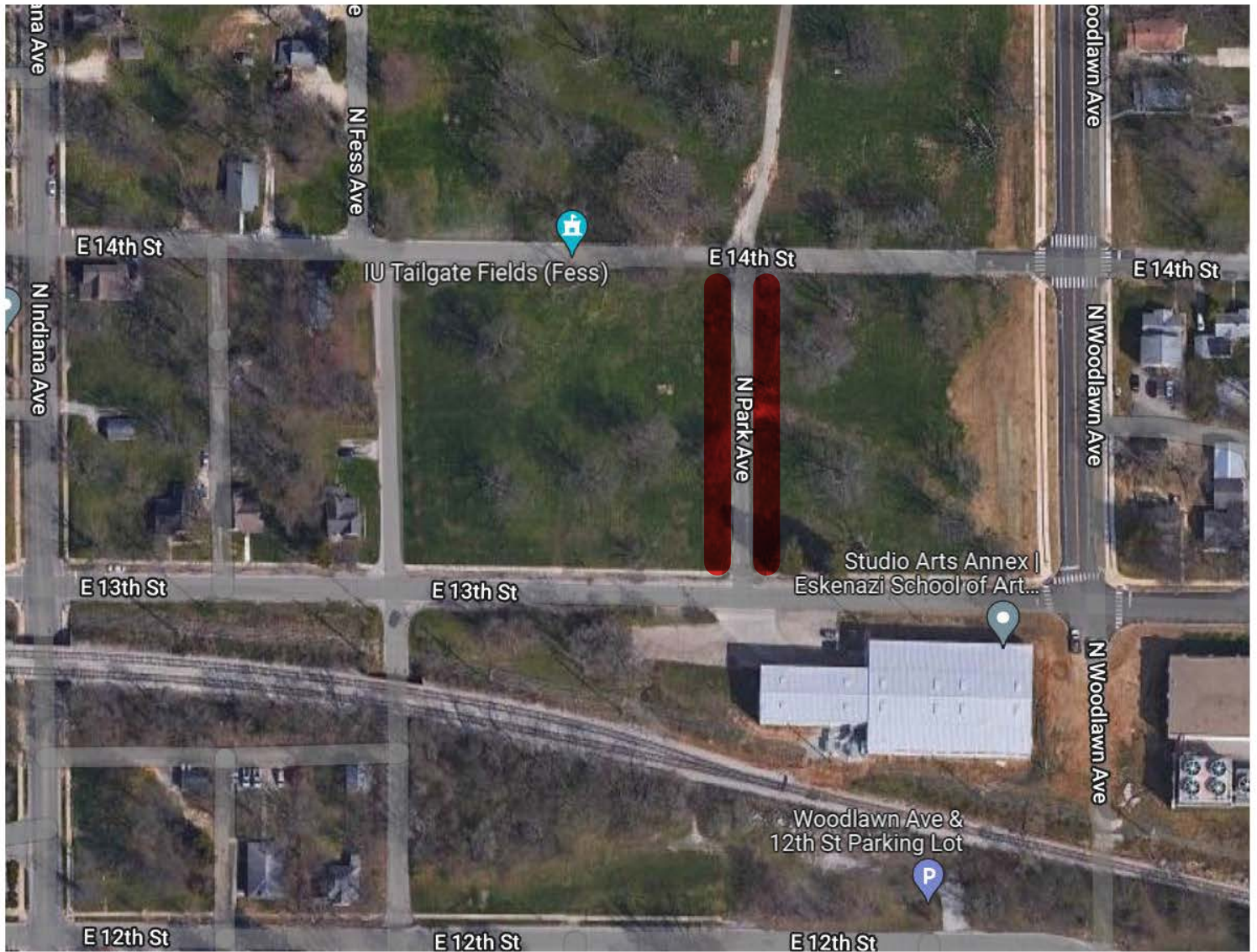
R2 is not present per C301; running slope 10.3%

REPAIR ASPHALT SIDEWALK TO ORIGINAL CONDITION

NEW SIDEWALK 2% CROSS SLOPE MAXIMUM RECONSTRUCT EXISTING ASPHALT WALK (SHADED) AS NECESSARY TO ENSURE THIS 2% CROSS SLOPE.

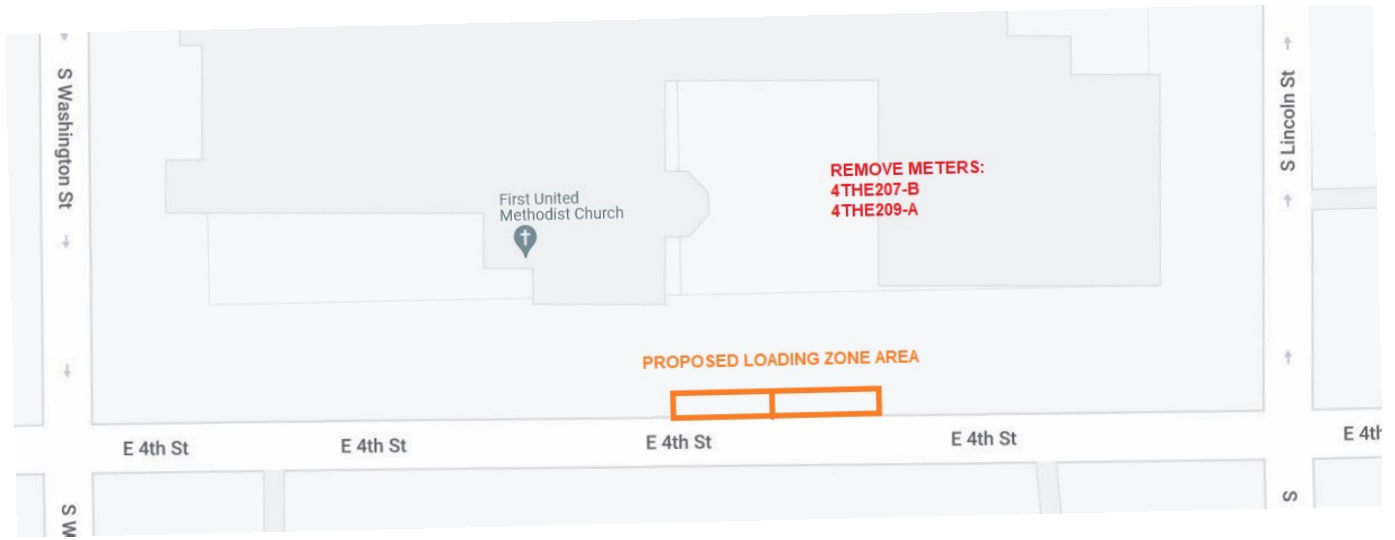
Section 6

Adding No Parking Spaces on Both Sides of N. Park Avenue between E. 13th Street and E. 14th Street

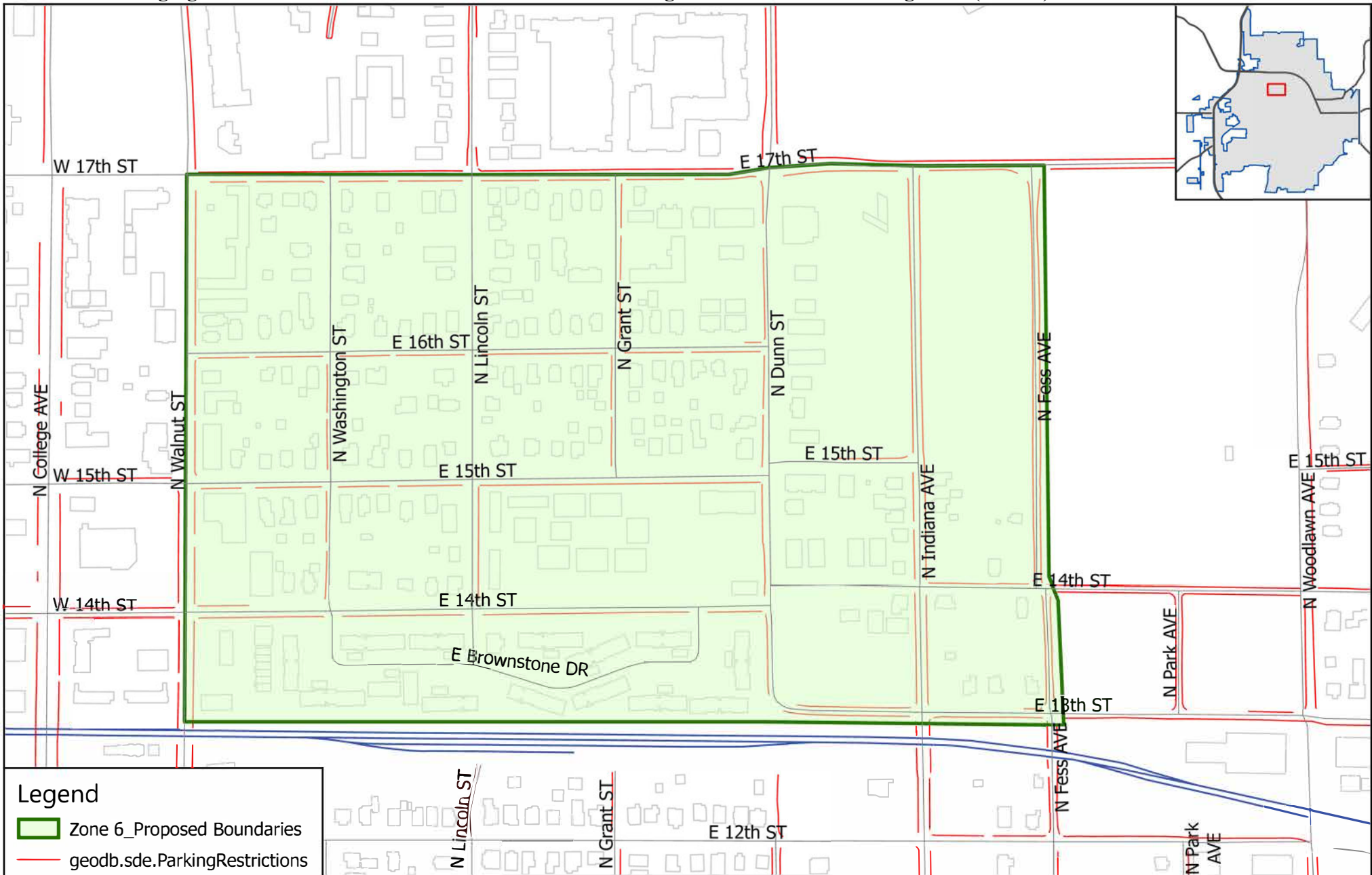


Section 7

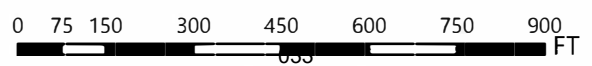
Adding Two Loading Zones on 200 E. 4th Street



Section 8 - Changing the Boundaries of the Garden Hill Residential Neighborhood Permit Parking Zone (Zone 6)

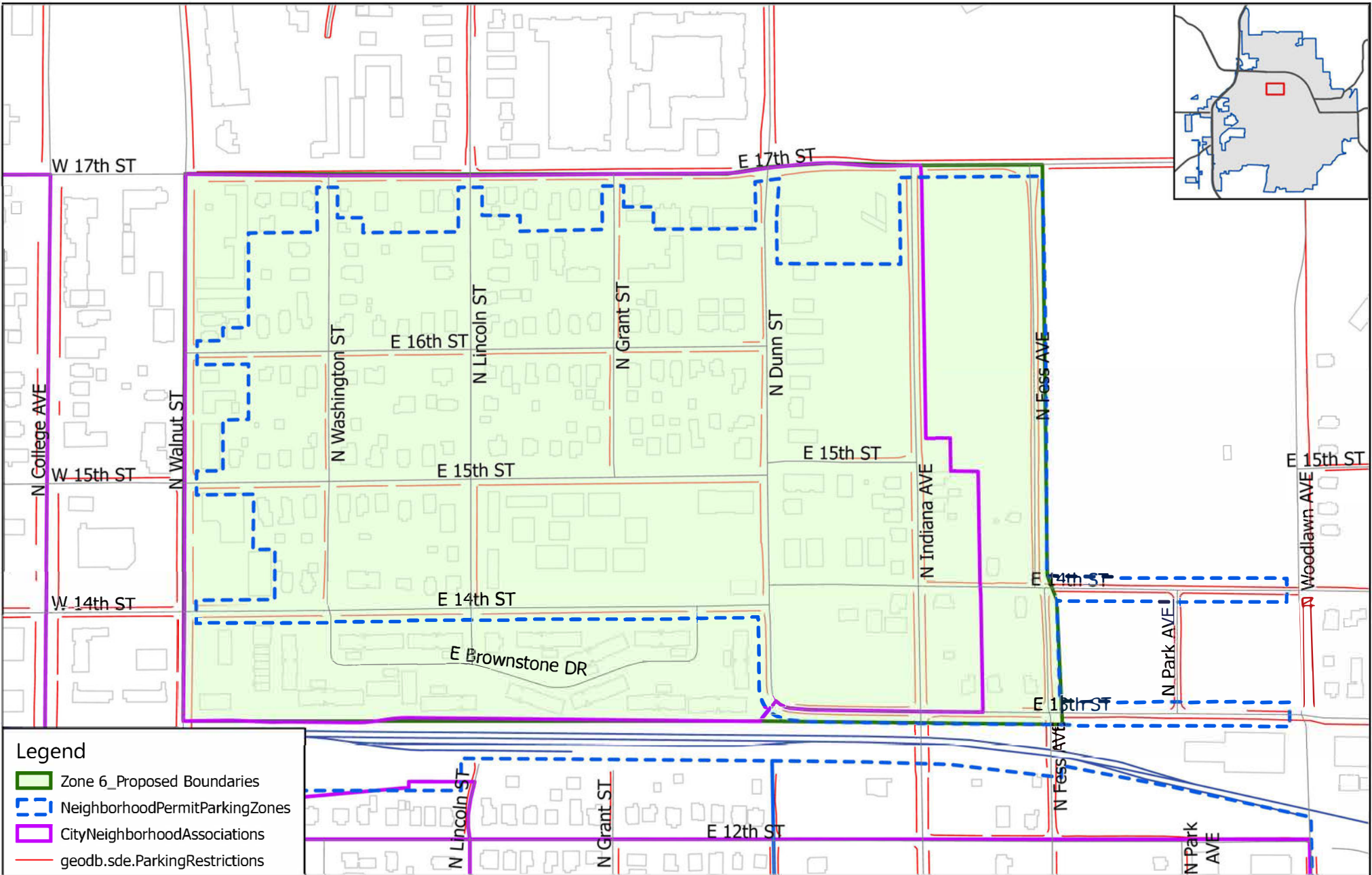


Residential Neighborhood Permit Parking Zone 6 Boundaries Changes







For use as map information only, information is NOT warranted.





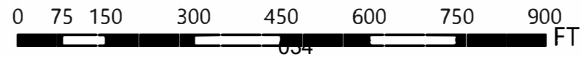
Legend

-  Zone 6_Proposed Boundaries
-  NeighborhoodPermitParkingZones
-  CityNeighborhoodAssociations
-  geodb.sde.ParkingRestrictions



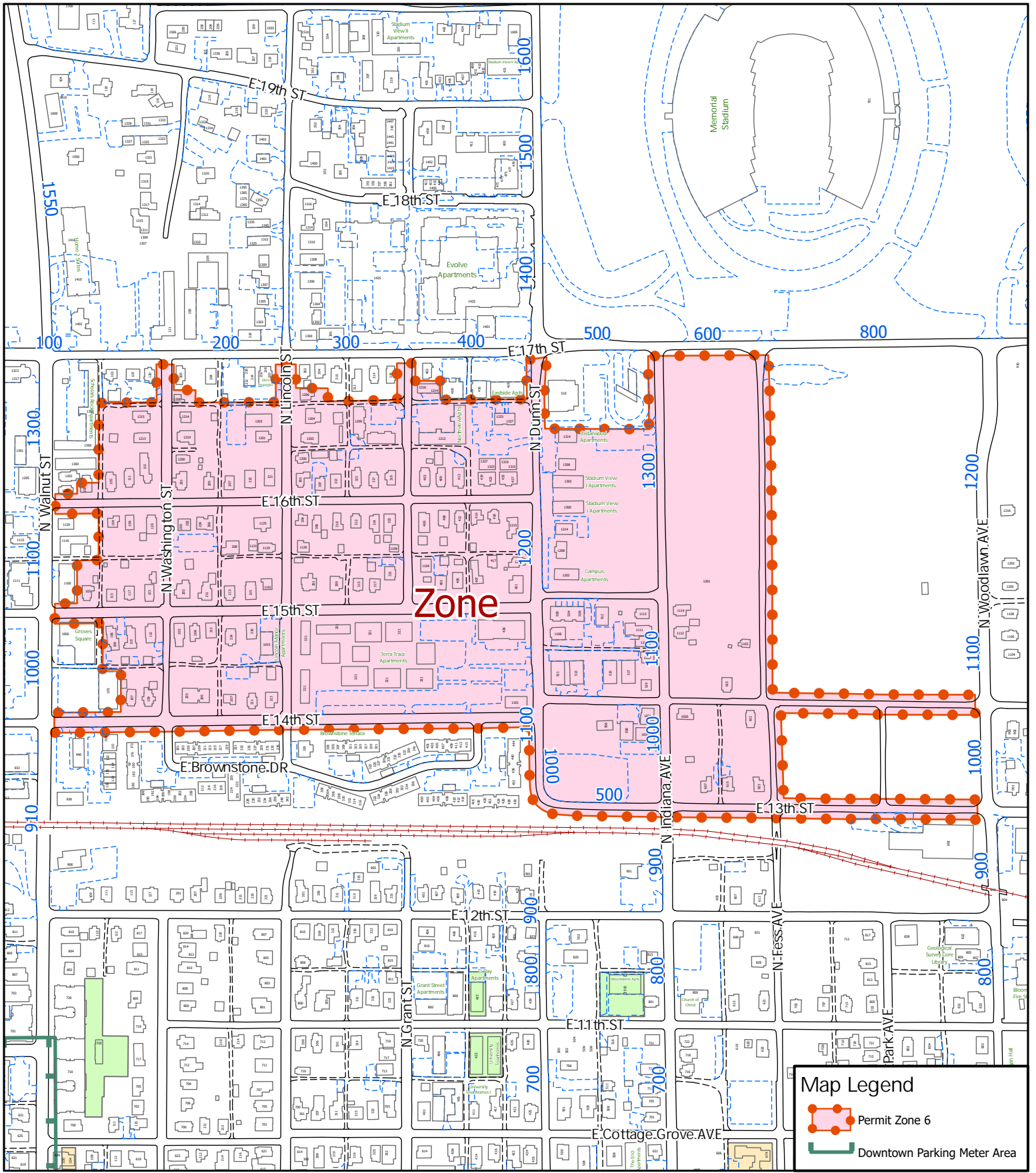
Residential Neighborhood Permit Parking Zone 6

Boundaries Changes



For use as map information only, information is NOT warranted.





Map Legend

- Permit Zone 6
- Downtown Parking Meter Area

Residential Permit Parking

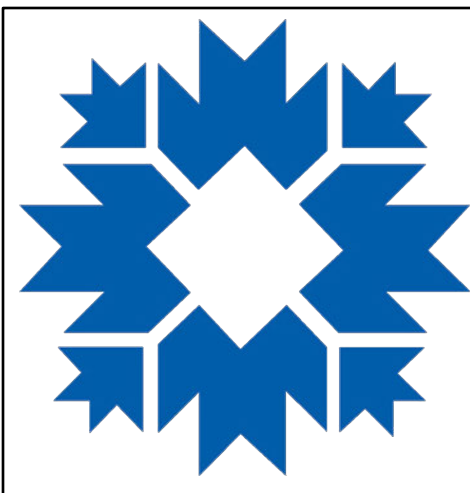
Zone 6



0 75 150 300 450 600 750 900 US Feet
 For use as map information only, information is NOT warranted.

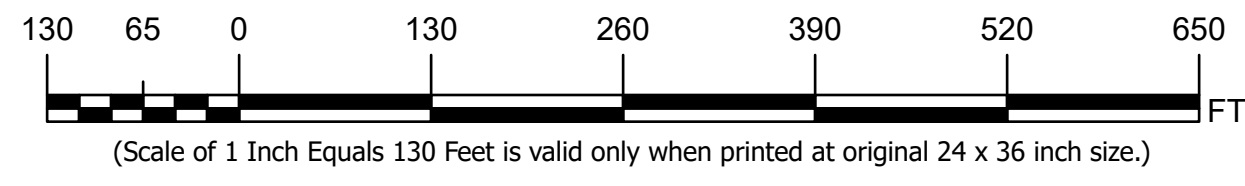
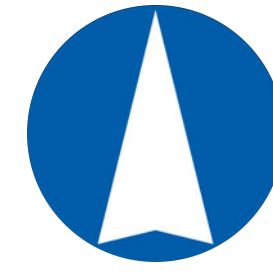
Produced: 3/1/2022
 Author: creek

 Project: ResidentialPermitParkingZonesLP
 File: Zone 6



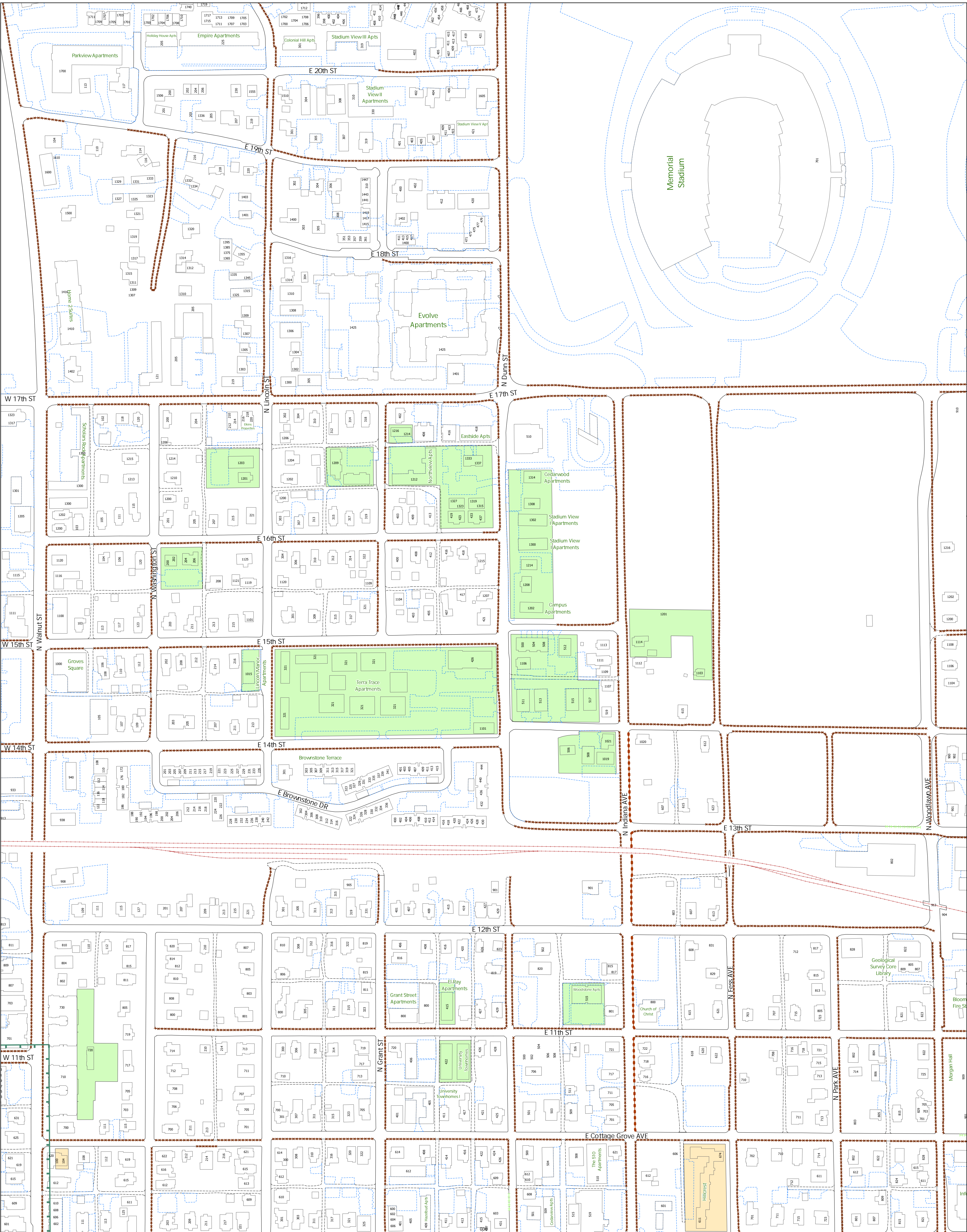
Map Legend

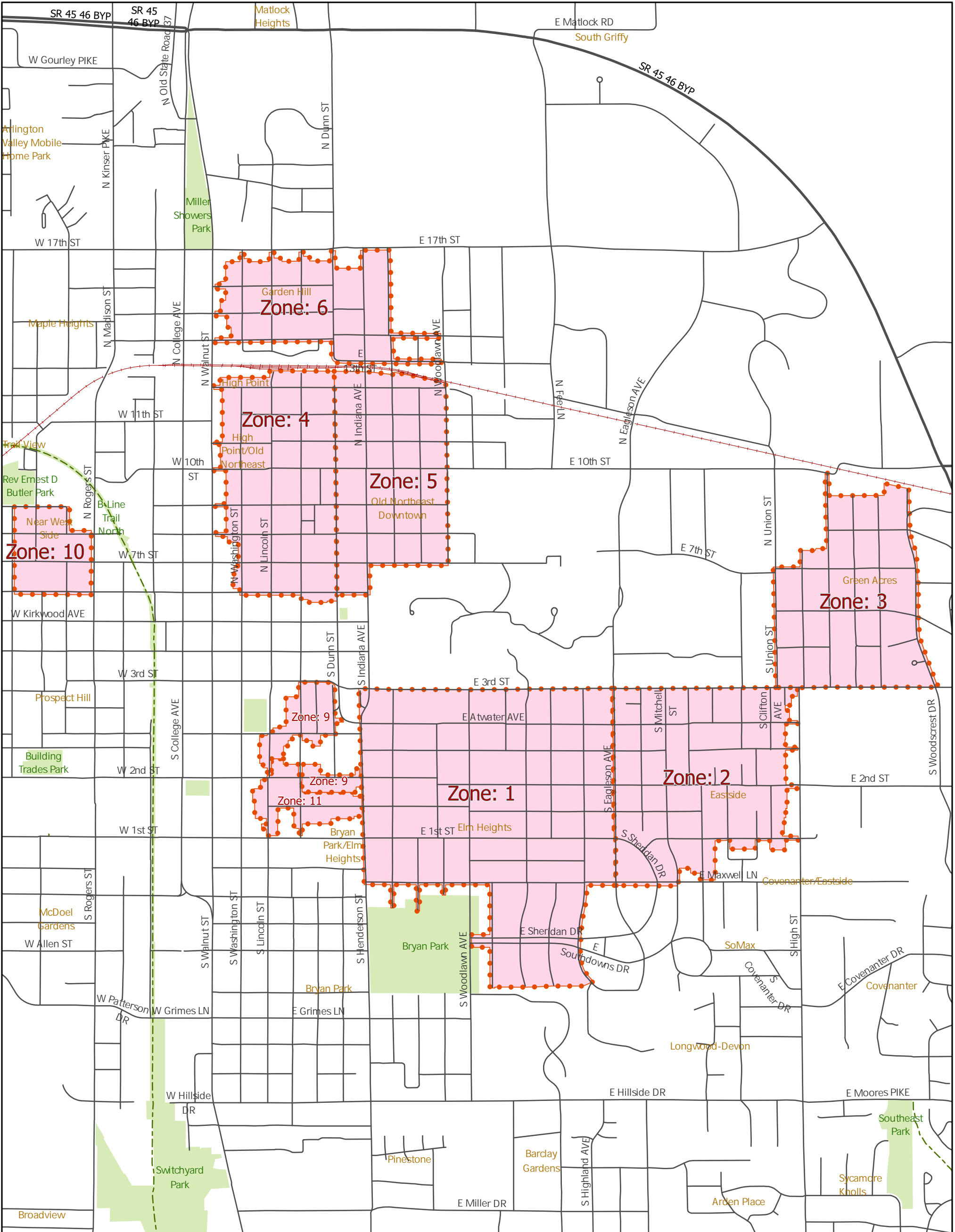
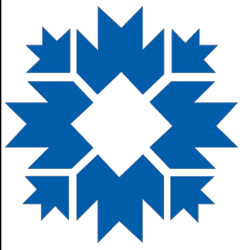
- Permit Zone 6
- Excluded Area
- Qualifying Area
- Other Restriction
- Combination Parking Zone
- Metered Parking Zone
- No Parking Anytime
- No Parking Posted Times
- Time Restricted Parking
- City Parking Facility



March 1st, 2022

This map was produced by the City of Bloomington GIS, for use by the City and general public as map information. The planimetric information is based on aerial orthorectified flown in March 2016. Supplemental updates are made from development drawings, plats, and other sources of information. The accuracy of information contained in this document is based on National Mapping Standards, however it is NOT warranted.
 The Corporation boundary reflects annexations effective January 1, 2002.





Map Legend

Neighborhood Permit Parking Zones



0 550 1,100 2,200 3,300 Feet

March 1st, 2022

Produced: 3/1/2022

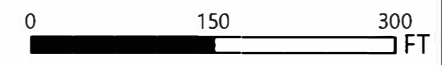


Author: creekr

File: ResidentialPermitParkingZonesOverview

**Garden Hill
Neighborhood
Association**

Neighborhood Associations
 Garden Hill

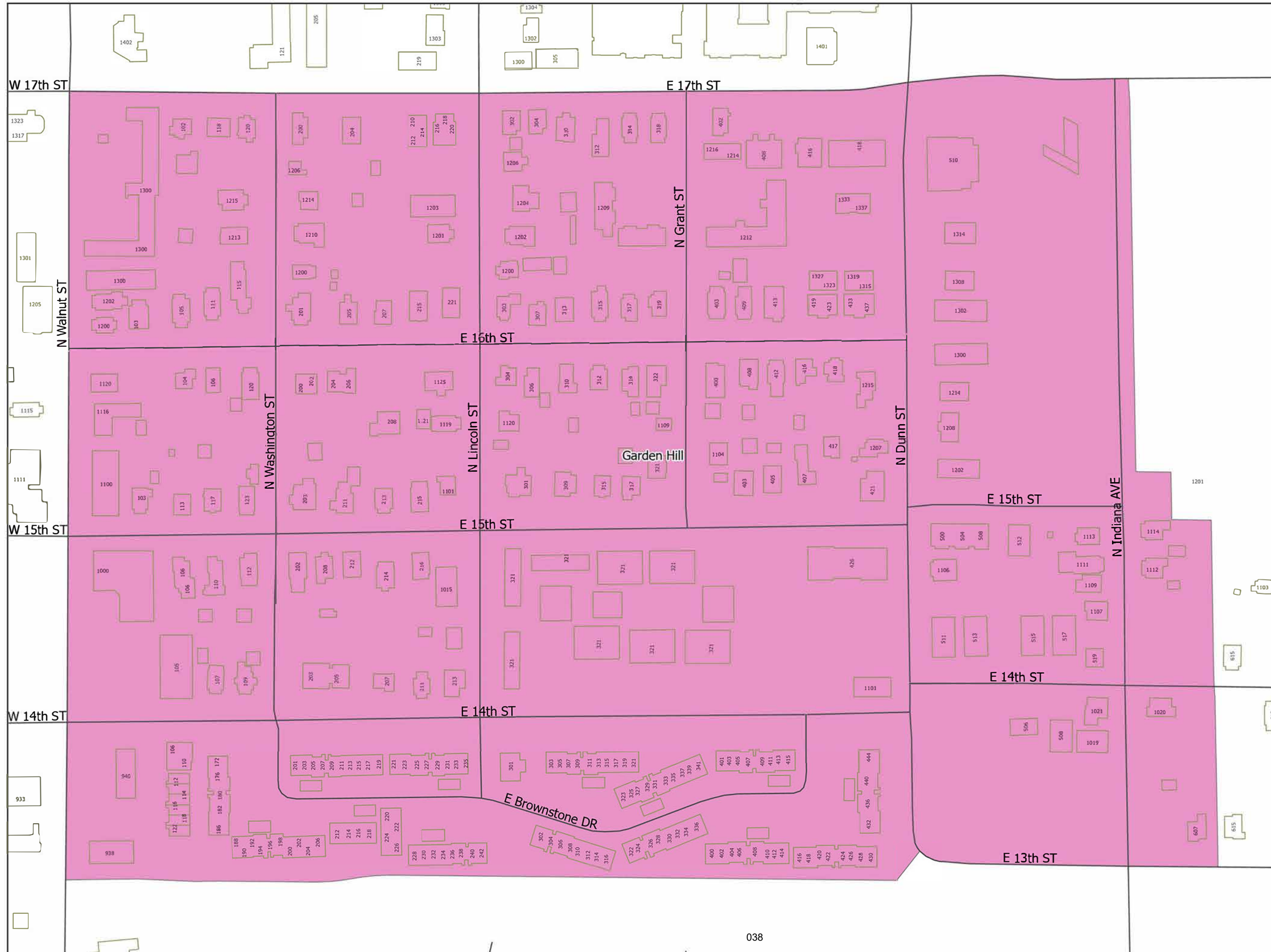


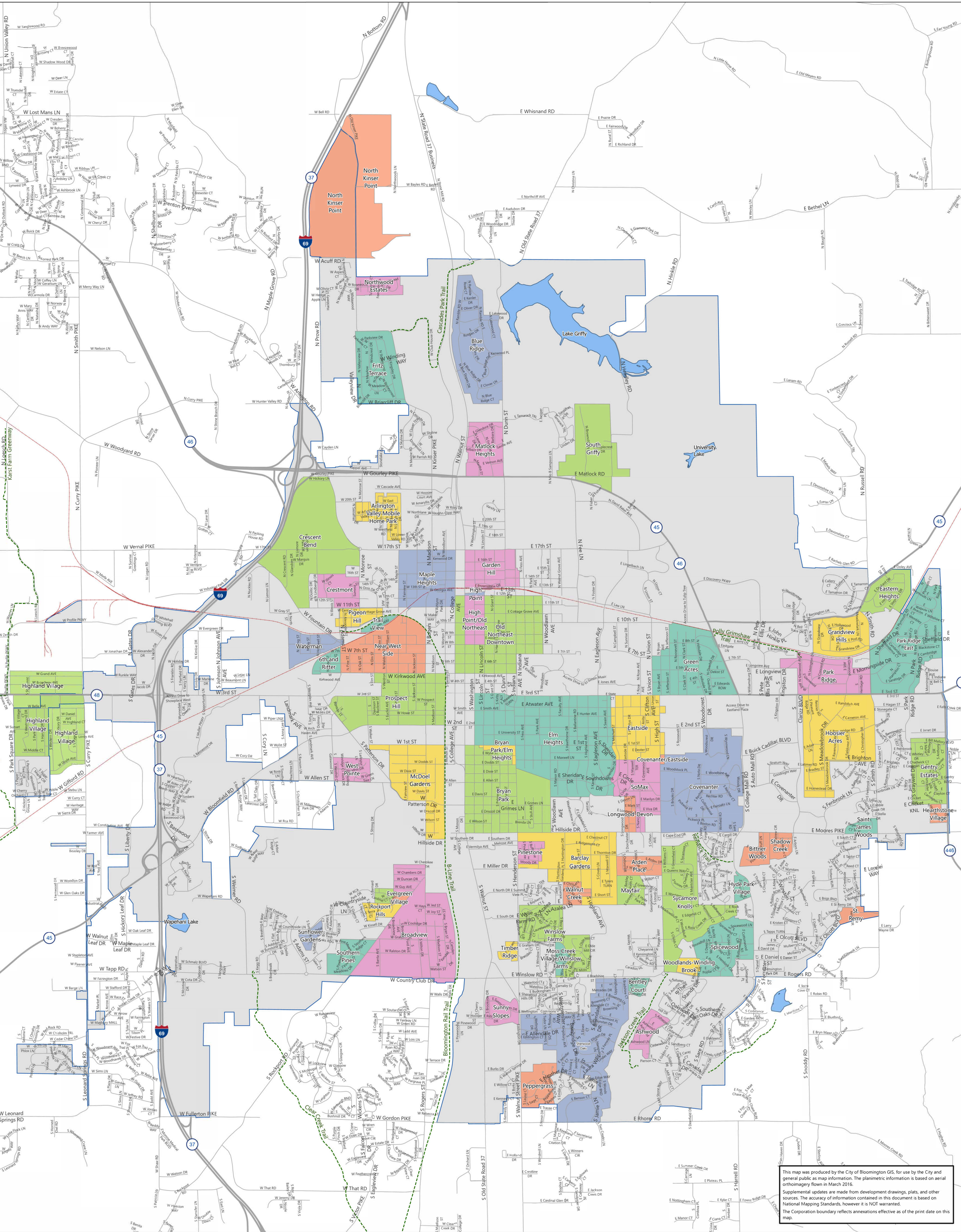
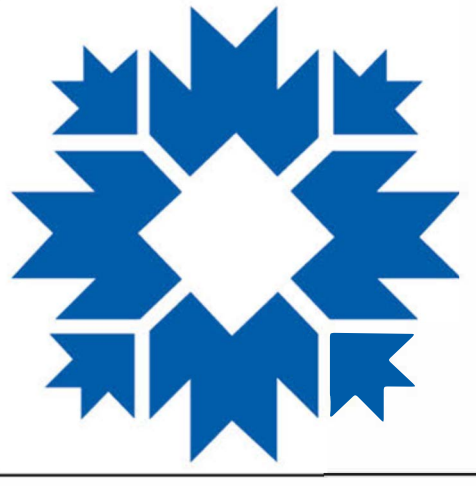
Produced: 4/7/2022



Project: NeighborhoodAssociationsLocations
 Layout: NeighborhoodAssociation_B

For use as map information only.
 information is NOT warranted.

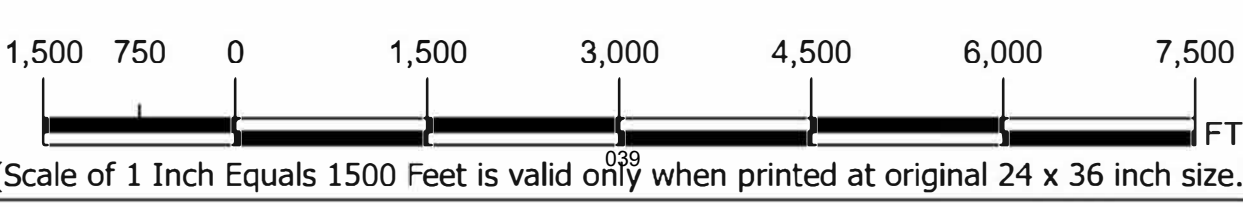




This map was produced by the City of Bloomington GIS, for use by the City and general public as map information. The planimetric information is based on aerial orthorectification from March 2016. Supplemental updates are made from development drawings, plats, and other sources. The accuracy of information contained in this document is based on National Mapping Standards, however it is NOT warranted. The Corporation boundary reflects annexations effective as of the print date on this map.

Map Legend

— Municipal Boundary



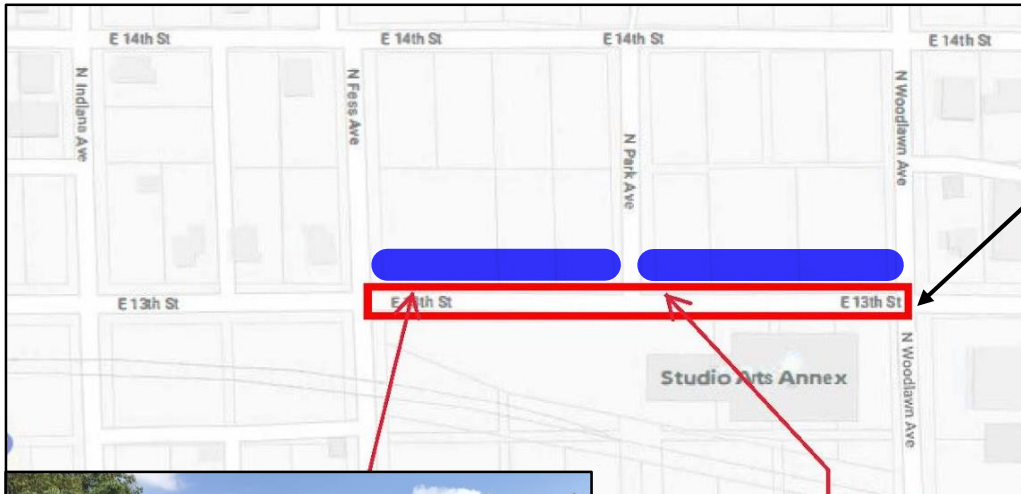
Produced: 4/8/2022



Layout: NeighborhoodAssocDP
Project: NeighborhoodAssociationsLayouts

Section 9

Adding Pay-by-Phone Parking Spaces on the North Side of E. 13th Street between N. Fess Avenue and N. Woodlawn Avenue. (Blue Areas on Map)

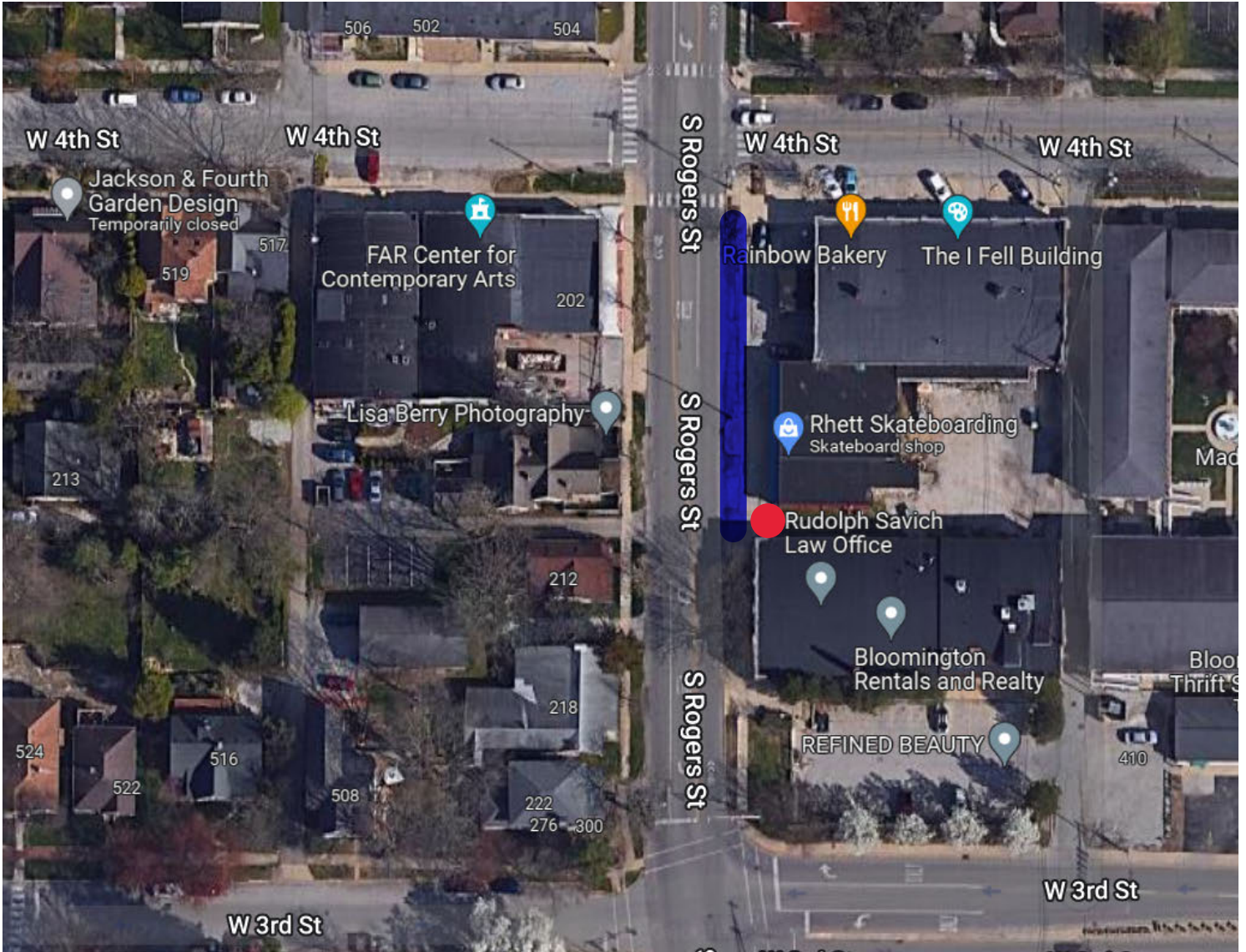


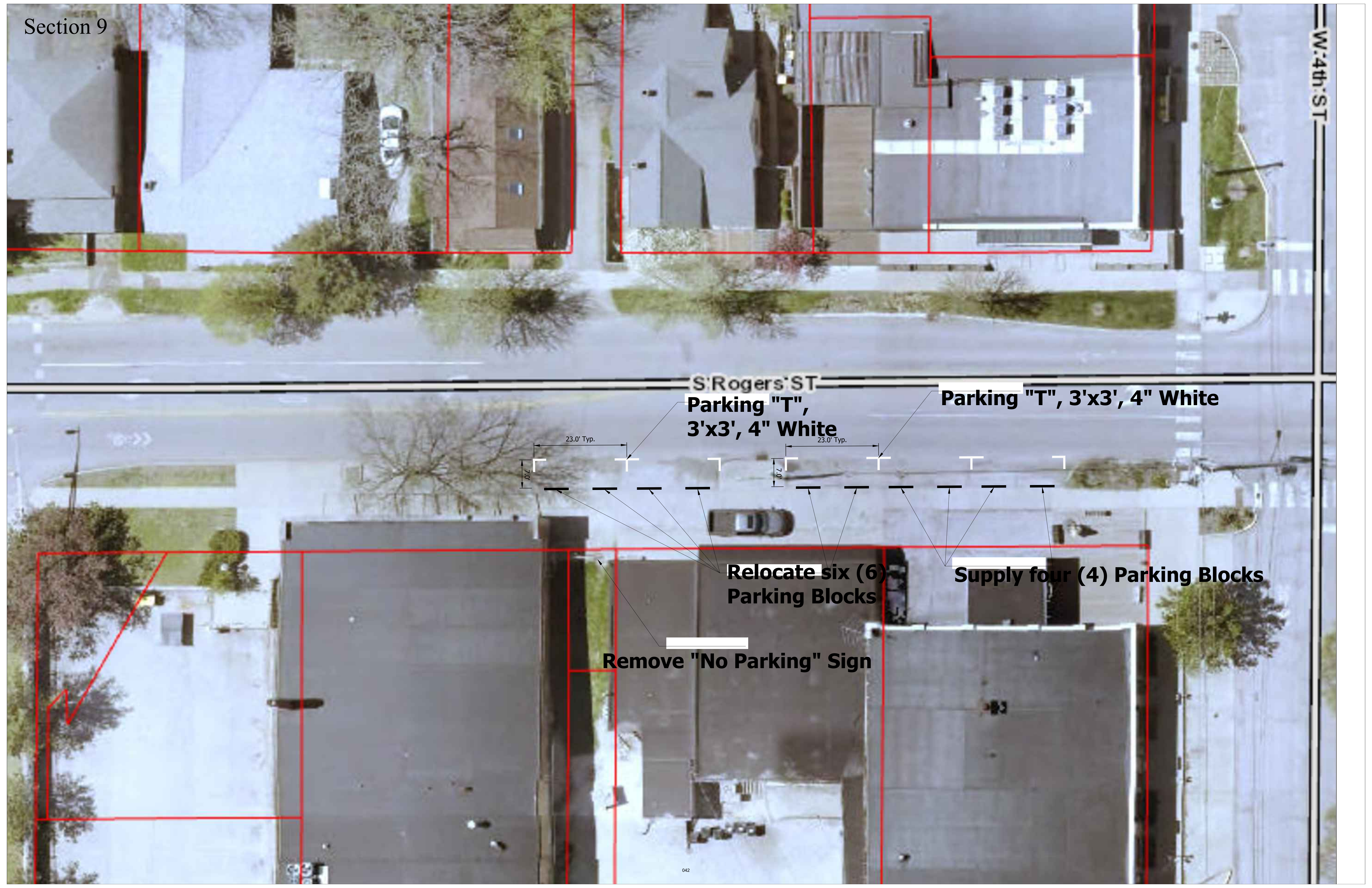
The above map and photo from September 2019 show E. 13th Street between N. Woodlawn Avenue and N. Fess Avenue.

Section 9

-Adding Pay-by-Phone Parking Spaces on the East side of 200 S. Rogers Street between W. 3rd Street and W. 4th Street (Blue Area on Map)

-Removing No Parking Sign on the East side of 200 S. Rogers Street (Red Area on Map)





S Rogers ST

Parking "T", 3'x3', 4" White

Parking "T", 3'x3', 4" White

23.0' Typ.
7.0'

23.0' Typ.
7.0'

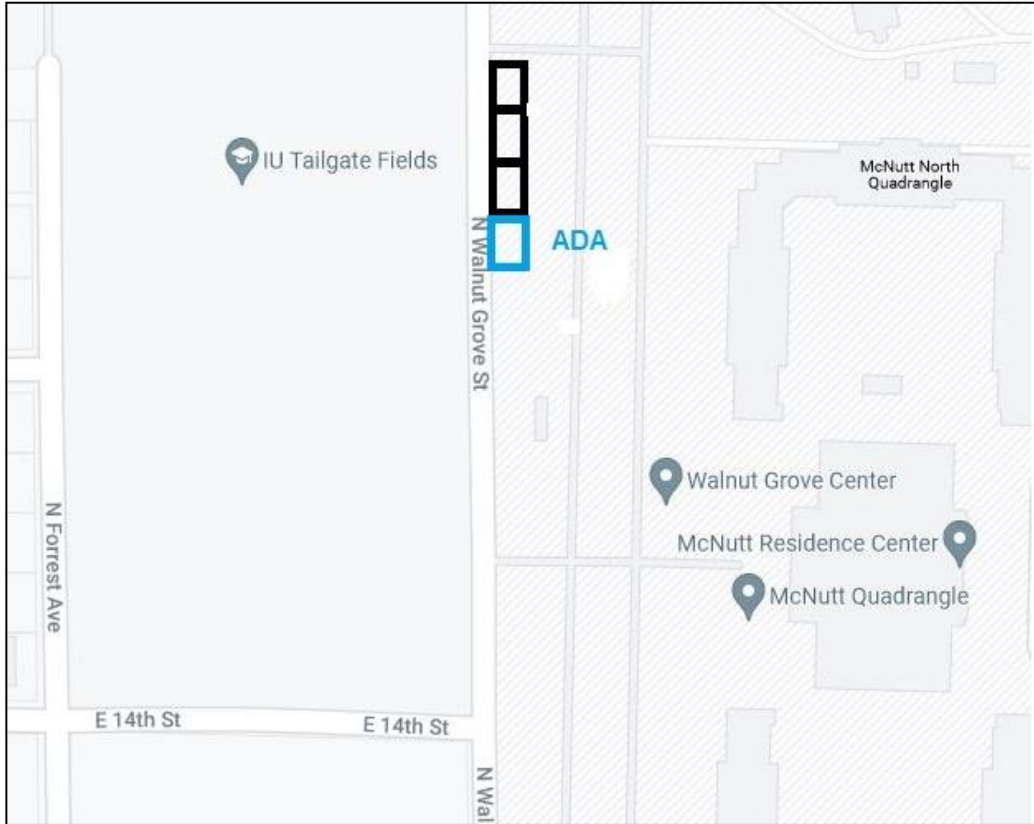
Relocate six (6) Parking Blocks

Supply four (4) Parking Blocks

Remove "No Parking" Sign

Section 9

Adding Pay-by-Phone Parking Spaces on the East Side of 1100 N. Walnut Grove Street



The above map and photo show 1100 N. Walnut Grove Street



MEMO FROM COUNCIL OFFICE ON:

Ordinance 22-21 – To Amend Title 8 of the Bloomington Municipal Code, Entitled “Historic Preservation and Protection” To Establish a Historic District – Re: Bethel A.M.E. Church and Parsonage (BETHEL A.M.E., Owner and Petitioner)

Synopsis

This ordinance amends Chapter 8.20 of the Bloomington Municipal Code entitled “The List of Designated Historic Districts” in order to designate the Bethel A.M.E. Church and Parsonage as a historic district. The owner and petitioner, Bethel A.M.E., sought this action and the Bloomington Historic Preservation Commission, after a public hearing on July 14, 2022, recommended that the structure be designated historic with a rating as “Outstanding.” This rating was based upon certain historic and architectural criteria set forth in BMC 8.08.101 (e) entitled “Historic District Criteria.” Local designation will provide the protection needed to ensure that this property is preserved.

Relevant Materials

- Ordinance 22-21
- Map of proposed historic district
- Staff Report from Bloomington Historic Preservation Commission
- Staff presentation slides

Summary

Ordinance 22-21 would add the Bethel A.M.E Church and Parsonage as a historic district under Title 8 of the Bloomington Municipal Code (entitled “Historic Preservation and Protection”). The provisions of Title 8 are enabled by state law under Indiana Code 36-7-11 (and following provisions) and are intended to:

- protect historic and architecturally-worthy properties that either impart a distinct aesthetic quality to the City or serve as visible reminders of our historic heritage;
- ensure the harmonious and orderly growth and development of the City;
- maintain established residential neighborhoods in danger of having their distinctiveness destroyed;
- enhance property values and attract new residents; and
- ensure the viability of the traditional downtown area and to enhance tourism.

The Historic Preservation Commission (“HPC”) is authorized to make recommendations to the Council regarding the establishment of historic districts either on its own accord or by petition of the property owner. In this case, the Bethel A.M.E Church petitioned for the local historic designation. The owner’s application materials submitted to the HPC can be found in the HPC’s July 14, 2022 meeting packet available here -

https://bloomington.in.gov/onboard/meetingFiles/download?meetingFile_id=10554.



City of Bloomington Indiana

City Hall | 401 N. Morton St. | Post Office Box 100 | Bloomington, Indiana 47402

Office of the Common Council | (812) 349-3409 | Fax: (812) 349-3570 | email: council@bloomington.in.gov

Under [BMC 08.08.020](#), once an area is designated as a historic district, a certificate of appropriateness must be issued by the HPC prior to the issuance of a permit for, or prior to work beginning on, any of the following within all areas of the historic district:

- The demolition of any building;
- The moving of any building;
- A conspicuous change in the exterior appearance of any historic building or any part of or appurtenance to such a building, including walls, fences, light fixtures, steps, paving, and signs by additions, reconstruction, alteration, or maintenance involving exterior color change if cited by individual ordinance, or
- Any new construction of a principal building or accessory building or structure subject to view from a public way.

The HPC promulgates [rules and procedures](#) for reviewing changes to properties within historic districts. Those reviews occur in the context of either granting or denying Certificates of Appropriateness for the proposed changes which, in some instances may be done by staff and other instances must be done by the Commission. Unless the property owner agrees to an extension, the action on the Certificate of Appropriateness must be taken within 30 days of submittal of the application. Persons who fail to comply with the Certificate of Appropriateness or other aspects of Title 8 are subject to fines and other actions set forth in BMC Chapter 8.16 (Administration and Enforcement).

According the BMC, in order to recommend the creation of a historic designation, the HPC must hold a public hearing and submit a map and staff report to the Council. The map identifies the district and classifies properties, and the Report explains these actions in terms of the historic and architectural criteria set forth in the ordinance (see also [BMC 08.08.010\(e\)](#)). These criteria provide the grounds for the designation. In this case, the ordinance rates the property as “Outstanding”, which means that the property has sufficient historic or architectural significance that it is already listed, or is eligible for individual listing, in the National Register of Historic Places.

In summary, [Ordinance 22-21](#):

- Approves the map and establishes the district, which provide the basis for the designation;
- Attaches the map and the report to the ordinance and incorporates them by reference;
- Describes the district and classifies the property;
- Inserts the newly-established district into the List of Historic and Conservation Districts contained within BMC 8.20.

Contact

Gloria Colom-Braña, Historic Preservation Program Manager, (812) 349-3507

ORDINANCE 22-21

**TO AMEND TITLE 8 OF THE BLOOMINGTON MUNICIPAL CODE, ENTITLED
“HISTORIC PRESERVATION AND PROTECTION”
TO ESTABLISH A HISTORIC DISTRICT –
Re: BETHEL A.M.E. CHURCH AND PARSONAGE
(BETHEL A.M.E., Owner and Petitioner)**

- WHEREAS, the Common Council adopted Ordinance 95-20 which created a Historic Preservation Commission (“Commission”) and established procedures for designating historic districts in the City of Bloomington; and
- WHEREAS, on July 14, 2022, the Commission held a public hearing for the purpose of allowing discussion and public comment on the proposed historic designation of the Bethel A.M.E Church and Parsonage located at 302 and 308 N. Rogers Street; and
- WHEREAS, at the same hearing, the Commission found that the building has historic and architectural significance that merits the protection of the property as a historic district; and
- WHEREAS, at the same hearing, the Commission approved a map and written report which accompanies the map and validates the proposed district by addressing the criteria outlined in Bloomington Municipal Code 8.08.010; and
- WHEREAS, at the same hearing the Commission voted to submit the map and report which recommend local historic designation of said properties to the Common Council; and
- WHEREAS, the report considered by the Commission at this hearing notes that this property consists of two structures. The Bethel African Methodist Episcopal Church (A.M.E.) was built in 1922 as a permanent and dedicated home to the Bethel A.M.E. congregation. The architect John Nichols, known for designing many of Bloomington’s notable buildings, designed the church in the classical revival, Tudor style (SHAARD 2014). The parsonage consists of a bungalow style cottage and is dated to 1925.

NOW THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. The map setting forth the proposed historic district for the site is hereby approved by the Common Council, and said historic district is hereby established. A copy of the map and report submitted by the Commission are attached to this ordinance and incorporated herein by reference and two copies of them are on file in the Office of the Clerk for public inspection.

The legal description of this property is further described as:

013-50530-00 ORIG PLATS 289 & 290 in the City of Bloomington, Monroe County, Indiana.

SECTION 2. The property at “302 and 308 N Rogers Street.” shall be classified as “Outstanding”.

SECTION 3. Chapter 8.20 of the Bloomington Municipal Code, entitled “List of Designated Historic and Conservation Districts,” is hereby amended to insert “The Bethel A.M.E. Church and Parsonage” and such entry shall read as follows:

The Bethel A.M.E. Church and Parsonage

302 and 308 N. Rogers Street

SECTION 4. If any section, sentence, or provision of this ordinance, or the application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions, or applications of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

SECTION 5. This ordinance shall be in full force and effect from and after its passage by the Common Council of the City of Bloomington and approval of the Mayor.

PASSED AND ADOPTED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2022.

SUSAN SANDBERG, President
City of Bloomington

ATTEST:

NICOLE BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2022.

NICOLE BOLDEN, Clerk
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2022.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

This ordinance amends Chapter 8.20 of the Bloomington Municipal Code entitled “The List of Designated Historic Districts” in order to designate the Bethel A.M.E. Church and Parsonage as a historic district. The owner and petitioner, Bethel A.M.E., sought this action and the Bloomington Historic Preservation Commission, after a public hearing on July 14, 2022, recommended that the structure be designated historic with a rating as “Outstanding.” This rating was based upon certain historic and architectural criteria set forth in BMC 8.08.101 (e) entitled “Historic District Criteria.” Local designation will provide the protection needed to ensure that this property is preserved.

TO: Common Council
FROM: Gloria M. Colom Braña, Historic Preservation Program
Manager
RE: Historic District Nomination for the Bethel A.M.E. Church and
Parsonage located at 302 and 308 N Rogers Street
DATE: July 19, 2022

At the July 14, 2022 meeting, the Historic Preservation Commission voted to recommend historic designation for the Bethel A.M.E. Church and Parsonage with a vote 8-0 based on the following criteria:

- (1) Historic:
 - a) Has significant character, interest, or value as part of the development, heritage, or cultural characteristics of the city, state, or nation; or is associated with a person who played a significant role in local, state, or national history; or
 - c) Exemplifies the cultural, political, economic, social, or historic heritage of the community.

- (2) Architectural:
 - b) Is the work of a designer whose individual work has significantly influenced the development of the community; or
 - g) Exemplifies the built environment in an era of history characterized by a distinctive architectural style

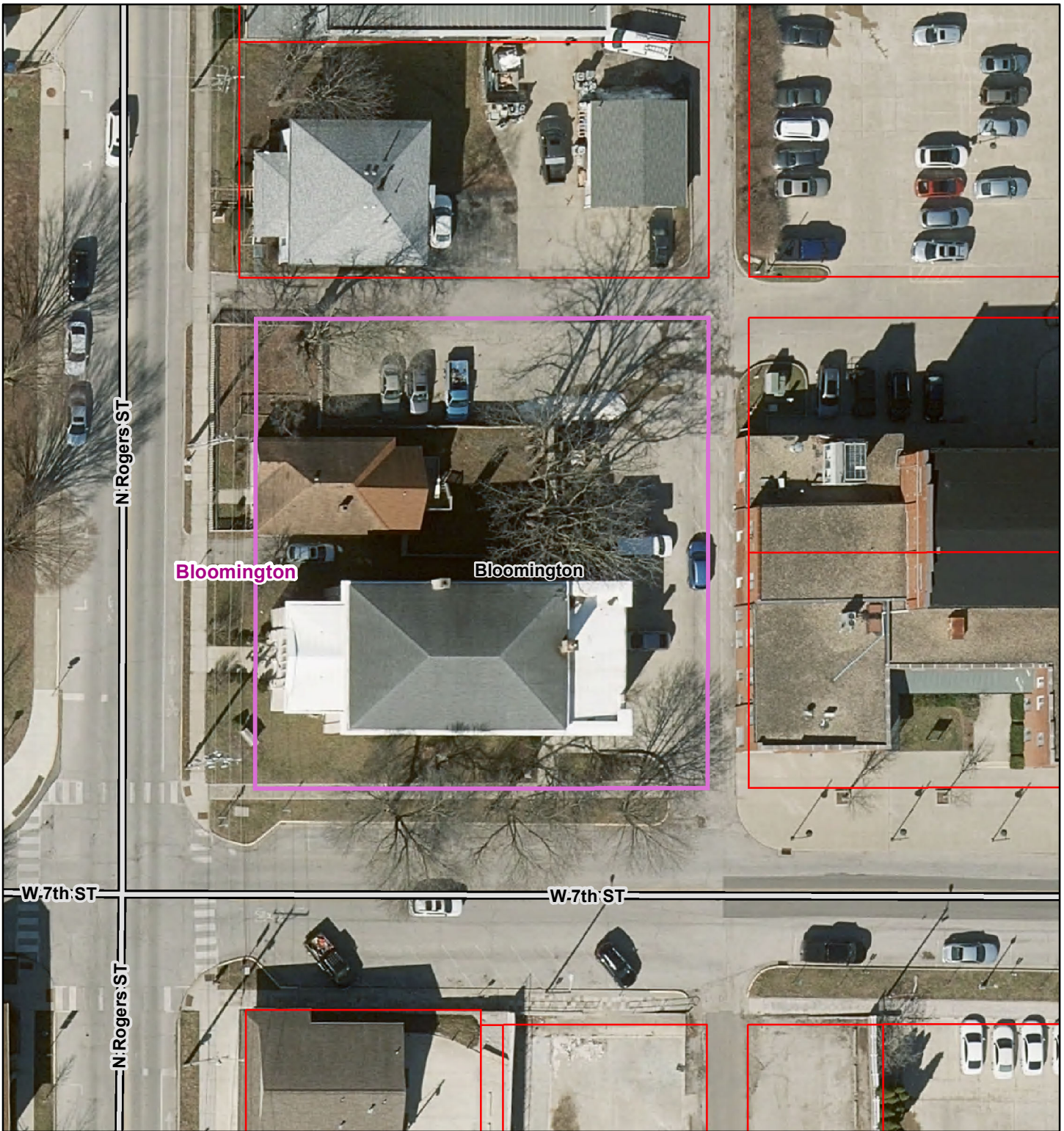
Synopsis

The Bethel African Methodist Episcopal Church (A.M.E.) was built in 1922 as a permanent and dedicated home to the Bethel A.M.E. congregation. The architect John Nichols, known for designing many of Bloomington's notable buildings designed the church in the classical revival, Tudor style (SHAARD 2014). The parsonage consists of a bungalow style cottage and is dated to 1925.

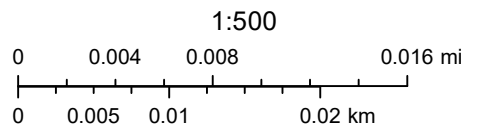
The Bethel African Methodist Episcopal church has existed in Bloomington since September 17, 1870 when it was organized by Reverend John. W. Malone and has served as one of the main religious institutions for Bloomington's Black community since then. The church doubled as a social cultural unifier, providing a space for creativity, social cohesion, and a place where Indiana University's Black students could find community as well. Bloomington's Bethel A.M.E.

The staff report, map, and presentation are included with this memorandum.

302 and 308 N Rogers Street (Bethel AME Church and Parsonage)



**CITY OF
BLOOMINGTON**
HOUSING AND NEIGHBORHOOD DEVELOPMENT



 **Boundary Line**

The property at 302 and 308 N Rogers Street qualifies for local designation under the following highlighted criteria found in Ordinance 95-20 of the Municipal Code (1) a, c // (2) b, g

- 1) Historic:
 - a) **Has significant character, interest, or value as part of the development, heritage, or cultural characteristics of the city, state, or nation; or is associated with a person who played a significant role in local, state, or national history; or**
 - b) Is the site of an historic event; or
 - c) **Exemplifies the cultural, political, economic, social, or historic heritage of the community.**

- 2) Architectural:
 - a) Embodies distinguishing characteristics of an architectural or engineering type;
or
 - b) **Is the work of a designer whose individual work has significantly influenced the development of the community; or**
 - c) Is the work of a designer of such prominence that such work gains its value from the designer's reputation; or
 - d) Contains elements of design, detail, materials, or craftsmanship which represent a significant innovation; or
 - e) Contains any architectural style, detail, or other element in danger of being lost; or
 - f) Owing to its unique location or physical characteristics, represents an established and familiar visual feature of the city; or
 - g) **Exemplifies the built environment in an era of history characterized by a distinctive architectural style**

Case Background

The proposed district consists of two buildings on the lot legally recorded as 013-50530-00 ORIG PLATS 289 & 290. The area is zoned as Zoning MC-DC Mixed Use Downtown. The Bethel African Methodist Episcopal Church (A.M.E.) was built in 1922 as a permanent and dedicated home to the Bethel A.M.E. congregation. The architect John Nichols, known for designing many of Bloomington's notable buildings designed the church in the classical revival, Tudor style (SHAARD 2014). The parsonage consists of a bungalow style cottage and is dated to 1925.

Historic surveys rating and designations:

The two buildings are currently included in the Bloomington West Side Historic District in the National Register of Historic Places. This historic district features a combination of residential, commercial, industrial, and institutional buildings that were centered on the Showers Brothers furniture factory. Both the Bethel A.M.E. Church and the Second Baptist Church located across the street towards the north end of the block served and continue to serve Bloomington's Black community, many who lived in the West Side neighborhood and worked at the Showers Brothers Factory, Johnson's Creamery, Indiana University, and other industries. Although the Bethel A.M.E. Church is included in the West Side Historic District (at a federal level), it is not included in any of the local historic districts that currently surround it such as The Near West Side Conservation District, The Showers Brother Factory Historic District, or the Johnson's Creamery Historic District, amongst others.

The main church is listed in both the Indiana Historic Sites & Structures Inventory and the Bloomington Historic Sites and Structures List as "Outstanding." The parish house on the north of the property is listed as "Contributing". Neither of the two buildings within the property are located within a local historic or local conservation district under the jurisdiction of the Bloomington Historic Preservation Commission.

Financial Impact Statement:

There is no anticipated fiscal impact associated with this Ordinance.

Historical Significance, 1 (a): Has significant character, interest, or value as part of the development, heritage, or cultural characteristics of the city, state, or nation; or is associated with a person who played a significant role in local, state, or national history

The Bethel African Methodist Episcopal church has existed in Bloomington since September 17, 1870 when it was organized by Reverend John. W. Malone and has served as one of the main religious institutions for Bloomington's Black community since then. The church doubled as a social cultural unifier, providing a space for creativity, social cohesion, and a place where Indiana University's Black students could find community as well. Bloomington's Bethel A.M.E. The congregation was originally housed in a former Presbyterian church located on 6th Street just west of the city square (Halsell Gilliam 1985,30). When the congregation grew to about 64 members, they purchased the current lot on the north east corner of 7th and Rogers Street, and hired John L. Nichols, one of Bloomington's early architects to design a new building. The congregation was seeking a new space as the old church suffered from "lack inside plumbing, central heat, and rowdiness in the neighborhood by derelicts (Henning Byfield)." The new building cost \$35,000, measured 42 by 97 feet and was two stories high (Halsell Gilliam 1985,30).

Mrs. Mattie Jacobs Fuller, an accomplished singer and organ player, as well as a founding member of the church, raised money to buy the lot by performing (Image 2). Mrs. Fuller's incessant work and contribution to the community cannot be understated. Playing her portable organ, Mrs. Fuller raised more than \$13,000 that went to the church (Herald Times 2006). Additional money was acquired from selling the old Presbyterian church to the Smallwood Brothers (World Telephone 1945, 1). Stone was donated from several of the local quarries.

The paying off of the mortgage was done through extensive community collaboration, including constant fundraisers through meals and community bonding events. When Reverend J. E. Reddick was appointed to Bethel A.M.E. Church at Bloomington in 1940, the church had a congregation of 102 people and the mortgage debt was \$7,500. Through a consistent campaign, more than half of that money was paid off by 1945. On Sunday July 29, 1945 the congregation raised \$3,515.81 which was enough to pay off the remaining \$2,700 and have money left over to make additional repairs. Paying off the mortgage was a significant event and celebrated by burning the mortgage papers on August 26, 1945 (The World Telephone 1945).

The parsonage located on 308 N Rogers Street, just north of the main church structure is made of wood and was originally built for a foreman who worked for Nurre Glass (Krause 2014, D2). The bungalow is reminiscent in scale and massing to many of the working class homes built at that time in the Near West Side Neighborhood. A huge effort with the collaboration of multiple non-profit organizations and community members was done in 2014 to restore and save the bungalow.

Historical Significance, 1 (c): Exemplifies the cultural, political, economic, social, or historic heritage of the community

The current Bethel AMC building represents the story of a community, where many members of Bloomington's Black community met to fulfill spiritual needs, but also found community in other ways such as singing, cooking, eating, putting on plays, and hosting Indiana University's small but growing Black student population. Many of the large and small events were recorded on the "Among the Colored People" news section within the *Daily Telephone* newspaper. Members of Bethel A.M.E. worked together with the other two major Black congregations from the Second Baptist Church and Taylor Chapel ME at different times. In 1912 the social announcement for Bethel was canceled when Mrs. William Montgomery, a prominent member of Second Baptist Church died unexpectedly (Daily Telephone 1912). This is just one instance out of a myriad of examples of the close knit community that worked together across denominations within Bloomington's Black community.

Architectural Significance, 2(b): Is the work of a designer whose individual work has significantly influenced the development of the community

John L. Nichols, the architect who designed Bethel A.M.E., was one of Bloomington's earliest prolific trained architects. Nichols designed many of Indiana University's prominent buildings such as Wylie Hall and the observatory as well as over two hundred houses, and institutional buildings. Nichols experimented with the different architectural styles available at the time including neoclassical, Queen Anne, eclectic, and Romanesque amongst others.

Architectural Significance, 2(g): Exemplifies the built environment in an era of history characterized by a distinctive architectural style

The main church “reflects the Arts and Crafts style and shows Moorish influence in its triangular arched stained glass windows on the second floor and bartered Walls (NRHP Nomination 1997, 14),” using locally sourced Indiana Limestone. With a clean and symmetrical design, the building features a large opening consisting of ten windows on the front elevation. The side elevations each have five bays with original wood windows. The windows conform to the land gradation. Storm windows were installed to protect the stained glass windows.

“The side elevations are composed of five bays. Each of these bays have stained-glass windows placed in triplets with pentagonal windows above. Below, six-over-six wood sash windows vary in size responding to the grade of the land. The rear wing has eight-over-eight wood sash windows on the upper floor and four-over-four wood sash windows on the lower, as well as a pair of replaced doors and a six-paned transom. The side elevations mirror each other, except for the presence of the doors only on the south elevation (SHAARD 2014).”

Recommendation: Approval

Staff recommends property parcel 53-05-32-413-099.000-005 (The Bethel A.M.E. Church and Parsonage) be designated as a local historic district. After careful consideration of the application and review of the Historic District Criteria as found in Ordinance 95-20 of the Municipal Code, staff finds that the property not only meets, but exceeds the minimum criteria listed in the code.

The property meets Criteria 1(a) because the church provided a safe communal space for many members of Bloomington’s Black community in the early twentieth century. Prominent members such as Mrs. Mattie Jacobs Fuller raised a considerable amount of money to make sure that the congregation owned the land and the building.

The property meets Criteria 1(c) because it has served as one of the main socio-cultural hubs for Bloomington’s Black community, as a social hub for congregants and Indiana University students alike.

The property meets Criteria 2(b) because this is an exemplary example of John L. Nichols, perhaps Bloomington’s earliest architect of renown’s work.

The property meets Criteria 2(g) because the building itself through its elegant design provides a solid example of the late Classical revival style.

Bibliography

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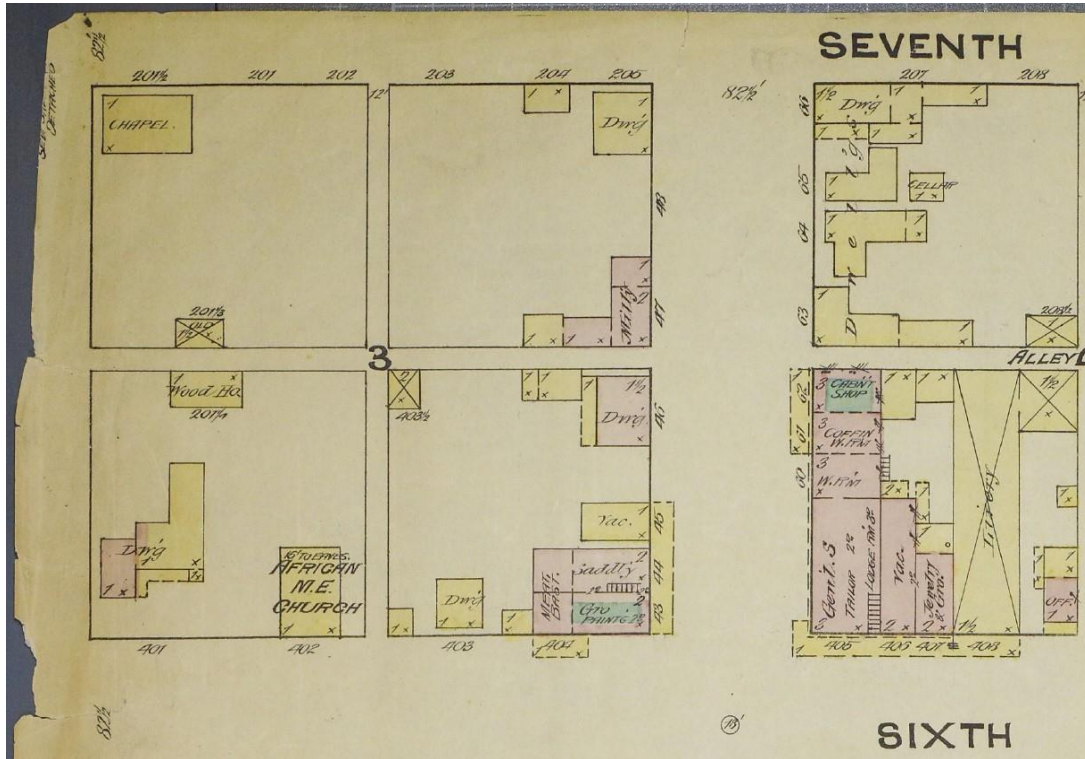


Image 1: Sanborn Map 1833 Original Location of Bethel A.M.E. on 6th Street



Image 2: The steeple of the original Bethel A.M.E. location is visible at the of the row of buildings on the north side of the Square in this undated photo (Courtesy of the Monroe County History Center)



Figure 3: Bethel A.M.E. with the congregation standing in front of the building, (1945?)
(City of Bloomington)

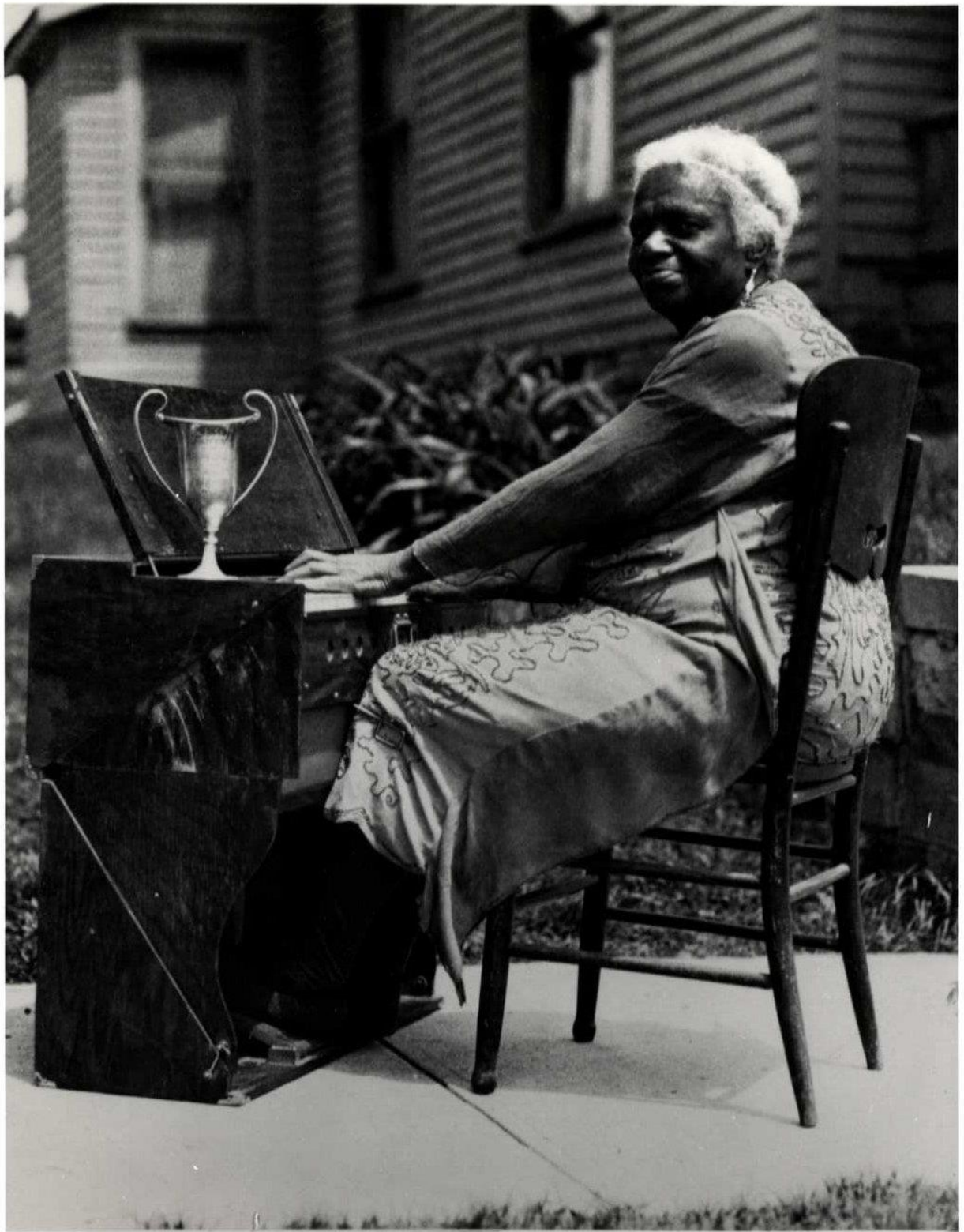


Image 4: Mattie Jacobs Fuller, acclaimed singer and organ player and prominent member of the community helped raise money to buy the land where the current church is located with her musical performances, undated (Courtesy of the Monroe County History Center)



Image 5: "Mortgage Burning for Bethel A.M.E., center left Rev & Mrs. Reddick, center right Rev & Mrs. Porter," Undated although most likely August 26, 1945 (Courtesy of the Monroe County History Center)



Image 6: Front of the church structure facing west, 2022 (photo by staff)



Image 7: Southern facade, showcasing six rows of windows, 2022 (photo by staff)



Image 8: Front of the church structure facing west, stone lintel with the A.M.E. carved, 2022 (photo by staff)



Image 9: Original windows facing southward, 2022 (photo by staff)



Image 10: Back of the church facing eastward, 2022 (photo by staff)



Image 11: Northwest of the parsonage, bungalow, 2022 (photo by staff)



Image 12: West facade with the front entrance of the parsonage, bungalow, 2022 (photo by staff)

Historic District Nomination: Bethel A.M.E. Church and Parsonage at 302 and 308 N Rogers Street

HISTORIC DISTRICT NOMINATION: HD 22-03

STAFF RECOMMENDATION



SUMMARY

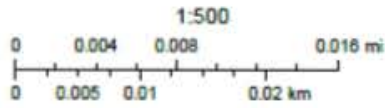
- **Address:** 702 & 708 N Rogers St. (Bethel AME Church and Parsonage)
- **Petitioner:** Bethel AME
- **Recommendation:** Staff recommends property parcel 53-05-32-413-099.000-005 (The Bethel AME Church and Parsonage) be designated as a local historic district. After careful consideration of the application and review of the Historic District Criteria as found in Ordinance 95-20 of the Municipal Code, staff finds that the property not only meets, but exceeds the minimum criteria listed in the code.

OUTSTANDING

COMMENTS

- **The property meets Criteria 1(a)** because the church provided a safe communal space for many members of Bloomington's Black community in the early twentieth century. Prominent members such as Mrs. Mattie Jacobs Fuller raised a considerable amount of money to make sure that the congregation owned the land and the building.
- **The property meets Criteria 1(c)** because it has served as one of the main socio-cultural hubs for Bloomington's Black community, as a social hub for congregants and Indiana University students alike.
- **The property meets Criteria 2(b)** because this is an exemplary example of John L. Nichols, perhaps Bloomington's earliest architect of renown's work.
- **The property meets Criteria 2(g)** because the building itself through its elegant design provides a solid example of the late Classical revival style.

302 and 308 N Rogers Street (Bethel AME Church and Parsonage)



 Boundary Line

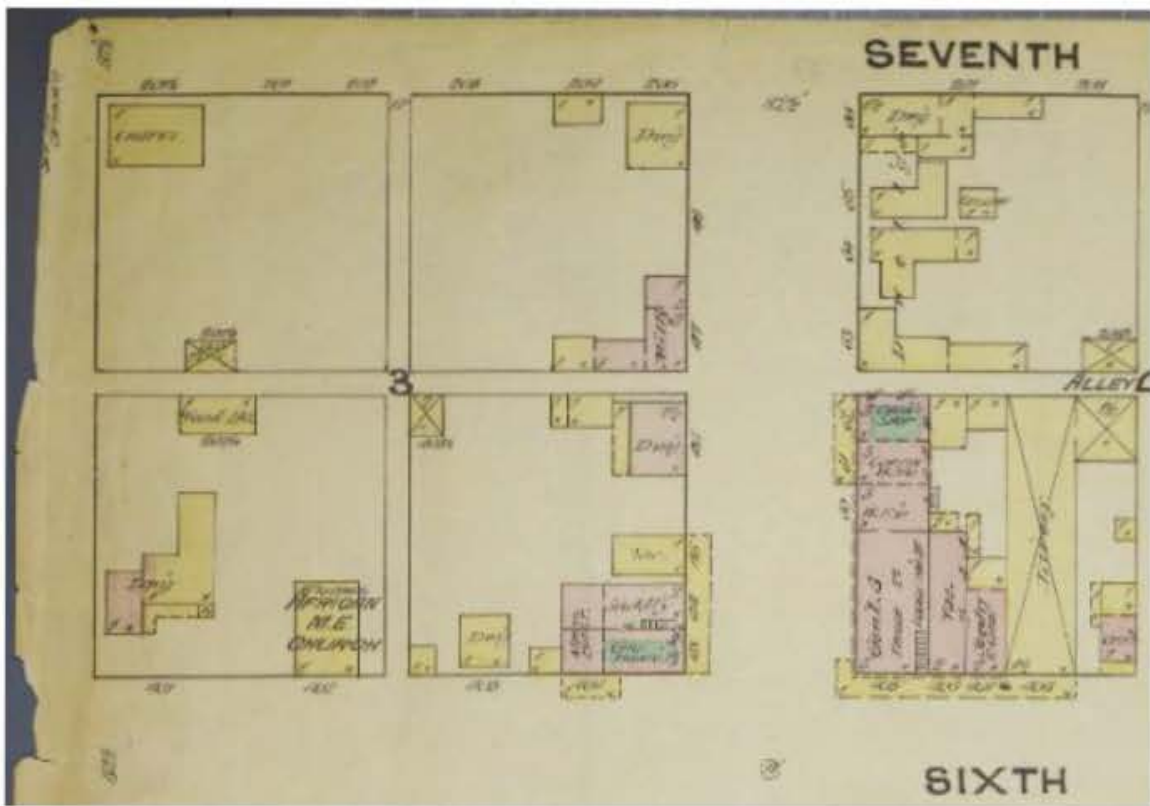


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