



To: Council Members  
From: Council Office  
Re: Weekly Packet Memo  
Date: July 16, 2004

## Packet Related Material

### **Memo**

### **Agenda**

### **Calendar**

### **Notices and Agendas:**

**Notice of Change in Schedule** for Common Council Departmental Budget Hearings

**Notice of Council Sidewalk Meeting** in the McCloskey Room on Wednesday, July 21<sup>st</sup> at noon.

### **Legislation for Final Action:**

**Ord 04-17** To Authorize Reestablishment of the Cumulative Capital Development Fund (Revised)

- Revised version of the ordinance; *(Please see the packet distributed on July 9<sup>th</sup> for the July 14<sup>th</sup> meetings for the initial piece of legislation, summary, and background material)*

Contact: Susan Clark at 349-3416 or [clarks@bloomington.in.gov](mailto:clarks@bloomington.in.gov)

### **Three Ordinance Package Related to Downtown Parking**

**Ord 04-14** To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles And Traffic" - Re: Changes to Chapter 15.32 (Parking Controls), Chapter 15.37 (Residential Neighborhood Permit Parking), Chapter 15.38 (Public Employee Parking Permits), Chapter 15.40 (Municipal Parking Lots, Garages, and On-Street Metered Parking), Chapter 15.48 (Emergency Removal and Impoundment of Vehicles), and Chapter 15.64 (Traffic Violation Schedule)

**Ord 04-15** To Amend the Ordinance which Fixed the Salaries of Appointed Officers and Employees of the Civil City (Ordinance 03-20) for the Year 2004 - Re: Adding a Parking Enforcement Officer and Two Security Guards in the Parking Enforcement Division

**App Ord 04-04** To Specially Appropriate from the Parking Enforcement Fund Expenditures Not Otherwise Appropriated (Appropriating Funds from the Parking Enforcement Fund for New Positions in the Parking Enforcement Division and the Purchase of Equipment)

- Summary of Upcoming Amendments from Deputy Mayor James McNamara; Memo from Deputy Mayor James McNamara (placed online on June 15<sup>th</sup>)

*(Please see the packet distributed on July 9<sup>th</sup> for the July 14<sup>th</sup> meetings for legislation, summary, and other background material)*

Contact: James McNamara at 349-3406 or [mcnamarj@bloomington.in.gov](mailto:mcnamarj@bloomington.in.gov)

**Amendment to Salary Ordinance – Reclassification Committee Recommendations**

**Ord 04-16** To Amend the Ordinance which Fixed the Salaries of Appointed Officers and Employees of the Civil City (Ordinance 03-20) for the Year 2004 - Re: Positions in the Clerk's Office and the Council Office

*(Please see the packet distributed on July 9<sup>th</sup> for the July 14<sup>th</sup> meetings for legislation, summary, and other background material)*

Contact: Daniel Grundmann at 349-3578 or [grundmad@bloomington.in.gov](mailto:grundmad@bloomington.in.gov)

Daniel Sherman at 349-3562 or [shermad@bloomington.in.gov](mailto:shermad@bloomington.in.gov)

Regina Moore at 349-3408 or [moorer@bloomington.in.gov](mailto:moorer@bloomington.in.gov)

**Legislation and Background Material for First Reading:**

**Ord 04-13** To Amend the Bloomington Zoning Maps from RS 4.5 to PUD and to Approve the Preliminary Plan for the South Dunn Street Planned Unit Development (PUD) - Re: 1330 South Dunn Street (Neighborhood Solutions LLC, Petitioner)

- Certification (7 – 3); Map of Surrounding Area; Memo from James Roach, Senior Zoning Planner; Memo from Environmental Commission; Memo from Bicycle and Pedestrian Commission; Memo from Fire Chief; Staff Report for June 14<sup>th</sup> and May 10<sup>th</sup> Meetings; Preliminary Plan Materials; Letters in Support; Letters in Opposition: Excerpts from the Growth Policies Plan

Contact: James Roach at 349-3527 or [roachja@bloomington.in.gov](mailto:roachja@bloomington.in.gov)

**Minutes from Regular Session:**

March 24<sup>th</sup>

July 14<sup>th</sup>

**Memo**

**Dates for Departmental Budget Hearings Changed**

The dates for the departmental budget hearings will change next week. The first night of hearings will be on Tuesday (rather than Monday) and cover the two departments (Transit and Utilities) originally scheduled for that evening. The remaining budget hearings will then be held on Monday, July 26<sup>th</sup>, Tuesday, July 27<sup>th</sup>,



and Thursday, July 29<sup>th</sup>. This change in schedule will give the Administration another week to prepare materials for the Council.

**Five Items Ready for Final Action and One Item Ready to be Introduced at the Regular Session on Wednesday, July 21<sup>st</sup>**

There are five items ready for final action and one item ready for introduction next Wednesday. The five items ready for final action were distributed and can be found in the packet for the July 14<sup>th</sup> meeting. Please note the additional information regarding those items which indicated in italics below. The one new item is included in this packet and briefly summarized further in this memo.

Regular Session on Wednesday, July 21<sup>st</sup> - Items Ready for Final Action

**Ord 04-17**      Reestablishing the Cumulative Capital Development Fund (with a Tax Levy of .05 per \$100 of Assessed Valuation) *(See Revised Version in this Packet)*

Package of Three Ordinances Regarding Downtown Parking  
*(See memo from Deputy Mayor McNamara in this Packet)*

**Ord 04-14**      Amending Title 15 (Vehicles and Traffic) - Re: Changes to Chapter 15.32 (Parking Controls), Chapter 15.37 (Residential Neighborhood Permit Parking), Chapter 15.38 (Public Employee Parking Permits), Chapter 15.40 (Municipal Parking Lots, Garages, and On-Street Metered Parking), Chapter 15.48 (Emergency Removal and Impoundment of Vehicles), and Chapter 15.64 (Traffic Violation Schedule)

Possible Amendment from Councilmember Sabbagh would:

- Provide weekend parking for public in Lot 11 until 11:00 p.m.

Proposed Amendments from the Administration would: *(See Memo from Deputy Mayor James McNamara for more details)*

- Withdraw the proposed South Downtown Residential Neighborhood Permit Parking Zone (Zone 8)

- Expand the Monday – Friday, 5:00 a.m. – 5:00 p.m. enforcement zone to include one block of East 6<sup>th</sup>, one block of Washington Street, and portions of West 4<sup>th</sup> and South Madison (around WonderLab)
- Provide parking on West Kirkwood Avenue in front of The Kirkwood Manor.
- Add limited parking spaces on North College just north of 10<sup>th</sup> Street for the retail uses in the new building going up there.
- Offer some parking alternatives for the long-term downtown residents (e.g. Chris Smith’s concern)
- Make some other “housekeeping” changes

**Ord 04-15** Amending the Civil City Salary Ordinance (Ord 03-20) to Add Parking Enforcement Officer and Two Security Guards in the Parking Enforcement Division

**App Ord 04-04** Appropriating \$108,341 from the Parking Enforcement Fund for New Positions in the Parking Enforcement Division and the Purchase of Equipment

Reclassification Committee Recommendations for Changes in the Salary Ordinance

**Ord 04-16** Amending the Civil City Salary Ordinance (Ord 03-20) to Reclassify Positions in the Clerk and Council Office

Regular Session on Wednesday, July 21<sup>st</sup> - Items Ready for Introduction

**Ord 04-13** Rezoning 6.9 Acres at 1330 South Dunn Street from RS 4.5 to PUD and to Approving the Preliminary Plan for the South Dunn Street Planned Unit Development (PUD) - Re: 1330 South Dunn Street (Neighborhood Solutions LLC, Petitioner)

## First Readings

### **Ord 04-13 Rezoning 6.9 acres from RS4.5 to PUD and Approving a Preliminary Plan for 43 Single Family Lots and 3 Mixed Used Lots – 1330 South Dunn Street (Neighborhood Solutions, LLC)**

**Ord 04-13** rezones approximated 6.9 acres of land on South Dunn Street from RS4.5 to PUD and approves a Preliminary Plan for 43 single family lots and 3 mixed use lots along Hillside Drive. It is being proposed by Neighborhood Solutions, LLC (Matt Press) with the help of Kirkwood Design Studios. Matt Press purchased the main parcel of land from Steve Smith and Tim Henke after they withdrew their proposal for this area last fall in the face of strong neighborhood opposition.

Jim Roach, Senior Zoning Planner, has provided a memo to the Council, which describes this proposal and analyses it terms of the Growth Policies Plan and current zoning ordinance. Please refer to his memo and the background material for a good understanding of this project.

Rather than summarizing it here again, this memo will orient you to the project by highlighting the significant changes over the previous proposal and then discussing one of them in more detail.

Here are some of the significant differences between this and the proposal presented to the Council last September:

- The developer has worked collaboratively with and gained the support of much of the surrounding neighborhood;
- There are rigorous architectural and site guidelines and accompanying covenants that dictate the appearance, placement, and use of structures throughout the PUD and are intended to assure the establishment of a “neo” traditional neighborhood. This effort follows upon the form district created by the City along West Kirkwood (The West Kirkwood Plan) and takes it a step further by fitting a new six-block neighborhood into an older single-family area at the edge of the Core Residential Area.
- This neighborhood would also include privately owned and maintained 3,500 s.f. “pocket park;”
- The single family densities will increase from 30 to 43 lots with a corresponding reduction in lot dimensions and most right-of-ways, and without a requirement for off-street parking;

- South Dunn will become a narrow “queuing” street and, as a result of a request of the Fire Department, the single-family properties will be equipped with residential sprinklers;
- There is a Neighborhood Activity Center (NAC) along Hillside in a maximum of five buildings, with as much as 15,650 s.f. of commercial space on the first floor and 21,150 s.f. of office space on the basement and second floors;
- Rather than the 20 apartment units under the previous proposal, the multi-family component of the NAC would have 16 units, if solely occupying the second floor of the buildings, or as much as 32 units if the commercial space does not work and the first floors become apartments (with a maximum of 48 bedrooms);
- The parking for the NAC will be reduced by at least the 83 spaces ordinarily required by code and will include 34 spaces of angled-parking on Hillside; and
- The site has been enlarged to include the lot at the northwest corner of Henderson and Hillside and portions of the City’s right-of-way that must be acquired through the Redevelopment Commission.

### Neighborhood Activity Center and Related Parking

The Plan Commission spent much of its time discussing the Neighborhood Activity Center (NAC) and the proposed angle-parking on Hillside Drive and, for that reason, the rest of this memo will cover this topic.

**GPP Policies.** The NAC is a new and evolving concept for the GPP. It is intended to serve as the focus for a neighborhood and to give residents a place to walk rather than drive their cars.

The nature and scale of these commercial nodes should draw upon the needs of the neighborhood and their site designs should minimize adverse impacts on the surrounding neighborhood as well as include a full complement of pedestrian amenities. With that said, they may also include residential or office space on the upper floors.

They are intended to offer an alternative to the new commercial growth at the City’s edge and should be near transit routes, on primary and secondary collectors (not arterials), and should be surrounded by an adequate network of sidewalks. While the GPP saw them cropping up in the center as well as the edge of neighborhoods, developers recently have placed them along the major thoroughfares and argued that they are only viable if conveniently accessible by car.

**Appearance, Use and Scale of this NAC.** This NAC will occupy three lots along Hillside Drive (Lots A, B & C) in as many as 5 2-story buildings with usable space on all floors approaching 36,800 square feet. The building on the corner of Henderson and Hillside (Lot C) will look like a series of storefronts, and the buildings to the west (on Lot B and Lot A) may either look like storefronts or small apartment buildings.

They are intended to have commercial uses on the first floor (15,650 s.f.) and office uses on the upper floors (15,650 s.f.) as well as in the basement of Lot C (5,500 s.f.). These uses have been tailored to serve the neighborhood and include:

- Grocery/Convenience Stores
- Offices (on first and second floors and basement)
- Personal Services (excluding tattoo parlors and on-site dry cleaning)
- Retail Services (excluding liquor stores and adult uses)
- Restaurants

*But specifically exclude all drive-through and gasoline sale uses (COA #3).*

In the alternative, these buildings may be occupied by as many as 32 dwelling units with a total 48 bedrooms. In order to provide an incentive for commercial uses, the dwelling units on the first floors must be 1-bedroom studio apartments.

While the list of uses may serve the surrounding neighborhood, there is some question whether the 36,800 s.f. of commercial/office space is also at the appropriate scale. The memo from Jim Roach includes a table comparing this project with other recent NAC's including Renwick (32,000 s.f.) and the McDoel Grocery (5,500 s.f.) as well as other commercial nodes around the community.

**Reductions in Parking and Angled-Parking on Hillside.** This PUD calls for reductions in parking for both the single-family and mixed use portions of the project. If the NAC were to fully develop as a commercial/office project, the Planning memo indicates that our code would require 143 off-street parking spaces. This project offers only 26 off-street spaces and provides for 34 spaces of angled parking on Hillside for a total of 60, which is 83 less than what the code would require. Given these numbers, the NAC may not have enough parking to be viable or, in the alternative, may bring cars that it cannot accommodate and which will park in the surrounding neighborhoods.

The 26 spaces of angled parking generated much discussion at the Plan Commission because the cars would pull out onto Hillside, which is a secondary arterial. The developer addressed this concern by agreeing to purchase enough right-of-way from the City so that the length of each stall can be extended an extra 5 feet (from 20'3" to 25').

The Plan Commission explored three options before accepting the developer's proposal for Hillside. These options are in the packet and included:

- Parallel parking on Hillside with an 8' "pull-off area" (which was consistent with the Master Thoroughfare Plan and more acceptable to City's emergency services, but was opposed by the developer because of the loss of parking that could only be regained by removing a single-family lot).
- No on-street parking, but more off-street parking behind the buildings (but this would threaten the viability of the NAC, remove the traffic-calming effect of pull-off parking, and require the loss of a single-family lot).
- Angled-parking on Hillside with a curbed island separating the area from the travel lane (but this would reduce on-street parking, move the mixed use buildings away from the street front, and perhaps eliminate the building on the corner of Henderson and Hillside).

The Plan Commission eventually supported angled parking despite its inconsistencies with the NAC and the Master Thoroughfare Plan recommendations because of:

- Examples of angled-parking along other arterials;
- The presence of a signalized intersection and a school speed zone; and
- The necessity of the parking for the success of the NAC.

### **Conditions of Approval**

After hearings on May 10<sup>th</sup> and June 14<sup>th</sup> the Plan Commission approved this petition by a vote of 7 – 3 subject to the following conditions:

- It declared the intent to amend GPP to designate 1 acre along Hillside Drive as a Neighborhood Activity Center (COA #1);
- It allowed the petitioner to reduce travel lanes on interior public streets to 7.5 feet as long as the single family homes are equipped with residential sprinklers and inspected by the Fire Department prior to occupancy (COA #2); and

It required the Petitioner to:

- Exclude drive-through uses and gasoline sales from the list of permitted uses (COA #3);
- Maintain travel lanes of 11' on Grimes Lane and a 12' on Hillside Drive when making improvements in the right-of-way (COA #4);
- Locate all public improvements (on-street parking, street trees, and sidewalks) within the right-of-way (COA #5);
- Provide a 20' radius of right-of-way lines at the intersection of Hillside and Henderson (COA #6);
- Dedicate 50' of right-of-way for South Dunn and construct all of it from Grimes Lane to Hillside Drive at the time of the final plan for the single family lots (COA #7);
- Provide 8' wide parallel parking spaces along South Dunn Street adjacent to the commercial buildings (COA #8);
- Arrange for the park to be privately owned and maintained, which shall be reviewed by the Plan Commission at final plan stage (COA #9);
- Limit residential occupancies to no more than 3 unrelated adults pursuant to the local definition of "family" in single family zones (COA #10);
- Obtain final approval of the drainage and utility plans from CBU by the time of the final plan (COA #11);
- Purchase land along Hillside Drive from the Redevelopment Commission prior to filing the final plan (COA #12);
- Preserve the regulated sight triangle for all on-street parking which requires at least 20' from crosswalks and 30' from approaches to stop signs (COA #13);
- Design the north/south alleys by the mixed used buildings along Hillside Drive to accommodate truck traffic and the turning movements needed for deliveries (COA #14);
- Construct a sidewalk along the south side of Grimes Lane from this project to the corner of Henderson once the design has been approved as part of the final plan (COA #15);
- Place deed restrictions on the potential commercial lots along Hillside Drive that will be enforceable either by the future owners of the properties or the City and will restrict:
  - the hours of operation;
  - the size of delivery vehicles and times of deliveries in a manner that is reasonably designed to avoid noise problems with neighbors and traffic problems on Hillside Drive;
  - after consultation with Plan Staff and with review by the Plan Commission during final plan stage (COA #16); and

- Not extend Wilson Street east of the alley between Dunn Street and Henderson (and the revised site plan will be reviewed by the Plan Commission during the final plan stage) (COA #17).





City of  
Bloomington  
Indiana

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**To: Council Members**  
**From: Council Office**  
**Re: Calendar for the Week of**  
**July 19, 2004 – July 24, 2004**  
**Date: July 16, 2004**

Monday, July 19, 2004

12:00 pm Safe and Civil City Advisory Board, McCloskey  
3:00 pm Smoking Policy Committee, Hooker Room  
4:00 pm Council for Community Accessibility, McCloskey  
4:00 pm Community and Family Resources Commission, Dunlap  
5:00 pm Farmers' Market Advisory Council, Parks  
5:30 pm Bicycle & Pedestrian Safety Commission, Hooker Room

Tuesday, July 20, 2004

3:00 pm Bloomington Community Farmers' Market, Plaza  
4:00 pm Board of Public Safety – 220 E. Third Street  
4:00 pm Board of Park Commissioners, Council Chambers  
5:30 pm Public Transportation Corporation Board, Transportation Center – 130 W. Grimes  
6:30 pm Animal Control Commission, McCloskey  
7:00 pm Common Council – Departmental Budget Hearings, Council Chambers

Wednesday, July 21, 2004

9:30 pm Tree Commission, Hooker Room  
12:00 pm Common Council Sidewalk Committee, McCloskey  
2:00 pm Hearing Officer, Kelly  
7:00 pm Council of Neighborhood Associations, McCloskey  
7:30 pm Common Council – Regular Session, Council Chambers

Thursday, July 22, 2004

7:00 pm Environmental Commission, McCloskey

Friday, July 23, 2004

*There are no meetings scheduled for today*

Saturday, July 24, 2004

7:00 am Bloomington Community Farmers' Market, Showers Common



City of Bloomington  
Office of the Common Council

## **NOTICE OF CHANGE IN SCHEDULE FOR THE COMMON COUNCIL DEPARTMENTAL BUDGET HEARINGS**

The Common Council will not begin the departmental budget hearings on Monday, July 19, 2004 at 7:00 p.m. as originally scheduled. The first departmental budget hearings will begin on Tuesday, July 20<sup>th</sup> at 7:00 p.m. in the Council Chambers and include the budget for Transit and the Utilities Department. The remaining departmental budget hearings will occur the following week and notice of those meetings will be posted in the near future.

This statement is provided in accordance with the Indiana Open Door Law and provides notice that this meeting will occur and is open for the public to attend, observe, and record what transpires.

Posted: Thursday, July 15, 2004



City of Bloomington  
Office of the Common Council

# MEETING NOTICE

## Common Council Sidewalk Committee

The Common Council Sidewalk Committee will meet at 12:00 p.m. on this Wednesday, July 21, 2004. The meeting will be held in the McCloskey Room, at City Hall (401 N. Morton Street). The purpose of the meeting is to discuss sidewalk projects and procedures for 2004. Because a quorum of the Council may be present, this meeting would also constitute a meeting of the Council as well as of this committee under the Indiana Open Door Law. For that reason, this statement is providing notice that this meeting will occur and is open for the public to attend, observe, and record what transpires.

Dated and Posted: Friday, July 16, 2004

**ORDINANCE 04-17**

**TO AUTHORIZE REESTABLISHMENT OF THE CUMULATIVE CAPITAL DEVELOPMENT FUND**

- WHEREAS, Indiana Code §36-9-15.5 provides for the establishment of a Municipal Cumulative Capital Development Fund; and
- WHEREAS, The City established a Cumulative Capital Development Fund in 1984 by Ordinance 84-28 as reauthorized by, Ordinance 87-24, Ordinance 90-33 and Ordinance 93-38; and
- WHEREAS, The Capital Development Fund tax rate for the year 1983 pay 1984 was \$0.15 per \$100 of assessed valuation; and
- WHEREAS, As a result of various general reassessments throughout the years, the Capital Development Fund tax rate for the year 2003 pay 2004 was .0284 per \$100 of assessed valuation; and
- WHEREAS, The City desires to increase the Capital Development Fund tax rate as allowed by Indiana Code §36-9-15.5;

NOW, THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION I. Ordinance 93-38 is hereby rescinded.

SECTION II. The Cumulative Capital Development Fund (Fund) is hereby reestablished with a tax rate not to exceed \$0.05 per \$100 of assessed valuation beginning with taxes payable in 2005, continuing until reduced or rescinded.

SECTION III. The Fund shall be used in accordance with the applicable Indiana Code provisions which include but are not limited to the following: to purchase, construct, equip, and maintain buildings for public purposes; to acquire the land, and any improvements on it, that are necessary for the construction of public buildings; to demolish any improvements on land acquired under this section, and to level, grade, and prepare the land for the construction of a public building; to acquire land or rights-of-way to be used as a public way or other means of ingress or egress to land acquired for the construction of a public building; to improve or construct any public way or other means of ingress or egress to land acquired for the construction of a public building; to acquire land or rights-of-way to be used for public ways or sidewalks; to construct and maintain public ways or sidewalks; to acquire land or rights-of-way for the construction of sanitary or storm sewers, or both; to construct and maintain sanitary or storm sewers, or both; to acquire, by purchase or lease, or to pay all or part of the purchase price of a utility; to purchase or lease land, buildings, or rights-of-way for the use of any utility that is acquired or operated by the unit; to purchase or acquire land, with or without buildings, for park or recreation purposes; to purchase, lease, or pay all or part of the purchase price of motor vehicles for the use of the police or fire department, or both, including ambulances and firefighting vehicles with the necessary equipment, ladders, and hoses; to retire in whole or in part any general obligation bonds of the unit that were issued for the purpose of acquiring or constructing improvements or properties that would qualify for the use of cumulative capital improvement funds; to purchase or lease equipment and other nonconsumable personal property needed by the unit for any public transportation use; to purchase or lease equipment to be used to illuminate a public way or sidewalk; to purchase, lease, upgrade, maintain, or repair computer hardware, computer software, wiring and computer networks, communication access systems used to connect with computer networks or electronic gateways; or to pay for the services of full-time or part-time computer maintenance employees.

SECTION IV. In each of the years during which the Fund is authorized, fifteen percent (15%) of the property tax levy shall be used specifically for the acquisition of land for purposes including but not limited to greenspace, passive recreation, and/or conservation.

SECTION V. All expenditures from the Fund shall be subject to appropriation by the city's fiscal body.

SECTION VI. If any section, sentence or provision of this ordinance, or the application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions, or applications of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

SECTION VII. This ordinance shall be in full force and effect from and after its passage by the Common Council of the City of Bloomington and approval of the Mayor.

PASSED AND ADOPTED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
MIKE DIEKHOF, President  
Bloomington Common Council

ATTEST:

\_\_\_\_\_  
REGINA MOORE, Clerk  
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
REGINA MOORE, Clerk  
City of Bloomington

SIGNED and APPROVED by me upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
MARK KRUZAN, Mayor  
City of Bloomington

#### SYNOPSIS

This ordinance reestablishes the Cumulative Capital Development Fund with a property tax rate of \$0.05 per \$100 of assessed valuation. It also authorizes an annual transfer of fifteen percent (15%) of the property tax levy for the acquisition of land for purposes including, but not limited to, greenspace, passive recreation, and/or conservation.

*Note: This ordinance was revised after it was distributed in the packet and before it was introduced at the Regular Session in order to raise the set aside for the acquisition of land from 10% to 15% and to clarify in the synopsis that the purpose for these acquisitions includes, but is not limited to, providing for greenspace, passive recreation and/or the conservation of land.*

The memos on the following pages are related to the legislation listed below.

**Ordinance 04-14**: To Amend Title 15 of the Bloomington Municipal Code entitled “Vehicles and Traffic.”

**Ordinance 04-15**: To Amend the Ordinance Which Fixed the Salaries of Appointed Officers and Employees of the Civil City (Ordinance 03-20) for the Year 2004.

**Appropriation Ordinance 04-04** To Specially Appropriate from the Parking Enforcement Fund Expenditures Not Otherwise Appropriated (Appropriating Funds from the Parking Enforcement Fund for New Positions in the Parking Enforcement Division and the Purchase of Equipment)



**City of Bloomington  
Office of the Mayor**

Mark Kruzan

Date: July 16, 2004  
To: Councilmembers  
From: James McNamara  
Subj: Amendments to parking legislation

As I indicated at the Committee of the Whole meeting last Wednesday I will be bringing forward amendments to this Wednesday's regular session to the parking legislation we discussed. All the amendments are to Ordinance 04-14, to Amend Title 15. While I have not yet prepared the actual amendment documents, I wanted to give you an idea of what I saw coming forward for your consideration. I will also be talking with Dan Sherman about which of these ideas can be combined into a single amendment rather than addressing each point with a separate amendment upon which you need to vote.

Proposed amendments:

- Withdraw the creation of new Zone 8 residential parking zone from this legislation. The proposal could be brought back for future consideration in the fall if the Council wishes. I now feel we need more time to communicate about the creation of this zone and get feedback, including Council feedback, about its boundaries and general desirability
- Expand the Monday - Saturday, 5:00 am to 5:00 pm, enforcement zone to include a block of east 6<sup>th</sup> Street, a block of South Washington Street (I mentioned this at the meeting,) and to include two block faces on W Fourth and South Madison for the area around the WonderLab.
- Add parking spaces on the south side of West Kirkwood in front of the The Kirkwood housing development just east of Madison Street. Current Code prohibits parking there because of the location's previous incarnation as a grocery store with wide cuts into Kirkwood Avenue from the grocery store parking lot.
- Add limited time (less than 2 hours) parking spaces in front of the 10<sup>th</sup> and College residential development on the west side of College just north of 10<sup>th</sup> Street to accommodate some retail in that project.
- Something to address at least some portion of the issues raised by Chris Smith at Wednesday's Committee meeting.
- A "housekeeping" amendment to correct clerical errors and omissions I made in drafting the original legislation.

While this may seem like a lot of revision, I don't think you will find any of it contentious and hope you can support the amendments. Please let me know if you have any questions. Feel free to call me on my cell phone over the weekend or at any time.





**City of Bloomington**  
**Office of the Mayor**  
Mark Kruzan

Date: July 10, 2004  
To: Councilmembers  
From: James McNamara  
Subj: Parking Legislation

The parking legislation on your agenda comes to you as a package of three ordinances:

- Ordinance 04-14: Amends Title 15 of the Bloomington Municipal Code and constitutes the bulk of the proposed changes
- Ordinance 04-15: An Amendment to the 2004 Salary Ordinance that allows the hiring of two security guards and an additional parking enforcement officer
- Appropriation Ordinance 04-04: Which appropriates money from the Parking Enforcement Fund to pay for security personnel, enforcement officer, security equipment and signage.

Each ordinance is described in greater detail below. It is worth noting at the beginning that the Parking Enforcement Fund is funded entirely with revenues from parking fees and fines. No tax dollars are allocated to this Fund.

The legislation comes to you with the overarching goal of getting specific, concrete proposals on the table for your consideration. Other primary and interrelated goals include preparing for the occupancy of more than 1300 new bedrooms in the downtown area in the next two months (with a heavy concentration in the downtown's westside,) and doing the best we can to ensure turnover of the on-street parking spaces in this area in order to increase the availability of that parking. We also want to use this opportunity to take measures to enhance the security at the parking garages in this area.

These ordinances represent an incremental approach to downtown parking issues. We do not presume that this legislation addresses all downtown parking issues. We anticipate that additional measures may need to be taken, but we want to first learn from our experience of the next several months, both with the new downtown residents and with this legislation. The focus of this legislation is on an area bounded by Kirkwood Avenue on the south, Rogers Street on the west, 11<sup>th</sup> Street on the north and Walnut Street on the east. It also includes the establishment of a new neighborhood parking zone in an area bounded roughly by First, Lincoln, 3<sup>rd</sup> and Dunn Streets.

The legislation addresses the following topics that this memo will address in turn.

- On-street parking restrictions
- City Hall Parking
- Parking Garage Security
- Neighborhood Parking Zone
- Enforcement & Collections
- Revenue Enhancement
- Housekeeping

### On-street Parking Restrictions

Ordinance 04 –14 Sections: 1-6, 18

The legislation makes several changes to the Municipal Code regarding on-street parking. First it extends the boundaries of the area where the 2-hour parking limits, found throughout most of the downtown area, apply. It extends the covered area to Rogers Street on the west and 11<sup>th</sup> street on the north, including those streets.

Another change is an attempt to deal with what is sometimes called “storage parking”. We’ve heard from many downtown stakeholders about on-street parking spaces being occupied on nights and weekends and thus unavailable to those who might visit downtown during those hours. Currently the 2 hour downtown parking limit applies Monday through Friday from 8:00 a.m. to 5:00. Ordinance 04-14 would change those hours so that the two hour limit would apply Monday through Saturday between 5:00 a.m. and 5:00 p.m. This means that a vehicle parked in a regulated space at 5:00 a.m. would be violating the two hour limit, and eligible for a parking ticket, if it was still parked there at 7:01 a.m. It is our belief/hope that the necessity of moving one’s vehicle before 7:00 a.m. will discourage a significant percentage of those who might otherwise park their vehicles there overnight. Obviously, the City would need to change its staffing schedule to enforce these changes.

We’ve also had requests to increase the number of short-term parking spaces available in the courthouse square area. We believe an incremental approach is most prudent here and, accordingly, we propose to convert eight spaces on the inside of the courthouse square from 2 hour to 1 hour limits. Four spaces would be clustered around the northwest and southeast corners of the inside of the square. We also propose to add two 30 minute spaces on the northeastern end of Morton Street just south of Seventh Street.

### City Hall Parking

Ordinance 04-14 Sections: 7, 9, 10, 11, 12, 13, 23, 24, 25, 27, 29

Appropriation Ordinance: 04-04

With so much of the new residential development going up very near City Hall, we want to do what we can to ensure that parking remains accessible to those visiting City Hall (on business, for meetings, etc.) and to our employees. We are proposing to designate 15 spaces on Morton

Street just south of Eighth Street as being exclusively for those visiting City Hall between 5:00 a.m. and 5:00 p.m. Those hours were chosen to discourage overnight parking and the possibility the spaces would then still be occupied after 8:00 a.m. We hope to make parking in a space designated for City Hall visitors by those not actually in City Hall a \$50 fine and a towable expense.

We also want to protect the employee parking spaces on the south and north ends of City Hall in a similar fashion by prohibiting parking there without an employee permit between 5:00 am and 5:00pm, and prohibiting parking there by anyone other than those inside City Hall at all other times. The spaces would remain available after 5:00 for those attending a meeting, for example. But parking by anyone else on nights and weekends would be prohibited. We hope to make this a \$50 fine for violations and a towable offense. Exceptions are made in the ordinance to the above provisions for Farmers' Market.

It will take extensive signage to make the new rules clear to those searching for parking spaces. The appropriation ordinance that is part of this legislative package includes \$13,800 for such signage and the accompanying necessary poles and fixtures. The money comes from the Parking Enforcement Fund.

Parking Garage Security

Ordinance 04 – 14 Section 16

Ordinance 04 – 15

Appropriation Ordinance 04-04

**ORDINANCE 04-13**

**TO AMEND THE BLOOMINGTON ZONING MAPS FROM RS 4.5 TO PUD  
AND TO APPROVE THE PRELIMINARY PLAN FOR  
THE SOUTH DUNN STREET PLANNED UNIT DEVELOPMENT (PUD)  
RE: 1330 South Dunn Street  
(Neighborhood Solutions LLC, Petitioner)**

WHEREAS, on May 1, 1995 the Common Council adopted Ordinance 95-21, which repealed and replaced Title 20 of the Bloomington Municipal Code entitled “Zoning”, including the incorporated zoning maps, and Title 21, entitled “Land Use and Development;” and

WHEREAS, the Plan Commission has considered this case, PUD-09-04, and recommended that the petitioner, Neighborhood Solutions LLC, be granted a rezone of the property located at 1330 South Dunn Street from RS4.5 to Planned Unit Development and also be granted a preliminary plan approval for the South Dunn Street PUD. The Plan Commission thereby requests that the Common Council consider this petition;

NOW, THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION I. Through the authority of IC 36-7-4 and pursuant to Chapter 20.05.09 of the Bloomington Municipal Code, the property located at 1330 South Dunn Street shall be rezoned from RS4.5 to Planned Unit Development (PUD) and the Preliminary Plan shall be approved. The property is further described as follows:

A part of southwest quarter of Section 4, Township 9 North, Range 1 West, Monroe County, Indiana, being more particularly described as follows:

A part of the West half of Seminary Lot Eighty (80) in Township Eight (8) North, Range 1 West, not platted Rolling Park Addition, and described as follows, to-wit: Beginning at a point Four Hundred Fifty (450) feet West of a corner stone in the Northeast corner of Lot Number 1 in Rolling Park Addition to the City of Bloomington, as shown by the recorded plat thereof in the office of the Recorder in Monroe County, thence East One Hundred Thirty-two (132) feet, thence South to the Southeast corner of Lot Number Fifty-one (51) in said Rolling Park Addition, thence West on and over the South line of said Lot Number Fifty-one (51) in said addition, a distance of One Hundred Thirty-two (132) feet, thence North to the place of beginning;

Also a part of the East half of Seminary Lot Eighty (80) in Township Eight (8) North, Range 1 West, not platted Rolling Park Addition, and described as follows, to-wit: Beginning at a point One Hundred Forty (140) feet West of a corner stone in the Northeast corner of Lot Number 1 in Rolling Park Addition to the City of Bloomington, as shown by the recorded plat thereof in the office of the Recorder in Monroe County, thence West One Hundred Thirty-two (132) feet, thence South to the Northwest corner of Lot Number Eighteen (18) in said Rolling Park Addition, thence East on and over the North line of said Lot Number Eighteen (18) in said addition, a distance of One Hundred Thirty-two (132) feet, thence North to the place of beginning;

Also, fifteen (15) feet by parallel lines off the South side of Lot Number Fifteen (15) in Rolling Park Addition to the City of Bloomington, Indiana, and also Lot Number Sixteen (16) in said Rolling Park Addition, the plat of which is recorded in Plat Book No. 3 at page 68 in the Office of the Recorder of Monroe County, Indiana.

SECTION II. The Preliminary Plan shall be attached hereto and made a part thereof.

SECTION III. This ordinance shall be in full force and effect from and after its passage by the

Common Council and approval by the Mayor.

PASSED AND ADOPTED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
MIKE DIEKHOFF, President  
Bloomington Common Council

ATTEST:

\_\_\_\_\_  
REGINA MOORE, Clerk  
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
REGINA MOORE, Clerk  
City of Bloomington

SIGNED and APPROVED by me upon this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
MARK KRUZAN, Mayor  
City of Bloomington

#### SYNOPSIS

This ordinance rezones approximately 6.9 gross acres along South Dunn Street between Hillside Drive and Grimes Lane from RS4.5 to Planned Unit Development. It also approves a preliminary plan that allows for up to 43 single family lots to the north and three lots of commercial/ multi-family uses along Hillside Drive.

\*\*\*\*ORDINANCE CERTIFICATION\*\*\*\*

In accordance with IC 36-7-4-605 I hereby certify that the attached Ordinance Number 04-13 is a true and complete copy of Plan Commission Case Number PUD-09-04 which was given a recommendation of approval by a vote of 7 Ayes, 3 Nays, and 0 Abstentions by the Bloomington City Plan Commission at a public hearing held on June 14, 2004.

Date: June 18, 2004

\_\_\_\_\_  
Thomas B. Micuda, Secretary  
Plan Commission

Received by the Common Council Office this \_\_\_\_\_ day of \_\_\_\_\_, 2004.

\_\_\_\_\_  
Regina Moore, City Clerk

Appropriation Ordinance #	_____	Fiscal Impact Statement Ordinance #	_____	Resolution #	_____
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Type of Legislation:

Appropriation	End of Program	Penal Ordinance
Budget Transfer	New Program	Grant Approval
Salary Change	Bonding	Administrative Change
Zoning Change	Investments	Short-Term Borrowing
New Fees	Annexation	Other

If the legislation directly affects City funds, the following must be completed by the City Controller:

Cause of Request:

Planned Expenditure	_____	Emergency	_____
Unforeseen Need	_____	Other	_____

Funds Affected by Request:

Fund(s) Affected	_____	_____
Fund Balance as of January 1	\$ _____	\$ _____
Revenue to Date	\$ _____	\$ _____
Revenue Expected for Rest of year	\$ _____	\$ _____
Appropriations to Date	\$ _____	\$ _____
Unappropriated Balance	\$ _____	\$ _____
Effect of Proposed Legislation (+/-)	\$ _____	\$ _____
Projected Balance	\$ _____	\$ _____

Signature of Controller

Will the legislation have a major impact on existing City appropriations, fiscal liability or revenues?

Yes \_\_\_\_\_ No \_\_\_\_\_

If the legislation will not have a major fiscal impact, explain briefly the reason for your conclusion.

If the legislation will have a major fiscal impact, explain briefly what the effect on City costs and revenues will be and include factors which could lead to significant additional expenditures in the future. Be as specific as possible. (Continue on second sheet if necessary.)

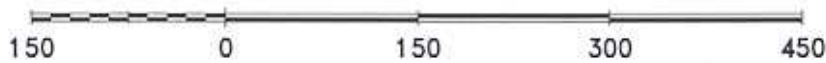






1998 Aerial

By: roachja  
20 Apr 04



For reference only; map information NOT warranted.



City of Bloomington  
Planning



Scale: 1" = 150'



## Interdepartmental Memo

**To: Members of the Common Council**  
**From: James Roach, Senior Zoning Planner**  
**Subject: Case # PUD-09-04**  
**Date: June 21, 2004**

Attached are the staff report, petitioners' statement, and map exhibits which pertain to Plan Commission Case # PUD-09-04. The Plan Commission heard this petition at its June 14, 2004 meeting and voted 7-3 to send this petition to the Common Council with a favorable recommendation.

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**REQUEST:** The petitioners are requesting a rezoning of 6.9 gross acres from RS4.5 to Planned Unit Development (PUD) and preliminary plan approval for 43 single family lots and three mixed use lots. Also requested is a *Growth Policies Plan* Amendment to change the classification of part of the property from "Core Residential" to "Neighborhood Activity Center".

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### BACKGROUND:

<b>Area:</b>	6.9 gross acres
<b>Current Zoning:</b>	RS4.5
<b>GPP Designation:</b>	Core Residential
<b>Existing Land Use:</b>	Mostly vacant, rolling meadow terrain, scattered trees, 6 existing homes
<b>Proposed Land Use:</b>	Mixed residential and commercial
<b>Proposed Density:</b>	
<b>Total:</b>	up to 10.9 u/a (75 units on 6.9 acres)
<b>Single Family:</b>	up to 7.3 u/a (43 lots on 5.9 acres).
<b>Multi-family:</b>	up to 32 u/a (32 units on 1 acre)
<b>Proposed Commercial:</b>	15,650 maximum square feet retail + 21,150 maximum SF of office
<b>Surrounding Uses:</b>	<b>North, West</b> – Single family residential <b>East</b> – Single family residential and Templeton Elementary School <b>South</b> – Mixed single and multi-family residential

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**REPORT SUMMARY:** The property in question is approximately 6.9 acres in the Bryan Park Neighborhood. It is bounded by E. Grimes Lane to the north, E. Hillside Drive to the south, existing homes on S. Palmer Street to the west and existing homes and S. Henderson Street to the east. Surrounding lots were created in the late 1920s. Since that time, a portion of E. Driscoll Street was constructed into the property and extended up the Dunn St. right-of-way to Grimes Ln. Also, six small houses were constructed and used as rentals. These houses are in poor condition and not historic in nature. One house was recently moved to the northeast corner of Henderson and E. Miller Drive.

Under the current RS4.5 zoning, the property could conceptually be developed with 26 lots. Instead, the petitioners, Neighborhood Solutions LLC, are requesting that the zoning be changed to Planned Unit Development (PUD) and a preliminary plan be approved to allow for 43 single family lots and 3 mixed use lots.

The proposal includes the creation of new alleys to access rear garages and parking areas. Internal public streets would be developed with on-street parking and narrow travel lanes. The homes will be built with front porches, in locations close to the right-of-way, and in styles similar to those in the existing neighborhood. The mixed use buildings would utilize on-street parking, outdoor “plazas”, and first floor commercial uses with residential or office uses on the second floor. Dunn St. would be extended through the property to Hillside. Two other rights-of-way, E. Driscoll Street and E. Wilson Street, stub into the property from both the east and the west. Driscoll St. is proposed to be extended through the property to Palmer St. while the Plan Commission required that Wilson St. end at the eastern edge of this property and not be extended onto Henderson St. All of these elements integrate into the “neo-traditional” or “new urbanist” feel that the petitioners are trying to create.

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**GROWTH POLICIES PLAN:** The *Growth Policies Plan* designates this property as “Core Residential”, but the petitioners have requested that the *GPP* be amended to include the southern part of the property as a “Neighborhood Activity Center” (NAC). The *GPP* notes that “while several NACs have been identified on the land use map, more could be designated in the future as further study is done and appropriate locations have been identified” (Pg. 33).

In general, the Plan Commission found that this proposal meets many of the policies and recommendations of the *GPP*. This proposal furthers goals of compact urban form and neighborhood scaled commercial centers. The dominant land use element of this project is still owner occupied single family homes. This matches the general land use goal for “Core Residential” areas. The single family lots also meet multiple *GPP* recommendations such as providing garages accessed by alleys, utilizing a grid street pattern with multiple points of connectivity and providing compatible architecture and site design for new construction.

The mixed use component has been designed to satisfy the recommendations of the *GPP* for “Neighborhood Activity Centers.” These buildings are designed at a scale so that they serve the neighborhood without attracting an influx of usage from surrounding areas. The proposed uses are also compatible with the neighborhood. Residential uses are mostly located on the second floors of the buildings. The buildings are pushed to the front edge of the site, framing the four corners of the commercial area at the street intersection. Parking is provided

both on street and behind buildings and is at a scale to ensure that parking is not a large attractor for commercial users.

This proposal promotes compact urban form in that it is a dense, infill project in an area that already contains city services. This proposal preserves community character by maintaining the architectural character of the older core neighborhoods. Improved connectivity is ensured through the linkage of street stubs adjacent to the property. Finally, the character of the neighborhood is ensured through continuation of narrowed streets into the development.

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## **LAND USE:**

**Single Family:** Single family uses are currently permitted in this zoning district. The *GPP* states that the Core Residential land use district is designed for higher density single family residential uses. The *GPP* also encourages maintenance of existing housing stock and utilization of City funds to provide incentives for increased owner occupancy and affordable housing construction.

**Commercial:** The Plan Commission approved non-residential uses on the three mixed use lots along Hillside Dr. and an amendment to the *GPP* to designate this area as a NAC. Approximately one acre of land at the SW corner of Hillside and Henderson is already zoned Limited Commercial (CL) and is designated as a NAC. The *GPP* notes that other NACs may be designated in the future after further study.

The proposed uses are very similar to the uses currently permitted in the CL zoning district. The *GPP* states that within NACs, the commercial uses should be “at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes” (Pg. 33). The *GPP* also notes that “neighborhood-serving commercial uses, and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations” (pg. 30).

The proposed uses are as follows:

- Grocery/Convenience Store
- Offices
  - Permitted on first and second floors
- Personal Services
  - Tattoo Parlors not permitted
  - On-site Dry-Cleaning not permitted
- Retail Sales
  - Liquor stores not permitted
  - Adult uses not permitted
- Restaurants
- Drive-through uses are specifically excluded
- Gasoline sales is specifically excluded

**Multi-family:** The Plan Commission approved multi-family uses on the second floors of all the mixed use buildings at the southern end of the development. The petitioners also proposed that if commercial uses are not successful at this location that all of the first floors of these building be allowed to be used for studio apartment. They described these apartments as single rooms with a bathroom and kitchenette. This will allow for both the easiest possible conversion back to commercial space and the greatest flexibility of uses for the building owner. The petitioners believe that there will be an incentive to utilize this space commercially because of the lower rents that can be expected with studio apartments as apposed to commercial space.

If all of the first floors were developed with commercial uses, the lots would be permitted a maximum of sixteen (16) two-bedroom units, for a total of 32 bedrooms. Without any commercial on the first floor, the maximum number of units increases to 32 units, including sixteen (16) two-bedroom units and sixteen (16) studio units (48 total bedrooms).

**Corner Lots/Accessory Dwelling Units:** The petitioners also proposed that the “side street lots” along Wilson St. and Driscoll St. be allowed to be developed with either: 1) a garage for the house on the adjoining corner lot, 2) a separate house on a small lot, or 3) a garage with an accessory apartment on the second floor. The petitioners also proposed that the accessory apartment only be permitted if the main house, on the corner, is owner occupied. This will require a covenant within the PUD to assure this arrangement. The *GPP* notes that “granny flats” may be an appropriate way to allow additional densities while still protecting community and neighborhood character (pg. 17).

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## **INTENSITY OF USES:**

**Single family density, lot size and lot width:** The petitioners propose a maximum single family density of 7.3 units per acre (43 units/5.9 acres). This density would be reached if every corner lot was developed with two houses, or a house and an accessory apartment over a detached garage. If all corner lots were developed with only one unit, this density drops to 6.1 units per acre (36 lots/5.9 acres).

The single family lot layout includes a minimum lot size of 2,266 square feet and minimum lot width of 40 feet. The smallest lots would be the “side street lots” along Driscoll and Wilson. The largest lots, at 5,480 square feet, would be those same corner locations if the two lots were developed with only one house.

Setback requirements are specific to the lot type. The petitioners have proposed five different lot types, including Corner Lot, Side Street Lot, Mid Block Lot, Grimes Lane Lot and Live/Work Lot. All lots along Dunn and Grimes include a 10 foot build-to line for the required porches and an 18 foot build-to line for the main mass of the house. Lots along side streets include a 6 foot build-to line for the required porches and a 12 foot build-to line for the main mass of the house. In general, one and one and a half story houses are permitted to have 6 foot side yard setbacks, while two story houses are required to have 8 foot side yard setbacks. Rear setbacks are more restrictive than the current code requirement of 25 feet, except on side street lots.

The maximum proposed single family density of 7.3 units per acre is higher than the average block density between Hillside, Grimes, Walnut and Henderson of 5.8 units per acre. Were this property to be developed based on current zoning requirements, no lot could be smaller than 7,200 square feet. There is a pattern of half lot splits on corners in this neighborhood.

While the minimum lot widths proposed are inconsistent with the lots in the immediately adjacent blocks, which range from 54 to 62 feet in width, those lots were not developed along alleys. Those lots require greater widths to accommodate driveways. The proposed 40 foot minimum lot width is consistent with other lots located in the neighborhood further to the west along Walnut, Washington, Lincoln and Grant. The blocks along these streets, between Grimes and Hillside, were platted with widths of 40 and 41.14 feet and were also developed with alleys.

**Overall Density:** Overall, the proposed maximum density is 10.9 units per acre (75 units/6.9 acres). This density is obtained if all corner lots are developed with two houses and no commercial uses are developed along Hillside. If none of the corner lots are developed with two houses and the mixed use building is developed in the “residential building type” with commercial on the first floor, the density associated with the petition drops to 7.5 units per acre (52 units/6.9 acres). While this development is denser than immediately surrounding blocks,

the density is comparable to other blocks in the neighborhood, especially those served by alleys. Surrounding densities range from 1.8 to 10 units per acre, with the densest blocks being along Washington Street.

**Commercial Square footage:** A maximum build-out of the mixed use buildings on Hillside would allow for a maximum of 15,650 square feet of first floor commercial uses and 21,150 square feet of second floor and basement office uses. The petitioners have provided a list comparing their project to other small retail uses in terms of square footage and street frontage. The GPP states that within NACs, the commercial uses should be “at a scale that serves the immediate neighborhood” without “attracting an influx of usage from surrounding areas” (Pg. 33). The GPP also states that in NACs, “commercial uses should be restricted to ensure their neighborhood focus” (Pg. 33).

The following chart was prepared by Staff and compares the proposed commercial space to other existing or proposed commercial spaces in the City.

	<b>Location</b>	<b>First Floor</b>	<b>Second Floor</b>
<b>Proposal</b>	Hillside and Henderson	15,650 square feet (SF)	21,150 SF of office (including basement of Lot C)
<b>Renwick PUD Village Center</b> (Not yet approved)	Moore's Pike and Sare Rd.	22,000 SF	10,000 SF of office
<b>Covenanter Hill PUD Prof. Center</b> (Not yet built)	Covenanter Dr. and Clarizz Blvd.	20,000 SF	12,000 SF
<b>McDoel Grocery Thomson Area PUD</b> (Not yet built)	Patterson and Rogers	5,500 SF	Residential
<b>The Shoppes</b>	800 block of College Mall Rd	37,500 SF	N/A
<b>Colstone Square</b>	3 <sup>rd</sup> and Dunn	12,000 SF	Residential
<b>Whitehall Park</b>	3400 block of W. 3 <sup>rd</sup> St.	20,000 SF per building	N/A

**Multi-family density:** In terms of density, the proposed apartments range from 14 units per acre to 32 units per acre, depending on the ultimate building type constructed and whether the first floors are developed with apartments. While this density is higher than the surrounding neighborhood, which varies from 1.8 to 10 units per acre, the density is mitigated by the architecture of the buildings, small unit size (studio and 2 bedroom units) and location adjacent to Hillside Dr. In addition, the petitioners propose that any residential use on the first floor only be studio apartments. These apartments will be single rooms with a bathroom and kitchenette to allow for the easiest possible conversion back to commercial space. The petitioners believe that there will be a financial incentive to utilize this space commercially because of the lower rents that can be expected with studio apartments.

Staff research determined that the proposed multi-family portion of this development is consistent with other small, neighborhood sized, multi-family developments in the area. Existing small-scale multi-family within the neighborhood ranges in density from 15 to 40 units per acre. While these densities are greater than any multi-family zoning district would allow, some of these developments are as small as 10 units.

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**SITE DESIGN:**

**Single Family Architecture:** The petitioners’ proposal includes six specific house types for lots in this development. Not all house types will be allowed on all lots. They have designed the house types to replicate housing types found in the neighborhood. Some features of the houses include a first floor elevation a few feet above the elevation of the street, specific paint schemes, first floor ceiling heights between 9 and 10 feet, large front porches, large double hung windows and a prohibition against vinyl siding.

Specific requirements for architecture are listed on Figure 11. Examples of features regulated include siding material, porch construction, roof pitch and type, porch roof pitch and type, porch width as a percentage of house width (70-85%), window type, roof and siding color and window arrangement.

The *GPP* encourages infill development in core neighborhoods to maintain and respect the historic context and architectural character of the existing homes. The Plan Commission found that the listed architectural elements, as well as the small front yard setback, limited height of homes and alley accessed garages, provide a sufficient commitment to houses that are of high quality, and are compatible with and complementary to existing houses in the area.

Housing Styles, Locations and Features			
		Lots	Features
Type A	1 Story Bungalow	Corner Lot Side Street Lot 40' Mid Block Lot 42' Mid Block Lot	Max. 3 bedrooms Max. 1400-finished sf

		Grimes Lane Lot Live/ Work Lot	
Type B	1 1/2 Story Bungalow	Corner Lot 40' Mid Block Lot 42' Mid Block Lot Grimes Lane Lot Live/Work Lot	Max. 4 bedrooms Max. 2000-finished sf Max. 1400 sf footprint
Type C	Foursquare	Corner Lot 42' Mid Block Lot Grimes Lane Lot Live/Work Lot	Max. 4 bedrooms Max. 2000-finished sf Max. 1160 sf footprint
Type D	1 1/2 Story Cottage	Side Street Lot	Max. 2 bedrooms Max. 1100-finished sf Max. 700 sf footprint
Type E	2 Story Cottage	Side Street Lot	Max. 3 bedrooms Max. 1100-finished sf Max. 550 sf footprint
Type F	Relocated BRI House	Side Street Lot	Two of the existing houses will be relocated and remodeled by Bloomington Restorations, Inc.

**Mixed Use Building Architecture:** The mixed use buildings at the south end of the development are proposed at two stories. The building on Lot C will also have a partially exposed basement because of grade changes. The architecture of these buildings will be either a residential or storefront design. The petitioners committed that Lot C, the lot at the corner of Hillside Dr. and Henderson St., must have the storefront design. The “residential type” was designed based on a small-scale apartment building. This building type would still allow commercial uses on the first floor. The “storefront type” would be as the name implies: a series of storefronts, broken into bays, which give the impression of several buildings, instead on one larger building.

**Access and Connectivity:** Access to individual lots and off-street parking for the mixed use buildings would be from newly constructed alleys on both sides of the development. This portion of the Bryan Park neighborhood was not platted with alleys. Both of the proposed alleys would be 12 feet in width, within 15 foot wide right-of-ways, and extend from Grimes to Hillside. The Public Works and Engineering Departments are willing to accept these alleys as public rights-of-way and are willing to review the 12 foot width requirement if the alleys must be narrower in spots to avoid natural features on the site.



With this development, Dunn St. would be constructed through the property from Grimes Ln. to Hillside Dr. Wilson St. and Driscoll St. would also be extended to connect to Palmer St. Off-site sections of these roads would be constructed with sidewalk on one side of the street.

The Plan Commission found that extending Wilson St. to Henderson St. was not necessary to serve connectivity. At the request of the Plan Commission, staff researched planning literature and other communities' ordinances concerning acceptable levels of connectivity for developments. This research determined that communities typically use prohibitions against cul-de-sacs or maximum block length requirements to ensure connectivity. Some communities use a "Connectivity Index" to measure the level of connectivity in a development. While all of these communities use slightly different methods to determine the index and have different levels of connectivity they find acceptable, all calculate the index by dividing the number of links (road segments) by the number of nodes (intersections or cul-de-sacs). Using these methods, the proposed PUD, both with or without the extension of Wilson St. to Henderson St., would exceed all of these communities' standards.

Community	Minimum Acceptable	S. Dunn with Wilson	S. Dunn without Wilson
Cary, NC	1.2	1.4	1.25
Orlando, FL	1.4	2.38	2.29
Hillsborough County, FL	2.0	3.5	3.0
San Antonio, TX	1.2	3.5	3.0
Concord, NC	1.4	3.5	3.0



**Figures 3.3A, 3.3B, and 3.3C.** (Left) Cary rules: Include nodes with arterials but no external links. (Center) Orlando rules: Include nodes with arterials and one link beyond the last node. (Right) San Antonio, Concord, and Hillsborough rules: Do not include nodes on arterials.

Handy, Susan; Robert G. Paterson and Kent Butler. (2003). *Planning for Street Connectivity: Getting from Here to There*. Planning Advisory Service Report No. 515. Chicago, IL: American Planning Association.

The Plan Commission found that while the proposed intersection of Wilson St. and Henderson St. met minimum safety requirements for line of site, it would

create traffic hazards for both vehicles and pedestrian. The Plan Commission was concerned about the dog-leg situation created with Brenda Ln., the potential for cut-through traffic trying to avoid the light at Henderson St. and Hillside Dr., pedestrian safety in crossing mid-block to Templeton Elementary School and the impacts of this road on the immediately adjacent property owners.

To address the issue of pedestrian safety, the petitioners committed to constructing a sidewalk on the south side of Grimes Ln. between the development and the intersection with Henderson St. This sidewalk will facilitate an additional safe point for pedestrians to cross Henderson St.

**Street Standards:** The petitioners propose that all interior streets provide 7 ½ foot travel lanes with additional pavement width for parking on one side of the street. These narrowed travel lanes create a queuing arrangement where vehicles must stop and wait for a car traveling in the opposite direction to pass before proceeding. The proposed intersections are also slightly narrowed using “bump-outs” to protect the street parking. The intersections are proposed to include curbs with narrowed, but “mountable,” turning radii. The petitioners provided examples of existing streets in the City that have a queuing arrangement. These examples are in older, “Core Residential” areas. It should be noted that queuing streets are identified in the AASHTO “Green Book” manual as being common in single family areas, where a low number of short distance trips can be expected.

The Plan Commission approved these reduced street cross sections after consultation with the Bloomington Fire Department. The Fire Department’s ideal solution was for the petitioners to agree to install residential sprinkler systems in every house. With the petitioners’ commitment for residential sprinklers, the Fire Department believes the 7 ½ foot travel lanes in a queuing street arrangement would not hinder fire protection.

**Right-of-way:** Fifty feet of right-of-way was required on Dunn Street. The Plan Commission allowed the petitioners to dedicated only 42 feet of right-of-way for Driscoll St. and Wilson St. This is appropriate because the existing sections of these streets only have 37 feet of right-of-way.

The Plan Commission approved a right-of-way dedication reduction on Grimes. Instead of the required 32.5 feet from centerline, the Plan Commission allowed only 29 feet to be dedicated. This provides for the necessary half right-of-way needed to allow for 11 foot wide travel lanes, a parking lane, tree plot and sidewalk.

**Purchase of Hillside Right-of-way:** The petitioners have requested that the Bloomington Redevelopment Commission consider giving up some of the land it owns along Hillside Dr. This land was purchased by the City in order to facilitate improvements to the intersection of Hillside Dr. and Henderson St. However, the

Redevelopment Commission owns more land than required for the long term needs of Hillside Dr., based on the Thoroughfare Plan. The petitioners intend to make an offer to purchase part of this land if the rezoning request is approved by the Common Council. Staff notes that the schematic location of the right-of-way purchase only includes those areas outside of the 40 foot from centerline Thoroughfare Plan requirement that are not needed for public improvements, such as parking, street trees and sidewalk.

**Single Family Parking:** The Zoning Ordinance requires that all single family lots provide a minimum of two off-street parking spaces. This can take place in an attached or detached garage or on a parking pad or driveway. The petitioners propose that no lot be required to provide off-street spaces. In addition, under some scenarios the corner lots are prohibited from providing parking. Only detached garages or an 18' by 18' parking pad would be permitted. All off-street parking access will be from the proposed alleys, which is consistent with the GPP recommendation for core residential neighborhoods (Pg. 30).

**Mixed Use Parking:** The following chart summarizes the parking approved by the Plan Commission with this petition versus *Zoning Ordinance* requirements. This parking requirement was calculated based on these lots being developed with the "storefront building type" and the buildings being used exclusively for commercial uses. Other build out scenarios would require less parking. The Plan Commission believed that this reduction was appropriate due to both the presence of new on-street parking spaces as well as the neighborhood-focused, mixed use nature of the proposed land uses. Staff notes that some on-street parking must be eliminated in order to meet minimum standards for distance to intersections and cross walks and to keep parked cars out of the "sight triangle."

		<b>Required</b>	<b>Proposed</b>
<b>Single Family Parking</b>	Off-street	86 spaces (2 per lot)	0 spaces required
	On-street	0 spaces	64 max spaces (1.5 per lot)
<b>Maximum Mixed Use Parking</b>	Off-street	143 spaces	26 spaces
	On-street	0 spaces	34 spaces
<b>Total Mixed Use</b>		143 spaces	60 spaces

**Parking on Hillside and Alternatives:** The Plan Commission approved a plan that included on-street, pull-in parking spaces along Hillside Dr. The petitioners stated that on-street parking was necessary if the proposed commercial uses on Hillside Dr. were to be viable. The petitioners note that this parking would slow traffic in a school zone and provide more spaces than a parallel parking scenario. They also note that other examples of pull-in, angled parking along arterial roadways can be found in the City on S. Rogers Street, W. 17<sup>th</sup> Street, E. 3<sup>rd</sup> Street, E. 10<sup>th</sup> Street, and S. Henderson Street.

The AASHTO “Green Book” manual notes that angled parking is allowable under certain circumstances. The manual states that any on-street parking “decreases through traffic capacity, impedes traffic flow, and increase crash potential.” It does however state that in urban areas the “existing and developing land uses may necessitate the consideration of on-street parking.” It also notes that angled parking presents problems because of the varying length of vehicles the need for good sight distance (pg. 377). The petitioners’ plan includes parking stalls that are 25 feet deep instead of the 20 feet and 3 inches required by the Zoning Ordinance. These deeper parking stalls would increase visibility of exiting vehicles, by providing drivers with increased maneuvering space and sight distance, and provide for a higher level of safety.

Hillside is classified as a secondary arterial roadway and has traffic counts of approximately 8,200 average daily trips. Hillside is currently functioning at Level of Service (LOS) “C” along most of its length. While the posted speed limit on this street is 30 mph, the average speed is 32/34 mph and the 85th percentile speed is 38/39 mph. A disadvantage of pull-in on-street parking is that it would introduce an additional point of vehicular conflict on a street meant to serve a high degree of vehicular mobility.

The Plan Commission asked staff to put together schematic illustrations of alternatives to angled parking on Hillside Dr. For the benefit of the Council, three alternatives are presented in the packet. These alternatives include: 1) parallel parking on Hillside Dr. with an 8- foot “pull-off area,” 2) no on street parking on Hillside Drive, but increased parking behind the buildings, and 3) angled spaces along Hillside Drive with a curbed island separating this parking from the travel

lane.

The first alternative, parallel parking on Hillside Dr., is supported by the Master Thoroughfare Plan and is more acceptable to City emergency service providers than pull-in spaces. This alternative would mean a reduction of about half of the on-street parking on Hillside Dr. Some of this parking could be regained behind the mixed use buildings but would necessitate the loss of one single family lot.

The second alternative, no parking on Hillside Dr., would likely mean that commercial uses would not be viable at this location. There would simply be no parking presence on the street to pull-in customers from Hillside. Also, there would be no traffic calming benefit associated with pull-in parking off Hillside Dr. Some lost parking could be regained behind the mixed use buildings, but this would necessitate the loss of one single family lot.

Finally, the Plan Commission suggested an alternative that would involve pull-in parking between the street and the proposed buildings, but be separated from the lane of traffic with some sort of physical barrier. Some of the disadvantages of this plan include the removal of valuable on-street parking spaces, relocation of the proposed mixed use buildings away from the street edge, and possible elimination of the proposed mixed use building at Henderson and Hillside.

The GPP has several statements related to parking in NACs and along arterial roadways. The GPP states that parking in NACs should not be “used as an attractor for commercial users” and that “parking should be located in the side or rear of buildings” in order to “minimize street cuts in front of buildings” (Pg. 34). The Master Thoroughfare Plan notes that if on-street parking is provided on secondary arterials it should be in the form of an 8 foot parking lane, but it does not list on-street parking as a priority for the right-of-way or as a recommended “traffic management feature” (Pg. 88). Finally, the Thoroughfare Plan notes that arterial streets serve a “higher degree of mobility” and “longer trips within the city” than do collector streets. Arterial streets serve a “larger city-wide function” and “connect major destinations” (Pg. 81).

The Plan Commission approved the on-street parking plan because they believed that the safety impacts of the parking were mitigated by such factors as the proposal for longer parking stall lengths, location of the project near a signalized intersection and the location of the project in a restricted speed/school zone. They also believed that on-street parking was necessary for the commercial uses to be successful.

**Private Park:** The petitioners have proposed an approximately 3,500 square foot private “pocket park” at the southeast corner of Dunn and Wilson. This park will be owned and maintained by either a homeowner’s association or some other non-profit group.

**Environmental Issues:** The property includes several large trees, mainly silver maples. While these trees are not of high quality, the petitioners intend to preserve them where possible. On some lots, the sidewalk may need to meander from the road to preserve these existing trees.

**Utilities:** This site has adequate utility service for both water and sanitary sewer. Schematic plans have been submitted to CBU and have received conceptual approval.

**Stormwater:** A schematic drainage plan has been submitted to CBU and has received conceptual approval. This plan includes off-site improvements and piping of stormwater to the north to discharge into the creek along Davis Street and to the south to discharge closer to Clear Creek.

**Transit:** The north side of the property along Grimes and the far southeast corner of the property along Henderson are on Bloomington Transit's route #2. The GPP encourages the City to place higher density residential development within walking distance to transit routes.

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**Recommendation:** The Plan Commission voted 7-3 to send this petition to the Common Council with a favorable recommendation with the following conditions:

1. The GPP is hereby amended to designate the southern one (1) acre, which includes the three mixed use buildings, as a "Neighborhood Activity Center."
2. All of the proposed single family homes located within the PUD are required to be equipped with residential sprinklers. These homes must all be inspected by the City Fire Department prior to occupancy. Fire hydrants shall be located on the side the street with no parking. As part of this condition, travel lanes along all interior public streets may be reduced to seven (7.5) feet in width.
3. Drive-through uses and gasoline sales are specifically excluded from the list of permitted uses.
4. In association with the public improvements constructed by the petitioners, Grimes Ln. must maintain eleven (11) foot wide travel lanes and Hillside Dr. and Henderson St. must maintain twelve (12) foot wide travel lanes per the Thoroughfare Plan.
5. All public improvements, including on-street parking, street trees and a minimum of five (5) foot wide sidewalks must be located within the road right-of-way.
6. A twenty (20) foot radius of the right-of-way lines at the intersection of Hillside and Henderson is required.
7. Dunn Street must be constructed through to Hillside in conjunction with final plans for the single family lots, not the mixed use buildings. Fifty (50) feet of right-of-way must be dedicated for Dunn St.
8. Parallel parking adjacent to commercial buildings along Dunn Street must be eight (8) feet wide.

9. The proposed “park” must be privately owned and maintained. Ownership and maintenance arrangements shall be reviewed by the Plan Commission at Final Plan stage.
10. Occupancy of all lots and multi-family units are subject to the single family definition of “family”, which includes not more than three (3) unrelated adults.
11. Final approval of drainage and utility plans by CBU is required in conjunction with final plan approval.
12. Land purchase from the Bloomington Redevelopment Commission must be finalized prior to final plan filing.
13. All on-street parking spaces must be clear of the regulated sight triangle, not be within twenty (20) feet of a crosswalk and not be within thirty (30) feet of the approach to a stop sign.
14. The proposed north south alleys adjacent to the mixed use buildings shall be designed to accommodate truck traffic and turning movements to handle deliveries.
15. A sidewalk is required on the south side of Grimes Ln. from Dunn St. to Henderson St. Design of this sidewalk link shall be determined at final plan stage.
16. The petitioners will place deed restrictions, enforceable by the neighborhood (i.e., future owners of property in the parcel) or the City, on the potential commercial lots along Hillside Drive, restricting the size of regular delivery vehicles and the hours of regular deliveries in a manner reasonably designed to avoid noise problems in the neighborhood and traffic problems on Hillside Drive, with the specific restrictions to be determined in consultation with staff. Also, the operating hours of the commercial establishments will be reasonably restricted, with the specific restrictions to be determined in consultation with staff. *(Staff note: These deed restrictions will be reviewed by the Plan Commission during the Final Plan stage)*
17. The extension of Wilson Street will not be constructed east of the alley between Dunn and Henderson. *(Staff note: A reworked site plan, removing this road from the plan, will be reviewed by the Plan Commission during the Final Plan stage)*

### **Selected quotes from the 2002 GROWTH POLICIES PLAN:**

An analysis of the Growth Policies Plan is included throughout the staff report. Following are quotes from the GPP that staff believes directly relate to the proposal.

- **Core Residential:** Core Residential areas are characterized by a grid-like street system, alley access to garages, small street setbacks, and a mixture of owner occupants and rental units. The unique character, urban form and land use pattern of the near-downtown residential areas must be protected and enhanced. (pg. 30)
- **Core Residential:** [The Core Residential] district is designed primarily for higher density single family residential use. The existing single family housing stock and development pattern must be maintained... (pg. 30)
- **Core Residential:** Multi-family (medium and high-density) residential and neighborhood serving commercial uses may be appropriate for this district when compatibly designed and properly located to respect and compliment single family dwellings. (pg. 30)
- **Core Residential:** Neighborhood-serving commercial uses, and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations. (pg. 30)
- **Core Residential:** [The City should] allow multi-family redevelopment along designated major streets...and when appropriately integrated with adjacent uses per adopted form district requirements. (pg. 30)
- **Core Residential:** [The City should] utilize targeted tax abatements and grant programs in specific neighborhoods to provide incentives for increased owner occupancy and affordable housing construction. (pg. 30)
- **Core Residential:** Core Residential development should emphasize building and site compatibility with existing densities, intensities, building types, landscaping and other site planning features. (pg. 30)
- **Core Residential:** Residential parking should be encouraged to utilize garages accessed by alleys to the rear of properties, while front yard parking shall be prohibited, (Pg. 30)
- **Core Residential:** [The City should] explore opportunities to introduce nodes of appropriately designed, neighborhood scaled commercial uses within the core neighborhoods. (pg. 30)
- **Core Residential:** [The City should] promote neighborhood enhancements of public improvements such as sidewalks, streetlights, street trees and landscaping, and playgrounds and play areas. (pg. 30)
- **Neighborhood Activity Center (NAC):** The NAC must be designed so that it serves the neighborhood adequately without attracting an influx of usage from surrounding areas. (Pg. 33)
- **NAC:** The main focus of the NAC should be commercial uses at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes. (Pg. 33)
- **NAC:** Office uses and public/semi-public uses are acceptable when built to generate minimal traffic attraction to the neighborhood. (Pg. 33)



- **NAC:** Residential uses should be limited to multifamily development, ideally on floors above street level commercial uses. (Pg. 33)
- **NAC:** Commercial uses should be restricted to ensure their neighborhood focus. (Pg. 33)
- **NAC:** Public Transit as an urban service must be a key element in the location of the NAC, providing access to people outside the neighborhood without the need for personal vehicles. All newly developed NAC's must be located within walking distance (5-10 minutes) of a major public transit stop. (Pg. 33)
- **NAC:** The roadways that a NAC is developed around should be Collectors (Secondary or Primary) as designated on the City's Master Thoroughfare Plan. (Pg. 33)
- **NAC:** The development of an NAC should include coordination on the completion of an adequate sidewalk network throughout the immediate neighborhood it serves, if no such network exists at the time of development. (Pg. 33)
- **NAC:** Compatibility with surrounding established neighborhoods is one of the most important factors in the development of a Neighborhood Activity Center. (Pg. 33)
- **NAC:** The introduction of a commercial node into a primarily residential area requires great sensitivity to the design and scale of the existing structures, as well as responsiveness to the needs of the surrounding residents. NACs must relate to surrounding residential neighborhoods and not adversely affect the livability of these neighborhoods through traffic, lighting, noise, litter or other impacts. (Pg. 33)
- **NAC:** In order to define the center, buildings should be pushed to the front edge of the site, framing the four corners of the commercial node at the street intersection. (Pg. 34)
- **NAC:** Any parking that is provided for a NAC should be primarily serving any residential units that are a part of the development rather than used as an attractor for commercial users. (Pg. 34)
- **NAC:** Parking should be located in the side or rear of buildings and can be made accessible from an improved alley system in order to minimize street cuts in front of buildings. (Pg. 34)
- **NAC:** All parking areas should also be heavily landscaped in order to soften their impact on the neighborhood. (Pg. 34)
- **Master Thoroughfare Plan (MTP): Street Classifications/Arterials:** The arterial street is designed to provide a higher degree of mobility than the collector streets and most often serve longer trips within the city. The arterial street network serves a larger city-wide function facilitating employment generated trips and connecting major destinations such as the downtown district, industrial areas, suburban commercial centers, residential areas, and other key activity centers. (Pg. 81)
- **MTP: Purpose/Preservation of Capacity:** Preserve the capacity of major transportation facilities. The plan establishes a rationale for access

management based on existing and anticipated development along the major transportation corridors. (Pg. 80)

- **MTP: Purpose/Preservation of neighborhood character:** Preserve the character of the existing neighborhoods...traditional neighborhood street character is an important quality that the City must preserve. (pg. 80)
- **MTP: Typical Characteristics of a Secondary Arterial:** 12 foot travel lanes. 8 foot parking lanes may be added to higher density developments with limited on-site parking. (Pg. 81)
- **MTP: Secondary Arterials: Priority for Right-of-way:** Parking is not listed as a primary or secondary priority element for secondary arterials. (Pg. 88)
- **MTP: Typical Characteristics of a Neighborhood Street:** 10 foot travel lanes. 8 foot parking lanes may be added to higher density developments with limited on-site parking.
- **MTP: Neighborhood Streets: Priority for Right-of-way:** A primary priority is residential access while a secondary is on-street parking,
- **MTP: Neighborhood Streets: Traffic Management Features:** On-street parking and narrower travel lanes are both listed as traffic management features for neighborhood streets.
- **MTP: Access Control Guidelines:** "...[D]riveway spacing [on secondary arterials] may be closer than 600 feet, provided no driveway exit is within 200 feet of the approach to an existing signalized intersection." (Pg. 94)
- **Compact Urban Form:** [Compact urban form] does not imply the intrusion of higher density development into established neighborhoods, crowding, or high rise development of a scale more appropriate to larger cities. (pg. 5)
- **Compact Urban Form:** Compact form is not to be achieved at the expense of greenspace, environmental protection, and other policies. (pg. 5)
- **Compact Urban Form:** ...denser infill development in areas that already contain city services must be encouraged. (pg. 6)
- **Mitigate Traffic:** [The City should] require the siting of future high density multi-family and commercial projects within walking distance to transit routes. (pg. 14, MT-2)
- **Mitigate Traffic:** [The City should] ensure the provision and linkage of street stubs to improved connectivity within all sectors of the community. (pg. 16, MT-13)
- **Conserve Community Character:** New development that alters the architectural character of [Bloomington's] neighborhoods should be avoided. (pg. 17)
- **Conserve Community Character:** ...it is essential to maintain the historic context and architectural character of the older core neighborhoods. (pg. 17)
- **Conserve Community Character:** Neighborhood character can evolve in a gradual and compatible way to allow additional density through subdividing lots and the creation of granny flats and duplexes (pg. 17)

# MEMORANDUM

**TO:** City of Bloomington Plan Commission  
**FROM:** Environmental Commission  
**LIAISON:** Linda Thompson, Senior Environmental Planner  
**DATE:** May 5, 2004  
**SUBJECT:** PUD-09-04 South Dunn Street

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This memorandum contains environmental information and recommendations regarding the planned unit development containing 43 single-family lots and three mixed use buildings. The Bloomington Environmental Commission (EC) has reviewed the petition and has the following comments and recommendations that should be included into the plan prior to approval of the development. The recommendations that are italicized behind the symbol "►" are the EC's highest priorities.

## **Site Description:**

The site is urban, containing 6.9 acres near Bryan Park. There are currently scattered trees.

## **Environmental Concerns:**

Recommendations:

### **Forest & Tree Preservation:**

1. ► *During construction, use protective fencing around the entire drip line of the existing trees.*

### **Landscaping:**

2. ► *Currently all proposed street trees are sunset maple. The EC recommends that the street tree planting plan be diversified by using a mixture of at least three types of large canopy native trees.*
3. Plant redbud in replacement of crabapple trees.

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## Bicycle and Pedestrian Safety Commission

### Review and Comments: South Dunn Street Development

Mitch Rice, Chairperson

At our May 3, 2004 Work Session, Matt Press and Marc Cornett presented plans for the South Dunn Street Project.

**General Commission Comment: This is a very good project. See Concerns.**

Specific Comments:

“This is the best project I’ve seen in my more than ten years on the Commission.”

“This project makes me want to live there!”

The project embodies the key characteristics of a pedestrian oriented neighborhood infill project. It is relatively dense (compact form), it appears compatible with the surrounding neighborhood, it includes significant mixed use, and it fully integrates pedestrians into the overall project design. Sidewalks are wide with tree plots. On-street parking helps to separate and protect sidewalks. Presentation drawings show that buildings are also oriented to the pedestrian. Porches are placed close to the sidewalk and driveways are kept to the rear. This overall design format reflects a traditional style of development, not for its nostalgic attractions, but because it works to encouraging walking.

The inclusion of the mixed-use, commercial style buildings along Hillside is especially positive and essential to the project. A pedestrian oriented neighborhood must include a wide range of typical ‘destinations’ within a short walking distance of residents. The buildings along Hillside will provide jobs, services, apartments, and retail easily accessible on foot.

It is essential that new projects in Bloomington begin to reduce the need for excessive dependence on driving. (About ten trips per house are now estimated for most typical projects.) With its proximity to Templeton Elementary School, this project shows that it is very possible to build attractive developments which reduce the need to drive.

### **Concerns:**

#### **Street Width**

Street width within the current proposal is excessive and will result in unnecessary speeding. Our Commission’s experience with traffic calming gives us some background on this issue. If built as currently designed we would expect to see applications for traffic calming in a few years. The City Engineering Department recommends against using stop signs to slow traffic. Discussions over the years with Bloomington police points to the fact that enforcement is no answer to speeding. It is sporadic at best. There are simply too many miles of streets.

The unavoidable fact is that the design of the street itself is what determines speeds, and street width is probably the most important aspect. We have too many examples of

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speeding on streets of excessive width. Wider is not better. The traffic volume on the internal streets of this project will be low enough that meeting another vehicle will be the exception rather than the rule. The net effect experienced by most drivers will be a very wide open street.

The internal streets should be designed as “queing streets” in which street width allows one vehicle through at a time. Intermittent on-street parking provides a temporary pull over for oncoming vehicles. This ‘natural’ traffic calming is a proven technique on many surrounding streets. For example, University St, in the blocks west of Woodlawn, is less than twenty feet, curb to curb. It has two way traffic and parking on one side. Second Street, a street which carries far more traffic than any of the streets within this development, is only about twenty four feet wide with parking on one side. On-street parking needs should be calculated so that pull-over spaces will likely exist between parked cars. In the future, if traffic counts require it, some parking could be removed.

Street design should include consideration of emergency vehicles, but mandates from the Fire Department should not be allowed to create a street which encourages speeding. Empirical data from existing streets should be utilized. For example, are the many hundreds (probably thousands) of Bloomington residents living on existing streets which do not meet current Fire Department ‘standards’ at risk from fire? If they are at risk, should they be notified? The appropriate selection and use of fire fighting equipment is essential to not only serving existing neighborhoods, but must also take into account the effects to the design and overall design public safety of new developments.

Fire safety design should include encouragement for developers to provide multiple access routes to the development. This project provides many routes into the development. With alleys and connecting streets, fires can be approached from the front and back and from two directions.

### **Speeding on Hillside**

Speeding is very excessive on Hillside. A street with the volume of Hillside with a 85<sup>th</sup> percentile traffic speed of 38 mph is frightening, especially in a school zone. While 38 mph may not sound that significant, our Commission’s experience with traffic calming recommends a closer look at the speed study. (Not presented) It is very likely that a significant number of vehicles are traveling well over 40 mph.

Note: About 50% of pedestrians die when struck by a vehicle traveling at 30 mph.

The current speeding problem should not be seen as a reason to prohibit parking on Hillside. The proposal should become an opportunity to help mitigate an existing, dangerous situation. Our Commission recommends that the Engineering Department study possible traffic calming infrastructure changes to Hillside ‘upstream’ from the project. Possibilities include curb build-outs, lane narrowings, raised crosswalks, no right turns on red, etc.



June 8, 2004

To the members of the Planning Commission:

I have reviewed the plans for the PUD on South Dunn Street in the Bryan Park neighborhood. I have also discussed a number of options regarding street width, connectivity, and residential sprinklers. The Indiana Fire Code section 503.1.1 requires approved fire apparatus access roads that can reach within 150 feet of portion of the exterior wall of any building. The section 503.1.1 also allows for an exception to this requirement if buildings are protected throughout by a supervised automatic fire sprinkler system and not used for high piled combustible storage in excess of 12,000 square feet.

The access road specifications are outlined in a subsequent section, specifically, Indiana Fire Code (IFC) section 503.2 and include dimension requirements of not less than 20 feet of unobstructed width. Based upon the fact that the exception to IFC 503.1.1 does not require fire department access roads if buildings are protected throughout by a supervised automatic fire sprinkler system and not used for high piled combustible storage in excess of 12,000 square feet, the Bloomington Fire Department can support a 15 feet wide unobstructed street width if and only if the entire development proposal includes installation of supervised, fully automatic, fire sprinkler systems throughout each building, as required by the 503.1.1 exception.

Based on the developer's commitment to include residential sprinklers in all buildings in the development, the fire code has been met by the exception to section 503.1.1.

Residential sprinklers work. For complete information including technical drawings go to the U.S. Fire Administration's web site at: <http://www.usfa.fema.gov/public/hfs/sprinklers.shtm> . Some of the information from that site is presented here:

Fires in residences have taken a high toll of life and property. In 2002 there were:

- 401,000 residential fires
- 2,695 civilian fire deaths
- 14,050 civilian fire injuries
- Over \$6 billion in property damage

*Data Source: Fire Loss in the U.S. During 2002, NFPA. August 2003.*

Studies by the Federal Emergency Management Agency's United States Fire Administration indicate that the installation of residential fire sprinkler systems could have saved thousands of

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lives; prevented a large portion of those injuries; and eliminated hundreds of millions of dollars in property losses.

The fire loss in this country in residential occupancies is alarming. Manual firefighting methods are not the answer. The way to attack the problem is to limit the fire growth where it occurs in dwellings. We have the technology to do that. Residential Automatic Sprinkler Systems. Ordinance No. 745; Adopted May 28, 1969; by the San Clemente, California City Council. Proposition 13 was a major factor in promoting the ordinance. There is also a shift within the fire service toward more fire prevention and less suppression emphasis. San Clemente and Corte Madera, California were some of the first communities in the United States to enact a home sprinkler ordinance. Other communities that have initiated or plan to initiate residential sprinkler ordinances include:

- Livermore, California
- Montgomery County, MD
- Long Grove, Illinois
- Chapel Hill, North Carolina
- Germantown, Tennessee
- Cobb County, Georgia
- Scottsdale, Arizona

Altamonte Springs, Florida

Sprinklers are a good investment for the homebuyer.

- A fire occurs in a residential structure every 79 seconds, according to the U.S. Fire Administration. To the homebuilder, this fact means that a large share of potential customers now have knowledge of the terror and destruction caused by fire.
- Families with children, senior citizens, and handicapped members have special fire protection needs. Home sprinkler systems provide added protection for these people.
- In case of a home fire, firefighters will have less risk of injury or life loss since they will be fighting a fire of less intensity.
- Allocation of community resources can be improved with the adoption of home sprinkler technology.
- Communities will be able to make better utilization of available land and thereby increase their tax base.

The Bloomington Fire Department is supportive of the residential sprinkler requirement throughout this development and with such a requirement, the Fire Department can support the smaller street widths of 15 feet or greater.

Yours in Fire Protection,

Chief Jeff Barlow

**BLOOMINGTON PLAN COMMISSION  
FINAL REPORT  
LOCATION: 1330 S. Dunn Street**

**CASE NO: PUD-09-04  
DATE: June 14, 2004**

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**PETITIONER:** Neighborhood Solutions, LLC (Matt Press)  
601 W. Dodds Street, Bloomington

**COUNSEL:** Kirkwood Design Studio  
108 ½ E. Kirkwood Ave., #3, Bloomington

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**REQUEST:** The petitioners are requesting a rezoning of 6.9 gross acres from RS4.5 to Planned Unit Development (PUD) and preliminary plan approval for 43 single family lots and three mixed use lots. Also requested is a *Growth Policies Plan Amendment* to change the classification of part of the property from “Core Residential” to “Neighborhood Activity Center”.

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**BACKGROUND:**

**Area:** 6.9 gross acres  
**Current Zoning:** RS4.5  
**GPP Designation:** Core Residential  
**Existing Land Use:** Mostly vacant, rolling meadow terrain, scattered trees, 5 existing houses  
**Proposed Land Use:** Mixed residential and commercial  
**Proposed Density:**  
    **Total:** up to 10.9 u/a (75 units on 6.9 acres)  
    **Single Family:** up to 7.3 u/a (43 lots on 5.9 acres).  
    **Multi-family:** up to 32 u/a (32 units on 1 acre)  
**Proposed Commercial:** 15,650 square feet retail + 21,150 SF of office  
**Surrounding Uses:** **North, West** – Single family residential  
**East** – Single family residential and Templeton Elementary School  
**South** – Mixed single and multi-family residential

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**REPORT SUMMARY:** The property in question is approximately 6.9 acres in the Bryan Park Neighborhood. It is bounded by E. Grimes Lane to the north, E. Hillside Drive to the south, existing homes on S. Palmer Street to the west and existing homes and S. Henderson Street to the east. Surrounding lots were created in the late 1920s. Since that time, a portion of E. Driscoll Street was constructed into the property and extended up the Dunn St. right-of-way to Grimes Ln. Also, six small houses were constructed and used as rentals. These houses are in poor condition and not historic in nature. One house was recently moved to the northeast corner of Henderson and E. Miller Drive.

Under the current RS4.5 zoning, the property could conceptually be developed with 26 lots. Instead, the petitioners, Neighborhood Solutions LLC, are requesting



that the zoning be changed to Planned Unit Development (PUD) and a preliminary plan be approved to allow for 43 single family lots and 3 mixed use lots.

This project was last heard at the May 10, 2004 Plan Commission meeting. At this meeting the Commissioners requested additional information and research concerning the proposed Hillside Dr. parking and the extension of Wilson St. to Henderson St. There has been only one major change to the proposal since the May hearing. The petitioners now propose that all streets in the development be designed as “queuing streets.” This type of street includes very narrow travel lanes (7.5 feet) and does not allow two cars to pass each other. A driver must wait in line, or queue, until there is an opening to go.

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**GROWTH POLICIES PLAN:** The *Growth Policies Plan* designates this property as “Core Residential”, but the petitioners have requested that the *GPP* be amended to include the southern part of the property as a “Neighborhood Activity Center” (NAC). The *GPP* notes that “while several NACs have been identified on the land use map, more could be designated in the future as further study is done and appropriate locations have been identified” (Pg. 33).

In general, staff finds that this proposal meets many of the policies and recommendations of the *GPP*. This proposal furthers goals of compact urban form and neighborhood scaled commercial centers. The dominant land use element of this project is still owner occupied single family homes. This matches the general land use goal for “Core Residential” areas. The single family lots also meet multiple *GPP* recommendations such as providing garages accessed by alleys, utilizing a grid street pattern with multiple points of connectivity and providing compatible architecture and site design for new construction.

The mixed use component has been designed to satisfy the recommendations of the *GPP* for “Neighborhood Activity Centers.” These buildings are designed at a scale so that they serve the neighborhood without attracting an influx of usage from surrounding areas. The proposed uses are also compatible with the neighborhood. Residential uses are mostly located on the second floors of the buildings. The buildings are pushed to the front edge of the site, framing the four corners of the commercial area at the street intersection. Parking is provided both on street and behind buildings and is at a scale to ensure that parking is not a large attractor for commercial users.

This proposal promotes compact urban form in that it is a dense, infill project in an area that already contains city services. This proposal preserves community character by maintaining the architectural character of the older core neighborhoods. Improved connectivity is ensured through the linkage of street stubs adjacent to the property. Finally, the character of the neighborhood is ensured through continuation of narrowed streets into the development.

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**LAND USE & INTENSITY OF USES:**

**Commercial Uses:** There have been no major changes to the proposed list of permitted uses. Based on recommendations from the Plan Commission in May, staff recommends that all drive-through uses be excluded and gasoline sales be excluded from the list of permitted uses.

**Overall Density:** At the request of the Plan Commission, a density map is provided in the packet. This map illustrates the density in units per acre for the surrounding blocks. Overall, the proposed maximum density is 10.9 units per acre (75 units/6.9 acres). This density is obtained if all corner lots are developed with two houses and no commercial uses are developed along Hillside. While this development is denser than immediately surrounding blocks, the density is comparable to other blocks in the neighborhood, especially those served by alleys. Surrounding densities range from 1.8 to 10 units per acre, with the most dense blocks being along Washington Street.

**Commercial Square footage:** A maximum build-out of the mixed use buildings on Hillside would allow for a maximum of 15,650 square feet of first floor commercial uses and 21,150 square feet of second floor and basement office uses. The petitioners have provided a list comparing their project to other small retail uses in terms of square footage and street frontage. The following chart was prepared by Staff and compares the proposed commercial space to other existing or proposed commercial spaces in the City.

	<b>Location</b>	<b>First Floor</b>	<b>Second Floor</b>
<b>Proposal</b>	Hillside and Henderson	15,650 square feet (SF)	21,150 SF of office (including basement of Lot C)
<b>Renwick PUD Village Center</b> (Not yet approved)	Moore's Pike and Sare	22,000 SF	10,000 SF of office
<b>Covenanter Hill PUD Prof. Center</b> (Not yet built)	Covenanter and Clarizz	20,000 SF	12,000 SF
<b>McDoel Grocery Thomson Area PUD</b> (Not yet built)	Patterson and Rogers	5,500 SF	Residential
<b>The Shoppes</b>	800 block of College Mall Rd	37,500 SF	N/A
<b>Colstone Square</b>	3 <sup>rd</sup> and Dunn	12,000 SF	Residential
<b>Whitehall Park</b>	3400 block of W. 3 <sup>rd</sup> St.	20,000 SF per building	N/A

**Multi-family density:** In terms of density, the proposed apartments range from 14 units per acre to 32 units per acre, depending on the ultimate building type constructed and whether the first floors are developed with apartments. While

this density is higher than the surrounding neighborhood, which varies from 1.8 to 10 units per acre, the density is mitigated by the architecture of the buildings, small unit size (studio and 2 bedroom units) and location adjacent to Hillside. In addition, the petitioners have proposed that any residential use on the first floor only be studio apartments. They have described these apartments as single rooms with a bathroom and kitchenette. This will allow for both the easiest possible conversion back to commercial space and the greatest flexibility of uses for the building owner. The petitioners believe that there will be an incentive to utilize this space commercially because of the lower rents that can be expected with studio apartments as apposed to commercial space.

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**SITE DESIGN:**

**Access and Connectivity:** Since the last hearing, the petitioners have committed to 12 foot wide, public alleys. The City Engineering Department has also stated that at the Final Plan stage they are willing to review this width requirement if the alley must be more narrow in spots to avoid natural features on the site.

In order to assess the need for Wilson St. to be extended to Henderson St., the Plan Commission asked the staff to analyze general principles in planning literature and other communities’ ordinances concerning acceptable levels of connectivity for developments. This research has determined that communities typically use prohibitions against cul-de-sacs or requirements for maximum block lengths to ensure connectivity. Some communities do use a “Connectivity Index” to measure the level of connectivity in a development. While all of these communities use slightly different methods to determine the index and have different levels of connectivity they find acceptable, all calculate the index by dividing the number of links (road segments) by the number of nodes (intersections or cul-de-sacs). Using these methods, the proposed PUD, both with and without the extension of Wilson St. to Henderson St., would exceed all of these communities’ standards.

Community	Minimum Acceptable	S. Dunn with Wilson	S. Dunn without Wilson
Cary, NC	1.2	1.4	1.25
Orlando, FL	1.4	2.38	2.29
Hillsborough County, FL	2.0	3.5	3.0
San Antonio, TX	1.2	3.5	3.0
Concord, NC	1.4	3.5	3.0



**Figures 3.3A, 3.3B, and 3.3C.** (Left) Cary rules: Include nodes with arterials but no external links. (Center) Orlando rules: Include nodes with arterials and one link beyond the last node. (Right) San Antonio, Concord, and Hillsborough rules: Do not include nodes on arterials.

Handy, Susan; Robert G. Paterson and Kent Butler. (2003). *Planning for Street Connectivity: Getting from Here to There*. Planning Advisory Service Report No. 515. Chicago, IL: American Planning Association.

The proposed intersection of Wilson St. and Henderson St. has been reviewed by the City Engineering Department and was found to meet minimum safety requirements. The AASHTO Manual requires a minimum of 200 feet of sight distance for a road with a 30 MPH speed limit. The proposed intersection has at least 215 feet of sight distance, thus satisfying the minimum requirements.

The petitioners believe that extending Wilson St. to Henderson St. is necessary because it is an “integral part of the platted, grid network” of streets. Finally, increased connectivity and additional points of access are supported by the Bloomington Fire Department as well as other emergency service providers.

Apart from vehicular safety, staff was asked to look at pedestrian safety at this proposed intersection and at the intersection of Henderson St. and Brenda Ln. With an increased number of homes west of the school and new points of connectivity, increased pedestrian traffic can be anticipated.

The petitioners have proposed that no sidewalk be constructed on the eastern off-site section of Wilson St. so as to not encourage pedestrian traffic crossing at the mid-block. A sign could also be added to discourage pedestrians from using Wilson St. to access Henderson St. At the recommendation of the Bicycle and Pedestrian Safety Commission, the developer has verbally agreed to instead construct a sidewalk on the south side of Grimes Ln. between the development and the intersection with Henderson.

Another possible solution that was discussed involves construction of sidewalk along the west side of Henderson St. to direct pedestrians either north or south. Staff believes that even with this additional sidewalk, the problem of pedestrians

crossing at mid block to the school would not necessarily be resolved.

Staff believes that the petitioners' proposal is the most logical means to discourage pedestrians from crossing Henderson St. from Wilson. With this proposal two sidewalk connections would be made to controlled intersections, allowing pedestrians to cross Henderson St. safely.

**Street Standards:** The primary change to the proposal is the narrowing of all interior streets to provide 7 ½ -foot travel lanes with additional pavement width for parking on one side of the street. The petitioners had originally proposed 9 foot wide travel lanes on Wilson St. and Driscoll St. and 10 foot wide lanes on Dunn St., which would be the main fire access. The petitioners have submitted examples of existing streets in the City they have a queuing arrangement. Most of these are located in older, "Core Residential" areas. The petitioners also note that queuing streets are identified in the AASHTO "Green Book" manual as common in single family areas, where a low number of short distance trips can be expected.

The Fire Department's ideal solution would be for the petitioners to agree to install residential sprinkler systems in every house. If sprinklers are installed, the Fire Department believes the 7 ½ foot travel lanes in a queuing street arrangement would not hinder fire protection. The petitioners have committed to imposing this requirement and staff recommends that the queuing streets be approved with the sprinkler commitment. However, understanding that this proposal may not acceptable to the Plan Commission, the City Fire Chief has outlined the following alternative recommendations in order of preference:

1. 10 foot travel lanes on Dunn as specified in the State of Indiana fire code and shown on the original plan;
2. 10 foot travel lanes on Driscoll St. and Wilson St. and the section of Dunn St. between Driscoll St. and Wilson St. This would give the Fire Department access as required by the fire code within 150 feet of all structures. Fire hydrant locations would still need to be worked out;
3. Mid-block fire lanes, as proposed by petitioners, coupled with 10 foot travel lanes on Wilson St. and Driscoll St. It is not yet clear if this option would meet the code requirement. Fire hydrant locations would still have to be worked out. There may also be fire protection issues if cars are illegally parked in the fire lanes or if snow is piled in these areas in the winter.

Unlike the previous proposal, this new street arrangement would include on-street parking on only one side of Dunn St. through the residential sections as well as the on-site sections of Driscoll St. The petitioners have stated that less residential on-street parking may be necessary because many future homeowners are likely to build garages for off-street parking. The proposed intersections are slightly narrowed using "bump-outs" to protect the street

parking. The intersections are also proposed to include curbs with narrowed, but “mountable,” turning radii. If the residential sprinkler recommendation of the City Fire Chief is not accepted by the Plan Commission, staff would recommend that Dunn St. be widened to include 10 foot travel lanes and the side streets be widened to 9 foot travel lanes.

**Right-of-way:** The petitioners have proposed 42 foot right-of-ways for Driscoll and Wilson. While this is less than the normally required 50 feet of dedication, the Planning and Engineering Departments have no objection to this request because the existing sections of these streets only have 37 feet of right-of-way. Staff notes that if 9 foot travel lanes are required by the Plan Commission, this dedication width will likely increase to 45 feet.

Another right-of-way dedication reduction is required on Grimes. The petitioners wish to only dedicate the half right-of-way needed to allow for a travel lane, a parking lane, tree plot and sidewalk. Staff estimates this at 29 feet instead of the required 32.5 feet from centerline. The petitioners argue that no other lots along Grimes have allocated this much land, and houses would have to be torn down to accommodate a full 65 foot right-of-way. Staff has no objection to this request as long as 11 foot wide travel lanes are maintained on Grimes Ln. and there is enough right-of-way to accommodate the required public improvements.

Finally, the petitioners are proposing to dedicate 44 feet of right-of-way in the residential sections of Dunn St. The Thoroughfare Plan and Zoning Ordinance require 50 feet of right-of-way for local streets. Staff recommends that a minimum 50 foot wide right-of-way be dedicated for Dunn St.

**Purchase of Hillside Right-of-way:** If this rezoning request is approved, the petitioners intend to acquire land owned by the Bloomington Redevelopment Commission along Hillside Dr. This land was purchased by the City in order to facilitate improvements to the intersection of Hillside Dr. and Henderson St. However, the Redevelopment Commission owns more land than required for the long term needs of Hillside Dr., based on the Thoroughfare Plan. The petitioners intend to make an offer to purchase part of this land if the rezoning request is approved by the Common Council. Staff notes that the schematic location of the right-of-way purchase has been altered to include only those areas outside of the 40 foot from centerline Thoroughfare Plan requirement.

**Mixed Use Parking:** The following chart summarizes the revised parking provided with this petition versus *Zoning Ordinance* requirements. Staff supports the proposed reduction in off-street parking due to both the presence of new on-street parking spaces as well as the neighborhood-focused, mixed use nature of the proposed land uses. Staff notes that some on-street parking must be eliminated in order to meet minimum standards for distance to intersections and cross walks and to keep parked cars out of the “sight triangle.”

		<b>Required</b>	<b>Proposed</b>
<b>Single Family Parking</b>	Off-street	86 spaces (2 per lot)	0 spaces required
	On-street	0 spaces	64 max spaces (1.5 per lot)
<b>Maximum Mixed Use Parking</b>	Off-street	143 spaces	26 spaces
	On-street	0 spaces	34 spaces
<b>Total Mixed Use</b>		143 spaces	60 spaces

### **Alternatives to Pull-in On-Street Parking on Hillside**

The Plan Commission asked staff to put together schematic illustrations of alternatives to angled parking on Hillside Dr. For the benefit of the Commission, three alternatives are presented in the packet. These alternatives include: 1) parallel parking on Hillside Dr. with an 8- foot “pull-off area,” 2) no on street parking on Hillside Drive, but increased parking behind the buildings, and 3) angled spaces along Hillside Drive with a curbed island separating this parking from the travel lane.

The first alternative, parallel parking on Hillside Dr., is supported by AASHTO standards and the Master Thoroughfare Plan and more acceptable to City emergency service providers than pull-in spaces. This alternative would mean a reduction of about half of the on-street parking on Hillside Dr. Some of this parking could be regained behind the mixed use buildings but would necessitate the loss of one single family lot.

The second alternative, no parking on Hillside Dr., would likely mean that commercial uses would not be viable at this location. There would simply be no parking presence on the street to pull-in customers from Hillside. Also, there would be no traffic calming benefit associated with pull-in parking off Hillside Dr. Some lost parking could be regained behind the mixed use buildings, but this would necessitate the loss of one single family lot.

Finally, the Plan Commission suggested an alternative that would involve pull-in parking between the street and the proposed buildings, but be separated from the lane of traffic with some sort of physical barrier. Some of the disadvantages of this plan include the removal of valuable on-street parking spaces, relocation of the proposed mixed use buildings away from the street edge, and possible elimination of the proposed mixed use building at Henderson and Hillside.

As stated during the first hearing, staff continues to support the proposed pull-in, angled parking off of Hillside Dr. While the options presented above are technically feasible, they create either a significant loss of on-street parking, loss of the building forward design concept, or the elimination of the commercial services component altogether. These effects severely damage this proposal’s goal of achieving a mixed use, “new urbanist” character to the development.

Moreover, the safety impacts of pull-in parking are mitigated by such factors as the proposal for longer parking stall lengths, location of the project near a signalized intersection and the location of the project in a restricted speed/school zone. Staff note that other examples of pull-in, angled parking along arterial roadways can be found in the City on S. Rogers Street, W. 17<sup>th</sup> Street, E. 3<sup>rd</sup> Street, E. 10<sup>th</sup> Street, and S. Henderson Street.

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**RECOMMENDATION:** Staff recommends approval of Case# PUD-09-04 with the following conditions of approval:

1. The GPP is hereby amended to designate the southern one (1) acre, which includes the three mixed use buildings, as a "Neighborhood Activity Center."
2. All of the proposed single family homes located within the PUD are required to be equipped with residential sprinklers. These homes must all be inspected by the City Fire Department prior to occupancy. Fire hydrants shall be located on the side the street with no parking. As part of this condition, travel lanes along all interior public streets may be reduced to seven (7.5) feet in width.
3. Drive-through uses and gasoline sales are specifically excluded from the list of permitted uses.
4. In association with the public improvements constructed by the petitioners, Grimes Ln. must maintain eleven (11) foot wide travel lanes and Hillside Dr. and Henderson St. must maintain twelve (12) foot wide travel lanes per the Thoroughfare Plan.
5. All public improvements, including on-street parking, street trees and a minimum of five (5) foot wide sidewalks must be located within the road right-of-way.
6. A twenty (20) foot radius of the right-of-way lines at the intersection of Hillside and Henderson is required.
7. Dunn Street must be constructed through to Hillside in conjunction with final plans for the single family lots, not the mixed use buildings. Fifty (50) feet of right-of-way must be dedicated for Dunn St.
8. Parallel parking adjacent to commercial buildings along Dunn Street must be eight (8) feet wide.
9. The proposed "park" must be privately owned and maintained. Ownership and maintenance arrangements shall be reviewed by the Plan Commission at Final Plan stage.
10. Occupancy of all lots and multi-family units are subject to the single family definition of "family", which includes not more than three (3) unrelated adults.
11. Final approval of drainage and utility plans by CBU is required in conjunction with final plan approval.
12. Land purchase from the Bloomington Redevelopment Commission must be finalized prior to final plan filing.

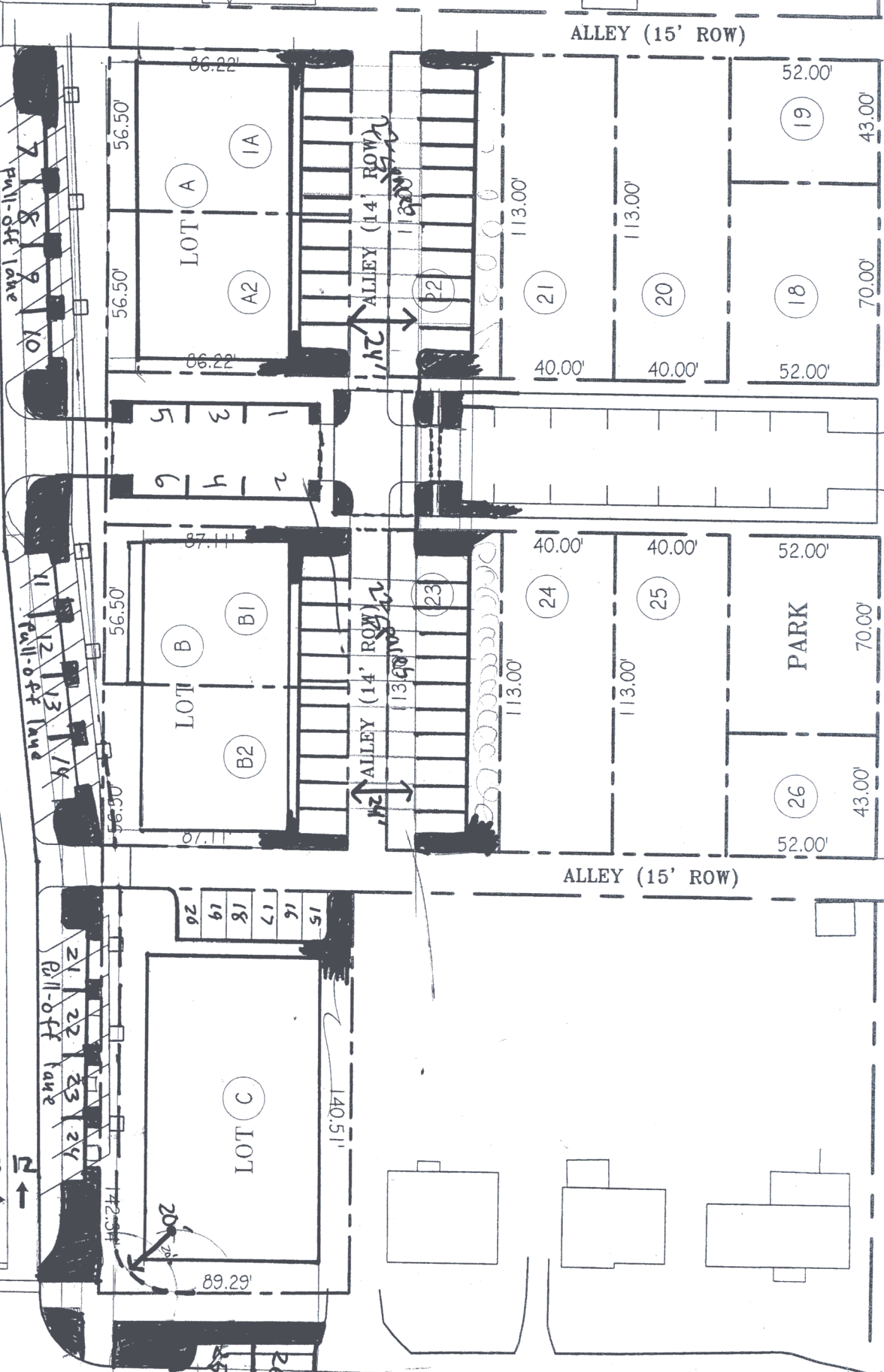


13. All on-street parking spaces must be clear of the regulated sight triangle, not be within twenty (20) feet of a crosswalk and not be within thirty (30) feet of the approach to a stop sign.
14. The proposed north south alleys adjacent to the mixed use buildings shall be designed to accommodate truck traffic and turning movements to handle deliveries.
15. A sidewalk is required on the south side of Grimes Ln. from Dunn St. to Henderson St. Design of this sidewalk link shall be determined at final plan stage.

12 vs 26

lot 02  
20 total  
5D off street  
parking to DS

Hillside Drive



Alternative #2  
Parallel Parking on Hillside

19  
12  
13

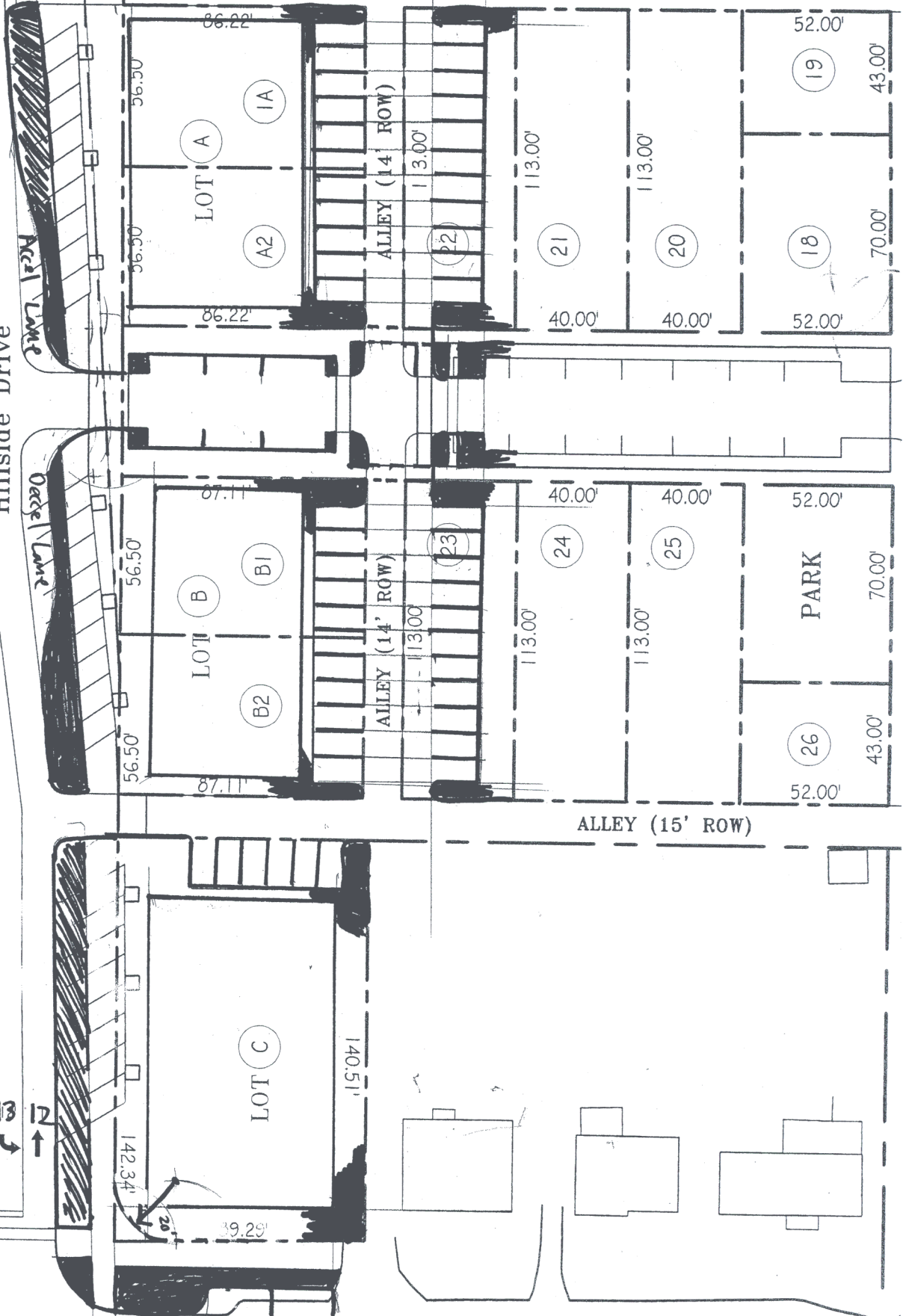
No Street Parking on Hillside  
 8 spaces on street  
 50 off-street spaces  
 58 total

Hillside Drive

Accl Lane

Deck Lane

12  
 13  
 2

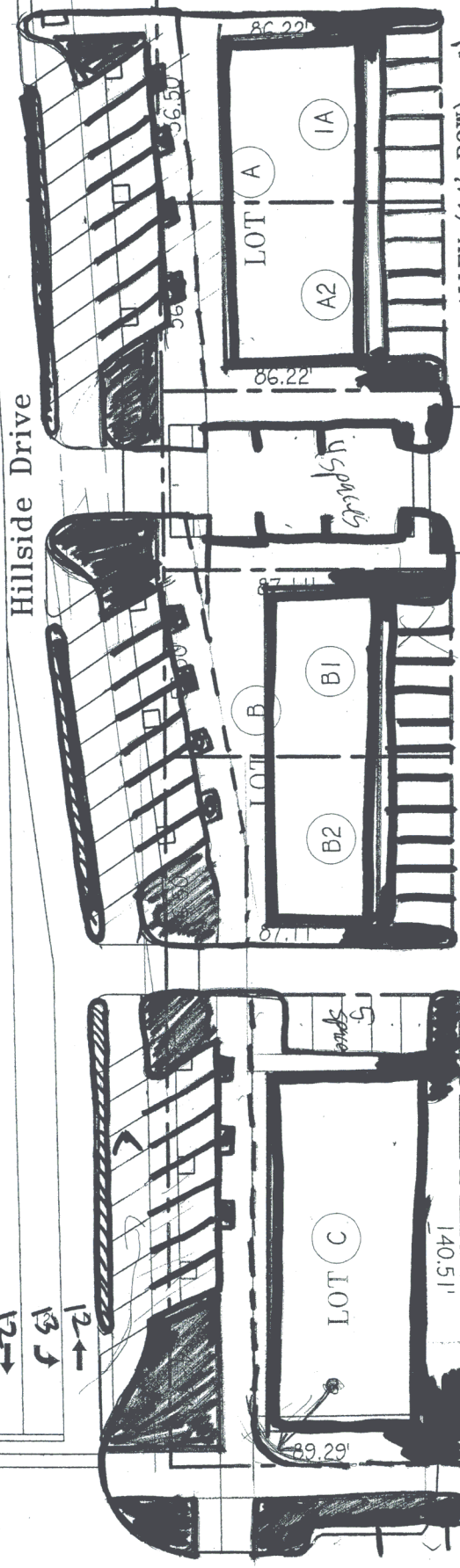


Alternative #2  
 No parking on Hillside

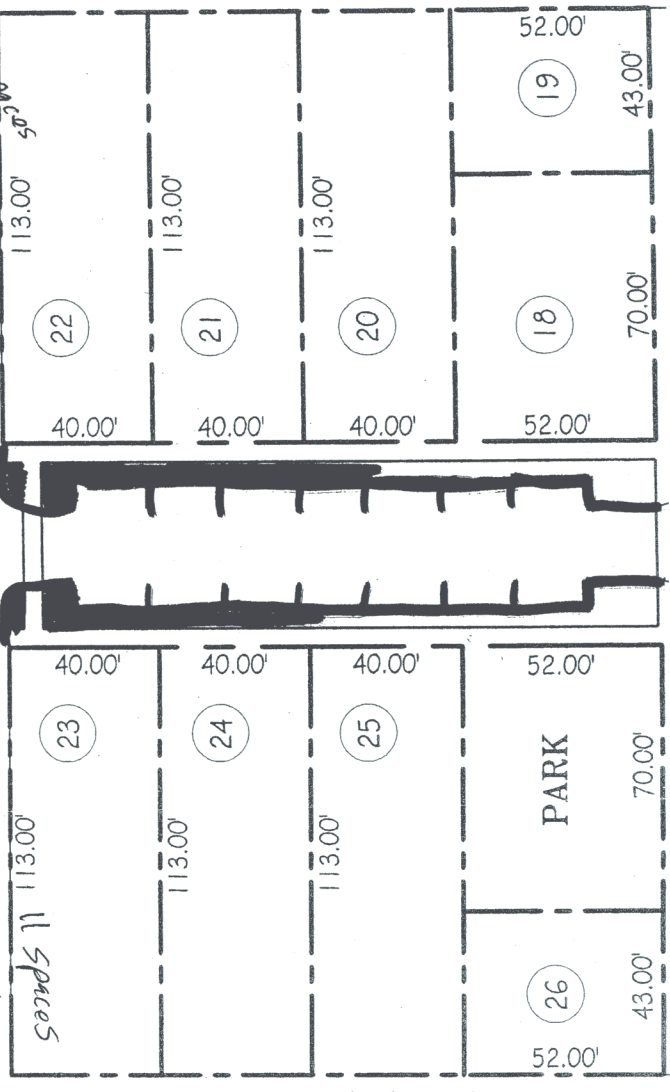
31 off-street spaces  
 25 on-street spaces  
 56 total

Hillside Drive

12  
 13  
 12



ALLEY (14' ROW)



ALLEY (15' ROW)

ALLEY (15' ROW)



Alternative #3  
 Parking separated from travel lane

**BLOOMINGTON PLAN COMMISSION  
PRELIMINARY REPORT  
LOCATION: 1330 S. Dunn Street**

**CASE NO: PUD-09-04  
DATE: May 10, 2004**

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**PETITIONER:** Neighborhood Solutions, LLC (Matt Press)  
601 W. Dodds Street, Bloomington

**COUNSEL:** Kirkwood Design Studio  
108 ½ E. Kirkwood Ave., #3, Bloomington

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**REQUEST:** The petitioners are requesting a rezoning of 6.9 gross acres from RS4.5 to Planned Unit Development (PUD) and preliminary plan approval for 43 single family lots and three mixed use lots. Also requested is a *Growth Policies Plan Amendment* to change the classification of part of the property from “Core Residential” to “Neighborhood Activity Center”.

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**BACKGROUND:**

**Area:** 6.9 gross acres  
**Current Zoning:** RS4.5  
**GPP Designation:** Core Residential  
**Existing Land Use:** Mostly vacant, rolling meadow terrain, scattered trees, 6 existing homes  
**Proposed Land Use:** Mixed residential and commercial  
**Proposed Density:**  
    **Total:** up to 10.9 u/a (75 units on 6.9 acres)  
    **Single Family:** up to 7.3 u/a (43 lots on 5.9 acres).  
    **Multi-family:** up to 32 u/a (32 units on 1 acre)  
**Proposed Commercial:** 15,650 square feet retail + 21,150 SF of office  
**Surrounding Uses:**           **North, West** – Single family residential  
                                  **East** – Single family residential and Templeton Elementary School  
                                  **South** – Mixed single and multi-family residential

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**REPORT SUMMARY:** The property in question is approximately 6.9 acres in the Bryan Park Neighborhood. It is bounded by E. Grimes Lane to the north, E. Hillside Drive to the south, existing homes on S. Palmer Street to the west and existing homes and S. Henderson Street to the east. Surrounding lots were created in the late 1920s. Since that time, a portion of E. Driscoll Street was constructed into the property and extended up the Dunn right-of-way to Grimes. Also, six small houses were constructed and used as rentals. These houses are in poor condition and not historic in nature. A permit has already been approved to move one of these houses to the northeast corner of Henderson and E. Miller Drive.

Under the current RS4.5 zoning, the property could conceptually be developed

with approximately 26 lots. Instead, the petitioner, Neighborhood Solutions LLC, is requesting that the zoning be changed to Planned Unit Development (PUD) and a preliminary plan be approved to allow for 43 single family lots and 3 mixed use lots.

A previous proposal for this property by South Dunn Street, LLC. was reviewed by the Plan Commission in 2003. After considerable debate and three hearings, the Plan Commission voted 7-3 to forward a positive recommendation to the Common Council in August. Prior to review by the Council, the petitioners withdrew the project from consideration.

The biggest changes between this proposal and the one previously reviewed is the inclusion of commercial uses on Hillside and the addition of an additional lot at the northwest corner of Hillside and Henderson into the petition area. Staff does not recommend that the Plan Commission review this petition by comparing it to the previous petition. The Plan Commission should review this petition based on its merits and its compliance with the policies and recommendation and the *Growth Policies Plan*. As a point of reference though, staff felt that providing the Commission with some basic density and parking comparisons was appropriate.

	<b>Bryan Park Place</b>	<b>Petitioner's Proposal</b>
<b>Overall Density</b>	7.6 units per acre (u/a)	10.9 u/a
<b># of Single Family Lots</b>	30 lots	43 lots max.
<b>Minimum lot size</b>	5,100 square feet	2,266 square feet
<b>Minimum lot width</b>	44 feet	40 feet
<b>Largest lot proposed</b>	7,200 square feet	6,044 square feet
<b>Single Family Density</b>	5.6 u/a	5.9 u/a max.
<b># of Multi-family Units</b>	20 units	32 units max.
<b>Multi-family Density</b>	16.7 u/a	32 u/a max
<b>Commercial Square Footage</b>	0 square feet	15,650 square feet max + 21,150 SF of office
<b>Reduction in required parking proposed</b>	0 spaces	66 spaces

This proposal includes the creation of new alleys to access rear garages and parking areas. Internal public streets would be developed with on-street parking and narrow travel lanes. The homes would be built with front porches, in locations close to the right-of-way, and in styles similar to those in the existing neighborhood. The mixed use buildings would utilize on-street parking, outdoor "plazas", and first floor commercial uses with residential or office uses on the second floor. Dunn would be extended through the property to Hillside. Two other rights-of-way, Driscoll and E. Wilson Street, stub into the property from both the east and the west. The petitioners propose to connect both of these streets to Henderson and S. Palmer Street. All of these elements integrate into the "neo-traditional" or "new urbanist" feel that the petitioner is trying to create.



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**GROWTH POLICIES PLAN:** An analysis of the *Growth Policies Plan* is included throughout the report. While this property is designated as “Core Residential” by the *GPP*, the petitioners have requested that the *GPP* be amended with this petition to include the southern part of the property as a “Neighborhood Activity Center” (NAC). The *GPP* notes that “while several NACs have been identified on the land use map, more could be designated in the future as further study is done and appropriate locations have been identified” (Pg. 33). This report reviews this petition based on both the Core Residential and Neighborhood Activity Center recommendations.

In general, staff finds that this proposal meets many of the policies and recommendations of the *GPP*. This proposal furthers goals of compact urban form and neighborhood scaled centers. The dominant land use element of this project is owner occupied single family homes. This matches the general land use goal for “Core Residential” areas. The single family lots also meet multiple *GPP* recommendations such as providing garages accessed by alleys, utilizing a grid street pattern with multiple points of connectivity and providing compatible architecture and site design for new construction.

The Plan Commission must determine if the proposed commercial uses and mixed use buildings are scaled to serve the immediate neighborhood and ensure their neighborhood focus.

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## **LAND USE:**

**Single Family:** Single family uses are currently permitted in this zoning district. The *GPP* states that the Core Residential land use district is designed for higher density single family residential uses. The *GPP* also encourages maintenance of existing housing stock and utilization of City funds to provide incentives for increased owner occupancy and affordable housing construction. In 2001 the City designated the Bryan Park Neighborhood as a “Homeownership Zone.” This zone encourages homeownership through low interest loans to income eligible homeowners and through targeted infrastructure investment. In this respect, the petitioner’s proposal for variable lot sizes, housing types, and smaller units is very compatible with the *GPP*.

**Commercial:** The petitioner has proposed non-residential uses on the three mixed use lot along Hillside and have proposed an amendment to the *GPP* to designate this area as a NAC. Approximately one acre of land at the SW corner of Hillside and Henderson is already zoned Limited Commercial (CL) and is designated as a NAC. The *GPP* notes that other NACs may be designated in the future after further study.

The petitioner’s proposed uses are very similar to the uses currently permitted in the CL zoning district. The *GPP* states that within NACs, the commercial uses should be “at a scale that serves the immediate neighborhood, including such

services as small food stores, video rental, or small cafes” (Pg. 33). The *GPP* also notes that “neighborhood-serving commercial uses and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations” (pg. 30). Hillside and Henderson are both secondary arterials. However, the *GPP* also states that NACs “should be [on] collectors (Secondary or Primary)” (Pg. 33). Finally, the *GPP* states that in NACs, “commercial uses should be restricted to ensure their neighborhood focus” (Pg. 33).

The proposed uses are as follows:

- Grocery/Convenience Store
- Offices
  - Permitted on first and second floors
- Personal Services
  - Tattoo Parlors not permitted
  - On-site Dry-Cleaning not permitted
- Retail Sales
  - Liquor stores not permitted
  - Adult uses not permitted
- Restaurants
  - Drive-throughs not permitted

**Multi-family:** The petitioner has also proposed multi-family uses on the second floor of all three buildings at the southern end of the development. In addition, they have proposed that if commercial uses do not survive at this location that all of the first floors of these building be allowed to be used for multi-family residential uses. With all of the first floors developed as commercial, the lots would be permitted a maximum of sixteen (16) two-bedroom units, for a total of 32 bedrooms. Without any commercial on the first floor, the maximum number of units increases to 32 units, including sixteen (16) two-bedroom units and sixteen (16) studio units (48 total bedrooms).

The *GPP* states that medium and high-density multi-family residential uses may be appropriate for the Core Residential areas if the units are compatibility designed and located to respect and compliment single family dwellings. The *GPP* also states that if allowed, multi-family uses should be located along major streets, such as Hillside, which is designated a Secondary Arterial (Pg. 30). Finally, the *GPP* states that within NACs, multi-family uses would be ideally located on the “floors above street level commercial uses” (pg. 33).

**Accessory Dwelling Units:** The petitioner has also proposed that the “side street lots” along Wilson, Driscoll, and Grimes be allowed to be developed with either: 1) a garage for the house on the adjoining corner lot, 2) a separate house on a small lot, or 3) a garage with an accessory apartment on the second floor. The petitioner also proposes that the accessory apartment only be permitted if



the main house, on the corner, is owner occupied. This would require a covenant within the PUD to assure this arrangement. Inclusion of these accessory apartment units do not change the maximum allowable number of units or the density, because these ½ lots could also be developed with stand alone houses. The *GPP* notes that “granny flats” may be an appropriate way to allow additional densities while still protecting community and neighborhood character (pg. 17).

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## **INTENSITY OF USES:**

**Overall Density:** Overall, the proposed maximum density is 10.9 units per acre (75 units/6.9 acres). This density is obtained if all corner lots are developed with two houses and no commercial uses are developed along Hillside. If none of the corner lots are developed with two houses and the mixed use building is developed in the “residential building type” with commercial on the first floor, the density associated with the petition drops to 7.2 units per acre (50 units/6.9 acres). While this development is denser than immediately surrounding blocks, the density is comparable to other blocks in the neighborhood, especially those served by alleys. Surrounding densities range from 1.8 to 10 units per acre, with the most dense blocks being along Washington Street.

The *GPP* encourages denser infill development in areas that already contain city services (Pg. 6). The *GPP* also notes that compact urban form does not imply “the intrusion of higher density development into established neighborhoods [or] crowding” and is “not to be achieved at the expense of greenspace, environmental protection, and other policies” (pg. 5). Finally, the *GPP* notes that in core residential areas, “development should emphasize building and site compatibility with existing densities [and] intensities....” (pg. 30).

**Single family density, lot size and lot width:** The petitioner is proposing a maximum single family density of 7.3 units per acre (43 units/5.9 acres). This density would be reached if every corner lot was developed with two houses, or a house and an accessory apartment over a detached garage. If all corner lots were developed with only one unit, this density drops to 6.1 units per acre (36 lots/5.9 acres).

The single family lot layout includes a minimum lot size of 2,266 square feet and minimum lot width of 40 feet. The smallest lots would be the “side street lots” along Driscoll and Wilson. The largest lots, at 5,480 square feet, would be those same corner locations if the two lots were developed with only one house.

Setback requirements are specific to the lot type. The petitioner has proposed five different lot types, including Corner Lot, Side Street Lot, Mid Block Lot, Grimes Lane Lot and Live/Work Lot. All lots along Dunn and Grimes include a 10 foot build-to line for the required porches and an 18 foot build-to line for the main mass of the house. Lots along side streets include a 6 foot build-to line for

the required porches and a 12 foot build-to line for the main mass of the house. In general, one and one and a half story houses are permitted to have 6 foot side yard setbacks, while two story houses are required to have 8 foot side yard setbacks. Rear setbacks are more restrictive than the current code requirement of 25 feet, except on side street lots.

The maximum proposed single family density of 7.3 units per acre is higher than the average block density between Hillside, Grimes, Walnut and Henderson of 5.8 units per acre. Were this property to be developed based on current zoning requirements, no lot could be smaller than 7,200 square feet. There is a pattern of half lot splits on corners in this neighborhood.

While the minimum lot widths proposed are inconsistent with the lots in the immediately adjacent blocks, which range from 54 to 62 feet in width, those lots were not developed along alleys. Those lots require greater widths to accommodate driveways. The proposed 40 foot minimum lot width is consistent with other lots located in the neighborhood further to the west along Walnut, Washington, Lincoln and Grant. The blocks along these streets, between Grimes and Hillside, were platted with widths of 40 and 41.14 feet and were also developed with alleys.

**Commercial Square footage:** A maximum build-out of the mixed use buildings on Hillside would allow for a maximum of 15,650 square feet of first floor commercial uses and 21,150 square feet of second floor and basement office uses. The following chart compares this commercial space to other existing or proposed commercial spaces in the City.

	<b>Location</b>	<b>First Floor</b>	<b>Second Floor</b>
<b>Proposal</b>	Hillside and Henderson	15,650 square feet (SF)	21,150 SF of office (including basement of Lot C)
<b>Renwick PUD Village Center</b> (Not yet approved)	Moore's Pike and Sare	22,000 SF	10,000 SF of office
<b>Covenanter Hill PUD Prof. Center</b> (Not yet built)	Covenanter and Clarizz	20,000 SF	12,000 SF
<b>McDoel Grocery Thomson Area PUD</b> (Not yet built)	Patterson and Rogers	5,500 SF	Residential
<b>The Shoppes</b>	800 block of College Mall Rd	37,500 SF	N/A
<b>Colstone Square</b>	3 <sup>rd</sup> and Dunn	12,000 SF	Residential
<b>Whitehall Park</b>	3400 block of W. 3 <sup>rd</sup> St.	20,000 SF per building	N/A

The *GPP* states that within NACs, the commercial uses should be “at a scale that serves the immediate neighborhood” without “attracting an influx of usage from surrounding areas”(Pg. 33). The *GPP* also states that in NACs, “commercial uses should be restricted to ensure their neighborhood focus” (Pg. 33). The Plan Commission must determine if the proposed commercial uses and buildings are scaled to serve the immediate neighborhood and ensure their neighborhood focus.

**Multi-family density:** The number of multi-family units associated with the project varies from 14 units (28 bedrooms) to 32 units (48) depending on which building type is constructed and whether the first floor is used for commercial tenants. The petitioner proposes that the second floor of all mixed use buildings be used for either office space or apartments. The first floor of these buildings could also be used for “studio apartments” in case commercial space cannot be sustained at this location, but they would be designed to be easily converted to commercial uses.

In terms of density, the proposed apartments range from 14 units per acre to 32 units per acre. While this density is higher than the surrounding neighborhood, which varies from 1.8 to 10 units per acre, the density is mitigated by the architecture of the buildings, small unit size (studio and 2 bedroom units) and location adjacent to Hillside.

Staff research has determined that the proposed multi-family portion of this development is consistent with other small, neighborhood sized multi-family developments in the area. Existing small-scale multi-family within the neighborhood ranges in density from 15 to 40 units per acre. While these densities are greater than any multi-family zoning district would allow, some of these developments are as small as 10 units. The Plan Commission must determine if the proposed multi-family densities are compatible with surrounding land uses and whether the first floor of the mixed use buildings should be allowed to be used as apartments.

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## **SITE DESIGN:**

**Access and Connectivity:** Access to individual lots and off-street parking for the mixed use buildings would be from newly constructed alleys on both sides of the development. This portion of the Bryan Park neighborhood was not platted with alleys. Both of the proposed alleys would be 10 feet in width, within 15 foot wide right-of-ways, and extend from Grimes to Hillside. The Public Works Department is only willing to accept these alleys as public rights-of-way if they are a built to a minimum width of 12 feet. Future homeowners must also be made aware that the City of Bloomington will not provide public services, such as trash collection and snow removal, within the alleys.

With this development, Dunn would be constructed through the property from

Grimes to Hillside. Wilson and Driscoll would also be extended to connect to Palmer and to Henderson. These off-site improvements would be constructed with sidewalk on one side of the street and reduced travel lane width. The previous petition on this property did not propose to extend Wilson to Henderson.

The petitioner believes that extending Wilson to Henderson is necessary because it is an “integral part of the platted, grid network” of streets. The petitioner has submitted a study which looks at the sight distance available for drivers on Henderson at the proposed Wilson intersection. This study is being reviewed by the Engineering Department.

The Plan Commission must determine if the proposed extension of Wilson to Henderson and the extension of the alleys to Hillside will be safe for vehicles and pedestrians.

**Street Standards:** Dunn has been designed with 10 foot travel lanes and 7 foot wide on-street parking lanes on both sides of the street. All internal streets would be developed with a five foot sidewalk and five foot tree plot on both sides of the street. The travel lanes for Wilson and Driscoll are proposed to be reduced to 9 feet. The off-site sections of Wilson and Driscoll are proposed with 8 foot travel lanes. These 8 foot lanes are not acceptable to the Engineering departments and should be widened to 9 feet. Dunn would include on-street parking on both sides and the on-site sections of Driscoll and Wilson would include parking on one side.

**Right-of-way:** All necessary right-of-way will be dedicated on Dunn. The petitioner has proposed 45 foot right-of-ways for Driscoll and Wilson. While this is less than the normally required 50 feet of dedication, the Planning and Engineering Departments have no objection to this request because the existing sections of these streets only have 37 feet of right-of-way.

Another right-of-way dedication reduction is required on Grimes. The petitioner wishes to only dedicate the half right-of-way needed to allow for a travel lane, a parking lane, tree plot and sidewalk. Staff estimates this at 29 feet instead of the required 32.5 feet from centerline. The petitioner argues that no other lots along Grimes have allocated this much land, and houses would have to be torn down to accommodate a full 65 foot right-of-way. Staff has no objection to this request but would note that the petitioner's schematic utility plan shows sanitary sewer laterals and stormwater mains within the front setback of the lots on Grimes. These facilities are typically located in right-of-way.

**Purchase of Hillside Right-of-way:** The petitioner has requested that the Bloomington Redevelopment Commission consider giving up some of the land it owns along Hillside. When the intersection of Hillside and Henderson was improved, the Redevelopment Commission used Federal funds to purchase more right-of-way than was needed to improve the intersection and more than is required by the current *Thoroughfare Plan*. The City currently controls approximately 70 feet of land from the center of Hillside, whereas the *Thoroughfare Plan* suggests that only 40 feet is needed for Hillside in the future. The petitioner has suggested that about 30 feet of this unneeded City property be included as part of this project. The Redevelopment Commission has been receptive to this idea, but State law requires that the land be offered to the highest bidder. If this rezoning request is approved, the petitioner intends to begin the process of getting this land surveyed, appraised and purchased from the Redevelopment Commission. Staff would note that only land beyond 40 feet from centerline that is not needed for public improvements, such as parking, street trees and sidewalk, would be sold.

**Pedestrian Facilities:** Five foot wide concrete sidewalks are proposed on both sides of Dunn, Wilson and Driscoll and along one side of Driscoll and Wilson. A five foot sidewalk would also be provided on the south side of Grimes. Finally,

along Hillside, the petitioner proposes a wide “plaza space” that would extend from the front of the buildings to the on-street parking. Part of this space would be public sidewalk and part would be private plaza for outdoor seating and other activities. Staff would note that at least five feet of ADA accessible sidewalk along Hillside must be located in the right-of-way and unobstructed. Final sidewalk design would be reviewed at Final Plan stage.

**Private Park:** The petitioner has proposed an approximately 3,500 square foot private “pocket park” at the southeast corner of Dunn and Wilson. This park will be owned and maintained by either a homeowner’s association or some other non-profit group. Staff is very supportive of this commitment to providing on-site recreation opportunities.

**Single Family Parking:** The *Zoning Ordinance* requires that all single family lots provide a minimum of two off-street parking spaces. This can take place in an attached or detached garage or on a parking pad or driveway. The petitioner has proposed that in this development no lot be required to provide off-street spaces. In addition, under some scenarios the corner lots are prohibited from providing parking. Only detached garages or an 18’ by 18’ parking pad would be permitted. All off-street parking access would be from the proposed alleys, which is consistent with the *GPP* recommendation for core residential neighborhoods (Pg. 30). The Plan Commission must determine if it is appropriate to not require any lots to provide off-street parking.

**Mixed Use Parking:** The *Zoning Ordinance* requires a minimum of 143 off-street parking spaces for the mixed use portion of this development. This parking requirement was calculated based on these lots being developed with the “storefront building type” and the buildings being used exclusively for commercial uses; with a grocery/convenience store on Lot C, office space in the basement of Lot C and the second floors of all buildings used for office uses instead of residential. Other build out scenarios would require less parking. Only 32 off-street spaces are proposed for these lots, but 31 new on-street spaces are being created.

The *GPP* makes several statements concerning parking in NACs. The *GPP* states that provided “parking should be designed to primarily serve residential units rather than used as an attractor for commercial users, be located in the side or rear of buildings to minimize street cuts in front of buildings and be heavily landscaped in order to soften [its] impact on the neighborhood” (Pg. 34).

The following chart summaries the parking provided with this petition versus *Zoning Ordinance* requirements.

		<b>Required</b>	<b>Proposed</b>
<b>Single Family Parking</b>	Off-street	86 spaces (2 per lot)	0 spaces required
	On-street	0 spaces	100 spaces

			(2.3 per lot)
<b>Maximum Mixed Use Parking</b>	Off-street	143 spaces	32 spaces
	On-street	0 spaces	31 spaces
<b>Total Parking</b>	Off-street	229 off-street spaces	32 spaces
	On-street	0 spaces	131 spaces
<b>Grand Total</b>		<b>229 spaces</b>	<b>163 spaces</b>

The Plan Commission must determine if the proposed parking has been designed to primarily serve residential units or if it is being used as an attractor for commercial users. The Plan Commission must also determine if the number of spaces, location, buffering and design is appropriate and minimizes the impact on the neighborhood.

**On-Street Parking on Hillside:** The petitioner has proposed on-street, pull-in parking spaces along Hillside. The petitioner states that on-street parking is necessary if the proposed commercial uses on Hillside are to be viable. Hillside is classified as a secondary arterial roadway and has traffic counts of approximately 8,200 average daily trips. Hillside is currently functioning at Level of Service (LOS) “C” along most of its length. While the posted speed limit on this street is 30 mph, the average speed is 32/34 mph and the 85<sup>th</sup> percentile speed is 38/39 mph.

#### Advantages of On-Street Parking

- Slows traffic
- Deeper parking stalls, as proposed, would increase visibility of exiting vehicles and provide for a higher level of safety
  - Zoning Ordinance requires 20’ 3” deep stalls
  - Petitioner proposes 25’ deep stalls.
- There are other examples of on-street pull-in parking in the community
- Provides a parking presence to attract customers driving along arterial streets
- Some kind of street parking may be necessary to make commercial uses viable
- Provides more spaces than a parallel parking scenario could possibly accomplish

#### Disadvantages of Pull-in On-Street Parking

- Not supported by Fire Department, Police Department or City Engineering. While still not desirable, these departments would prefer 8 foot parallel spaces with some separation from street
- Not supported by AASTHO standards. 8 foot parallel spaces would meet standards.
- Not supported by Master Thoroughfare Plan: 8 foot parking lane recommended.

- Spaces may be too close to intersection. Spaces are only about 65 feet from the Hillside and Henderson intersection.
- Parking would Interrupt traffic flow on a street meant to serve a high degree of vehicular mobility.
- Parking would introduce additional point of vehicular conflict.

Alternatives to pull-in spaces:

- 8 foot parallel parking lane with a 7-8 foot safety/pull-off lane
- Parallel or pull-in spaces with an access lane physically separated from main Hillside travel lane
- All parking provided behind buildings, accessed from Dunn and/or the alleys
- Reduce the amount of commercial and thereby reduce the parking needs

The *GPP* has several statements related to parking in NACs and along arterial roadways. The *GPP* states that parking in NACs should not be “used as an attractor for commercial users” and that “parking should be located in the side or rear of buildings” in order to “minimize street cuts in front of buildings” (Pg. 34).

The Master Thoroughfare Plan notes that if on-street parking is provided on secondary arterials it should be in the form of an 8 foot parking lane, but it does not list on-street parking as a priority for the right-of-way or as a recommended “traffic management feature” (Pg. 88). Driveway spacing on secondary arterials “may be closer than 600 feet, provided no driveway exit is within 200 feet of the approach to an existing signalized intersection” (Pg. 94). Staff notes that no part of this property’s frontage on Hillside is further than 200 feet from the approach to the Hillside and Henderson intersection. Finally, the Thoroughfare Plan notes that arterial streets serve a “higher degree of mobility” and “longer trips within the city” than do collector streets. Arterial streets serve a “larger city-wide function” and “connect major destinations” (Pg. 81).

The Plan Commission must determine if some kind of on-street parking is appropriate on Hillside. If found to be appropriate, the Plan Commission must also determine how the parking should be designed.

**Single Family Architecture:** The petitioner has provided six specific house types for lots in this development. Not all house types would be allowed on all lots. They have designed the house types to replicate housing types found in the neighborhood. Some features of the houses include a first floor elevation a few feet above the elevation of the street, specific paint schemes, first floor ceiling heights between 9 and 10 feet, large front porches, large double hung windows and a prohibition against vinyl siding.

Specific requirements for architecture are listed on Figure 11. Examples of features regulated include siding material, porch construction, roof pitch and



type, porch roof pitch and type, porch width as a percentage of house width (70-85%), window type, roof and siding color and window arrangement.

Housing Styles, Locations and Features			
		Lots	Features
Type A	1 Story Bungalow	Corner Lot Side Street Lot 40' Mid Block Lot 42' Mid Block Lot Grimes Lane Lot Live/ Work Lot	Max. 3 bedrooms Max. 1400-finished sf
Type B	1 1/2 Story Bungalow	Corner Lot 40' Mid Block Lot 42' Mid Block Lot Grimes Lane Lot Live/Work Lot	Max. 4 bedrooms Max. 2000-finished sf Max. 1400 sf footprint
Type C	Foursquare	Corner Lot 42' Mid Block Lot Grimes Lane Lot Live/Work Lot	Max. 4 bedrooms Max. 2000-finished sf Max. 1160 sf footprint
Type D	1 1/2 Story Cottage	Side Street Lot	Max. 2 bedrooms Max. 1100-finished sf Max. 700 sf footprint
Type E	2 Story Cottage	Side Street Lot	Max. 3 bedrooms Max. 1100-finished sf Max. 550 sf footprint
Type F	Relocated BRI House	Side Street Lot	Two of the existing houses will be relocated and remodeled by Bloomington Restorations, Inc.

The *GPP* encourages infill development in core neighborhoods to maintain and respect the historic context and architectural character of the existing homes. Staff finds that the listed architectural elements, as well as the small front yard setback, limited height of homes and alley accessed garages, provide a sufficient commitment to houses that are of high quality, and are compatible with and complementary to existing houses in the area.

**Mixed Use Building Architecture:** The mixed use buildings at the south end of the development are proposed at two stories. The building on Lot C would also have a partially exposed basement because of grade changes. The architecture of these buildings will be either a residential or storefront design. The petitioners state that Lot C, the lot at the corner of Hillside and Henderson, must have the storefront design. The Residential type was designed based on a small-scale apartment building. This building type would still allow commercial uses on the first floor. The storefront type would be as the name implies: a series of storefronts, broken into bays, that give the impression of several buildings instead on one larger building. Schematic elevations are included in the packet.

**Environmental Issues:** The property includes several large trees, mainly silver maples. While these trees are not of high quality, the petitioner intends to preserve them where possible. On some lots, the sidewalk may need to meander from the road to preserve these existing trees.

**Utilities:** This site has adequate utility service for both water and sanitary sewer. Schematic plans have been submitted to CBU and are under review.

**Stormwater:** A schematic drainage plan has been submitted to CBU and is under review. This plan includes off-site improvements and piping of stormwater to the north to discharge into the creek along Davis Street and to the south to discharge closer to Clear Creek.

**Transit:** The north side of the property along Grimes and the far southeast corner of the property along Henderson are on Bloomington Transit's route #2. The *GPP* encourages the City to place higher density residential development within walking distance to transit routes.

**Miscellaneous Development Standards:** The petitioner and City Planning and Engineering Department staff are not in agreement on several specific development standards associated with this project. Staff recommends that the following changes be made to the petition prior to second hearing.

<b>Standard</b>	<b>Code Req.</b>	<b>Proposal</b>	<b>Recommendation</b>
<b>Parking lane width</b>	8 feet	7 feet	7 feet for Residential 8 feet for Commercial
<b>Grimes Lane travel lane width</b>	11 feet	10 feet	11 feet
<b>Hillside travel lane widths</b>	12 feet	11 feet	12 feet
<b>Street intersection radius</b>	20 feet	10 feet	15 feet
<b>Street ROW radius at property corner</b>	20 feet	0 feet	20 feet
<b>Alley ROW radius at property corner</b>	15 feet	0 feet	15 feet
<b>Street width at Intersections</b>	Same as travel lanes	16 feet	20 feet on Dunn 18 feet on side streets
<b>Travel lane widths for off-site road extensions</b>	10 feet	8 feet	9 feet
<b>Public alley width</b>	12 feet	10 feet	12 feet

**NEIGHBORHOOD INPUT:** The petitioners has conducted numerous neighborhood meetings, including a week long “design charette” in February. These meetings were well attended by residents of the Bryan Park Neighborhood and some members of the Plan Commission and Common Council. Several letters of support are included in your packet from neighbors and the Neighborhood Association.

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**CONCLUSIONS:**

Staff finds that in general this project is a good example of compatible infill development that furthers many of the goals of the *GPP*. Some points for further discussion include:

- Are retail and other mixed uses appropriate at this location on Hillside? Is this an appropriate location for a new Neighborhood Activity Center (NAC)? Is the petitioner proposing too much non-residential use?
- Does the petition provide enough parking for the proposed mixed use buildings? Is the parking designed to serve residential units instead of “as an attractor for commercial users.”
- Should parking be permitted on Hillside? If parking is provided, should it be parallel or pull-in spaces?
- Is it necessary for the alleys to extend to Hillside? Will this be safe?
- Is it necessary for Wilson Street to be extended to Henderson? Will this be safe?
- Are the proposed lot sizes and widths compatible with existing lots in the neighborhood?
- Are other proposed reductions in standards appropriate?

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**RECOMMENDATION:** Staff recommends that this rezoning request be forwarded to the June 14, 2003 Plan Commission meeting.

# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Addendum to South Dunn Street PUD filed 6.1.04

June 9, 2004

### Alternate Street Design: Queuing Street with Residential Sprinklers

The width of a street is the major indicator of the speed of traffic and directly impacts pedestrian safety. In an effort to address this reality, we have worked with the Fire Department to find a way to achieve a narrow street width.

We propose that South Dunn Street from Grimes to the parking lane of the multi-use buildings, and Driscoll and Wilson Streets will be queuing streets. A queuing street is designed so that when there is traffic in two directions, cars must occasionally yield between parked cars before moving forward. This type of street permits only slow speed traffic on the most local of streets and works to discourage potential cut through traffic. These streets are proposed to be 22' wide with one 15' travel lane and one 7' parallel parking lane.

In order to implement a queuing street design, each house would be protected by a residential sprinklering system. Residential sprinkler systems have been in use for decades in the US and are mandatory in new houses in several cities and counties across the country. A residential sprinkler system can contain a fire from its beginning, minimizing the risk to life and property. This built-in measure of safety puts less pressure on emergency response times. Recognizing the tremendous benefit afforded by residential sprinklering, the fire code currently allows for an exception to the standard 20-foot road width when sprinklering is installed.

The developer will choose to implement the queuing street design with residential sprinklers if the various city departments involved will sufficiently support this effort.

Reference Attachments:      Alternate Street Design Site Plan dated 6.9.04  
   Alternate Street Sections  
   FEMA Home Fire Protection: Residential Fire Sprinkler Systems

Supplemental  
Petitioner's  
Statement

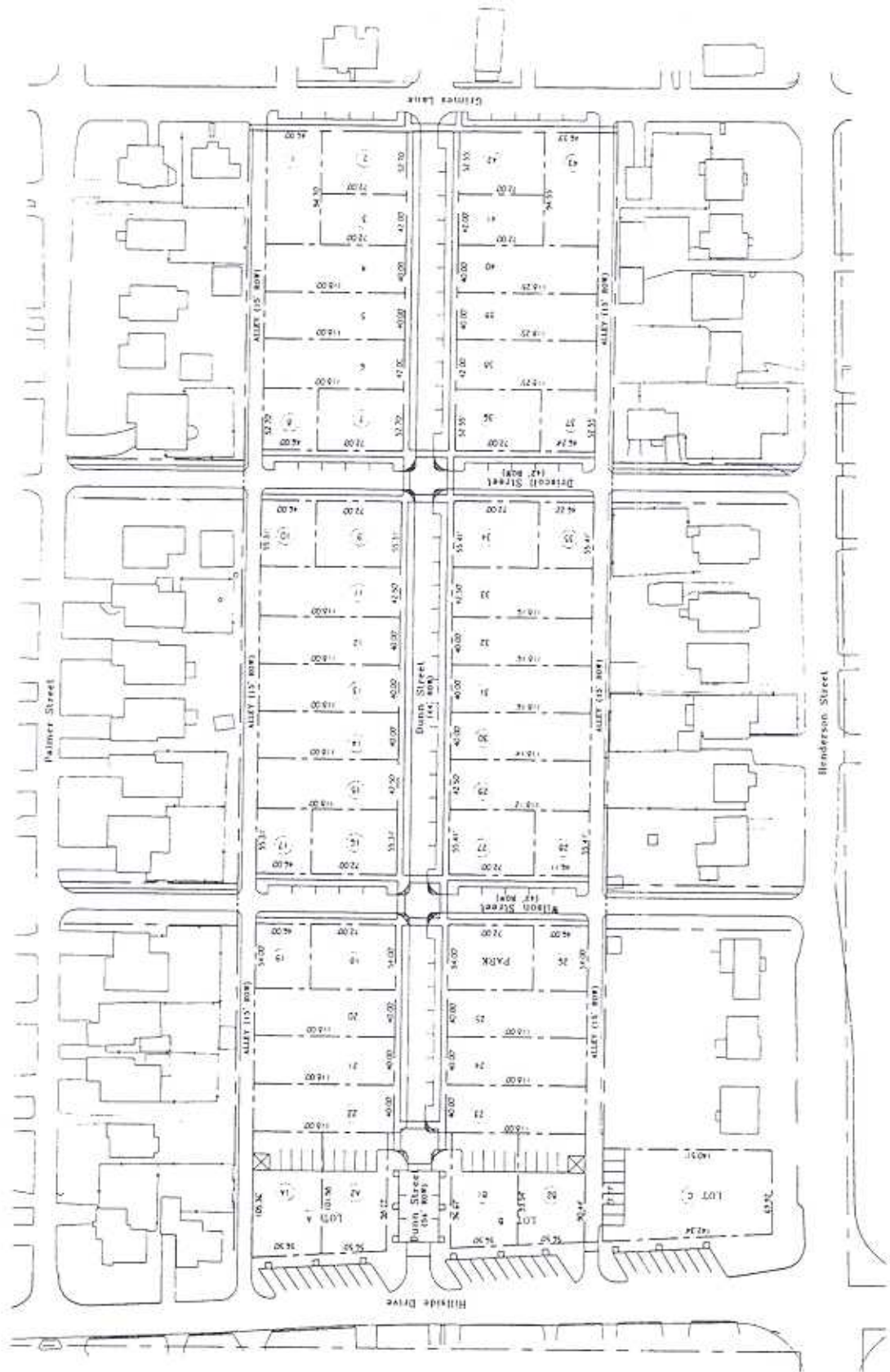


DUNN STREET TRADITIONAL NEIGHBORHOOD DEVELOPMENT  
NEIGHBORHOOD SOLUTIONS, LLC  
BRYAN PARK NEIGHBORHOOD, S. DUNN STREET  
BLOOMINGTON, IN

Site Plans







A SITE PLAN  
SCALE: 1/8" = 1'-0"





PUD-09-04  
SITE PLAN IN CONTEXT 49



# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

24 April, 2004

City of Bloomington Planning Dept.  
P.O. Box 100  
Bloomington, IN 47402  
James Roach, Senior Zoning Planner

Dear James,

Please find attached the South Dunn Street PUD proposal revisions that we discussed in our meeting last week.

We are also attaching an alternative design for Hillside based on our meeting with City Engineering.

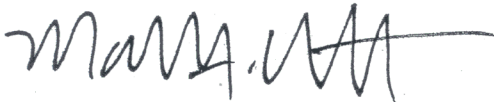
We want to thank you for your attention and consideration and we look forward to providing a new traditional neighborhood development that includes a mixed-use Neighborhood Activity Center along Hillside.

It is exciting to be able to contribute to the goals and implementation of the City of Bloomington Growth Policies Plan by creating a development that fits into the existing core neighborhood of Bryan Park.

Respectfully,



Matt Press,  
Neighborhood Solutions, LLC



Marc Cornett,  
KIRKWOOD DESIGN STUDIO, P.C.

c: file

Petitioner's  
Statement

April 12<sup>th</sup>, 2004

Plan Commission, City of Bloomington  
C/O Jim Roach, Planner  
P.O. Box 100  
Bloomington, IN 47402

Re.: South Dunn Street PUD proposal

Dear Jim and Plan Commissioners,

Last fall, I was presented with the opportunity to become involved with a truly unique piece of land in one of Bloomington's most vibrant core neighborhoods. The South Dunn Street property is approximately 6.9 acres of land between East Grimes Lane and East Hillside Drive, in the Bryan Park Neighborhood. Months of community outreach, city input, research and planning have culminated in this Planned Unit Development rezoning proposal that I now have the honor of presenting to you.

Every aspect of this proposal has been shaped by a philosophy of traditional neighborhood design. I, and those working with me, have turned to the resurgent historic core neighborhoods of Bloomington for inspiration and to the City's Growth Policies Plan for guidance in crafting this proposal. The resulting plan is committed to compact urban form, mixed use development, human-scaled design and a pedestrian-friendly environment. As such, the proposal necessitates the use of PUD zoning on the site. The PUD rezoning and its accompanying document will allow us to meet the very special needs of this particular property and allow for a very high level of certainty as to what will be built and how it will look and function.

I feel that the unique planning and design process undertaken for this project should be a role model for future urban infill projects in the city of Bloomington. I consider myself a New Urbanist and traditional neighborhood design activist as much as a property developer. As such, I felt it critical to the entire process that community input be prominent in the planning and design of this project.

To this end, communication with the surrounding Bryan Park neighborhood began the very day I purchased the property. I attended a neighborhood association meeting later that month and worked with those in attendance on a schedule for a series of workshops that would allow the neighborhood to have a real say in the shape of the project and the future of their neighborhood. These intensive workshops took place in January and covered literally every option and aspect of this project. I am very grateful for the hard work of the neighborhood and for the success we had in reaching consensus in those workshops. The process concluded with a public presentation of the workshop results. Those results are represented here, in this proposal.

This project attempts to follow in the footsteps of other innovative development projects that have recently come in front of the Bloomington Plan Commission. It is important that this

proposal be considered not only as a plan for this specific site, but as part of a larger trend in development in our city. This welcome trend places a much greater emphasis on the context of the development; on how it functions in regard to the surrounding neighborhood and city as a whole, and how it functions and evolves over time. Most importantly, this trend places the quality of life of the residents who will be affected by the development on an equal footing with other considerations shaping the design of the project.

I am very grateful for the opportunity to become involved in such a special project in a wonderful core neighborhood. I am particularly grateful for the ongoing input, guidance and support given to me by the residents of the Bryan Park neighborhood. They are the ones who will be living with the results of this project for many years to come. I sincerely hope that this project—and the process of design and approval—will serve them well.

Respectfully,

A handwritten signature in black ink, appearing to read "Matt Press". The signature is fluid and cursive, with a large loop at the end.

Matt Press  
Neighborhood Solutions, LLC

# SOUTH DUNN STREET

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NEIGHBORHOOD SOLUTIONS, LLC  
JUNE 1, 2004 REVISED

## Introduction

The goal of the South Dunn Street infill project is to complete an area of the Bryan Park neighborhood by seamlessly integrating into this historic core neighborhood while further enriching the quality of life for all the neighborhood residents.

This proposal addresses the development of property on South Dunn Street between Grimes Lane and Hillside Drive as well as an adjoining property on the northwest corner of the intersection of Hillside Drive and Henderson Street (fig. 1). The property, long known as the last remnant of the Young farm, is set within the original Rolling Park plat of the area. The site is comprised of approximately 6.9 acres and is located within the Bryan Park neighborhood.

The project proposes 43 single-family lots and the development of a Neighborhood Activity Center (NAC) comprised of (3) three neighborhood serving mixed-use buildings at the corner of Hillside Drive and Henderson Street and along Hillside Drive. These mixed-use buildings will have a commercial use on the first floor with between four and six apartments on the second floor of each building. A neighborhood pocket park is proposed at the southeast corner of South Dunn and Wilson Streets. (Reference attached site plan drawing fig.2)

In order to provide maximum connectivity, all of the existing streets will be connected through the site: South Dunn will run from Grimes Lane to Hillside Drive, Driscoll and Wilson Streets will run from Palmer Street to Henderson Street. Alleys that run north south from Grimes Lane to Hillside Drive will provide garage access for most of the single-family houses. A parking alley will run behind the mixed-use buildings providing service access for businesses and parking for tenants, employees and customers.

A philosophy of traditional neighborhood design shapes every aspect of this proposal. The project reflects the ideas of compact urban form and mixed-use development inherent in the Growth Policies Plan. A PUD rezoning of the site is required to integrate this project with the patterns of the existing core neighborhood and to provide the amenity of neighborhood-serving, commercial activity. The project also hopes to begin a transformation of Hillside Drive, through the realization of a NAC, to create a more human scaled and pedestrian oriented environment, and counteract urban monotony. This proposal promotes architecture that respects and builds upon the historic architectural character of the existing core neighborhood



# SOUTH DUNN STREET

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NEIGHBORHOOD SOLUTIONS, LLC  
JUNE 1, 2004 REVISED

## Growth Policies Plan

The Growth Policies Plan characterizes core residential as “areas of bungalows and cottages” characterized by “a grid-like street system, alley access to garages, small street setbacks and a mixture of owner occupants and rental units (p.30).” The South Dunn Street PUD incorporates all of these elements to reflect the unique architectural character and land use patterns of a core neighborhood.

The South Dunn Street PUD proposes a Neighborhood Activity Center (NAC) located along Hillside Drive and Henderson Street. The GPP encourages the exploration of “opportunities to introduce nodes of appropriately designed, neighborhood scaled commercial uses within the core neighborhoods (p.30).” The GPP further explains “neighborhood-serving commercial uses may be the most appropriate at the edge of Core Residential areas that front arterial street locations (p.30).” Both Hillside Drive and Henderson Street are classified as secondary arterials.

The proposed mixed-use buildings in the South Dunn Street project meets all aspects of Neighborhood Activity Center as defined in the Growth Policies Plan (p.33). The buildings will provide small-scale retail and business services to serve the neighborhood. The buildings will be located at the front of the site “to frame the street and intersection” with parking located at the side or rear of the buildings with access from an alley system. In addition, angled parking spaces off of Hillside will meet the critical need for convenience parking. The buildings will be fronted with a wide sidewalk and street trees to create a pleasant, pedestrian-scaled environment. The project will be adjacent to a number of bus routes. The South Dunn Street PUD contains all of the elements to realize a successful Neighborhood Activity Center for the Bryan Park Neighborhood.

## Street Design

The South Dunn Street PUD reflects the central traditional neighborhood design principle that a rich network of streets and alleys works to diffuse traffic equitably and promote pedestrian activity. This project, embedded within the larger Bryan Park core neighborhood, proposes to complete the grided road and alley system found in the existing neighborhood. The connections of South Dunn to Hillside, Driscoll and Wilson Streets from Palmer to Henderson will realize the original right-of-way layout to create as many alternate routes as possible for traffic leaving and entering the site.

The connection of the north south alleys from Grimes Lane to Hillside Drive will provide a consistent, pedestrian orientated streetscape along South Dunn Street by the elimination of curb cuts for driveways and garage access. In turn, curb continuity maximizes the amount of on-street parking spaces possible. The alleys, meant to serve only those properties in a given block, will handle two-way traffic.

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A 14-foot wide service lane behind the mixed-use buildings will allow for deliveries to serve the commercial buildings and provide access for tenant parking, employee and customer parking.

The South Dunn Street PUD street design emphasizes a safe, pedestrian-scaled street environment throughout the project. The residential portion of South Dunn Street, Driscoll Street and Wilson Street will be queuing streets that allow for one 14' wide drive lane so that traffic must occasionally yield between parked cars before moving forward. A queuing street design permits only slow speed traffic on the most local of streets and works to discourage potential cut through traffic. There will be a fire lane at the middle of each long block to provide a 20' wide clear operations area. These fire lanes, in addition to locations at the intersections, allow for access within 150' to the furthest part of a house located at a given block. This narrow street profile reflects the existing street widths and character of the Bryan Park neighborhood and Bloomington's other core neighborhoods.

All streets will have 5-foot wide sidewalks and be separated from the street edge by a 5-foot wide tree plot. On-street 7' parallel parking lanes on both sides of South Dunn Street and on one side of the side streets is designed to buffer pedestrians from traffic and provide a constricted sense of the street causing cars to move more slowly. Refer to Appendix A on 7' wide parking lanes.

South Dunn Street will transition from a 48' ROW with one 14' queuing lane in the residential area to a 54' ROW with (2) 10' lanes at the mixed-use buildings. This wider design at the southern part of the site anticipates a higher volume of traffic and the need for service access by trucks.

The off-site extensions of Driscoll and Wilson Streets consist of the existing 37' platted right-of-way. The proposed improvements within the platted right-of-way include a 14' drive lane, a 5' tree plot on both sides and a 5' sidewalk on the north side of each street. The construction of these sidewalks will hinge on the construction of sidewalks by the City of Bloomington along the west side of Henderson Street from Grimes Street to Hillside Drive. Refer to Appendix B on the Wilson Street connection to Henderson Street.

The radii of the internal intersections corners in a residential area will typically be 15' feet with a mountable curb at 5' to reduce the speed of turning vehicles and further reduce the in-street walking distance required for pedestrians to cross the street. Refer to Appendix D on Mountable Curbs.

As a traditionally planned infill project, the South Dunn Street PUD proposes new pedestrian destinations for the neighborhood. Neighborhood serving commercial buildings along Hillside will offer exciting, new opportunities for goods and services within a walkable distance. A pocket park on the corner of Wilson and South Dunn Street will provide a

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small scale, relaxing environment unlike the nearby Bryan Park. To ensure the success of these destinations, the pedestrian network must be continuous, safe and pleasant.

All curbs to be 6" standing curbs. Street lighting shall be pedestrian scaled, full cut-off lighting to minimize light pollution and glare.

## Street Descriptions

Reference attached fig. 3a-3c for on-site street sections.

South Dunn Street from Grimes Lane to Service Alley (48' Right-of-Way):

- (1) 14' traffic lane, 7' parallel parking both sides, 5' tree plot both sides, 5' sidewalks both sides

South Dunn Street from Grimes Lane to Hillside Drive (54' Right-of-Way):

- (2) 10' traffic lanes, 7' parallel parking both sides, 5' tree plot both sides, 5' sidewalks both sides

Driscoll and Wilson Streets - On the Project Site (41' Right-of-Way):

- (1) 14' traffic lane, 7' parallel parking one side, 5' tree plots both sides, 5' sidewalks both side

Driscoll and Wilson Streets – Off-site Extensions to Palmer Street and Henderson Street (37' Right-of Way):

- (1) 14' traffic lane, 5' tree plot one side, 5' sidewalk one side

South Side of Grimes Lane: (22.5' Right-of-Way from centerline – Existing ROW 40')

- 8' defined parallel parking lane, 5' tree plot, 5' sidewalk

North Side of Hillside Drive: (40' Right-of-Way from centerline – Existing ROW 80')

- 25' deep angled parking, wide plaza with street tree grates located partially in the right-of-way and partially on the site

## Alley Descriptions

North-south alley right-of-way shall be platted at 15' with a 12' paved alley.

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Alleys will be platted and dedicated to the City. The City will then maintain the alleys except for trash pickup and snow removal on the north-south alleys.

## Block Design

The South Dunn Street PUD lot layout is based on a block design that places emphasis on the corner lots and creates a desirable rhythm to the houses as they march down the street. The existing streetscape of South Washington Street illustrates this pleasing rhythm. Corner lots are widest to allow for some of the largest houses to be built there. The corner lots also allow for a house with an additional side bay. A side bay is a one or two story appendage, a maximum of 4' feet deep with a width 30% to 45% that of the house width. The middle block lots are the narrowest and permit only the smaller house types to be built there. There are a total of 43 single-family lots proposed. Reference fig. 4a-4e for a number of houses per street comparison.

### Subdivision at Corner Locations

Subdivision of the corner lots reflects an historic lot pattern found in the neighborhood. Reference fig. 4a for existing lot subdivision pattern. This subdivision offers flexibility so that the two lots could be sold together or separately. If sold to the same owner, there could be a main house on the corner lot and a garage or office/studio apartment over a garage on the side lot. If there is to be an office/studio apartment over a garage on the side lot then the main house must be owner occupied. This use will be linked to a covenant restriction. If the lots are sold to separate owners, a house could be built on the corner lot and a cottage house could be built on the side lot. The filling in of the side street lot with a building helps to visually "close-off" the alley and backyards as a completely private residential zone.

Two of the side street lots shall be reserved for the relocation of existing houses on the property. Bloomington Restoration Inc. will restore these houses through their affordable housing program.

### Live/Work Lots

Live/Work lots offer the opportunity to accommodate people who choose to work at home. They provide an affordable option for a small-scale business. In the South Dunn PUD, lots 22 and 23, located adjacent to the mixed-use buildings, are designated as Live/Work lots and are permitted house a business by right in addition to the residential unit. All home occupations that currently meet the Zoning Ordinance are permitted. Commercial signage will be restricted to two square feet and located on the building. The architecture will be



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limited to House Types A and B with a walkout basement or a garage. The business could be housed in the main house, walkout basement or garage.

Home occupations will be conditional on all other lots and will conform to the general requirements of the Zoning Ordinance.

## Lot Descriptions

Corner Lot – The corner lots are 52.55’ to 55.41’ wide and 70’ deep. They are located at all street intersections and are oriented to S. Dunn St.

Side Street Lot – The side street lots are 43’ to 43.24’ wide and 52.55’ to 55.41’ deep. They are oriented to Driscoll or Wilson Streets.

40’ Mid Block Lot – The mid-block lots are 40’ wide and 116’ to 116.25’ deep. They are located in the center of the block and are oriented to S. Dunn Street.

42’ Mid Block Lot -The mid-block lots are 42’ to 42.5’ wide and 70’ to 116.25’ deep. They are located to next to corner lots and are oriented to S. Dunn Street except for Lot Nos.1 and 43 that are oriented to Grimes.

Grimes Lane Lot – The lots are 43’ to 43.33’ wide and 94.55’ to 94.70’ deep. They located along the alleys and oriented to Grimes Lane.

Live/Work Lot – The work/live lots are 40’ wide are 116’ deep. They are located north to the mixed-use development and are oriented to S. Dunn Street

<b>Lot Type Location Table</b>	
<b>Lot Type</b>	<b>Lots Numbers</b>
Corner Lot	2, 7, 9, 16, 18, 27, 34, 36, 42
Side Street Lot	8, 10, 17, 19, 26, 28, 35, 37
40’ Middle Block Lot	4, 5, 12, 13, 14, 21, 24, 25, 30, 31, 32, 39, 40
42’ Mid Block Lot	3, 6, 11, 15, 20, 29, 33, 38, 41
Grimes Lane Lot	1, 43
Live/Work Lot	22, 23

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## Lot Regulations

Regulations governing how the lots will be built out will vary depending on the lot type and it's location on the site. These regulations specify lot size, build-to lines, setbacks, porch, garage zones, and encroachments. There are separate sets of regulations for Corner Lots, Side-Street Lots, 40' Mid-Block and 42' Mid Block Lots, Grimes Lane Lots, Live/Work Lots, and Mixed-Use Lots. Reference attached fig. 4b - fig.10 for specific regulations for each lot residential lot type.

## Single-Family Houses

The Bryan Park Neighborhood, platted from 1906 to 1928, is mainly composed of bungalow architecture from the period. The repetition of front facing gable bungalows with wide front porches creates a strong rhythm for many of the streets south of Grimes. The houses for South Dunn Street will be based on the historic building types found in the neighborhood: the One-Story Bungalow, the One and a Half Story Bungalow, the Two-Story Foursquare, the Cottage.

The understated, dignified character of these modest houses will be accomplished by sitting the house up a few steps over the street, exposed rafter tails for bungalows, multi-colored paint schemes, gracious front porches with refined porch rail and column details, and generous, well proportioned double hung windows. First floor ceiling heights will range between 9'-10' and second floor ceiling heights will range between 8'-9'. Reference attached materials list fig.11 for the houses. Where noted "to be approved" means to be approved by developer and developer's architect.

### House Types

Reference attached figs.12-16 for House Type illustrations of front elevations. A set of variable design components for each house type is listed to build variety into the architecture. Reference the Materials List fig. 11 in conjunction with the design components. Square footages listed below do not include finished basements. Basements will be permitted if allowed by subsurface soil conditions. Walk-out basements will be permitted if allowed by grade. All houses shall be consistent with the following house types:

House Type A - One Story Bungalow: Up to 3 bedrooms; up to 1400-finished sf

House Type B - One and a Half Story Bungalow: Up to 4 bedrooms; up to 2000-finished sf; maximum 1400 sf footprint

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House Type C - Foursquare: Up to 4 bedrooms; up to 2000-finished sf; maximum 1160 sf footprint

House Type D – One and a Half Story Cottage: Up to 2 bedrooms; up to 1100-finished sf; maximum 700 sf footprint

House Type E – Two Story Cottage: Up to 3 bedrooms; up to 1100-finished sf; maximum 550 sf footprint

House Type F – Relocated BRI House

**Lot Type to House Type Relationship Table**

Lot Type	House Types
Corner Lot	Type A (restricted to 850 sf), Type B (restricted to 850 sf footprint), Type C, Side Bays Permitted*
Side Street Lot	Type A (restricted to 850 sf), Type D, Type E, Type F
40' Mid Block Lot	Type A, Type B
42' Mid Block Lot	Type A, Type B, Type C
Grimes Lane Lot	Type A, Type B, Type C
Live/ Work Lot	Type A, Type B with/ walk-out basement

\* A side bay is a one or two story appendage, a maximum of 4' feet deep with a width 30% to 50% that of the house width.

## Garages

A garage is permitted on all residential lots except at the Corner Lots and Lots Nos. 3 and 41. All garages shall be detached except on the live/work lots where breezeways are permitted to connect the garage with the house. All garages will be accessed by alleys. There is no off-street parking requirement per house.

A garage or an office/studio apartment over a garage is permitted on Side Street Lots that are owned by the same owner of the adjacent Corner Lot. If there is to be an office/studio apartment over a garage on the side lot then the main house must be owner occupied. This use will be linked to a covenant restriction.

Garages shall match the architectural character of the house. The roof form and pitch of the garage shall resemble those of the house. Exterior finish materials such as siding, roof shingles, windows, gutters and downspouts shall match that of the house. All garage doors shall be single doors. Garages shall have a minimum 8' high ceiling.

A potting shed appendage 6' wide is permitted on the backyard sides of a garage.



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A parking area of 18' x 18' set adjacent to the alley will be permitted in lieu of a garage. This parking area may be of concrete, brick, gravel or porous pavers.

The maximum driveway width shall be 18 feet.

## Garage Types

Reference attached drawing fig. 17 for illustrations of garage types. All garages shall be consistent with the following garage types:

One-Story 1-Car Garage: Footprint size of 14' x 20'

One-Story 2-Car Garage: Footprint size of 22' x 22'

One and a Half Story 2-Car Garage: Footprint size of 22' x 22'; attic storage/ work space

One and a Half Story Office/ Studio Apartment over Garage: Footprint size of 22' x 22' with bathroom and kitchenette

Two Story Office/ Studio Apartment over Garage: Footprint size of 22' x 22' with bathroom and kitchenette; built only if the main house is a Foursquare House Type.

## **Mixed-Use Buildings**

The prospect of neighborhood serving retail is an important amenity for the Bryan Park Neighborhood and is integral to this project. The design of the mixed-use buildings as a Neighborhood Activity Center is intended to create an appropriate edge along Hillside Drive while complementing the neighborhood scale of the single-family houses behind. These buildings are brought forward on the site to define the street as an outdoor room. Large openings on the ground floor of these buildings will allow a breakdown of the interior and exterior to encourage activity along the sidewalk. Wide sidewalks shielded by deep canopies enliven the pedestrian space creating a zone for a number of possible activities including café seating and sidewalk displays. Street trees and on-street angled parking will further enhance the pedestrian experience protecting pedestrians from traffic. Angled parking spaces on Hillside Drive fill the critical need for convenience parking to serve the commercial uses. Reference Appendix C for an explanation of on-street, angled parking.

The mixed-use component of this project is composed of five separate lots: Lot A (1 & 2) along Hillside Drive and to the west of S. Dunn Street, Lot B (1 & 2) along Hillside Drive and to the east of S. Dunn Street, and Lot C at the corner of Henderson Street and Hillside Avenue. Lots A & B are subdivided to allow separate ownership of Residential type buildings.

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## Commercial Use Limitations

The commercial component of the mixed-use buildings is to be neighborhood serving in function and scale. The design of the buildings will do much to ensure this goal. Commercial Uses will be limited to: Grocery/ Convenience Stores, Offices, Personal Services, Retail Sales, Restaurants. Restaurants with drive-throughs, tattoo parlors, liquor stores, adult uses and on-site dry cleaning are not permitted. Office Uses shall be permitted on the first and second floors in all scenarios. Cafes/restaurants with outdoor seating or stores with outdoor displays during business hours are encouraged. All uses will be limited to hours of operation restricted from 2 am to 5 am. The limitation on the hours of operation will be stated in a lease agreement or in a deed restriction.

## Development Controls

The square footages and apartment counts listed below in all cases are the maximum numbers permitted. Square footages and apartments counts listed for the residential building type below are for each lot and not for each sub-lot or building. A month-to-month lease will control the conditional use of first floor space as studio apartments in the Storefront or Residential building types until a commercial tenant is found. Reference attached drawings fig.18 - fig. 22 for regulating plans that correspond to the different building type scenarios.

<b>Mixed-Use Development Controls</b>			
Lot	Floor	Building Type Scenarios	
		Storefront Building Type	Residential Building Type
Lot A (1 & 2)	1 <sup>st</sup> Floor	5650 sf of commercial space or (5) studio apts. conditional	4400 sf of commercial space* or (4) studio apts. conditional or (4) 2-bdrm apts.
	2 <sup>nd</sup> Floor	(5) 2-bdrm apts.	(4) 2-bdrm apts.
Lot B (1 & 2)	1 <sup>st</sup> Floor	4500 sf of commercial space or (5) studio apts. conditional	3800 sf of commercial space* or (4) studio apts. conditional
	2 <sup>nd</sup> Floor	(5) 2-bdrm apts.	(4) 2-bdrm apts.
Lot C	Basement	5500 of storage/office space	N/A
	1 <sup>st</sup> Floor	5500 of commercial space or (6) studio apts. conditional	
	2 <sup>nd</sup> Floor	(6) 2-bdrm apts.	

\* Square footage numbers do not include porches.



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## Mixed-Use Building Types

The architecture of the mixed-use buildings will be either a residential or storefront design depending on the lot location. Lot C will have a storefront design. Lot A or B can have either a storefront or residential design. Lot B cannot be built out with a residential building type until Lot A is built out with a residential building type. The storefront building type requires the same ownership of both Lots A1 and A2 or Lots B1 and B2.

**Residential Type:** This design is based on a small-scale apartment building type that was popular in the early part of the 20<sup>th</sup> century. The original design has two apartments on a second floor over two apartments on a first floor. The building is symmetrically organized around a common center stair hall. A modified version of the design presented here could allow commercial uses on the first floor. Reference attached drawing fig. 23 for architectural information.

**Storefront Type:** The small-scale urban storefront design has large openings on the first floor for commercial use with apartments above. The front elevation of the storefront will be broken down into a series of bays to impart a pedestrian scale to the building. Reference attached drawing fig. 24 for architectural information.

## Access to Commercial Buildings

South Dunn Street and the north/south alleys will provide the vital access needed for trucks to service the mixed-use buildings. Trucks will be able to pull into the north/south alleys from Hillside Drive to make deliveries, pick-ups etc. and then pass behind the commercial buildings to pull onto South Dunn Street and then out to Hillside Drive or vice-a-versa. This flexibility in access is critical to the viability of the retail. Trucks will not be permitted to park temporarily on Hillside Drive to make deliveries. This restriction will be stated in a lease agreement or in a deed restriction.

## Parking

Parking for the mixed-use buildings is limited to angled, on-street parking spaces at Hillside Drive, perpendicular spaces off the service alley in the rear of the buildings and off the side alley of Lot C, and parallel on street parking on South Dunn Street and Henderson Street. There are 61 total parking spaces to serve the mixed-use buildings: Lot A has 22 spaces, Lot B has 22 spaces and Lot C has 17 spaces. Reference Appendix C for an explanation of angled, on-street parking.

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Parking Space Breakdown			
Lot	On Street Parking	Off Street Parking	Total Parking Spaces
Lot A	12	10	22
- A1	4.5	5	9.5
- A2	7.5	5	13
Lot B	12	10	22
- B1	7.5	5	12.5
- B2	4.5	5	10
Lot C	10	7	17

An analysis of the number of parking spaces required for a maximum commercial scenario and a maximum apartment is shown in the table below. One parking space is figured per one bedroom and (3) three spaces per 1,000 sf of commercial/ office space. The storage/office space in the basement of Lot C is considered commercial space for purpose of this analysis.

Multi-Family Scenarios and Parking Requirements			
Scenario	Numbers	Parking Spaces Required	Parking Spaces Proposed
Ideal Scenario (Target)	(16) 2-bedroom apartments	32	
	15,650 sf commercial space (3-5 bldgs.)	47	
	5,500 sf office/storage	17	
		96 spaces	61 spaces
Max. Apt. Scenario (Interim Solution)	(16) 2-bedroom apartments	32	
	(16) studio apartments on a conditional basis	16	
	5,500 sf office/storage	17	
		65 spaces	61 spaces
Max. Comm. Scenario (Highly Unlikely)	15,650 sf commercial space (3-5 bldgs.)	47	
	15,650 sf office space (3-5 bldgs.)	47	
	5,500 sf office/storage	17	
		111 spaces	61 spaces

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# **SOUTH DUNN STREET**

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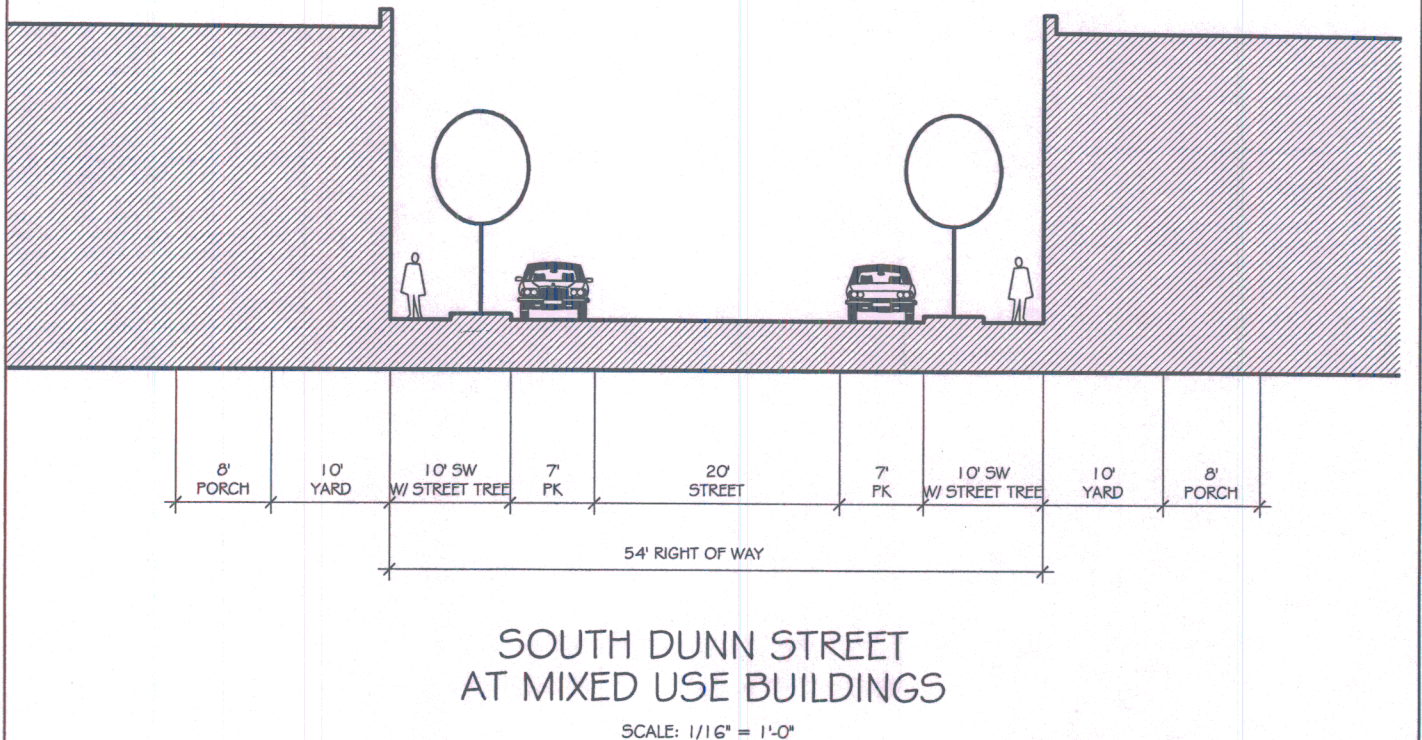
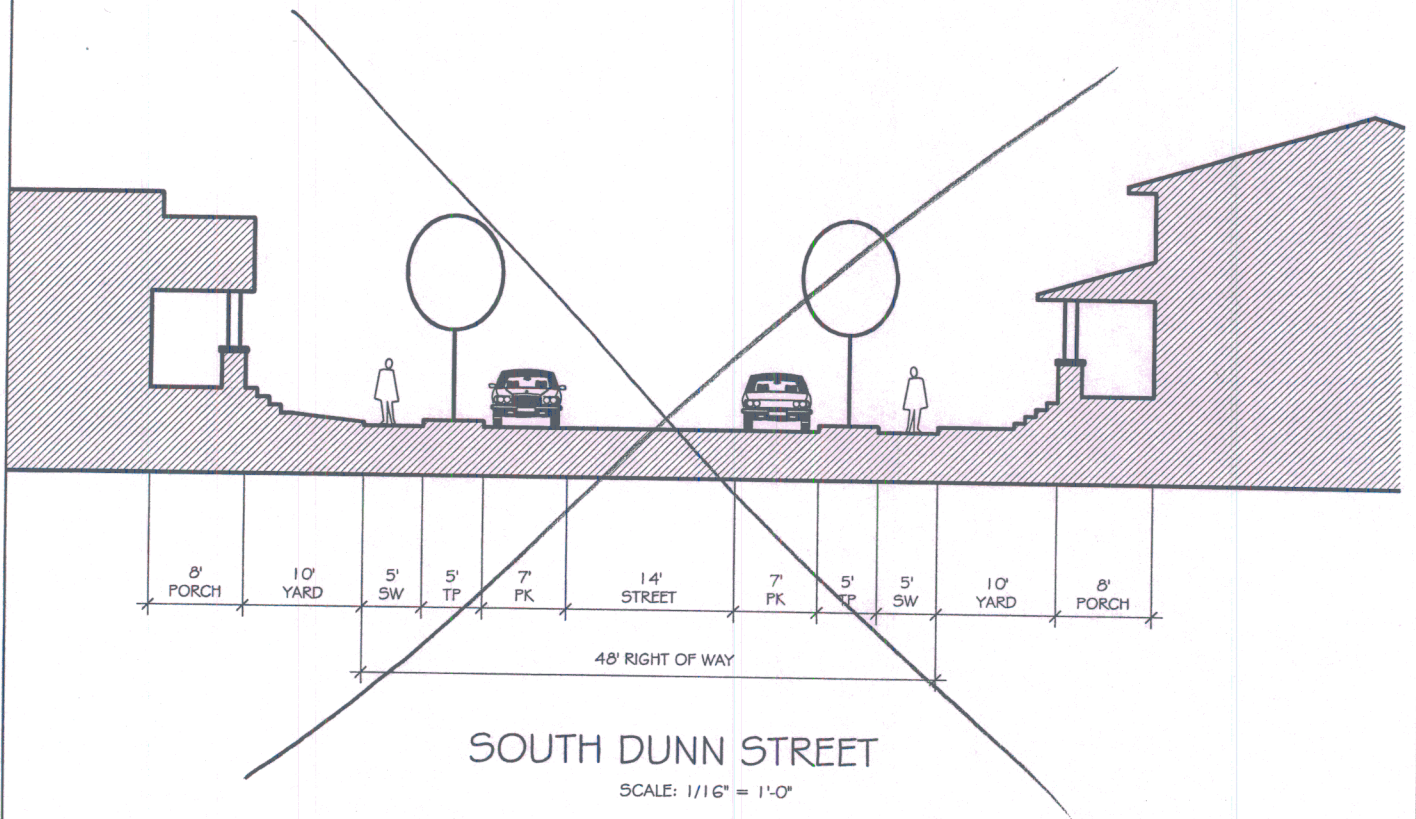
## **Neighborhood Pocket Park**

A neighborhood pocket park is proposed on the southeast corner lot of South Dunn and Wilson Street. The park would occupy a 52-foot by 70-foot corner lot. This small-scale park will provide an informal, restful place to sit and relax. The park would consist of a central grassy area ringed with a gravel path, trees and plantings on the periphery along with several benches.

Neighborhood Solutions will give the property to a non-profit group who will undertake the ownership and maintenance of the park.



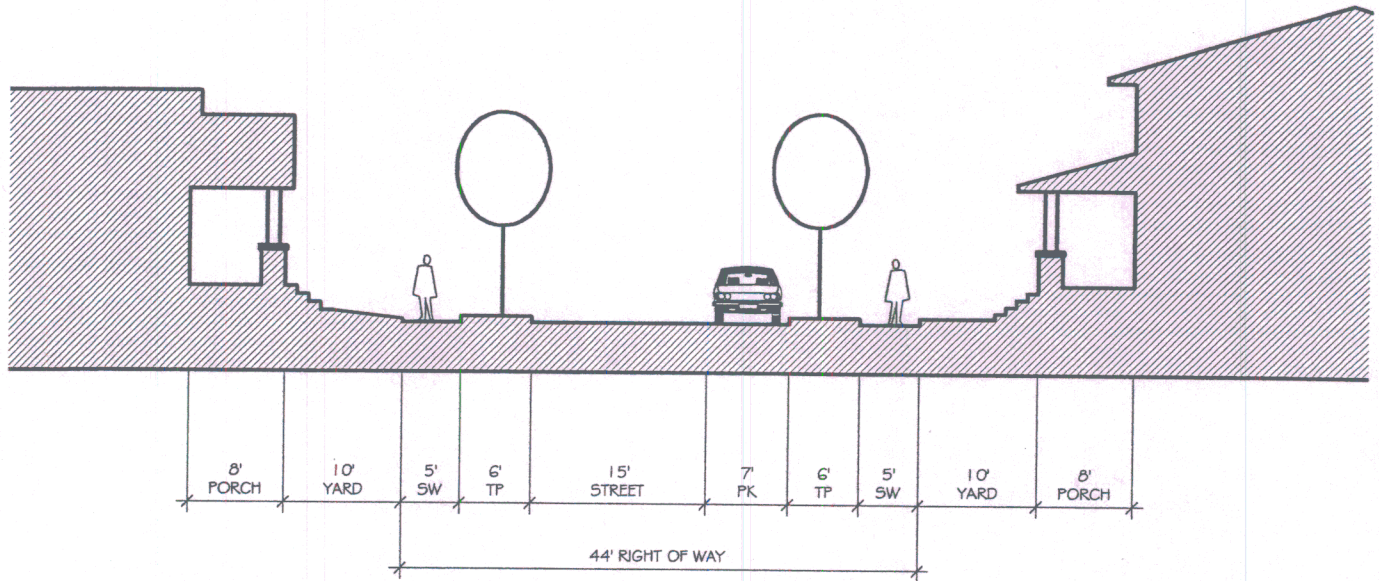
fig. 3a STREET SECTIONS



Street Sections

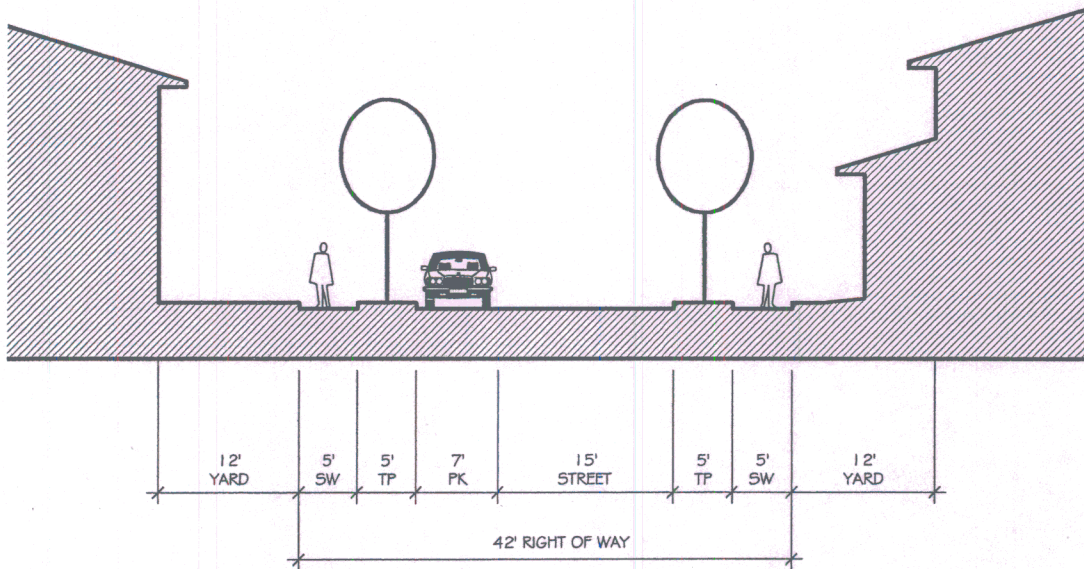


# SOUTH DUNN STREET SECTION



SCALE:  $\frac{1}{16}'' = 1'-0''$

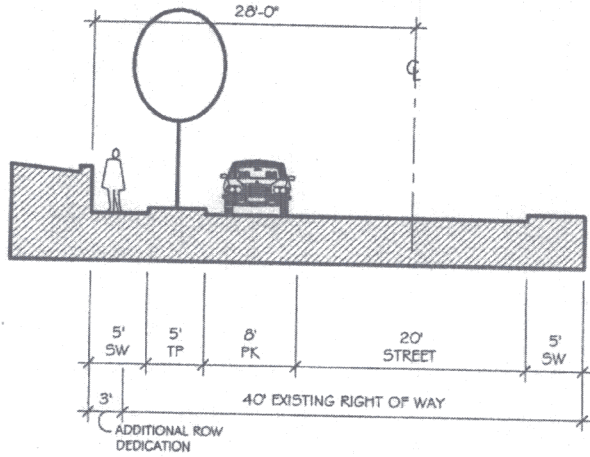
# DRISCOLL / WILSON STREET SECTION



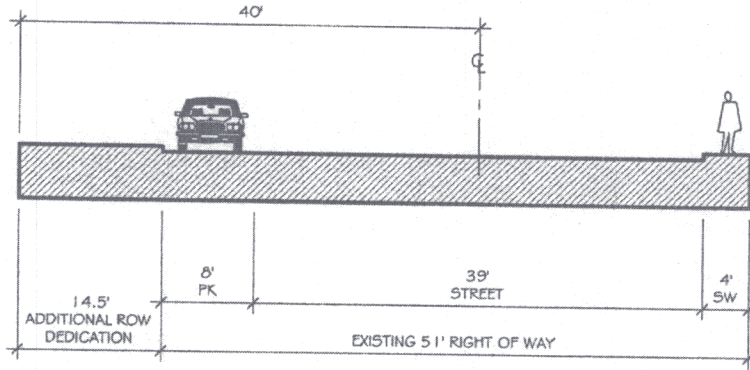
SCALE:  $\frac{1}{16}'' = 1'-0''$

39

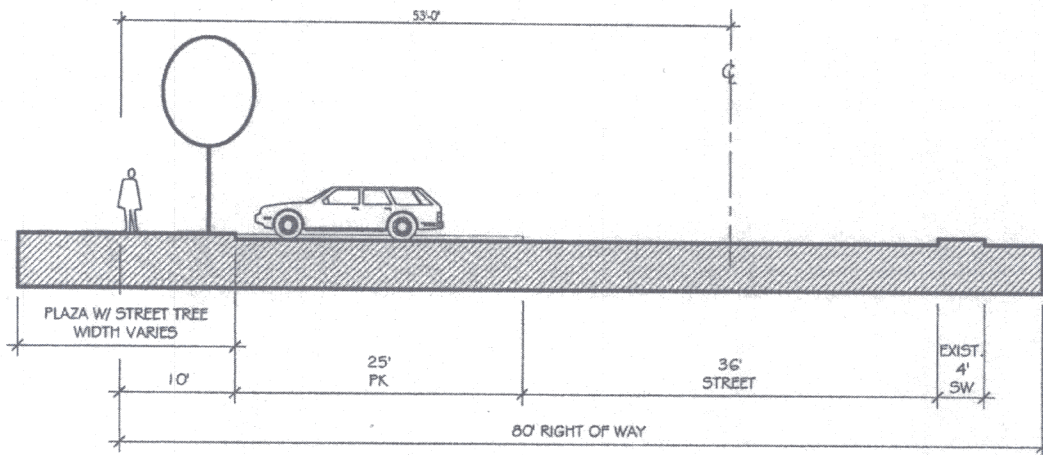
# fig. 3c STREET SECTIONS



**GRIMES STREET**  
SECTION LOOKING WEST  
SCALE: 1/16" = 1'-0"



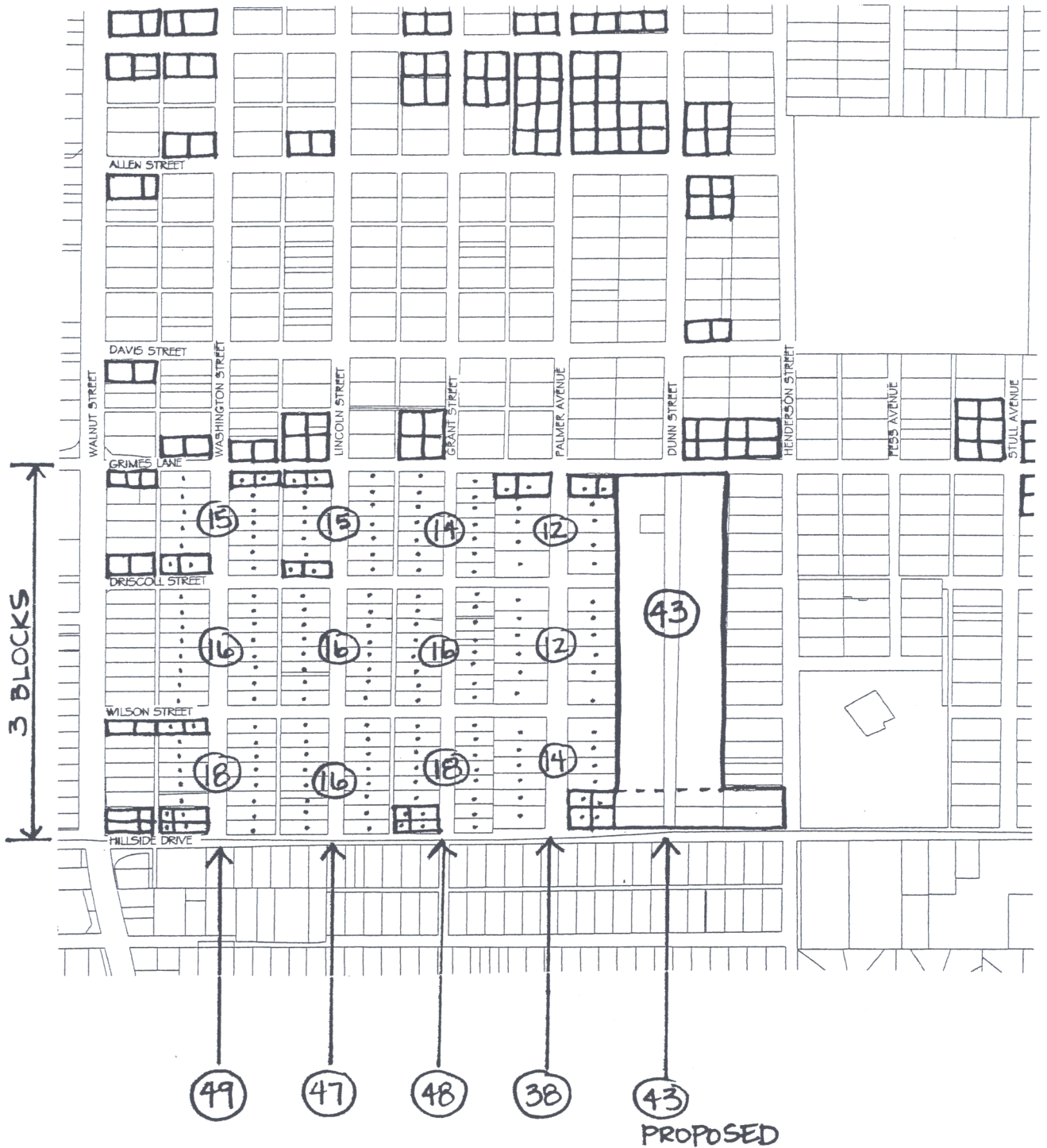
**HENDERSON STREET**  
SECTION LOOKING NORTH  
SCALE: 1/16" = 1'-0"



**HILLSIDE DRIVE**  
SECTION LOOKING EAST  
SCALE: 1/16" = 1'-0"

40

fig. 4a EXISTING DENSITY AND SPLIT LOTS IN THE SOUTH DUNN STREET AREA



 DENOTES SPLIT LOT

 NUMBER OF PROPERTIES ALONG STREET  
WITHIN DENOTED BLOCKS

1:5000





fig.4b SOUTH WASHINGTON STREET STREETSCAPE



WASHINGTON STREET LOOKING SOUTH



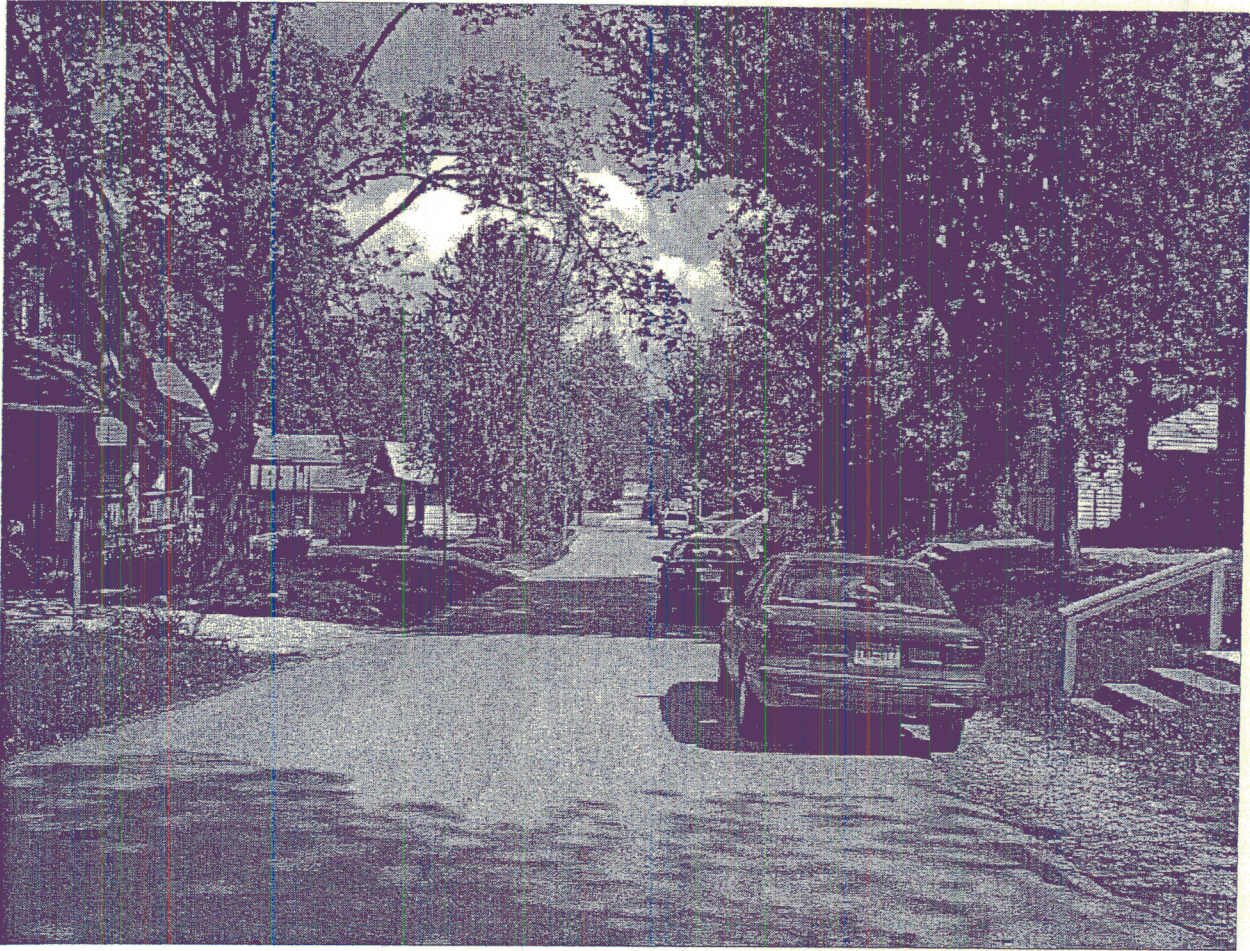
fig. 4c SOUTH LINCOLN STREET STREETScape



LINCOLN STREET LOOKING SOUTH



fig. 4d SOUTH GRANT STREET STREETSCAPE



GRANT STREET LOOKING NORTH



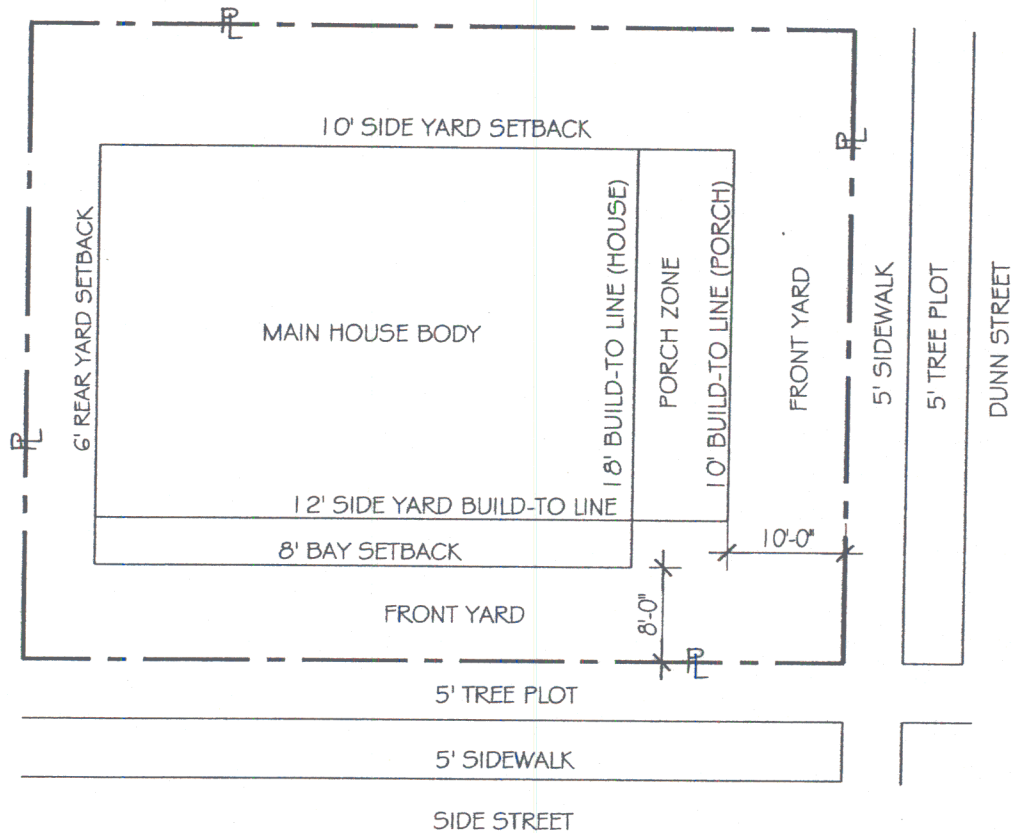
fig. 4e SOUTH PALMER STREET STREETScape



PALMER STREET LOOKING NORTH

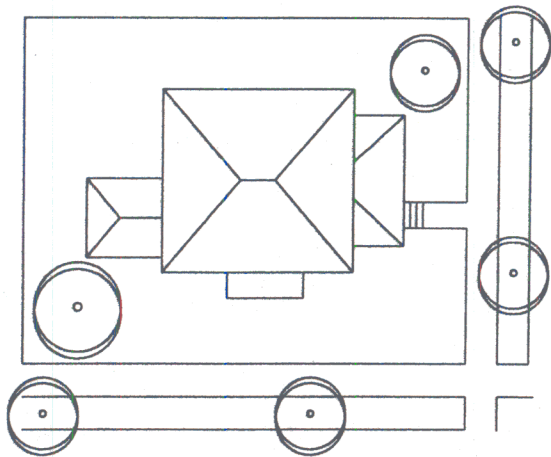


fig. 4 CORNER LOT



REGULATING PLAN

SCALE: 1/16" = 1'-0"



TYPICAL SITE PLAN

NO SCALE

SPECIFICATIONS:

LOT SIZE: APPROX. 53' x 70'

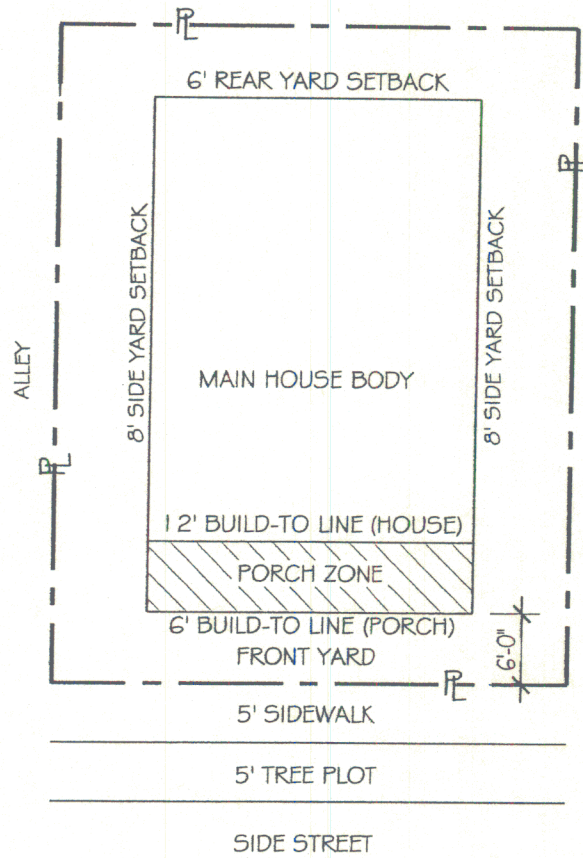
HOUSE TYPES A, B & C PERMITTED. REFER TO HOUSE TYPES FOR MAX. SQUARE FEET ALLOWED.

ENCROACHMENTS: PORCH STEPS MAY EXTEND INTO THE FRONT YARD AREA.

NO GARAGE PERMITTED ON THE CORNER LOT. A GARAGE FOR THE CORNER HOUSE IS PERMITTED ON THE ADJACENT SIDE STREET LOT WHEN BOTH LOTS HAVE THE SAME OWNER.

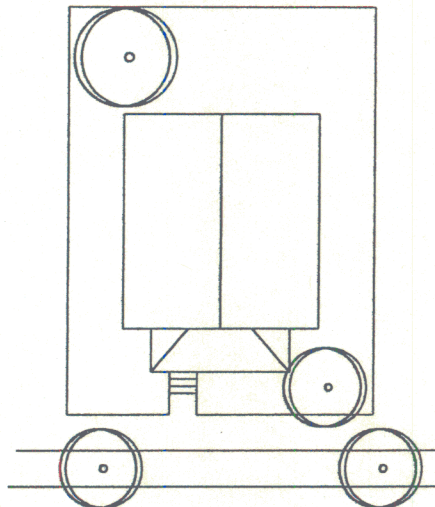
8' BAY SETBACK FOR A SIDE BAY THAT IS A ONE- OR TWO-STORY APPENDAGE, 4' DEEP MAXIMUM WITH A WIDTH THAT EQUALS 30-45% OF THE OVERALL HOUSE WIDTH.

fig. 5 SIDE STREET LOT - HOUSE



REGULATING PLAN

SCALE: 1/16" = 1'-0"



TYPICAL SITE PLAN

NO SCALE

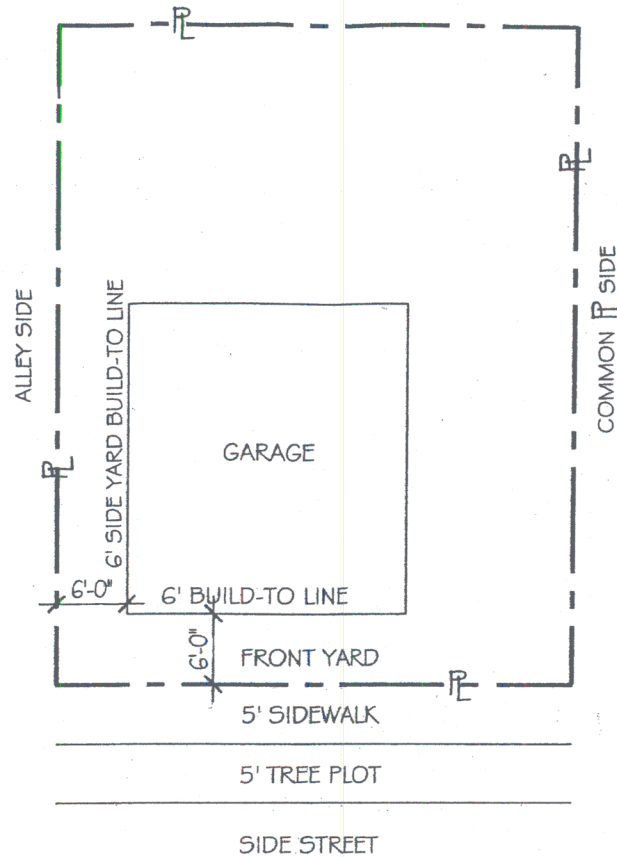
SPECIFICATIONS:

LOT SIZE: APPROX. 43' x 55'

HOUSE TYPE A, D & E PERMITTED. REFER TO HOUSE TYPES FOR MAX. SQUARE FEET ALLOWED.

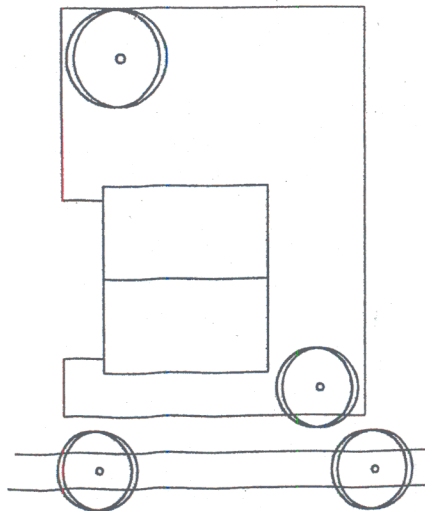
ENCROACHMENTS: PORCH STEPS MAY EXTEND INTO THE FRONT YARD AREA.

fig. 6 SIDE STREET LOT - GARAGE



REGULATING PLAN

SCALE: 1/16" = 1'-0"



TYPICAL SITE PLAN

NO SCALE

SPECIFICATIONS:

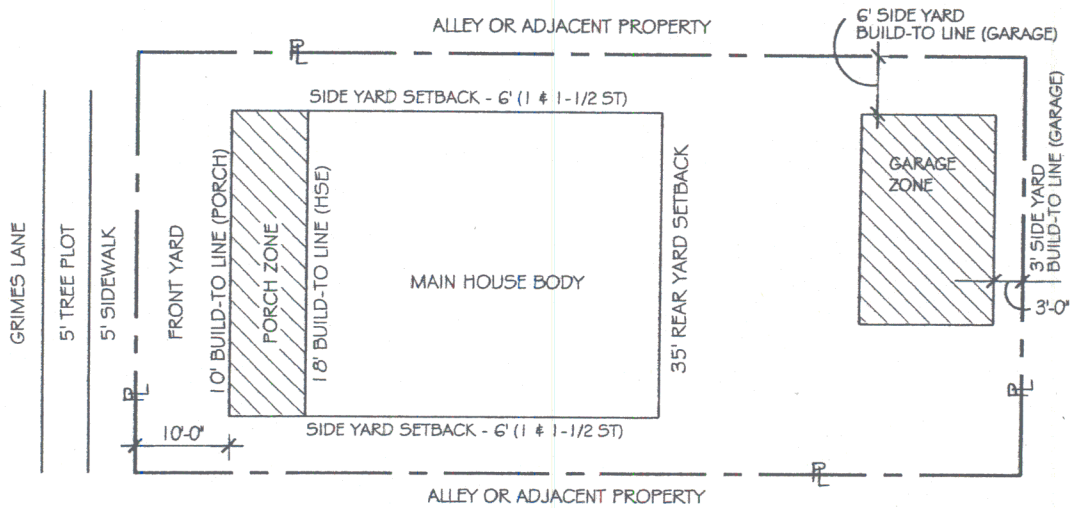
A GARAGE OR STUDIO/APARTMENT OVER GARAGE IS PERMITTED ON A SIDE STREET LOT THAT IS OWNED BY THE SAME OWNER OF THE ADJACENT CORNER LOT.

LOT SIZE: APPROX. 43' x 55'

GARAGE PERMITTED. REFER TO GARAGE TYPES FOR SQUARE FEET ALLOWED.

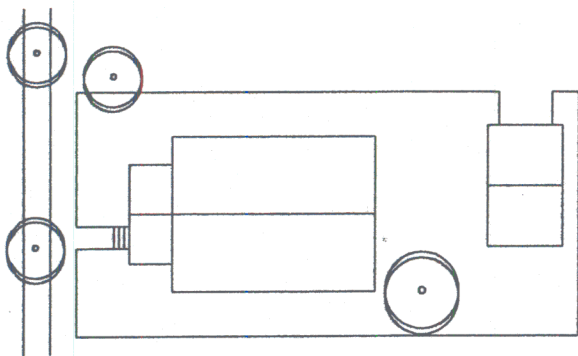
ENCROACHMENTS: SECOND STORY BALCONY, EXTERIOR STAIR, AND POTTING SHED MAY EXTEND INTO BACKYARD AREA.

fig. 8 GRIMES LANE LOT - SCENARIO A



### SCENARIO A: REGULATING PLAN

SCALE: 1" = 20'



TYPICAL SITE PLAN  
NO SCALE

### SPECIFICATIONS:

LOT SIZE: APPROX. 43' x 94'

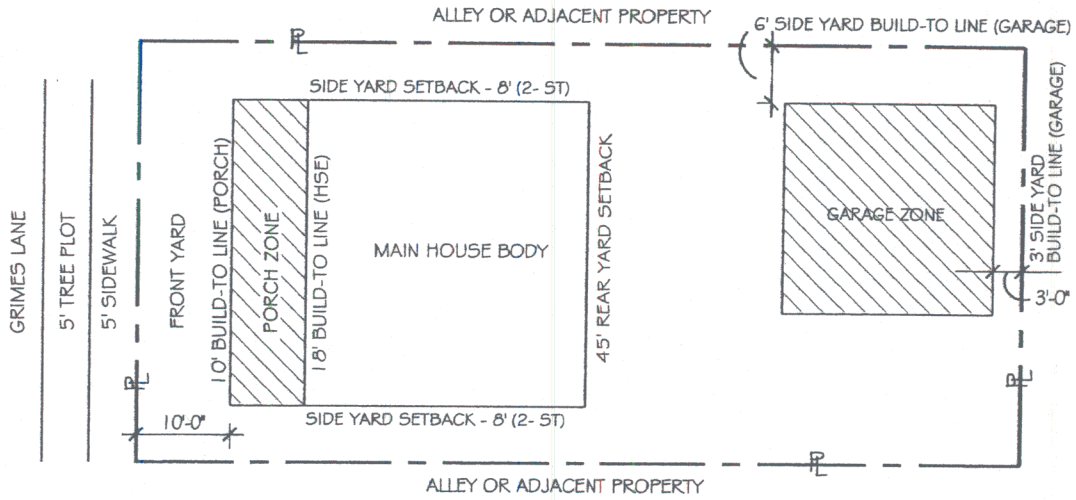
HOUSE TYPES A AND B WITH ONE CAR GARAGE PERMITTED. REFER TO HOUSE TYPE FOR MAXIMUM FOOT PRINT ALLOWED WITHIN SETBACKS.

GARAGE TO BE SIDE-LOADING AND BUILT ON EITHER THE EAST BUILD-TO LINE OR THE WEST BUILD-TO LINE.

ENCROACHMENTS: PORCH STEPS MAY EXTEND INTO THE FRONT YARD AREA. POTTING SHED AT GARAGE MAY EXTEND INTO BACKYARD AREA.

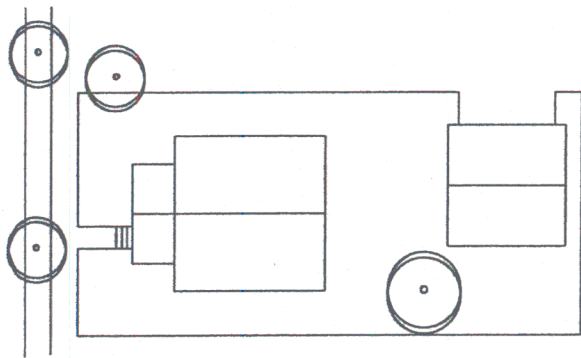


# fig. 9 GRIMES LANE LOT - SCENARIO B



## SCENARIO B: REGULATING PLAN

SCALE: 1" = 20'



## TYPICAL SITE PLAN

NO SCALE

## SPECIFICATIONS:

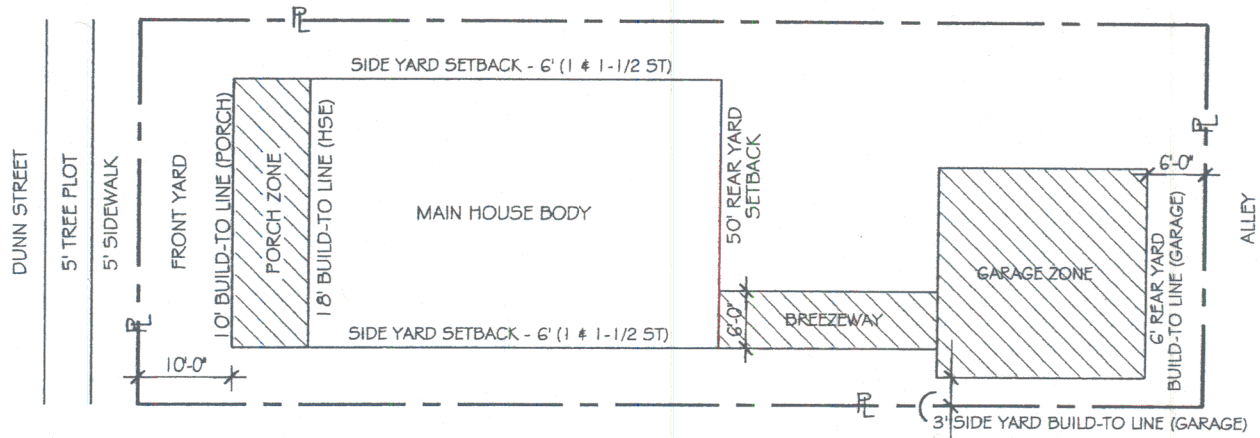
LOT SIZE: APPROX. 43' x 94'

HOUSE TYPE C WITH TWO CAR GARAGE PERMITTED. REFER TO HOUSE TYPE FOR MAXIMUM FOOT PRINT ALLOWED.

GARAGE TO BE SIDE-LOADING AND BUILT ON EITHER THE EAST BUILD-TO LINE OR THE WEST BUILD-TO LINE.

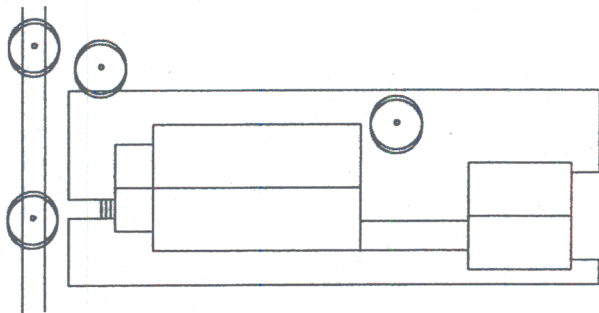
ENCROACHMENTS: PORCH STEPS MAY EXTEND INTO THE FRONT YARD AREA. POTTING SHED AT GARAGE MAY EXTEND INTO BACKYARD AREA.

fig. 10 LIVE / WORK LOT



### REGULATING PLAN

SCALE: 1" = 20'



### TYPICAL SITE PLAN

NO SCALE

### SPECIFICATIONS:

LOT SIZE: APPROX. 40' x 113'

HOUSE TYPES A OR B PERMITTED. REFER TO HOUSE TYPES FOR MAX. SQUARE FEET ALLOWED.

ENCROACHMENTS: PORCH STEPS MAY EXTEND INTO THE FRONT YARD AREA.

6'-MAX. WIDE OPEN OR 70% GLASS TRANSPARENT BREEZEWAY CONNECTION ALLOWED FROM HOUSE TO GARAGE.

# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Fig. 11 Exterior Materials List for Houses

Foundations: Split-face concrete masonry units above grade

Front Porch Floor: 1 x 4 tongue and groove, painted

Porch Ceiling: Bead board

Porch Construction: Trim, Rail and Boxed Columns - Wood or fiber-cement board, smooth side exposed, painted.

Masonry Columns and Partial Wall – Standard brick, random ashlar or cut limestone with limestone caps to be approved

Rear Deck or Porch: Treated lumber, painted

Exterior Cladding: Drop, flush or lap wood or fiber-cement board lap siding, 4" to 8" exposure, smooth side exposed, painted

Plain shingles, painted: Refer to houses elevations for permitted locations  
or

Standard brick with brick or limestone heads and sills to be approved

Windows: Wood, clad wood, or vinyl windows with or without simulated divided lites; Typical double hung windows vertical in proportion; Casement or awning windows limited to special locations to be approved

Exterior Doors: Full-lite door like French door style with clear glass with or without simulated divided lites; Storm and screen doors to match door elevation

Window, Door and Vent Trim: Wood or fiber cement board trim, smooth side exposed, painted

1 x 6 head with drip cap

1 x 4 jambs

1-1/2" sill

Exterior Trim: Wood or fiber cement board, smooth side exposed, painted

1 x 4 or 1x 6 corner trim

1 x 10 or 1 x 12 skirt board with water table

2 x 6 or 2 x 8 fly rafter

Exposed rafter tails for bungalows, one story, and one and a half story garages

Vents: Pre-manufactured composite or metal rectangular vent, vertical in proportion

Roof: Standard 3-tab, non-dimensional, asphalt shingle roof



fig. 12 HOUSE TYPE A - ONE-STORY BUNGALOW



TYPICAL FRONT ELEVATION

SCALE: 1/8" = 1'-0"

VARIABLE DESIGN COMPONENTS TO BE REFERENCED ALONG WITH MATERIALS LIST, FIG. 11

EXTERIOR CLADDING MATERIAL - WOOD OR FIBER-CEMENT BOARD, BRICK  
PORCH CONSTRUCTION - WOOD OR FIBER-CEMENT BOARD, BRICK, LIMESTONE  
MAIN ROOF TYPE - HIP, GABLE  
MAIN ROOF PITCH - HIP, 5:12 TO 7:12; GABLE, 6:12 TO 8:12  
PORCH ROOF TYPE - HIP, GABLE  
PORCH ROOF PITCH - HIP, 3.5:12 TO 4:12; GABLE, 3.5:12 TO 6:12  
PORCH DETAIL - WOOD RAIL (MUST BE APPROVED) NO RAIL (IF PERMISSIBLE BY GRADE)  
PORCH WIDTH - 70% TO 85% OF HOUSE WIDTH  
ROOF COLOR  
EXTERIOR PAINT SCHEME  
WINDOWS - WOOD, CLAD WOOD OR VINYL; WITH OR WITHOUT SIMULATED DIVIDED LITES  
WINDOW TYPES - DOUBLE HUNG WINDOWS; CASEMENT OR AWNING WINDOWS LIMITED TO SPECIAL LOCATIONS  
WINDOW ARRANGEMENT  
TRIM PATTERN - WOOD SHINGLES PERMITTED IN PORCH GABLE

PERMITTED LOT LOCATIONS: ALL

fig. 13  
HOUSE TYPE B - ONE AND A HALF STORY BUNGALOW



TYPICAL FRONT ELEVATION

SCALE: 1/8" = 1'-0"

VARIABLE DESIGN COMPONENTS

TO BE REFERENCED ALONG WITH  
MATERIALS LIST, FIG. 11

EXTERIOR CLADDING MATERIAL - WOOD OR FIBER-CEMENT BOARD, BRICK  
PORCH CONSTRUCTION - WOOD OR FIBER-CEMENT BOARD, BRICK, LIMESTONE  
MAIN ROOF PITCH - 9:12 TO 12:12 GABLE  
DORMER ROOF TYPE: SHED, GABLE  
DORMER ROOF PITCH: 3.5:12 SHED; 5:12 TO 7:12 GABLE  
PORCH ROOF TYPE - HIP, GABLE  
PORCH ROOF PITCH - HIP, 3.5:12 TO 4:12; GABLE, 3.5:12 TO 6:12  
PORCH DETAIL - WOOD RAIL (MUST BE APPROVED) NO RAIL (IF PERMISSIBLE BY GRADE)  
PORCH WIDTH - 70% TO 85% OF HOUSE WIDTH  
ROOF COLOR  
EXTERIOR PAINT SCHEME  
WINDOWS - WOOD, CLAD WOOD OR VINYL; WITH OR WITHOUT SIMULATED DIVIDED LITES  
WINDOW TYPE - DOUBLE HUNG WINDOWS; CASEMENT OR AWNING WINDOWS LIMITED TO  
SPECIAL LOCATIONS  
WINDOW ARRANGEMENT  
TRIM PATTERN - SHINGLES PERMITTED IN PORCH GABLE

PERMITTED LOT LOCATIONS: CORNER LOT, 40' & 42' MID BLOCK LOTS, GRIMES LANE LOT, LIVEWORK LOT

fig. 14 HOUSE TYPE C - FOURSQUARE



TYPICAL FRONT ELEVATION

SCALE: 1/8" = 1'-0"

VARIABLE DESIGN COMPONENTS TO BE REFERENCED ALONG WITH MATERIALS LIST, FIG. 11

EXTERIOR CLADDING MATERIAL - WOOD OR FIBER-CEMENT BOARD, BRICK  
PORCH CONSTRUCTION - WOOD OR FIBER-CEMENT BOARD, BRICK, LIMESTONE  
PORCH ROOF TYPE - HIP, GABLE  
PORCH ROOF PITCH - HIP, 3.5:12 TO 4:12; GABLE, 3.5:12 TO 6:12  
PORCH DETAIL - WOOD RAIL (MUST BE APPROVED) NO RAIL (IF PERMISSIBLE BY GRADE)  
PORCH WIDTH - 70% TO 85% OF HOUSE WIDTH  
ROOF COLOR  
EXTERIOR PAINT SCHEME  
WINDOWS - WOOD, CLAD WOOD, OR VINYL; WITH OR WITHOUT SIMULATED DIVIDED LITES  
WINDOW TYPES - DOUBLE HUNG WINDOWS; CASEMENT OR AWNING WINDOWS LIMITED TO SPECIAL LOCATIONS  
WINDOW ARRANGEMENT  
TRIM PATTERN - VARIABLE SIDING EXPOSURES, SHINGLE PERMITTED ABOVE SECOND FLOOR  
HORIZONTAL BAND, SHINGLE PERMITTED IN PORCH GABLE

PERMITTED LOT LOCATIONS: CORNER LOT, 42' MID BLOCK LOT, GRIMES LANE LOT

fig. 15  
HOUSE TYPE D - ONE AND A HALF STORY COTTAGE



TYPICAL FRONT ELEVATION

SCALE: 1/8" = 1'-0"

VARIABLE DESIGN COMPONENTS TO BE REFERENCED ALONG WITH MATERIALS LIST, FIG. 11

EXTERIOR CLADDING MATERIAL - WOOD OR FIBER-CEMENT BOARD, BRICK  
PORCH CONSTRUCTION - WOOD OR FIBER-CEMENT BOARD, BRICK, LIMESTONE  
MAIN ROOF PITCH - 9:12 TO 12:12 GABLE  
DORMER ROOF TYPE: SHED, GABLE  
DORMER ROOF PITCH: 3.5:12 SHED; 5:12 TO 7:12 GABLE  
PORCH ROOF TYPE - HIP, GABLE  
PORCH ROOF PITCH - HIP, 3.5:12 TO 4:12; GABLE, 3.5:12 TO 6:12  
PORCH DETAIL - WOOD RAIL (MUST BE APPROVED) NO RAIL (IF PERMISSIBLE BY GRADE)  
PORCH WIDTH - 70% TO 85% OF HOUSE WIDTH  
ROOF COLOR  
EXTERIOR PAINT SCHEME  
WINDOWS - WOOD, CLAD WOOD OR VINYL; WITH OR WITHOUT SIMULATED DIVIDED LITES  
WINDOW TYPES - DOUBLE HUNG WINDOWS; CASEMENT OR AWNING WINDOWS LIMITED TO SPECIAL LOCATIONS  
WINDOW ARRANGEMENT  
TRIM PATTERN - SHINGLES PERMITTED IN PORCH GABLE

PERMITTED LOT LOCATION: SIDE STREET LOT



# fig. 16 HOUSE TYPE E - TWO STORY COTTAGE



## TYPICAL FRONT ELEVATION

SCALE: 1/8" = 1'-0"

### VARIABLE DESIGN COMPONENTS TO BE REFERENCED ALONG WITH MATERIALS LIST, FIG. 11

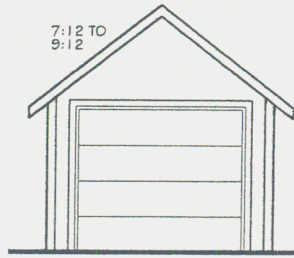
EXTERIOR CLADDING MATERIAL - WOOD OR FIBER-CEMENT BOARD, BRICK  
PORCH CONSTRUCTION - WOOD OR FIBER-CEMENT BOARD, BRICK, LIMESTONE  
PORCH ROOF TYPE - HIP, GABLE  
PORCH ROOF PITCH - HIP, 3.5:12 TO 4:12; GABLE, 4:12 TO 6:12  
PORCH DETAIL - WOOD RAIL (MUST BE APPROVED) NO RAIL (IF PERMISSIBLE BY GRADE)  
PORCH WIDTH - 70% - 85% OF HOUSE WIDTH  
ROOF COLOR  
EXTERIOR PAINT SCHEME  
WINDOWS - WOOD, CLAD WOOD OR VINYL; WITH OR WITHOUT SIMULATED DIVIDED LITES  
WINDOW TYPES - DOUBLE HUNG WINDOWS; CASEMENT OR AWNING WINDOWS LIMITED TO SPECIAL LOCATIONS  
WINDOW ARRANGEMENT  
TRIM PATTERN - VARIABLE SIDING EXPOSURES, SHINGLES PERMITTED ABOVE SECOND FLOOR

PERMITTED LOT LOCATION: SIDE STREET LOT

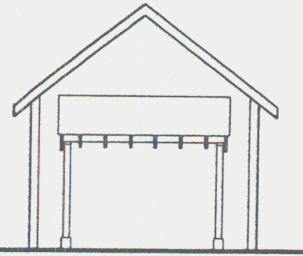


# fig. 17 GARAGE TYPES

## SAMPLE FRONT ELEVATIONS



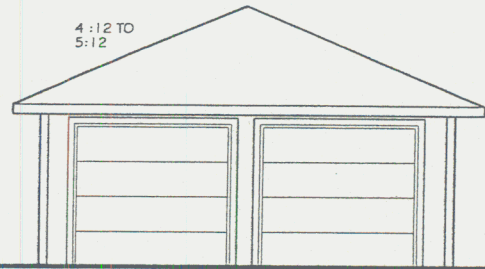
1-CAR GARAGE



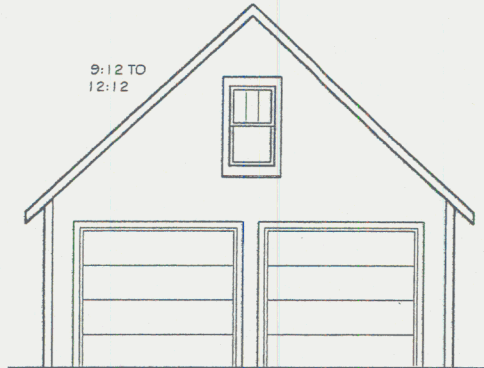
1-CAR GARAGE WITH  
POTTING SHED



2-CAR GARAGE WITH/  
GABLE ROOF



2-CAR GARAGE WITH/  
HIP ROOF



ONE AND A HALF STORY  
2-CAR GARAGE

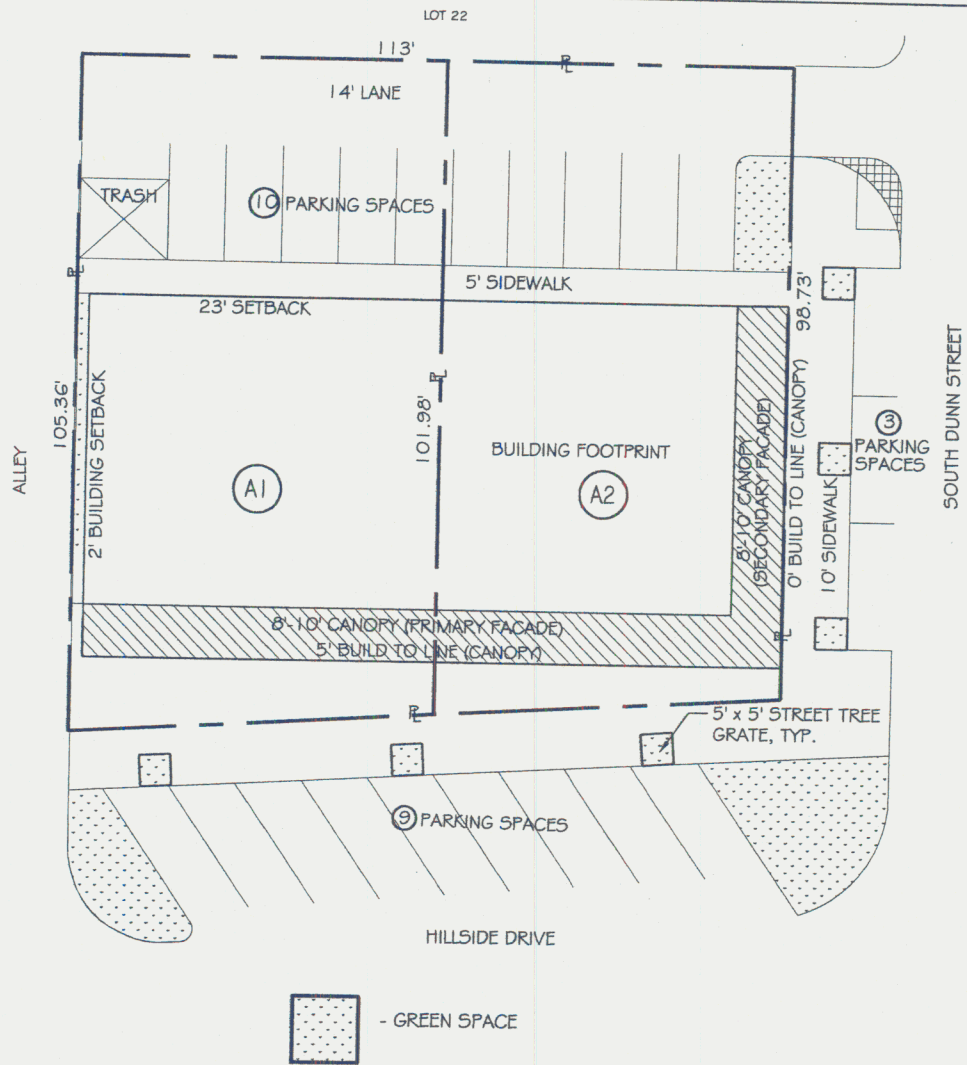


TWO STORY OFFICE/ STUDIO APT.  
OVER GARAGE



ONE AND A HALF STORY OFFICE/  
STUDIO APT. OVER GARAGE

fig. 18 LOT A - STOREFRONT TYPE



REGULATING PLAN

SCALE: 1" = 30'



SPECIFICATIONS:

LOT SIZE: 2 LOTS AT APPROX. 58' x 86' EACH

STOREFRONT BUILDING TYPE; MAXIMUM FOOT PRINT 5650 SQUARE FEET.

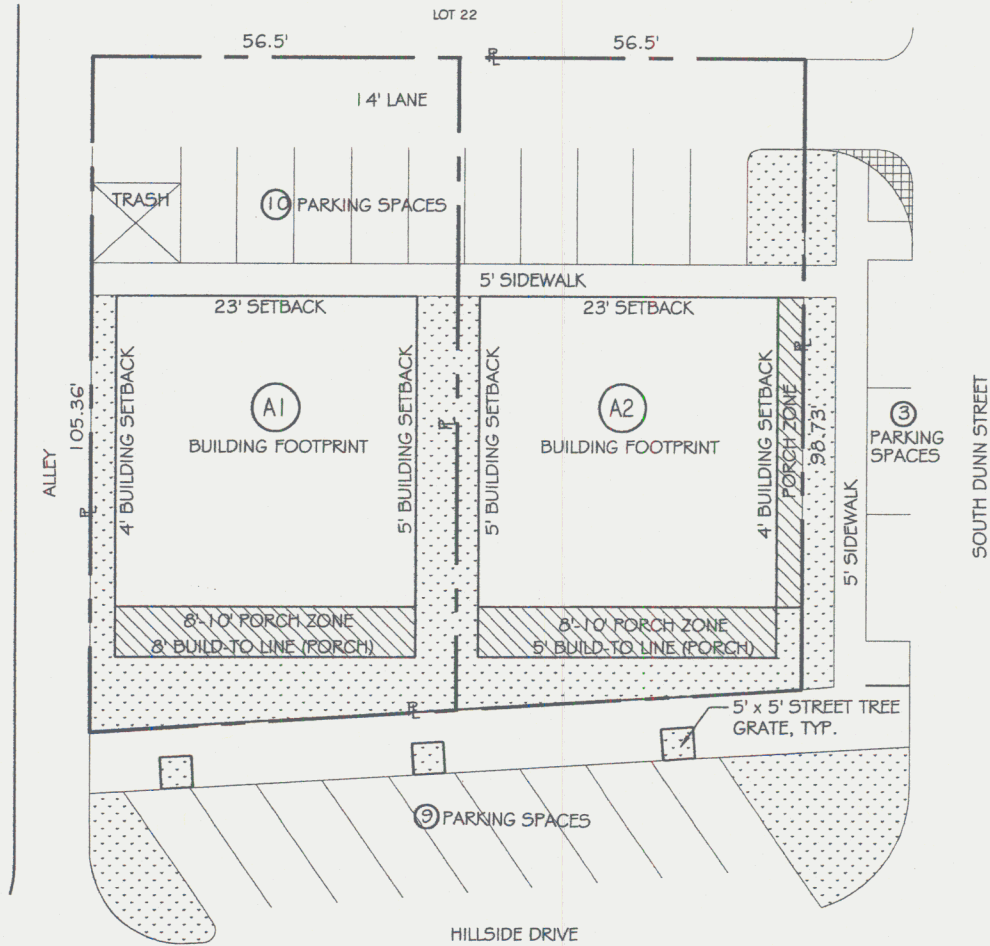
CANOPY: 8' - 10' DEEP ON PRIMARY & SECONDARY FACADE.

DEVELOPMENT CONTROLS: 1ST FLOOR - UP TO 5650 SQUARE FEET OF COMMERCIAL SPACE  
OR UP TO (5) STUDIO APARTMENTS CONDITIONAL  
2ND FLOOR - UP TO (5) 2-BEDROOM APARTMENTS

Individual  
Site Plans

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# fig. 19 LOT A - RESIDENTIAL BUILDING TYPE



## REGULATING PLAN

SCALE: 1" = 30'



### SPECIFICATIONS:

LOT SIZE: 2 LOTS AT APPROX. 58' x 86' EACH

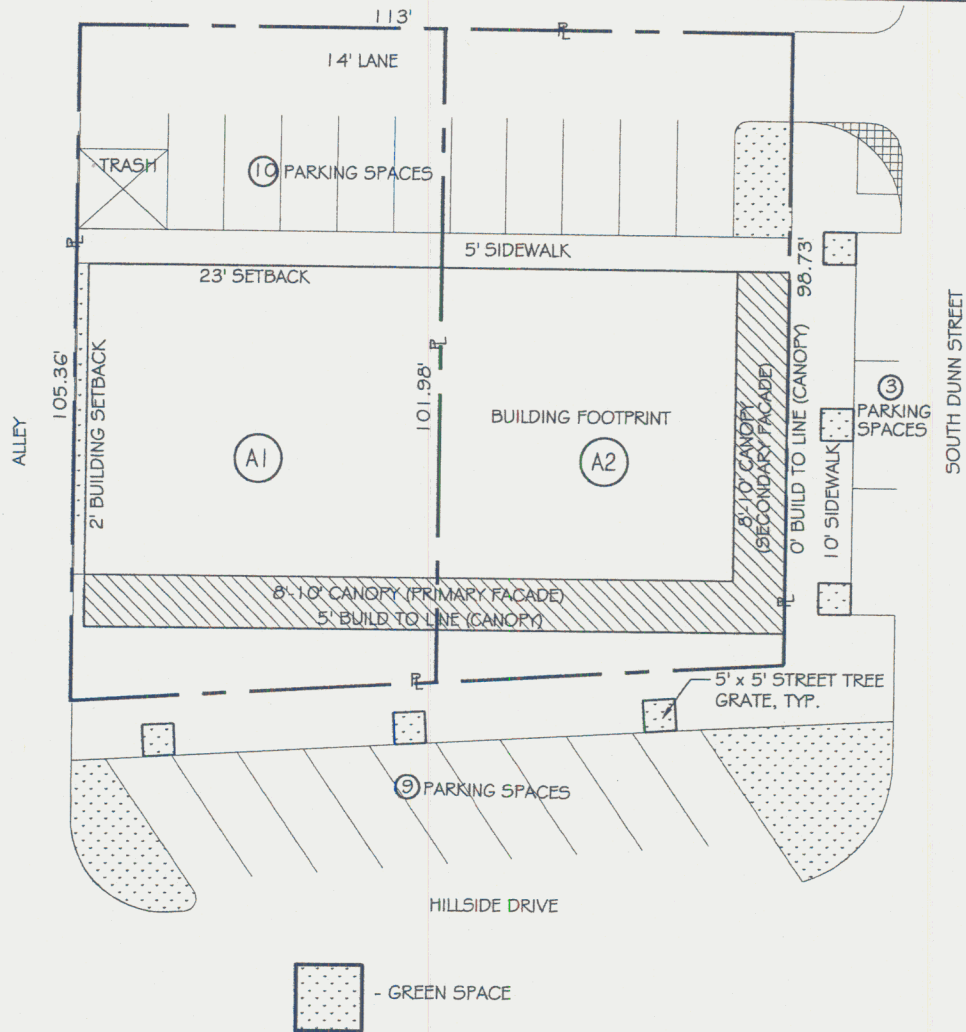
RESIDENTIAL BUILDING TYPE; MAXIMUM BUILDING FOOT PRINT 2360 SQUARE FEET

DEVELOPMENT CONTROLS: 1ST FLOOR - UP TO 5650 SQUARE FEET OF COMMERCIAL SPACE  
 OR UP TO (4) STUDIO APARTMENTS CONDITIONAL  
 OR (4) 2-BEDROOM APARTMENTS  
 2ND FLOOR - UP TO (4) 2-BEDROOM APARTMENTS

SIGN LOCATION: LAWN AREA IN FRONT OF EACH BUILDING

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fig. 20 LOT B - STOREFRONT TYPE



REGULATING PLAN

SCALE: 1" = 30'

SPECIFICATIONS:

LOT SIZE: 2 LOTS AT 58' x 87' EACH

STOREFRONT BUILDING TYPE; MAXIMUM FOOT PRINT 4500 SQUARE FEET

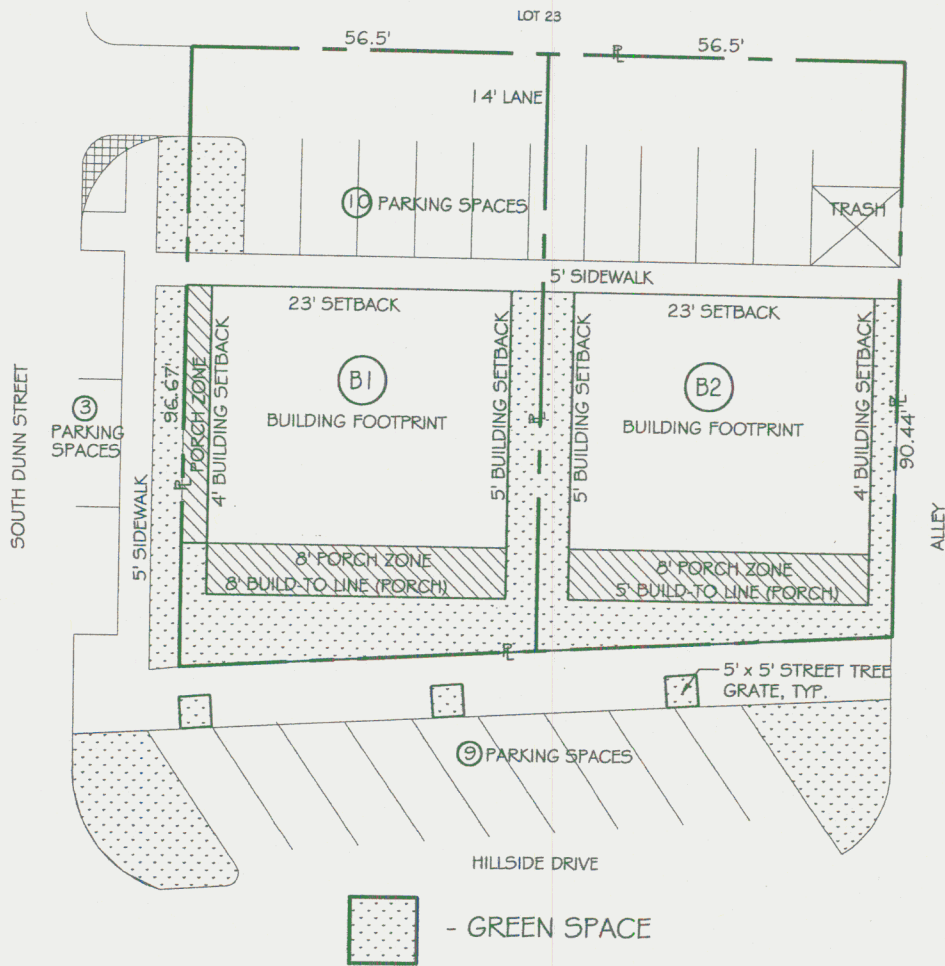
CANOPY: 8' - 10' DEEP ON PRIMARY & SECONDARY FACADE

DEVELOPMENT CONTROLS: 1ST FLOOR - UP TO 4500 SQUARE FEET OF COMMERCIAL SPACE  
OR (5) STUDIO APARTMENTS CONDITIONAL  
2ND FLOOR - UP TO (5) 2-BEDROOM APARTMENTS

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fig. 21 LOT B - RESIDENTIAL BUILDING TYPE



REGULATING PLAN

SCALE: 1" = 30'



SPECIFICATIONS:

LOT SIZE: 2 LOTS AT 58' x 87' EACH

RESIDENTIAL BUILDING TYPE; MAXIMUM BUILDING FOOT PRINT 1860 SQUARE FEET

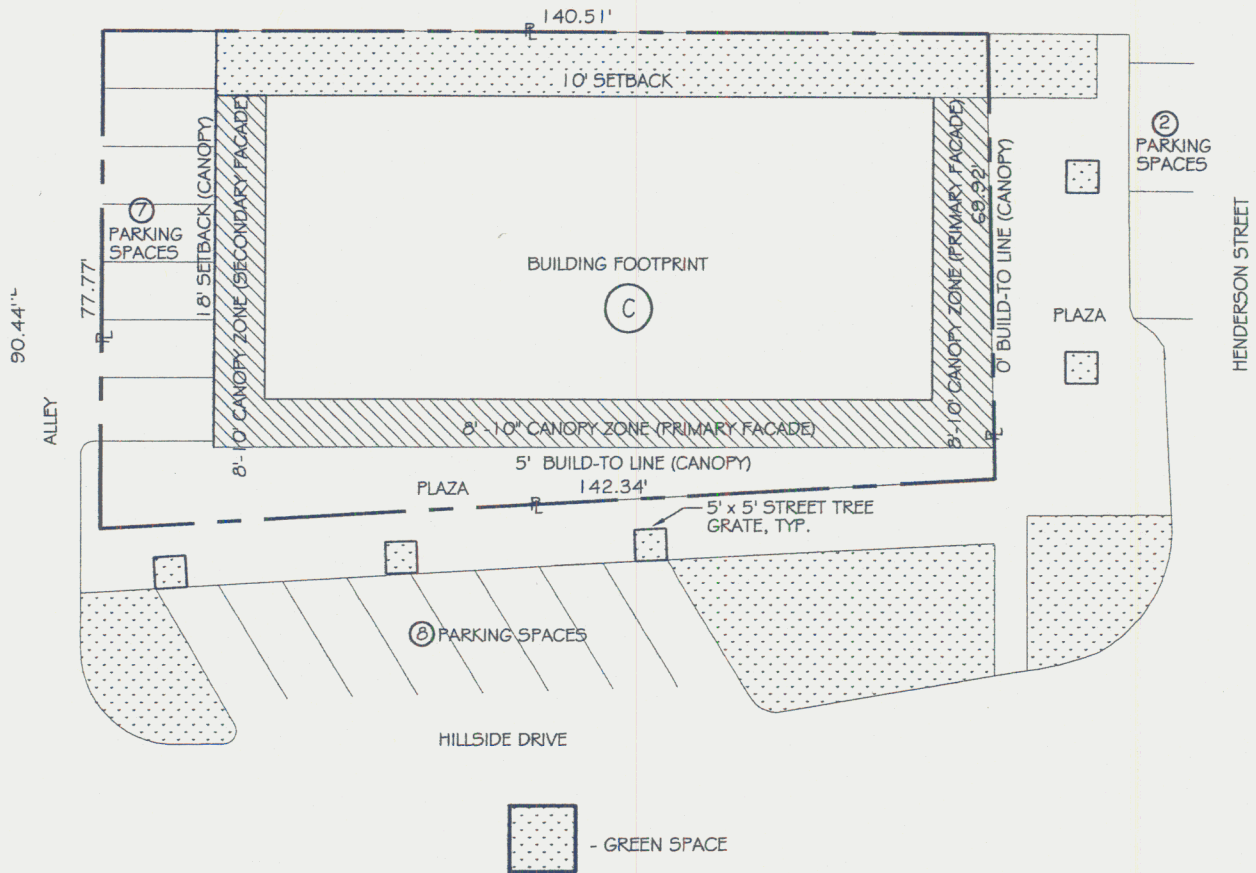
DEVELOPMENT CONTROLS: 1ST FLOOR - UP TO 4500 SQUARE FEET OF COMMERCIAL SPACE  
OR UP TO (4) STUDIO APARTMENTS CONDITIONAL  
2ND FLOOR - UP TO (4) 2-BEDROOM APARTMENTS

SIGN LOCATION: LAWN AREA IN FRONT OF EACH BUILDING

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fig. 22 LOT C - STOREFRONT TYPE



REGULATING PLAN

SCALE: 1" = 30'



SPECIFICATIONS:

LOT SIZE: APPROX. 89' x 140'

STOREFRONT BUILDING TYPE; MAXIMUM FOOT PRINT 5500 SQUARE FEET

CANOPY: 8' - 10' DEEP ON PRIMARY & SECONDARY FACADE

DEVELOPMENT CONTROLS: BASEMENT - UP TO 5500 SQUARE FEET OF STORAGE/OFFICE SPACE  
 1ST FLOOR - UP TO 5500 SQUARE FEET OF COMMERCIAL SPACE  
 OR UP TO (4) STUDIO APARTMENTS CONDITIONAL  
 2ND FLOOR - UP TO (6) 2-BEDROOM APARTMENTS

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fig. 23 RESIDENTIAL BUILDING TYPE



HILLSIDE DRIVE ELEVATION

SCALE: 3/32" = 1'-0"

## SPECIFICATIONS

NUMBER OF STORIES: 2 (PLUS BASEMENT)

1ST FLOOR: COMMERCIAL OR RESIDENTIAL

2ND FLOOR: RESIDENTIAL

1ST FLOOR TO SECOND FLOOR HEIGHT: 10' - 14'

2ND FLOOR CEILING HEIGHT: 8' - 12'

OVERALL BUILDING HEIGHT: 36'-0"

EXTERIOR CLAD MATERIALS: BRICK, WOOD/ FIBER-CEMENT BOARD SIDING, EIFS

PORCH CONSTRUCTION: WOOD/ FIBER-CEMENT BOARD OR BRICK

ROOF TYPE & PITCH: HIP 4:12 to 6:12, *GABLE, FLAT w/PARAPET*

PORCH ROOF TYPE: HIP 4:12 to 6:12, *etc.*

SIGNAGE LOCATIONS: ENTRY CANOPY, HANGING SIGN PERPENDICULAR TO FACADE, ON PORCH FLOOR ABOVE,  
GROUND SIGN IN LAWN IN FRONT OF BUILDING



## fig. 24 STOREFRONT BUILDING TYPE



### STREET ELEVATION

SCALE: 1/16" = 1'-0"

## SPECIFICATIONS

- NUMBER OF STORIES: 2 (PLUS BASEMENT)
- 1ST FLOOR TO 2ND FLOOR HEIGHT: 10' - 14'
- 2ND FLOOR CEILING HEIGHT: 8' - 12'
- OVERALL BUILDING HEIGHT: 32' - 36' FROM FIRST FLOOR
- NUMBER OF BAYS: 4 OR 5
- EXTERIOR CLADDING MATERIALS: BRICK, GROUND FACE CMU, LIMESTONE, EIFS
- EXTERIOR ACCENT MATERIALS: BRICK, GROUND FACE CMU, LIMESTONE, EIFS
- PARAPET WALL PROFILES: FLAT, STEPPED OR CURVED
- STOREFRONT WINDOW OPENINGS ON FIRST FLOOR: % OPENINGS
- SIGNAGE LOCATIONS: FRONT CANOPY EDGE, ON TRANSOM GLASS,  
HANGING SIGN PERPENDICULAR TO FACADE
- CANOPY: 8'-10' DEEP ALONG PRIMARY & SECONDARY FACADES,  
CANTILEVERED OR POST AND BEAM SUPPORTED
- MATERIALS: CANVAS, VINYL, STEEL, GLASS
- CONTOUR OF SITE: STEP BUILDING BAYS AS NEEDED TO WORK WITH SLOPE OF SITE  
OR PROVIDE WALK-OUT WHERE POSSIBLE



Building Type  
Scenarios 46

### HILLSIDE DRIVE ELEVATIONS



LOT A - STOREFRONT TYPE; LOT B - STOREFRONT TYPE; LOT C - STOREFRONT TYPE



LOT A - RESIDENTIAL TYPE; LOT B - STOREFRONT TYPE; LOT C - STOREFRONT TYPE



LOT A - RESIDENTIAL TYPE; LOT B - RESIDENTIAL TYPE; LOT C - STOREFRONT TYPE

# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## School Zone Traffic Improvements

May 28, 2004

The angled parking proposal in the South Dunn Street PUD aims to provide a traffic calming measure for Hillside Drive. Additional measures for better recognition of the Templeton School zone may be considered by the City of Bloomington in connection to this project. Below is a list of suggested options:

### 1. Signage

- "30/20 MPH Speed Limit" on Hillside, west of Henderson in both directions
- "No U-Turn Allowed" for Eastbound traffic on Hillside
- "Speeding Fines Doubled in School Zone" - *State requirement?*
- "School Speed Limit When Flashing" (w/ flashing lights)

### 2. Striping

- Striping added to delineate travel lanes
- Striping added to delineate back-out zone at NAC
- Improved crosswalk delineation, such as broad white stripes perpendicular to flow of traffic

### 3. Other Methods

- Actual speed posted by radar
- No right turn on red permitted in any direction at intersection



# SOUTH DUNN STREET

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NEIGHBORHOOD SOLUTIONS, LLC

## Appendix D

### Mountable Curbs at Intersections

June 1, 2004

A mountable curb is an extension of the curb at the corner of an intersection that works to slow motor vehicles and gives pedestrians a shorter distance to cross. It provides two radii at the corner: the larger radius provides a larger turning area that accommodates fire truck and emergency vehicle access when needed; the smaller radius serves to restrict cars and give a protected zone to pedestrians trying to cross the street. This design makes pedestrian crossings easier by providing a visual clue to motorists that they are on a non-arterial route. The mountable curb should be executed in a contrasting material to provide additional emphasis.





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# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Mixed-Use Buildings Financial Table Estimated Monthly Income

June 1, 2004

	<b>A: Retail Rental Income \$1/sf/month</b>	<b>B: Studio Rental Income \$600/unit/month</b>	<b>A-B: Retail Income Advantage</b>
<b>Lot A</b>	5650 sf = \$5650	5 units = \$3000	\$2650
<b>Lot B</b>	4500 sf = \$4500	5 units = \$3000	\$1500
<b>Lot C</b>	5500 sf = \$5500	6 units = \$3600	\$1900

# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Commercial Property Comparison Table

June 1, 2004

Address	Business Name	Lineal Feet of Street Frontage	Square Feet on Main Floor
309 East 3 <sup>rd</sup> Street	Tina's Carry-Out Cuisine	32	1,728
113 North Dunn Street	Bloomington Bagel Company	60	2,615
350 South Walnut Street	Summer House	28	3,120
118-120 North Rogers Street	formerly Insight	122	3,294
108 North Grant Street	Pygmalion's Art Supplies	134	3,400
401 West 7 <sup>th</sup> Street	Meridian Title Corporation	68	6,700
320 West 6 <sup>th</sup> Street	Encore Café	70	7,000
314 South Walnut Street	Lee's Martial Arts	60	7,920
211 South Rogers Street	formerly Grant Street	63	8,008
310 South Washington Street	Boxcar Books	72	9,984
350 West 11 <sup>th</sup> Street	Upland Brewing Company	100	10,719
421 East 3 <sup>rd</sup> Street	Coldstone Square	236	10,896

(Square footage numbers derived from Monroe County GIS website  
<http://in53.plexisgroup.com/map/monroe.mwf>)

\*\*\*The project proposes a maximum of 300 linear feet of commercial space total (includes all three lots). This street frontage is equal to approximately one side of the courthouse square.

# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Average Daily Trips Table

June 1, 2004

Type of Residence	Number of Units	7 Trips/Household*	10 Trips/Household*
Single Family Houses	43	301	430
Apartments	15	105	150
<b>TOTALS</b>		<b>406</b>	<b>580</b>

\*Average Daily Trips per household (ADT) is between 7-10 (as provided by Planning Staff).

The proposed project would provide 10 outlets (street and alley network) for vehicles to exit the neighborhood. Dividing the average daily trips shown above into the number of outlets, there would be an average of between 40 and 58 vehicles that would use each outlet on a daily basis.



# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Existing Driveway Cuts with and without Alley Network

June 8, 2004

Location	# of Blocks	# of Driveway Cuts	Alleys
Hillside (Walnut to Henderson) North side (alleys perpendicular) South side (no alleys)	6	5 22	Yes No
Henderson (Hillside to Grimes) West side (no alleys) East side (no alleys; includes Templeton School)	3	9 5	No No
Washington (Hillside to Grimes) West side (alleys parallel) East side (alleys parallel)	3	4 6	Yes Yes
Lincoln (Hillside to Grimes) West side (alleys parallel) East side (alleys parallel)	3	6 2	Yes Yes
Grant (Hillside to Grimes) West side (alleys parallel) East side (no alleys)	3	6 10	Yes No
Palmer (Hillside to Grimes) West side (no alleys) East side (no alleys)	3	15 16	No No
Proposed Dunn Street West side (alleys parallel) East side (alleys parallel)	3	1 1	Yes Yes

**Conclusion:** There are significantly less driveway cuts when an alley network is present.

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# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

## Existing Queuing Streets Table

June 8, 2004

This table represents only a sampling of queuing streets in the Bryan Park Neighborhood and in other Bloomington Core neighborhoods.

<b>Bryan Park Neighborhood</b>		
Streets	Width of Street	Parking on
Grant Btw. Grimes and Hillside	20'	one side
Palmer Btw. Grimes and Allen	18'	both sides
Dunn Btw. Grimes and Allen	19'	both sides
Stull Btw. Bryan Park and Wilson	20'	both sides
Park Btw. Bryan Park and Wilson	18'	both sides
Wilson btw. Park and Henderson	18'	both sides
Fess btw. Park and Henderson	21'	both sides
<b>Elm Heights Neighborhood</b>		
Streets	Width of Street	Parking on
Woodlawn South of Second	24'	one side
Second Street East of Woodlawn	24'	one side
Fess btw. Maxwell and 1 <sup>st</sup>	24'	one side
Hunter at Hawthorne	29'	both sides
University at Fess	22'	one side
Second Btw. Hawthorne and Woodlawn	24'	one side

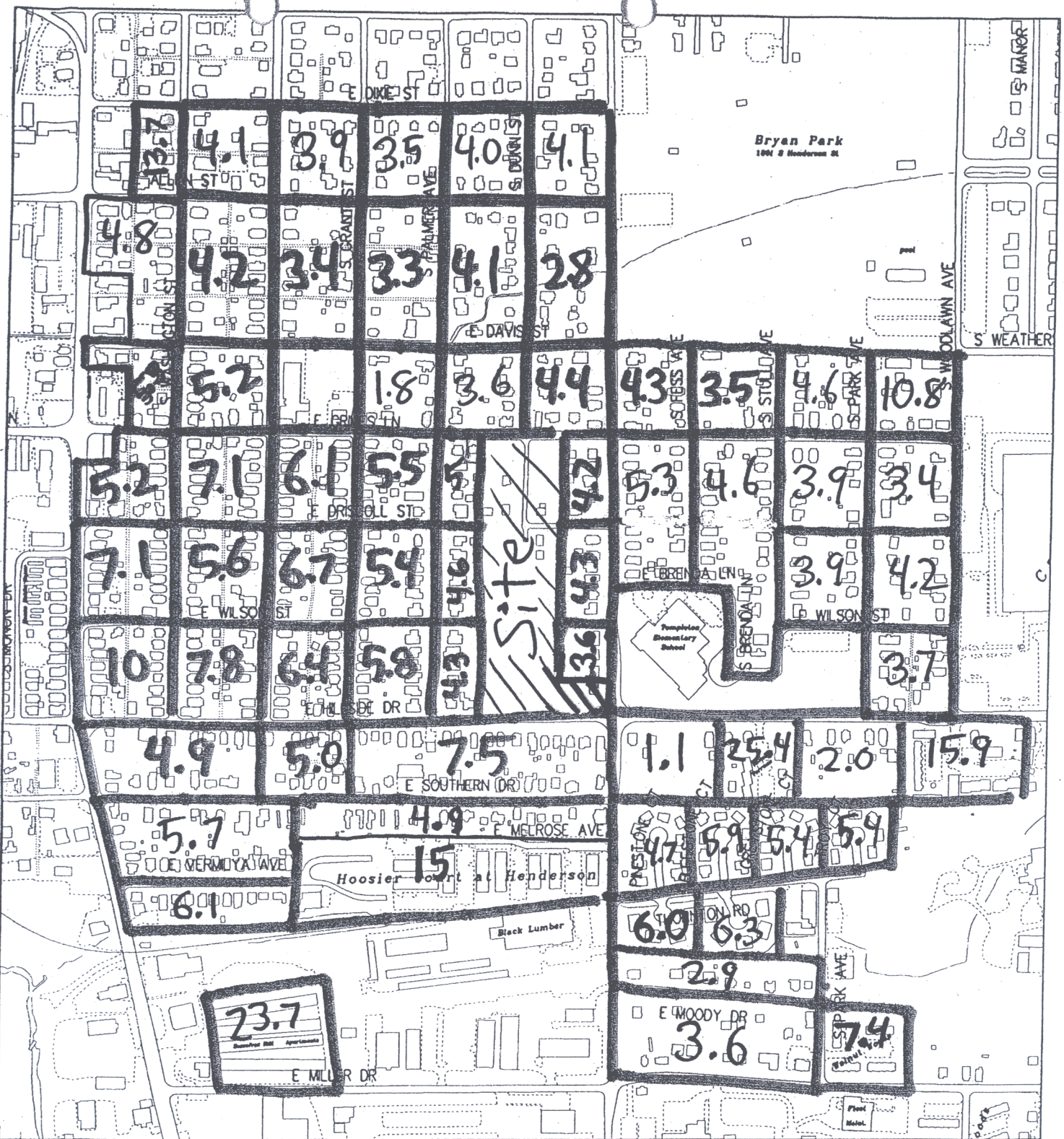
# SOUTH DUNN STREET

NEIGHBORHOOD SOLUTIONS, LLC

<b>Prospect Hill Neighborhood</b>		
<b>Streets</b>	<b>Width of Street</b>	<b>Parking on</b>
Maple btw. 2 <sup>nd</sup> and Kirkwood	20'	one side
Fourth btw. Jackson and Rose Hill	24'	one side
Third btw. Jackson and Rose Hill	20'	one side
<b>Near West Side Neighborhood</b>		
<b>Streets</b>	<b>Width of Street</b>	<b>Parking on</b>
Maple btw. Seventh and Nineth	22'	one side
Seventh btw. Fairview and Maple	24'	one side
Eighth btw. Fairview and Maple	24'	one side
<b>McDoel Gardens Neighborhood</b>		
<b>Streets</b>	<b>Width of Street</b>	<b>Parking on</b>
Wylie btw. Rogers and Fairview	20'	one side
Madison btw. Dodds and Allen	20'	both sides
Dodds btw. Rogers and Fairview	30'	both sides

55





**PUD-09-04**

1330 S. Dunn Street  
**Neighborhood Solutions LLC**  
 Surrounding densities in units per acre

By: roachja  
 29 Jun 03



City of Bloomington  
 Planning



Scale: 1" = 500'

For reference only; map information NOT warranted.

27



**Subject: South Dunn Street development proposal**

**Date:** Mon, 23 Feb 2004 22:39:54 -0500

**From:** "Jacqui Bauer" <jbauer@incap.org>

**To:** <alonsoj@bloomington.in.gov>, <mayor@bloomington.in.gov>, <micudat@bloomington.in.gov>, <walkerr@bloomington.in.gov>, <wykoffj@bloomington.in.gov>

**CC:** <haydenb@bloomington.in.us>, <evans@indiana.edu>

Hello!

I'm contacting you as a homeowner in the Bryan Park Neighborhood. My husband and I are very much in favor of the development being proposed by Matt Press, including the following:

- \*City approval for mixed-use development along Hillside,
- \*A city decision to vacate the city land along Hillside, in order to make mixed-use development feasible,
- \*Allowance of streetside parking along Hillside and in front of the proposed mixed-use development, and
- \*Permitting connectivity in north-south alleys to allow as many traffic options as possible.

My husband and I both strongly support this type of traditional neighborhood development, and think it is in keeping with Bloomington's Growth Policies Plan.

Please let me know if there is any additional information I can provide. Thank you for your consideration of this important issue.

Jacqui Bauer  
Tom Evans  
724 S. Washington  
812.323.0232



February 23, 2004

Mayor Mark Kruzan  
City of Bloomington  
401 North Morton Street, Suite 210  
Bloomington, IN 47404

Dear Mayor Kruzan:

The Bryan Park Neighborhood Association met recently to discuss neighborhood issues, including the proposed development of South Dunn Street. The BPNA believes that, as your administration begins to make decisions that will affect the development of South Dunn Street and the Bryan Park neighborhood, it is important for you to know of the Association's support for Matt Press' proposal.

The BPNA strongly supports the proposal for neighborhood-serving mixed use on Hillside Drive. We believe that this is a unique opportunity to enhance the health of our core neighborhood and reflects the vision of the Growth Policies Plan to strengthen core neighborhoods. We recognize that in order for the neighborhood-serving mixed-use to be viable the following ideas must be supported by the City: the vacation of right-of-way (lots acquired by the city during the intersection redesign) along Hillside, street-side parking along Hillside in front of the mixed-use buildings, the connections of the north-south alleys thru to Hillside and connections of the east-west cross streets to allow for a maximum distribution of traffic. These elements are critical to the success of the mixed-use. When built, there will be additional benefits of protecting pedestrians from traffic and slowing traffic as it travels through the neighborhood school zone while maintaining high traffic volume on Hillside and Henderson.

We are encouraged by the quality of life issues that resonated throughout your campaign. As you make these important policy decisions, we urge you to consider the Bryan Park Neighborhood Association's support of Matt Press and this extraordinary proposal for the South Dunn Street property.

On behalf of the Bryan Park Neighborhood Association,

The BPNA Committee on Development

Bill Hayden, BPNA President

Jim Opiat, BPNA Vice President

Jenny Bauer

Judith Brennan

Suzanne Eckes

Barre Klapper

Jon Lawrence

Jan Sorby

cc: Tom Micuda, Director of Planning;  
Julio Alonso, Director of Public Works;  
Justin Wykoff, Engineering Manager;  
Ron Walker, Economic Development Director

**Subject: South Dunn Street Development**

**Date:** Tue, 24 Feb 2004 08:56:53 -0500

**From:** "Chris Holly" <cjholly@bloomington.in.us>

**To:** <mayor@bloomington.in.gov>

**CC:** <micudat@bloomington.in.gov>, <alonsoj@bloomington.in.gov>, <wykoffj@bloomington.in.gov>, <walkerr@bloomington.in.gov>

Mayor Kruzan, I support Matt Press's South Dunn Street Development. He has worked hard and well with the neighborhood to come up with a compatible project. The city should accommodate the changes needed to make the project work:

- The vacation of right-of-way (lots acquired by the city during the intersection redesign) along Hillside to make the proposed mixed use along Hillside feasible

- Parking along Hillside to serve the mixed use and slow traffic along Hillside down (Hillside is becoming a drag strip and traffic needs to be made more aware of the neighborhood)

- North-south alley connections thru to Hillside and as much other connectivity as possible in order to allow for a maximum distribution of traffic within and from the site

With all the accommodation we have given downtown multistory apartment developers, this is the least we can do for a living, breathing neighborhood.

My Best, Chris

--

Chris Holly  
830 S. Fess Ave.  
Bloomington, IN 47401-4946  
812-339-0938

South Dunn Street Development

**Subject: South Dunn Street Development**

**Date:** Tue, 24 Feb 2004 09:11:18 -0500

**From:** "Ikranagara, Kay" <kikranag@indiana.edu>

**To:** <mayor@bloomington.in.gov>

**CC:** <micudat@bloomington.in.gov>, <alonsoj@bloomington.in.gov>, <wykoffj@bloomington.in.gov>, <walkerr@bloomington.in.gov>

Dear Mayor Kruzan

As a Bryan Park neighborhood member I would like to urge the city to support the multi-use proposal on Hillside of developer Matt Press of Neighborhood Solutions. This would involve the vacation of right-of-way along Hillside to make the proposed mixed use along Hillside feasible, parking along Hillside to serve the mixed use and slow traffic along Hillside, and north-south alley connections thru to Hillside and as much other connectivity as possible in order to allow for a maximum distribution of traffic within and from the site.

My husband and I attended several neighborhood meetings with the developer and were convinced that this solution is in the best interests of the neighborhood and the city. We support mixed use on Hillside and the policy changes necessary to make it work successfully.

Thank you,

Kay Ikranagara  
1321 S. Palmer Ave.  
Bloomington, IN 47408  
tel: 812 3378420



**Subject: South Dunn Street Development**

**Date:** Tue, 24 Feb 2004 09:29:42 -0500

**From:** "Arnold, Randy J" <rarnold@indiana.edu>

**To:** <mayor@bloomington.in.gov>

**CC:** <micudat@bloomington.in.gov>, <alonsoj@bloomington.in.gov>, <wykoffj@bloomington.in.gov>, <walkerr@bloomington.in.gov>, <Haydenb@bloomington.in.us>

Dear Mayor Kruzan:

Thank you for taking the time to listen to the residents of Bloomington regarding the South Dunn Street Development. I would like to voice my full support for Matt Press and the plans he worked on for the expansion of the Bryan Park Neighborhood. I fell in love with the city of Bloomington and became very fond of Bryan Park during my time here as an IU graduate student from 1994-1999. I have been fortunate to return to Bloomington as a member of the IU faculty in 2002, and currently rent a house across Grimes from the South Dunn Street property. I truly cherish the Bryan Park Neighborhood and hope to soon own a house in this part of Bloomington – possibly in the new development. I was able to participate in several of the workshops that Matt Press hosted in order to receive input on the development. I am truly impressed with the plans he has developed and hope that you will be fully supportive of them as well. I know that several compromises will be need on the part of the City in order to make Matt's plans a reality, in particular vacating the right-of-way along Hillside for proposed mixed use construction, allowing parking along Hillside for access to the mixed use facilities which will also slow traffic, and allowing north-south alley connections to Hillside for maximum connectivity to the mixed use and the rest of the new development. I hope that you will find these compromises in the best interest of the greater Bloomington community, as I do.

Best regards,

Randy Arnold

523 E. Grimes Lane

Bloomington, IN 47401

Randy J. Arnold, Ph.D., Manager

Proteomics R&D Facility

Department of Chemistry

Indiana University

Bloomington, IN 47405

(812) 856-0208 (phone)

(812) 855-8300 (fax)

From: Lawrence, Jon  
Sent: Tuesday, February 24, 2004 12:24 PM  
To: 'mayor@bloomington.in.gov'  
Cc: 'haydenb@bloomington.in.us'; 'micudat@bloomington.in.gov';  
'alonsoj@bloomington.in.gov'; 'wykoffj@bloomington.in.gov';  
'walkerr@bloomington.in.gov'  
Subject: South Dunn Street

Dear Mayor Kruzan:

I want to express my absolute support for Matt Press' proposed development on South Dunn St. Unlike the previous proposal by Steve Smith and Tim Henke, this proposal promises a design that will invigorate our neighborhood. At our monthly neighborhood meeting Sunday, Matt Press' design was UNANIMOUSLY supported. I can't tell you how great it is to be able to whole-heartedly support a development project in our neighborhood. And, how often do you get notes from neighbors supporting development in their own neighborhood!! Matt Press and Kirkwood Design have bent over backwards to make themselves accessible to the neighbors and have allowed the neighborhood incredible input into the design of this project. The outcome is a design that the overwhelming majority of our neighborhood supports. I would take a step further and even say that there's a feeling that the neighborhood has almost taken ownership of this design. We're excited to see it begin.

There are a few issues that are critical to the success of this project. Matt has purchased the house on the NW corner of Henderson and Hillside, which is adjacent to the project. In their new design, they envision a mixed usage zone from the corner of Henderson to South Dunn. Moving the retail to a corner property greatly enhances its ability to succeed. The problem is that the city owns 2 small parcels of land along Hillside that have been land-banked for the possibility of future widening of Hillside. Without this property, the commercial zone will not work. I whole-heartedly encourage the city to vacate that property to Matt Press. I hope that the city will put the viability of a truly urban village center above the possibility of placing a highway through a neighborhood. According to the GPP:

"Neighborhood-serving commercial uses, and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations."

This is exactly what Matt is suggesting. If the city does not vacate the land along Hillside, this will be impossible. We will have, instead, another set of apartments. It's amazing how excited the neighbors have been about a real neighborhood serving retail zone. We all envision walking to the coffee shop, grocery store, or whatever it happens to be. Their design would go a long way toward repairing the damage inflicted on Hillside and truly increase the quality of life in our neighborhood.

Their design includes angled parking off Hillside. The plan sets the parking far enough off Hillside that people could back up without exposing themselves to oncoming traffic. Think the downtown square. It works, people back out and no one thinks twice about it. Parked cars have the additional benefit of adding protection for the schoolchildren walking to and from the adjacent school. This is, again, another quality of life issue for our neighborhood. It works on the square and will work as a traffic-calming device in this "school zone."

I argue that it's more important to build a neighborhood center that serves the neighborhood rather than plan for a racetrack for people to get out of the city.

I ask that you please support the proposal as put forth to you so that we can continue to see our neighborhood protect the character that makes it such a wonderful place to live.

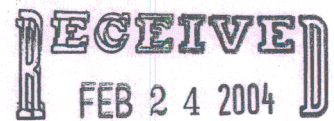
Jon Lawrence

\*\*\*\*\*

Jon Lawrence  
Director of Alumni Relations & Annual Giving  
Office of External Relations  
School of Education  
Indiana University  
812-856-8016



Angela J. Lexmond  
1302 S. Henderson  
Bloomington, Indiana 47401



BY:.....

February 24, 2004

Mayor Mark Kruzan  
City Hall – Room 210  
401 North Morton Street  
Bloomington, Indiana 47404

Dear Mayor Kruzan,

I write to advocate for the South Dunn Street development, as envisioned by Matt Press. Like so many others, my husband and I came to Bloomington for graduate school and decided to settle here. Life in Bloomington is vibrant and friendly, and with the birth of our two sons, we value it even more. We rent a house near Bryan Park (since 1997), and are ready to buy our first home. Our wish is to be walking distance from a park and an elementary school. We seek a simple, well-built home where we can be part of a neighborhood close to the heart of Bloomington.

We watched, with a great deal of interest, the proposals for the development of the Young farm property. The new urban design proposed by Matt Press is very creative, practical and desirable. We encourage you and other Bloomington officials to embrace this new paradigm to revitalize and develop existing neighborhoods. In our opinion, it is highly preferable to the common suburban projects that offer little by way of human scale design or community value.

With a home in the South Dunn Street area, we could walk to the park and to Templeton Elementary, and it would be fantastic to be able to walk to a grocery store or get a haircut as well! For these reasons, we support the creation of a mixed-use zone on Hillside, which would allow for commercial space with apartments above. We also hope that the city will support this vision by relinquishing the portion of land it owns so that the developer can build these commercial venues appropriately. We also like the idea of drivable alleys with complete connectivity to Grimes and Hillside streets.

In short, we are very interested in becoming homeowners in the development as currently proposed, and we add our voices to those of many others who would like to see the project unfold smoothly and in a timely manner (rather than getting ensnarled in policy, zoning, or political battles). We are confident that you will value the input of people living in the Bryan Park neighborhood because of the way you campaigned on the maintenance and improvement of Bloomington's unique neighborhoods.

Thank you for your time and attention to this proposal.

Sincerely,

A handwritten signature in cursive script that reads "Angela J. Lexmond". The signature is written in dark ink and is positioned above the printed name.

Angela Lexmond

cc: Tom Micuda, Planning Director  
Julio Alonso, Public Works Director  
Justin Wykoff, Engineering Department Manager  
Ron Walker, Economic Development Director





## Bryan Park Neighborhood Statement on the South Dunn Street PUD

April 27, 2004

The Bryan Park Neighborhood Association is pleased to support the PUD proposal for South Dunn Street put forth by Matt Press of Neighborhood Solutions. Upon purchasing this property, Mr. Press welcomed the neighbors to participate in a design charrette. The process consisted of meetings, lectures, neighborhood walks and much discussion with many neighbors in the Bryan Park neighborhood. We believe that the resultant PUD reflects the best historic architecture and traditional patterns of the Bryan Park neighborhood. The proposal gives the neighborhood a high level of predictability through a clear definition of house types, assurances as to the compatibility and quality of this infill project with our core neighborhood, and additional amenities of neighborhood serving retail and a pocket park.

The BPNA urges the Plan Commission to recognize and support this opportunity for an exceptional development. We believe that this proposal exemplifies the ideas of compact urban form as found in Bloomington's Growth Policy Plan. The PUD proposal reflects the philosophy of traditional neighborhood design that relies on an interrelationship of all components to create a rich and successful project. If you single out and deny certain aspects of this plan, the entire project is at risk. The BPNA specifically supports the following critical elements of the PUD:

**Mixed Use buildings along Hillside fronted by angled, on-street parking.** After a walk-through of the property and scrutiny of Hillside, neighbors agreed that a Neighborhood Activity Center (NAC) is the most appropriate use for the blocks fronting Hillside. We are encouraged that the city has identified the Henderson/Hillside corner specifically as a NAC. The GPP states (page 33):

- The main focus of the NAC should be commercial uses at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes.

We understand that the neighborhood alone cannot support businesses at this location, but know that the traffic along Hillside can help these new businesses thrive. Successful mixed-use development will only be possible with convenience parking along Hillside. Importantly, the on-street parking will serve as a visual cue to slow traffic down to speeds more acceptable in a school zone. The extra-deep parking spaces will provide clear sight lines for pedestrians and drivers and the parked cars will act as a safety barrier for pedestrians and children walking to Templeton Elementary School.

The neighborhood is excited about the potential of the proposed wide sidewalks to foster a lively pedestrian environment and help create a true neighborhood gathering place.

**North/south alleys from Grimes to Hillside and the Connections of Wilson and Driscoll Streets from Palmer to Henderson.** These street connections will complete the Bryan Park neighborhood grid platted in the 1920s. With the increased traffic from this development, it is essential that all the alleys and streets diffuse the traffic as much as possible. Only with full connectivity can the traffic increase be shared democratically, rather than a handful of households having to shoulder this burden.

As traffic becomes heavier on Henderson, the new public alleys will offer a safer egress opportunity for residents along Henderson to park off the alley rather than backing onto Henderson. These alleys must be two-way to allow for a maximum number of choices and greatest flexibility.



The BPNA is confident that the connection of Wilson Street to Henderson is worthwhile and safe. Due to the fall of the topography, the Wilson/Henderson intersection will have better sight lines than the existing Driscoll/Henderson intersection. Police records confirm that as far back as 1992 there are no recorded accidents at the Driscoll/Henderson intersection. Therefore, it follows that the connection of Wilson to Henderson, with better sight lines than Driscoll, will not pose any new safety problems. The BPNA will advocate for the extension of the sidewalks along the west side of Henderson to complete the pedestrian infrastructure in the area.

**South Dunn Street 30 feet wide.** The BPNA believes that S. Dunn Street as a local, residential core neighborhood street should be a maximum of 30 feet wide. The GPP makes special provisions for Existing Core Neighborhood Streets (page 84):

- Priority for the Right-of-Way:
  - On-street parking, Residential access, Neighborhood preservation
- In order to preserve neighborhood fabric, existing core Neighborhood Streets shall not be required to conform to the cross section standards that are being proposed for more suburban environments.
- Typical Characteristics of an Existing Core Neighborhood Street:
  - Travel lane widths – typically 8 to 10 feet.

The BPNA feels the thirty-foot wide street would feed into the existing seventeen-foot wide Dunn Street (north of Grimes) more smoothly and preserve the nature of the street. The BPNA is concerned that the proposed ten-foot wide lanes on S. Dunn Street will allow traffic to move much faster than desired. We know many of the existing streets in the neighborhood that have two travel lanes which are narrower than twenty-feet wide already suffer from speeding traffic. A wider road will most definitely exacerbate this problem.

In looking at other streets in the core neighborhoods, we think that the 900 block of E. Wylie Street and the 800 block of S. Stull Street, which have curbs and parking on both sides and are 30 feet wide are appropriate models for S. Dunn Street.

**Pocket Park.** Mr. Press has proposed a small “pocket park” in this PUD. The BPNA supports this amenity as a unique and informal park, wholly different in character and scale from the nearby Bryan Park. This passive park will be especially valuable for people living in the mixed-usage buildings at the south end of the project and create a new pedestrian destination for others in the neighborhood.

In conclusion, the BPNA feels neighborhoods cannot be expected to give up protective zoning without gaining measurable public benefits in return. The BPNA thinks that the South Dunn Street PUD provides several measurable benefits that will enhance the quality of life for all the neighbors and will offset the increase in density that this PUD requests. The BPNA urges you to forward this PUD intact to the City Council. The proposal, as is, will allow for a community enriching development with a successful, neighborhood-serving commercial zone on the southern end. People will feel safer crossing streets and commuters will be encouraged to support our businesses as they drive down Hillside. Children at Templeton Elementary School will feel safer walking to school. Changes to this proposal may result in the developer building yet more high-density apartments. Unquestionably this option would be more financially successful for the developer, but not good for the neighborhood. Mr. Press is willing to take a risk and do what he thinks is right for the neighborhood. We urge you to do the same and help create a new kind of development in Bloomington.

*Bill Hooper, President  
Bryan Park Neighborhood Assn.*



From: Scott O'Bryan <spobryan@hotmail.com>  
To: mpress@alumni@indiana.edu  
Subject: Strong Support for Dunn St. Project  
Sent: Thursday, May 6, 2004 9:13 PM  
Dear Matt,

It was a pleasure to meet yesterday again to review with you your proposal for the development bounded in part by Palmer, Hillside and Henderson streets.

I write with an interest in having my opinions of your project heard by the Bloomington Planning Commission at the upcoming hearing. I understand the meeting is scheduled for this Monday, May 10, a day, unfortunately, when I will be out of town.

I write as a new homeowner in the Bryant Park Neighborhood. Moreover, the new home I purchased (at 1412 S. Henderson St.) is directly affected by the project, arguably, in fact, \*the\* most directly affected home abutting the entire project, for my property is adjacent to the development on two sides--at the back and along the side where appropriate-use businesses are planned.

As a homeowner with such an immediate stake in the success of your project, I write to state that I strongly and unequivocally support the plans as you showed them to me yesterday, including especially your proposal for appropriate and neighborhood friendly commercial establishments fronting on Hillside up to and including the first lot on the corner of Hillside and Henderson. Indeed, the proposal for appropriate-use businesses along Hillside is an especially attractive aspect of your plan.

It is precisely because your plan promises to bring such local service businesses to the neighborhood that I decided to become a homeowner there. I want to be able to walk to my office and to my grocery and dry cleaners, and these sorts of lifestyles are made possible by your visionary project. There are so many people here in Bloomington who treasure this sort of opportunity to live in neighborhoods where people still walk, run into each other on the street, stop in to buy coffee or a loaf of bread, all within reasonable walking distance or within the distance of a short car trip \*within\* their own neighborhood. This is the sort of lifestyle that even I, as a forty-year old, remember having remnants of back in Louisville, KY and in Washington, D.C., where I spent time as a child. The hope of maintaining and strengthening these old, sensible, healthy, American traditions is something to be celebrated, and this project should be lauded for attempting to help in this regard in our town of Bloomington.

For such businesses as you are proposing to successfully serve the needs of our neighborhood, there should be included in the plan sites for appropriate parking, and I believe your plan is an elegant, safe, and indeed time-tested solution to this issue. I say time-tested in the sense that pull-in parking such as you are suggesting for these low-scale, convenient, service and amenity-type businesses, is just the sort of arrangement found in small and medium sized towns all over America that still care about maintaining vibrant, convenient, interactive living and shopping environments. Indeed, such pull-in parking has been the arrangement of choice for the better part of the twentieth century. One need only look at the structure of mature downtown squares and main streets all over this country to see that pull-in parking has worked successfully for Americans since the 1920s and 1930s.

I see this project as an attractive addition to the old core of our beautiful town. But it brings some very practical benefits as well that ought to garner the enthusiastic support of everyone involved:

- 1) It is a business-friendly proposal. Studies show that people use local businesses when they serve local needs and when there is reasonable convenience in terms of access. We want more businesses to grow in Bloomington proper, and we in the neighborhood will spend our money in these stores!
- 2) The proposal will bring an entire new set of homeowners of attractive properties into the heart of the old core. These homeowners will be tax payers, and, again, will be folks with consumer dollars to spend in the downtown neighborhoods of Bryan Park, Courthouse Square, etc.
- 3) The project will add charm and street-scape interest to our experiences of a core Bloomington neighborhood \*without\* impeding the movement of folks who are moving through our neighborhood in the course of their daily activities.

I respectfully and in the strongest possible terms urge the Planning Commission to approve this project. If Commission members have questions or would like further comments, they can reach me at [spobryan@hotmail.com](mailto:spobryan@hotmail.com).

Best regards,

Scott O'Bryan  
1412 S. Henderson St.  
812-331-1970





# Templeton Elementary School

1400 S. Brenda Lane Bloomington, IN 47401 812-330-7735 FAX 812-330-7779 ~www.mccsc.edu

May 10, 2004

Dear Plan Commissioner:

The Templeton Elementary School staff and parent communities are concerned about the high speed of the traffic on Hillside Drive as it passes through our 20/30 mph school zone. High traffic speeds compromise the safety of our students who walk to and from school and our busses as they exit on to Hillside Drive.

Matt Press' proposed development on South Dunn Street helps to address this issue in a manner that will help protect our school and its students. The introduction of on-street parking along Hillside Drive will provide a strong visual cue to drivers that they should slow down. In addition, on-street parking will create a protective barrier for pedestrians along this portion of Hillside Drive.

We are attempting to deal with student safety in a number of ways. Our efforts to look at utilizing Brenda Lane in different ways to ensure student safety are moving forward. By addressing speeds on Hillside Drive, we can continue to work collaboratively with the Neighborhood Association as an integral member of the neighborhood, in doing what is best for all children. We know that the safety of our students is as important to you as it is to us. Enforcement alone cannot reduce the speeds in our school zone. Therefore, we encourage you to support this project and promote the safety of our students.

Sincerely,

Catherine Diersing  
Templeton Principal

Letters  
58



## Bryan Park Neighborhood Statement on the South Dunn Street PUD

June 8, 2004

The Bryan Park Neighborhood Association is pleased to support the PUD proposal for South Dunn Street put forth by Matt Press of Neighborhood Solutions. Upon purchasing this property, Mr. Press welcomed the neighbors to participate in a design charrette. The process consisted of meetings, lectures, neighborhood walks and much discussion with many neighbors in the Bryan Park neighborhood. We believe that the resultant PUD reflects the best historic architecture and traditional patterns of the Bryan Park neighborhood. The proposal gives the neighborhood a high level of predictability through a clear definition of house types, assurances as to the compatibility and quality of this infill project with our core neighborhood, and additional amenities of neighborhood serving retail and a pocket park.

The BPNA urges the Plan Commission to recognize and support this opportunity for an exceptional development. We believe that this proposal exemplifies the ideas of compact urban form as found in Bloomington's Growth Policy Plan. The PUD proposal reflects the philosophy of traditional neighborhood design that relies on an interrelationship of all components to create a rich and successful project. If you single out and deny certain aspects of this plan, the entire project is at risk. The BPNA specifically supports the following critical elements of the PUD:

**Mixed Use buildings along Hillside fronted by angled, on-street parking.** After a walk-through of the property and scrutiny of Hillside, neighbors agreed that a Neighborhood Activity Center (NAC) is the most appropriate use for the blocks fronting Hillside. We are encouraged that the city has identified the Henderson/Hillside corner specifically as a NAC. The GPP states (page 33):

- The main focus of the NAC should be commercial uses at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes.

We understand that the neighborhood alone cannot support businesses at this location, but know that the traffic along Hillside can help these new businesses thrive. Successful mixed-use development will only be possible with convenience parking along Hillside. Importantly, the on-street parking will serve as a visual cue to slow traffic down to speeds more acceptable in a school zone. The extra-deep parking spaces will provide clear sight lines for pedestrians and drivers and the parked cars will act as a safety barrier for pedestrians and children walking to Templeton Elementary School.

The neighborhood is excited about the potential of the proposed wide sidewalks to foster a lively pedestrian environment and help create a true neighborhood gathering place.

**North/south alleys from Grimes to Hillside and the Connections of Wilson and Driscoll Streets from Palmer to Henderson.** These street connections will complete the Bryan Park neighborhood grid platted in the 1920s. With the increased traffic from this development, it is essential that all the alleys and streets diffuse the traffic as much as possible. Only with full connectivity can the traffic increase be shared democratically, rather than a handful of households having to shoulder this burden.

As traffic becomes heavier on Henderson, the new public alleys will offer a safer egress opportunity for residents along Henderson to park off the alley rather than backing onto Henderson. These alleys must be two-way to allow for a maximum number of choices and greatest flexibility.



The BPNA is confident that the connection of Wilson Street to Henderson is worthwhile and safe. Due to the fall of the topography, the Wilson/Henderson intersection will have better sight lines than the existing Driscoll/Henderson intersection. Police records confirm that as far back as 1992 there are no recorded accidents at the Driscoll/Henderson intersection. Therefore, it follows that the connection of Wilson to Henderson, with better sight lines than Driscoll, will not pose any new safety problems. The BPNA will advocate for the extension of the sidewalks along the west side of Henderson to complete the pedestrian infrastructure in the area.

**South Dunn Street 26-foot wide.** The BPNA believes that S. Dunn Street as a local, residential core neighborhood street should be a maximum of 26 feet wide. **The BPNA supports a condition of approval to narrow S. Dunn Street to a 26-foot wide dimension.** The GPP makes special provisions for Existing Core Neighborhood Streets (page 84):

- Priority for the Right-of-Way:
  - On-street parking, Residential access, Neighborhood preservation
- In order to preserve neighborhood fabric, existing core Neighborhood Streets shall not be required to conform to the cross section standards that are being proposed for more suburban environments.

The BPNA feels the twenty-six-foot wide street would feed into the existing seventeen-foot wide Dunn Street (north of Grimes) more smoothly and preserve the nature of the street. The BPNA is concerned that the proposed ten-foot wide lanes on S. Dunn Street will allow traffic to move much faster than desired. We know many of the existing streets in the neighborhood that have two travel lanes which are narrower than twenty-feet wide already suffer from speeding traffic. A wider road will most definitely exacerbate this problem.

**Pocket Park.** Mr. Press has proposed a small “pocket park” in this PUD. The BPNA supports this amenity as a unique and informal park, wholly different in character and scale from the nearby Bryan Park. This passive park will be especially valuable for people living in the mixed-use buildings at the south end of the project and create a new pedestrian destination for others in the neighborhood.

In conclusion, the BPNA feels neighborhoods cannot be expected to give up protective zoning without gaining measurable public benefits in return. The BPNA thinks that the South Dunn Street PUD provides several measurable benefits that will enhance the quality of life for all the neighbors and will offset the increase in density that this PUD requests. The BPNA urges you to forward this PUD intact to the City Council. The proposal, as is, will allow for a community enriching development with a successful, neighborhood-serving commercial zone on the southern end. People will feel safer crossing streets and commuters will be encouraged to support our businesses as they drive down Hillside. Children at Templeton Elementary School will feel safer walking to school. Changes to this proposal may result in the developer building yet more high-density apartments. Unquestionably this option would be more financially successful for the developer, but not good for the neighborhood. Mr. Press is willing to take a risk and do what he thinks is right for the neighborhood. We urge you to do the same and help create a new kind of development in Bloomington.

**Subject: S. DUNN DEVELOPMENT PROPOSAL : TO PLANNING COMMISSION MEMBERS**

**Date: Mon, 10 May 2004 14:23:41 EDT**

**From: LClark6380@aol.com**

**To: planning@bloomington.in.gov**

DEAR SIRs AND MADAMS,  
MY NAME IS LOUIS CLARK . I AM A ADJACENT PROPERTY OWNER TO THE PARCEL OF LAND UNDER  
CONSIDERATION.

I RESIDE AT 1408 S.HENDERSON ST. AND HAVE LIVED THERE WITH MY SPOUSE FOR MORE THAN  
SEVENTEEN YEARS.

THE RETAIL ASPECT OF THE PREPOSAL  
SHOULD NOT BE ALLOWED---- PERIOD.

THE DENSITY IS EVEN HIGHER THAN THE LAST DEVELOPER AND IS OUT OF SCALE  
WITH THE NEIGHBORHOOD.

WILSON ST. SHOULD NOT BE BROUGHT OUT TO HENDERSON ST.

THE ADDITION WILL ONLY MAKE THE STREET MORE DANGEROUS, AND WILL INCREASE THE NUMBER  
OF ACCIDENTS.

PLEASE DON'T DISREGARD THE CONCERNS OF THE IMMEDIATE NEIGHBORS!!

SINCERELY

LOUIS T. CLARK  
CINDY BARNARD



June 10, 2004

Planning Commissioners  
City of Bloomington

We the undersigned neighbors of the Bryan Park Neighborhood would like to address the following issues associated with the Bryan Place rezoning petition:

- **Proposed commercial development on Hillside**
- **East Wilson Street connection to Henderson Street**
- **Storm drainage**

The proposed commercial development on Hillside Dr. is in our opinion a great concept on paper. However, in reality it simply does not seem viable. Hillside at this point is not an appropriate placement for commercial business. Currently when businesses in town receive deliveries, trucks park in the alleys to unload. If this occurs with this particular development, there will be many early mornings and nights when we will have to listen to roaring engines. It also seems as though the proposed back-out parking along Hillside would be a continuous problem that would create a significant public safety risk.

Regarding the E. Wilson St. connection it has been mentioned several times that this is a serious public safety issue and appears to be an unnecessary benefit to the project. Without the E. Wilson connection there will still be nine entries and exits out of this project. There would be adequate access to all properties within this project -without this road. As we mentioned in the last hearing there are many school children that have to continue going to the intersection of Hillside and Henderson to utilize the crossing guard to safely cross to get to Templeton School. The grade of the hill on Henderson makes it impossible to have appropriate sight lines to enter and exit out of this connection.

The properties that sit adjacent to the Southeast end of this project also have issues currently with storm water drainage. Nothing has been discussed in the public forum to address this issue, and it is of great concern to us as neighbors.

To end, we realize that the majority of our neighbors agree with all aspects of Mr. Press' project. We have always realized that there would be development behind our homes, however nothing of this magnitude. As was stated at the last hearing, we are also willing to bear our share of the burden to support development on this property. However, the vast majority of the project's intensity and impacts (probably 90%) are immediately affecting our properties. We feel that at this point our voices are not being heard by our neighborhood association.

We would sincerely appreciate your consideration in the issues we have mentioned above. You are the individuals that determine the course of our neighborhood and the City of Bloomington.

Sincerely,

Stacy Ream  
Donna Kilpich  
Pat Kilpich  
Louis Levato  
Doris L Levato  
Meadford Proun

**Marc Cornett**

---

**From:** Tlayman200@aol.com  
**Sent:** Wednesday, May 05, 2004 5:04 PM  
**To:** mc@kdsarchitects.com  
**Subject:** South Dunn Street Development

Mr. Cornett, you asked me to research and comment on several development and traffic operational features to your proposed development plan for the referenced project. I have several comments to offer.

From a traffic engineering perspective new urbanism development offer good opportunities and challenges for new and redeveloped neighborhoods. The concept drawings for the Hillside Drive area provides such opportunities and challenges.

Having served on the Planning and Zoning Commission for my Community, I can appreciate the need to be informed of the possible impacts of the proposed development. In the case of Hillside Drive, the existing traffic is traveling at speeds of greater than 35 mph, which is too high for an area that has a school and is being proposed for commercial development.

New urbanism development can offer several very desirable quality of life features, such as: encouraging a walking environment, alleyways for garbage and garage access, commercial development to encourage auto free trips and open space for neighborhood activities. Commercial development is a critical feature of these developments due to the need to encourage walking for some basic needs shopping and to provide a local meeting place. To foster a good business environment requires good access for both the automobile and the pedestrian. The South Dunn Street development seems to provide these essential ingredients. But, I have several comments to share:

**1. The Provision of Diagonal Parking** --- In suburban shopping centers the desired parking area ratio to gross commercial floor space is approximately 1.5 to 1.0 to offer an environment for economic viability. In an urban infill area such ratios are usually not achievable. However, by providing diagonal parking near the store fronts helps to maximize the parking opportunity and the economic viability of the commercial ventures. It is very important to realize that parking spaces within reasonable walking distances are worth anywhere from \$20,000 to \$30,000 or more in gross sales per year for the businesses. As the businesses thrive so will the pedestrian environment and auto free trips. There is also another benefit to diagonal parking, the traffic calming effect on speeds of pass-by traffic. As a motorist enters an area with diagonal parking, there is the perception that a possible unparking maneuver will encroach into the driving lane. The perception will cause the motorist to slow down which will product a reduction in vehicle operating speeds. The South Dunn Street development plan offers two conditions that allows for reasonable



vehicle operation to occur in the diagonal parking area: good depth to the parking stalls which allows reasonable sight distance to the on-coming vehicles, and adequate street width where the motorist has room to maneuver.

**2. Reduce Curb Radius on Northwest Corner of Hillside Drive and Henderson Street** --- The existing curb radius encourages excessive speeds to occur with the vehicle turning right from Henderson Street onto Hillside Drive. The curb radius needs to be reduced to 10 feet. this adjustment will result in two improvements. The first being the traffic calming effect on the right turning vehicle and the resulting reaction time improvement on the driver as he enters the area of diagonal parking along Hillside Drive. The second improvement is the shorter walking time and exposure time a pedestrian has when crossing Hillside Drive, which results in a more pedestrian friendly environment. This improvement also supports a safer condition for the school crossings.

**3. The Provision of Traffic Calming Conditions for Hillside Drive** --- As previously discussed in item # 1, diagonal parking on Hillside drive provides a traffic calming condition. In addition, two other improvements in the intersection area will provide traffic calming effects, namely: providing textured sidewalks or raising the intersection about three inches to create a speed hump effect. Textured sidewalks help call attention to the possibility of a pedestrian and a resulting decrease in vehicle speed. The speed hump has been successful in many areas of the county. By raising the entire intersection it gives the motorist an alert signal that something is different and a speed reduction results. Both types of improvements are effective tools in reducing speeds, however there should be accompanying signage to notify the motorist there is something different ahead, such as "pedestrian crossing area ahead" or "congested area ahead" reduce speed.

**LOUIS T. CLARK**  
1408 S. HENDERSON ST.  
BLOOMINGTON, IN. 47401  
Home Phone 812-334-3285

June 10, 2004

Dear Plan Commission Member,

I am writing about my concern for the Bryan Park Place PUD that has been proposed for the remainder of the Young family farm on Hillside Drive.

I am an adjacent property owner, and have lived at my present address for sixteen years.

Prior to that I rented a house on Hillside Dr for nine years.

I strongly encourage you to support the preservation of the existing pattern of single family type homes only. I support a variety of lot sizes geared towards a diverse neighborhood of different income levels. but at the same time maintaining a less dense concentration of housing as proposed .

I know the proposed plan to put multi family housing(Apartment houses) and Retail businesses into our area is unneeded and unwanted.

If Retail businesses are allowed on one side of the street on hillside will it be allowed on the other?

If Hillside Dr. is a Secondary Arterial, and Henderson St is a Secondary Arterial,  
Can I as a property owner seek invertors and develop my land for retail or commercial use?

And if not ,why not?

How far down Hillside , how far up and down Henderson will retail be allowed?

Next, the extension of Wilson to Henderson st. will only cause more auto accidents in the future.

To use existing traffic accident records on Driscoll as a standard is misleading.

At present Dirscoll is effectively a dead end.

No one uses the road, so there are few, if no accidents.

If you open Driscoll from Henderson St to Walnut St. traffic accidents will rise significantly, especially left-handed turns going north.

The same will be true for Wilson St. if connected from Henderson to Walnut, if not worse.

**The B.P.N.A. DOES NOT REPRESENT THE ENTIRE NEIGHBORHOOD!**

**The adjacent neighbors at the southeast corner of the proposal**

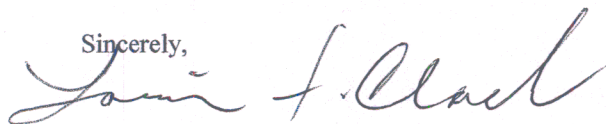
**DO NOT AGREE AT ALL WITH THE PROPOSAL, OR THE B.P.N.A.!**

Please take into your considerations the concerns of the existing adjacent neighbors

Thank You for your time.

I will be in attendance as this comes before the Plan Commission monday June 14th, 2004.

Sincerely,



Louis T. Clark



CINDY BARNARD

1408 S.HENDERSON ST  
BLOOMINGTON, IN. 47401  
Home Phone 812-334-3285

RECEIVED  
JUN 10 2004

BY:.....

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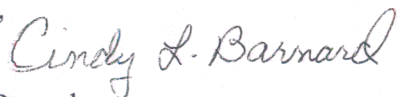
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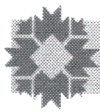
Thank You for your time.

I will be in attendance as this comes before the Plan Commission monday June 14th, 2004.

Sincerely,



Cindy L. Barnard



### Compact Urban Form

Compactness is a difficult characteristic to perceive if one is viewing land development on a site by site basis. It is the broader view of the community that provides the best observation of compactness. In essence, the value of compactness represents the value of spatial organization, planned growth and public fiscal responsibility. Compactness stands opposite of urban sprawl – as denoted by low density, discontinuous residential growth, and strip commercial development. Pragmatically, contained development is less costly to develop and serve. Spatial expansion costs more because sewer lines, roadways and school bus routes are longer. Additionally, discontinuous development at the urban periphery interrupts open space. Transit service is also negatively impacted by sprawl. Compact urban form creates an environment in which transit service can thrive.

Bloomington has a series of barriers beyond which urban development becomes either difficult or inappropriate. To the southeast, the Lake Monroe watershed restricts development. To the east, topography limits the magnitude of development possibilities. Beyond State Road 37 to the west, the peculiarities of karst topography create prohibitive development circumstances. To the north, the steep slopes of the Lake Griffy watershed also constrict development.

In order to achieve compact urban form, outward expansion of development must be limited through effective growth management policies. At the same time, these policies should be supplemented by strategies to increase housing densities within the planning jurisdiction. These strategies must also be coupled with a strategy for containing commercial development. Rather than open up large amounts of land for new commercial development on the periphery, the emphasis should be on providing incentives to re-use vacant or underutilized commercially zoned sites. An example of this would be redevelopment of the ST Semi-Con property in Downtown Bloomington.

To compact also means to integrate - to be drawn in. In Bloomington, there are strong environmental constraints which push development inward. Much of what makes Bloomington special is its shared “sense of place”. While this sense cannot always be defined to the satisfaction of all interests, it is irrevocably connected to

Bloomington’s town core and the harmony of its architecture, its neighborhoods and their respective contexts. Disorganized development sprawl dilutes this sense of place.

Compactness does not, however, mean or imply static population growth for the community as a whole. Within the planning horizon of the next ten years, Bloomington can physically accommodate more people, more employment, more homes and more activities within its current planning jurisdiction. Yet, in order to assure that population growth does not translate to a reduction in the perceived quality of life, Bloomington must grow with care, with conviction and with efficiency. Compactness implies directing growth - directing growth toward those locations where it is desirable, where it is in the public interest to grow, and where options conducive to future growth can be exercised.

Compact urban form refers to the overall development pattern. It does not imply the intrusion of higher density development into established neighborhoods, crowding, or high rise development of a scale more appropriate to larger cities. Compact form is not to be achieved at the expense of greenspace, environmental protection, and other policies.

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#### Compact Urban Form Goal

Create a compact land use development pattern that is sufficiently compact to provide efficient delivery of services, to effectively manage existing infrastructure resources, and to maximize return on public expenditures while limiting sprawl and maintaining the special nature of Bloomington.

#### Policy 1: Limit the Spatial Extent of Community Growth

As the community has expanded outward from its beginnings at the urban core, an ever-greater strain has been placed on the City’s ability to provide adequate urban services. Likewise, some citizens have stated that it has become increasingly difficult for residents to enjoy the quality of life that a diverse urban community like Bloomington offers. Bloomington must look inward for opportunities to accommodate continued growth within the existing limits of the community. The first part of any serious strategy to accomplish this is to explore methods of curtailing outward expansion.





## PART 1: Policy Essence

Land use planning policies offer one method of achieving this end. Clearly, the west and southwest sectors of Bloomington offer the most opportunities for residential growth, while areas to the east and southeast have been virtually built-out with residential development in recent years. Bloomington must work to shift more residential development to the west and southwest as development limits are reached in other sectors of the community. This development will not compromise nor diminish the conservation of sensitive areas and will take advantage of opportunities for greenspace preservation and acquisition.

Sewer extension policies can also be utilized as a tool for directing development to appropriate areas of the community. Currently, sewer extensions can be granted for developments located outside of the City's planning jurisdiction upon positive findings rendered by the Monroe County Plan Commission. This creates a scenario whereby another agency is providing policy guidance for the location of an important City service. A revision to this sewer approval process is clearly needed. The first step is to convene a series of meetings including both the City and County Planning Directors, the City Utilities Department, City and County decision-makers, and regional utility providers (Ellettsville Utilities, the Lake Monroe Regional Waste District, etc.). These meetings should be oriented towards determining likely areas of future growth as well as the associated geographic areas where service should logically be provided. The result of these discussions would be the creation of both sewer service districts as well as non-service areas. Once these determinations have been made, the City shall proceed in amending its existing Utility Service Overlay Map as well as its associated Utility Service Board rules and regulations regarding sewer extensions. In the interim, the City should strongly discourage the extension of sewers beyond its planning jurisdiction.

### Implementation Measures

- CUF-1** Encourage new housing starts toward the west and southwest sectors of the community; discourage urban development to the east of the east fork of Jackson Creek; and north and east of the State Road 45/46 Bypass.
- CUF-2** Develop revisions to the City's existing Utility Service Overlay Map as well as associated Utility Service Board rules and regulations to create sewer service districts as well as non-service areas. In the interim, strongly discourage sewer extensions beyond the planning jurisdiction.

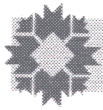
### Policy 2: Increase Residential Densities in the Urbanized Area

As a counterbalance to policies that limit the spatial expansion of growth, denser infill development in areas that already contain City services must be encouraged. Increasing the density of residential development within the community can provide several benefits. Concentrating densities in certain areas allows others to be preserved as greenspace, a vital urban amenity. Further, as densities increase, the efficiency and quality of urban services can be improved, and public transit becomes a much more feasible service.

The 1995 Zoning Ordinance attempted to address this issue by creating such incentives as the Planned Residential Overlay (PRO) district and the Downtown Development Opportunity Overlay (DDOO). Although overall residential densities have increased since the early 1990s, both overlay concepts have received some criticism from developers, neighborhood associations and historic preservation interests. While the overlay regulations achieved some measure of success, they were clearly not an adequate solution. These issues must be resolved as a crucial step in bringing residential growth and vitality back to the urbanized area.

### Implementation Measures

- CUF-3** Revise the Planned Residential Overlay requirements of the Zoning Ordinance to provide incentives for the development of mixed-use neighborhoods on infill development tracts. In general, however, multifamily residential developments that are likely to serve significant numbers of Indiana University students should be strongly discouraged in locations distant from the IU campus.
- CUF-4** Revise development regulations to increase the allowed level of residential density to 100 units per acre in the Downtown Commercial District. Link this increased density with required design standards (i.e. building setback, height, roof orientation, blank wall controls) and appropriate historic preservation for specific areas located within the Downtown Commercial District. Revise the Downtown Development Opportunity Overlay (DDOO) district to remove high-density incentives in specific areas which could result in a negative impact upon historic and culturally important buildings and districts.
- CUF-5** Revise development regulations for near-downtown and near-campus areas to encourage increased residential densities.



## PART 1: Policy Essence

### Policy 3: Redirect Commercial Development

Like residential growth, the continued spread of commercial growth has a profound impact on the community. As the locations of commercial centers move farther from established residential areas, people must spend more time driving to reach them, and thus create increased traffic throughout Bloomington. This is a pattern of development that has occurred over several decades, and has pushed the community away from the pedestrian-friendly vision that residents share.

A renewed emphasis must be placed on closely scrutinizing the location of new commercial development, especially on the periphery of the community, while providing incentives for infill-style commercial projects. This incentive strategy is particularly needed along arterial roadway corridors that are zoned for commercial development, but have been largely bypassed in favor of vacant land at the edges of the community. Commercial retail growth in well established commercial centers such as College Mall and Whitehall Crossing must be contained if such incentives are to have a substantial impact. To this end, no new regional commercial land has been identified in the plan, placing a greater focus on redevelopment of existing commercially designated land to meet future needs.

Redevelopment of vacant or underutilized commercial sites for uses compatible with prior uses of the same site can present significant advantages for the community. Such redevelopment reduces the need for consumption of raw land and avoids conflicts with neighborhoods over the impacts of new development. Such redevelopment, including adaptive reuse of existing commercial buildings, can help to achieve many of the GPP's goals. Therefore, such redevelopment should be encouraged through incentives and a streamlined, flexible regulatory process.

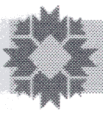
Redirecting commercial development also encompasses the form that commercial development should take in the future. Current commercial development patterns are characterized by one-story buildings set back great distances from roadways, large expanses of parking lots and frequent driveways along streets. The City should investigate zoning strategies that facilitate multi-story construction, mixed uses and potentially structured parking in the redevelopment of commercial areas. This would allow for the evolution of existing commercial areas into high-density nodes containing residential components and connected by transit and other alternative transportation facilities.

The final element of directing commercial growth goes back to the concept of a pedestrian friendly community. Certain neighborhoods may be able to support small-scale commercial development at strategic locations within them. This must only be done after the creation of neighborhood commercial development guidelines to ensure that any new commercial development is compatible in scale and design with existing neighborhoods. Neighborhood associations must be involved in the development of both the guidelines and site selection for new neighborhood commercial nodes.

#### Implementation Measures

- CUF-6** Direct commercial development to existing commercially zoned land, and provide incentives to encourage the re-use and improvement of vacant or under-developed commercial sites, particularly along arterial roadway corridors.
- CUF-7** Restrain new commercial development in the southeast sector of the community while providing opportunities for re-use and redevelopment of existing commercial land such as the College Mall shopping center.
- CUF-8** Revise the Zoning Ordinance to provide opportunities for mixed uses, multi-story construction and structured parking in community and regional scale activity centers.
- CUF-9** Amend the Zoning Ordinance to allow the development of appropriately located, designed and scaled neighborhood serving commercial centers in all geographic sectors of the community.





## PART 1: Policy Essence

### Conserve Community Character

Bloomington residents have a strong attachment to their community that emanates from a bundle of qualities that make Bloomington special and worthy of collective pride. Maintaining that community pride requires conservation, maintenance, and replication of those attributes that evoke positive feelings among residents. The challenge is to ensure that as growth occurs, community character is not lost. Future development and redevelopment should serve to strengthen the attachment that Bloomington residents feel toward their community.

Bloomington is often described as possessing “small town character.” The community boasts a wealth of amenities usually associated with much larger cities yet within the context of a more traditional “small town” environment. Landmarks such as community parks, a traditional and vital downtown, high quality public safety services, and active and diverse neighborhoods are just a few of the elements which are crucial to Bloomington’s character. In addition, community character has to do with the look and feel of Bloomington, especially the design of development in the downtown. Bloomington’s downtown has a history of traditional, “Mainstreet” design, and this legacy must be protected and reinforced in the future.

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#### Conserve Community Character Goal

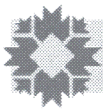
Conserve Bloomington’s unique community character through neighborhood protection, downtown investment and revitalization, and context-sensitive infill development.

### Policy 1: Protect and Enhance Neighborhoods

Central to the community character of Bloomington are its neighborhoods. These neighborhoods must be protected and invigorated. They contain a diversity of housing stock reflective of different periods of development, and which demonstrate a relatively compact pedestrian scale context. New development that alters the architectural character of these neighborhoods should be avoided. Additionally, the City shall promote structural maintenance and reinvestment of both owner and rental units and promote affordable housing. This includes the renovation of blighted, incompatible or functionally obsolete structures, in a manner that is sensitive to the existing residential context.

More specifically, Bloomington’s core neighborhoods, located in close proximity to the downtown, represent the historic identity of the city. These neighborhoods are an irreplaceable resource in terms of location and relative affordability. Additionally, it is essential to maintain the historic context and architectural character of the older core neighborhoods. In order to allow these neighborhoods to flourish and continue to grow in tradition, the maintenance of existing structures should be coupled with context sensitive development. Neighborhood character can evolve in a gradual and compatible way to allow additional density through subdividing lots, and the creation of granny flats and duplexes.

The City should strive to work with neighborhoods to improve service provision and to facilitate effective communication between the neighborhood and City agencies. This can be accomplished through the development of neighborhood plans, which can serve as guides for implementing public improvement projects and steering private investment in a specific neighborhood. An important component of this process is locating and empowering people in the community who have leadership talents and strong ties to their neighbors. The resulting plans will create mutual trust and bring together an association between citizens, business and government.



## PART 1: Policy Essence

In 1985, the City, in response to community concerns, changed the zoning ordinance to restrict the occupancy of single family homes to three (3) unrelated adults. The zoning ordinance was further amended in 1995 to place more properties within the single family occupancy restriction. This was carried out in order to prevent core neighborhoods from going to a majority of rental units. The effect of this regulation has been that the proportion of owner occupied units has increased in some core neighborhoods.

### Implementation Measures

- CCC-1** In coordination with the Housing and Neighborhood Development Department, adopt neighborhood plans that will be designed to address housing, land uses, the provision of public infrastructure and services, affordable housing and infill development strategies.
- CCC-2** Maintain the current maximum occupancy standard of three (3) unrelated adults within single family residential zoning districts.
- CCC-3** Revise the Zoning Ordinance to include standards for infill development in residential areas that are consistent and compatible with preexisting development.
- CCC-4** Develop a program that provides incentives (i.e. tax abatements or purchase/rehab assistance) for rehabilitating older housing stock in established neighborhoods for affordable housing.

### Policy 2: Improve Downtown Vitality

Downtown Bloomington represents an extraordinary collection of predominantly traditional late 19<sup>th</sup> and early 20<sup>th</sup> century commercial, residential and institutional buildings with a great variety of structural styles, ages, and masses. With that in mind, maintaining a vibrant and visually cohesive downtown is crucial to the future of Bloomington. This downtown serves as a cultural, recreational, and economic focal point as well as an employment center for the entire community. Bloomington's historic downtown character is a major economic development tool and must be nurtured and encouraged. The activities and services found in the Downtown are uniquely positioned to provide opportunities for a diverse array of users, from local residents to visitors from other communities. It is thus important that growth and development policies in Bloomington foster the continued vitality of the downtown by stimulating new downtown development and redevelopment of underutilized parcels and buildings.

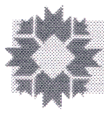
According to the Downtown Bloomington Commission, in the last 20 years, both the City and private property owners have demonstrated their commitment to the downtown by rehabilitating 112 existing buildings. This momentum can be maintained by continuing to rehabilitate and restore structures, build new structures on vacant or underutilized lots, and clean up environmentally contaminated sites downtown. In order for new development in the downtown to be successful, and to ensure compatibility of both function and form as growth progresses, the city will need to pursue more detailed design standards for the downtown area. These design standards (i.e. building setback, height, roof orientation, blank wall control) should be coupled with financial and zoning incentives, as well as design assistance to allow for innovative development.

In order to ensure that new downtown development and redevelopment does not detract from the area's historic and traditional character, properties and districts that contain important structures should be clearly identified. Once these structures are identified, mechanisms should be established to ensure that demolition of these structures is reviewed as a component of the Plan Commission or Board of Zoning Appeals hearing process.

### Implementation Measures

- CCC-5** Establish site planning and design standards for development and redevelopment in the downtown areas that emphasize compatibility of form with existing structures using a public community process directed by a professional urban design consultant.
- CCC-6** Expand the geographic boundaries of the Downtown Commercial zoning district while ensuring appropriate protection of structures and districts of historic importance. Work in cooperation with a professional urban design consultant in this process.
- CCC-7** Provide incentives (i.e. tax abatements, design assistance) to facilitate the construction of downtown development projects.
- CCC-8** Hire a professional urban planning firm to create, in cooperation with key stakeholder groups, a subarea plan which addresses the long-term viability of the downtown area.





## PART 1: Policy Essence

### Policy 3: Maintain Bloomington's Historic Character

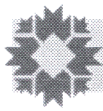
Historic preservation creates better communities by ensuring protection of culturally and historically significant structures and districts in downtowns and urban neighborhoods. From Queen Anne to Carpenter-Builder style structures, Bloomington's built environment features an array of architectural styles and eras. In some areas, preservation efforts such as local, State, and Federal districting have been utilized to help maintain the character that was evident years ago.

Preservation refers to the maintenance of a property without significant alteration to its current condition. This approach should be taken when it is appropriate to maintain a building or structure as it was originally constructed. A structure changes over its lifetime and each change represents a part of its history and integrity. The preservation of a historic building accepts those changes but maintains its historic integrity and as many original features as possible. In order to build on the community character of Bloomington, it is important to promote preservationist techniques (i.e. restoration, rehabilitation), in historic areas.

Currently, there are many sites in Bloomington with national, state, and local historic designations. The last historic sites and structures inventory was completed in 1988. Through the Historic Preservation Commission, an updated inventory of historic sites and structures is being completed. After the completion of this inventory, historic sites and districts can be sought out for national, state or local designation.

#### Implementation Measures

- CCC-9** Encourage public/private partnerships in the rehabilitation of existing structures downtown and in other areas of the community.
- CCC-10** Update the City of Bloomington *Interim Report on Indiana Historic Sites and Structures Inventory*.
- CCC-11** Protect historic areas against erosion and loss via demolition and alteration by using both Historic Conservation Districting and Local Historic Districting.
- CCC-12** Bring interested parties together to formulate and make recommendations regarding demolition delay provisions to be included in the municipal code.



### Mitigate Traffic

Traffic congestion is an increasingly apparent characteristic of urban growth, nationally as well as in Bloomington. Part of this congestion can be attributed to national trends such as smaller household sizes and increases in the number of personal vehicles. New development in general is commonly identified as a major culprit of traffic congestion. More accurately, it is the geographic disbursement and the type of such development that can cause traffic problems. In addition, the City of Bloomington is evolving into a regional center for commercial services, employment, and recreation that draws traffic from multiple counties.

In order to mitigate these national, regional, and local factors which are all contributing to increased traffic, Bloomington must strive to reduce the number of vehicle trips traveled per resident. Reducing automobile trip-making not only reduces congestion but improves air quality, saves energy, and increases bicycle and pedestrian safety within the transportation system. Fundamentally, traffic mitigation describes a set of public policies focused on actively reducing the demand for automobile trip-making.

Traffic mitigation is a logical principle to accept but presents difficulties due to existing national trends in vehicle utilization, current patterns of spatial separation between land uses, and lack of alternative transportation facilities. However, Bloomington, because it is relatively compact and contains a high ratio of university students, has an opportunity to change the pattern of automobile trip-making over time by embracing alternative forms of transportation. Walking is a widely underestimated mode of alternative transportation. Walking trips generally out-number biking and transit trips by about ten to one. In an effort to mitigate traffic, support for walking should be paramount. Additionally, trip-making patterns can also be altered through increasing mixed land use development, pursuing a compact development strategy, and achieving more interconnected street systems.

### Mitigate Traffic Goal

Enhance the community transportation system in a manner that reduces automobile dependency and increases access to multiple transportation modes such as walking, bicycling and transit.

### Policy 1: Enhance and Expand Public Transit Services

Through the development of a universal access system between Bloomington Transit and Indiana University, transit ridership rates increased significantly during the Year 2000. In fact, ridership has increased from 437,000 in 1982 to 1.37 million in 2000. This achievement prompted Metro Magazine, a national transit publication, to list Bloomington Transit as one of the 10 Most Improved Transit Systems in North America. Further gains in transit usage can be realized by more directly linking development form and location to city transit routes. For example, communities which have incorporated such features as building-forward orientation, attractive and convenient bus shelters, and safe bus pull-offs into new development and redevelopment projects have experienced a noticeable impact on ridership levels.

Ridership rates can also increase by ensuring high development density in direct proximity to transit routes. This synergy between population density and transit service can be readily seen in larger urban communities. These transit characteristics dovetail nicely with the Plan's principle of Compact Urban Form. Where development has already occurred outside the City limits, the City should not overlook the possibility of transit service agreements with large trip generators. An example of a possible future service area would be the new Ivy Tech facility located in the Park 48 Industrial Park on State Road 48. Additionally, the City and Indiana University should partner to continue the success of the existing Park and Ride system within the corporate boundaries.

### Implementation Measures

- MT-1** Develop transit-oriented site planning standards as a required component of development and redevelopment projects.
- MT-2** Require the siting of future high density multifamily and commercial projects within walking distance to transit routes.





## PART 1: Policy Essence

- MT-3** Expand the Park and Ride system by creating additional lots in under-served sectors of the City, particularly in proximity to arterial street corridors.
- MT-4** Pursue an integrated mass transit system between Bloomington Transit and Indiana University, either through the continuation of a universal bus pass system or a merger between the two service agencies.
- MT-5** Coordinate with Bloomington Transit to study the feasibility of allowing universal transit access for all citizens of Bloomington.

### Policy 2: Enhance Bicycle and Pedestrian Transportation Facilities

While most residents may not be able to walk or cycle to work, trips for entertainment and socializing are more discretionary. Nationally, less than one-fourth of all trips are to work, while a larger percentage of trips are for social and recreational purposes. If walking and cycling become more enjoyable, their potential trip percentage increase is large, especially since many destinations in Bloomington are within relatively easy walking distance.

A very high percentage of residents own, or have access to, motorized vehicles. For these residents, walking, biking, or taking transit is largely a matter of choice. This choice is determined not only by cost and convenience, but also by comfort. When sidewalks are immediately adjacent to moving vehicles, and lack shade and visual interest, residents will tend to use their cars, even if it means sitting in traffic jams.

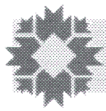
If walking is to compete with driving, the sidewalk environment must be very inviting. Separating sidewalks from moving traffic is essential. Wide tree plots, or in some cases, on-street parking, not only makes walking safer, but also buffers pedestrians from spray, dust, and noise. Good site design is also crucial to entice walkers. Regularly spaced, large species, street trees provide shade and beauty. Buildings placed close to the sidewalk offer visual interest and social interaction.

Bloomington is making progress in developing more options to foster non-automobile travel. For instance, in the Year 2000, the City created a Multi-Use Pathway fund of \$500,000 to be annually allocated for the development of sidewalks, sidepaths, bike lanes, and additions to the City's multi-use path system. While the intention of this fund is to be renewed annually, the City Parks Department has also pursued transportation enhancement grants to supplement this investment and facilitate trail development. For example, approximately

one million dollars of non-local money has been spent on development of the Clear Creek Trail system. Additionally, the City has adopted a 10-year Alternative Transportation and Greenways System Plan. This plan will act as a guide to facilitate annual investments in alternative transportation. Investments in the Greenways Plan, potentially beyond the money currently allocated, will be necessary to affect the trip-making patterns of Bloomington's citizens.

### Implementation Measures

- MT-6** Implement alternative transportation projects annually as outlined in the City's Alternative Transportation and Greenways System Plan. Seek to increase current local funding to ensure more rapid plan execution.
- MT-7** Identify and solicit transportation enhancement grants to assist in the funding of selected alternative transportation projects such as the construction of a multi-use trail along Jackson Creek and a multi-use trail along the CSX rail corridor.
- MT-8** Require the construction of pedestrian and bicycle facilities that provide safety and convenience in all new and redevelopment projects. Examples of features to be considered are sidewalks, pedestrian crosswalks, sidepaths, bicycle lanes, and bicycle racks.
- MT-9** Create true pedestrian corridors by increasing the number of large species, street trees in tree plots, and other pedestrian amenities within the right-of-way.
- MT-10** Ensure that designs for new construction and/or the retrofitting of existing intersections provide a safe environment for pedestrians to reduce crossing distances and include pedestrian signalization.



## PART 1: Policy Essence

### Policy 3: Implement Traffic Management Strategies

While the development of alternative transportation options is a key factor in mitigating traffic, systematic operational and regulatory changes must also be made by the City in order to optimize the efficiency of the existing roadway network. Substantial road widenings should only be considered as a last option after a thorough analysis of all alternatives. An on-going monitoring system to track traffic growth throughout the community must be established. The purpose of this monitoring is two-fold. First, it will allow for the prioritization of street improvements as identified in the *Bloomington/Monroe County Year 2025 Transportation Plan*. Second, it will allow congested roadways to be identified as areas that require special analysis during the development review process.

Regulatory approaches are also effective in managing the impacts of traffic growth. As development continues within the urbanized area, the demand for driveway cuts on arterial roadways will increase. In order to increase the efficiency and safety of major arterials, greater controls on the location and spacing of driveway cuts are necessary. The reduction of access points onto these street networks also greatly enhances the safety of bicyclists and pedestrians.

Decades of suburban-style development have created a transportation system that concentrates traffic on a limited number of major arterial roadways. As the community grows, more and more pressure is placed on these limited roadways to handle the traffic burden. One of the major factors underlying this trend is the lack of roadway connectivity from neighborhood to neighborhood, as well as from neighborhoods to commercial areas. Residential development trends have created enclaves of homes isolated from one another, all relying on the same arterial roadway to connect to each other and the wider community. Increased connectivity would provide for multiple routes of travel, relieving pressure on major arterials by providing options to the traveler.

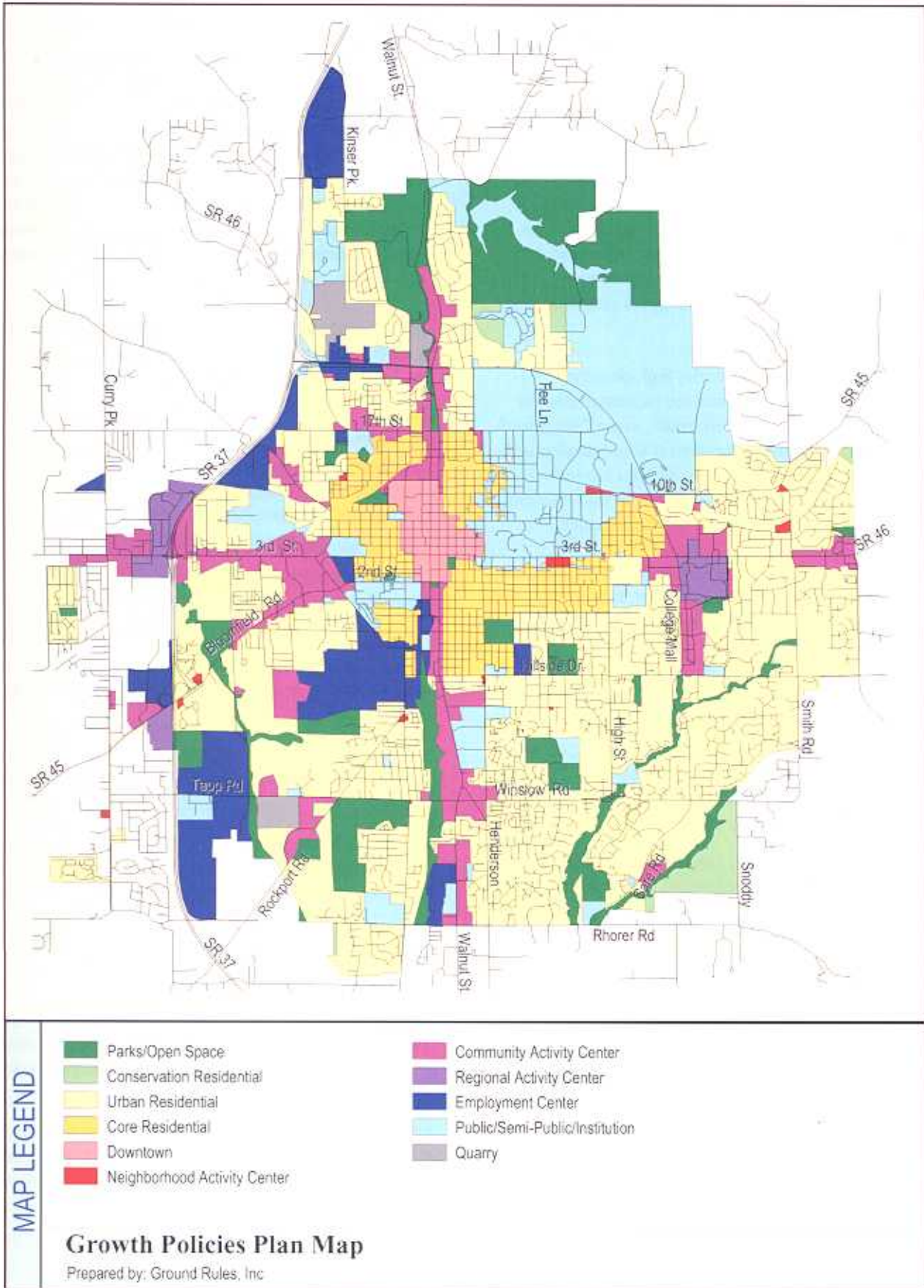
### Implementation Measures

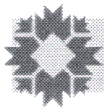
- MT-11** Continuously monitor traffic growth along major arterial corridors through the development of an on-going traffic counting program. Additionally, investigate the feasibility of creating an alternative transportation counting program.
- MT-12** Develop rigorous access management standards for collector and arterial level streets.
- MT-13** Ensure the provision and linkage of street stubs to improve connectivity within all sectors of the community.
- MT-14** For street stubs ending in vacant property, install signs indicating that these streets will be connected at the time of future development approval.





## PART 2: The Geography of the Policies





### Core Residential

#### Intent

This category encompasses those neighborhoods surrounding Bloomington's downtown and Indiana University. These areas are neighborhoods of cottages and bungalows (some architecturally and historically distinctive) built at higher densities than more recent residential development. Core Residential areas are characterized by a grid-like street system, alley access to garages, small street setbacks, and a mixture of owner occupants and rental tenants. The unique character, urban form and land use pattern of the near-downtown residential areas must be protected and enhanced.

#### Land Use

The predominant land use for this category is single family residential; however, redevelopment has introduced several uncharacteristic uses such as surface automobile parking, apartments, offices, retail space and institutional activities. This district is designed primarily for higher density single family residential use. The existing single family housing stock and development pattern should be maintained with an emphasis on limiting the conversion of dwellings to multi-family or commercial uses, and on encouraging ongoing maintenance and rehabilitation of single family structures. Multi-family (medium and high-density) residential and neighborhood-serving commercial uses may be appropriate for this district when compatibly designed and properly located to respect and compliment single family dwellings. Neighborhood-serving commercial uses, and possibly even office uses, may be most appropriate at the edge of Core Residential areas that front arterial street locations. More specific land use policies include:

- Allow multi-family redevelopment along designated major streets, in transition areas between the downtown and existing single family residential areas, and when appropriately integrated with adjacent uses per adopted form district requirements.
- Explore opportunities to introduce nodes of appropriately designed, neighborhood scaled commercial uses within the core neighborhoods.
- Discourage the conversion of single family homes to apartments.
- Utilize targeted tax abatements and grant programs in specific neighborhoods to provide incentives for increased owner occupancy and affordable housing construction.

#### Urban Services

Core Residential Areas have full accessibility to necessary urban services. Therefore, the main objective for these areas is to maintain adequate levels of urban service and where possible to improve the capacity and aesthetics of all urban services. In some core neighborhood areas, existing utilities infrastructure is outdated and deficient, and must be upgraded, with assistance from the City, as a component of infill development.

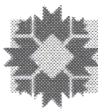
- Promote neighborhood enhancements of public improvements such as sidewalks, streetlights, street trees and landscaping, and playgrounds and play areas.
- Opportunities to repair and upgrade underground utilities must be pursued in order to preserve the capacity of aging utilities in the urban core.
- When major utilities projects are required, other urban amenities (sidewalks, landscaping, etc.) should be upgraded simultaneously to reduce the need for multiple construction processes.
- In new development or redevelopment projects, utilities should be placed underground and located so as to minimize potential conflicts with trees and other landscaping features.
- The City should reduce cost barriers for affordable housing providers by upgrading deficient utilities in core neighborhoods.

#### Site Design

The majority of core neighborhoods have been built out, so major changes will occur with redevelopment and property turnover. Redevelopment and rehabilitation of existing structures should respect the unique character and development pattern of the Core Residential areas. Core Residential development should emphasize building and site compatibility with existing densities, intensities, building types, landscaping and other site planning features.

- The Zoning Ordinance should include new site planning standards that reflect existing patterns of development in core neighborhoods (Form Districts).
- Residential parking should be encouraged to utilize garages accessed by alleys to the rear of properties, while front yard parking shall be prohibited.





## PART 2: The Geography of the Policies

### Neighborhood Activity Center (NAC)

#### Intent

The Neighborhood Activity Center (NAC) is a mixed commercial node that serves as the central focus of each neighborhood. The NAC must be designed so that it serves the neighborhood adequately without attracting an influx of usage from surrounding areas. It must also be located so that it is easily accessible by pedestrians, minimizing automotive traffic throughout the neighborhood. The Neighborhood Activity Center will provide small-scale retail and business services within the context of neighborhoods while maintaining compatibility within the existing fabric of development. It should be noted that while several NACs have been identified on the land use map, more could be designated in the future as further study is done and appropriate locations have been identified.

#### Land Use

A NAC should contain a mix of neighborhood scale retail and office space, as well as services such as day care and higher density housing. Housing elements are ideally integrated with nonresidential elements such that housing units are situated above commercial and office space. In some cases, a NAC can be located within the center of a Core Residential or Urban Residential area, most probably through the redevelopment of an existing non-residential use (i.e. the K & S Country Market on East 2nd Street). In other cases, a NAC will need to be located closer to the neighborhood edge in order to ensure greater compatibility and financial viability.

- The main focus of the NAC should be commercial uses at a scale that serves the immediate neighborhood, including such services as small food stores, video rental, or small cafes.
- Office uses and public/semi-public uses are acceptable when built to generate minimal traffic attraction to the neighborhood.
- Residential uses should be limited to multifamily development, ideally on floors above street level commercial uses.
- Commercial uses should be restricted to ensure their neighborhood focus.

#### Urban Services

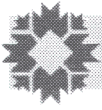
A Neighborhood Activity Center will be placed in a developed neighborhood, where most urban services have been previously provided. This includes access to sewer, water, electricity, and gas lines that should already be serving the existing neighborhood. This type of development is intended as an alternative to new commercial growth in areas where such utilities do not already exist.

- Public Transit as an urban service must be a key element in the location of the NAC, providing access to people outside the neighborhood without the need for personal vehicles. All newly developed NAC's must be located within walking distance (5-10 minutes) of a major public transit stop.
- The roadways that a NAC is developed around should be Collectors (Secondary or Primary) as designated on the City's Master Thoroughfare Plan.
- The development of an NAC should include coordination on the completion of an adequate sidewalk network throughout the immediate neighborhood it serves, if no such network exists at the time of development.
- In new development or redevelopment projects, utilities should be placed underground and located so as to minimize potential conflicts with trees and other landscaping features.

#### Site Design

Compatibility with surrounding established neighborhoods is one of the most important factors in the development of a Neighborhood Activity Center. Although it represents the smallest scale of commercial land use, the NAC is a high-density node of activity that will affect a neighborhood. The introduction of a commercial node into a primarily residential area requires great sensitivity to the design and scale of the existing structures, as well as responsiveness to the needs of the surrounding residents. NAC's must relate to surrounding residential neighborhoods and not adversely affect the livability of these neighborhoods through traffic, lighting, noise, litter or other impacts. The careful combination of pedestrian facilities and structural features will help to define the streetscape of the NAC.

- The height of new commercial structures in a NAC shall be limited to three stories in order to minimize the impact of such uses on surrounding residents.
- Sidewalks, street trees, pedestrian-scale lighting and other decorative features must be standard elements of the NAC streetscape.



## PART 2: The Geography of the Policies

- Bus stops, bus pull-offs, or shelters shall be incorporated to maximize transit trips to the NAC.
- In order to define the center, buildings should be pushed to the front edge of the site, framing the four corners of the commercial node at the street intersection.
- Any parking that is provided for a NAC should be primarily serving any residential units that are a part of the development rather than used as an attractor for commercial users.
- Parking should be located in the side or rear of buildings, and can be made accessible from an improved alley system in order to minimize street cuts in front of buildings.
- All parking areas should also be heavily landscaped in order to soften their impact on the neighborhood.



## Typical Cross Sections Introduction

On the following pages are typical cross sections and text descriptions for:

1. Existing Core Neighborhood .....	pg 84
2. Neighborhood Streets .....	pg 85
3. Secondary Collectors .....	pg 86
4. Primary Collectors .....	pg 87
5. Secondary Arterials .....	pg 88
6. Primary Arterials .....	pg 90

Each of these sections states the typical characteristics, priorities for the right-of-way, and examples of traffic management features. Pictures and illustrations are included to better depict the “typical” features of each type of street.

### Typical Street Cross Section Standards Overview

For quick reference, the below table notes the typical cross section standards by road classification.

### Typical Street Cross Section Standards

Functional Class	Minimum Right-of-Way	Number of Moving Lanes	Number of Parking Lanes	Median Divider	Pavement Cross Section					Border Section	
					Through Lanes	Auxiliary Lanes	Parking Lanes	Bike Lane	Curb & Gutter (each side)	Tree Plot	Sidewalk (each side)
Neighborhood Street	50'	2	1 if density requires	None	10'	None	8'	None	6"	4.5'	5'
Secondary Collector	55'	2	1 if density requires	None	11'	None	8'	None	6"	6.5'	5'
Primary Collector	65'	2	1 if density requires	None	11'	None	8'	4' (b)	6"	7.5'	5'
Secondary Arterial	80' (a)	2	1 if density requires	14' (c)	12'	12'	8'	4' (b)	6"	7.5'	5'
Primary Arterial	100' (a)	4	1 if density requires	14' (c)	12'	12'	8'	4' (b)	6"	7.5'	5'

Notes: (a) Wider right-of-way near intersections might be required for turn lanes. Right-of-way may be reduced in built out areas.

(b) An 8' sidepath accommodating both bicycle and pedestrian traffic may be substituted provided the street has a limited number of access points or is located in a low-density area.

(c) A 14' left-turn lane might be required when a median does not provide traffic separation.





### Existing Core Neighborhood Streets

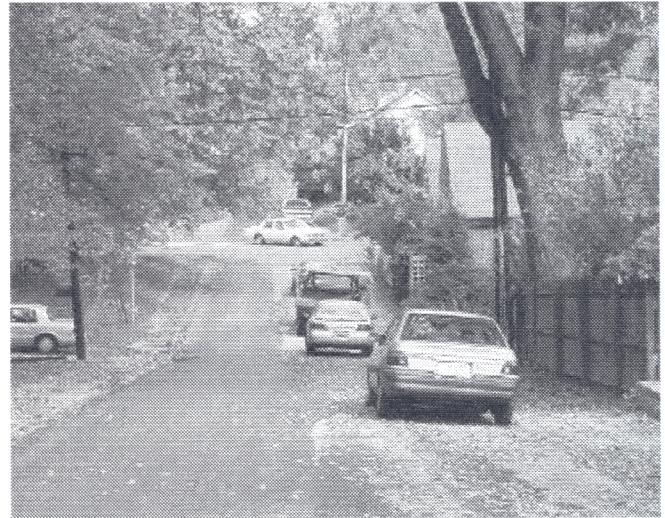
The core neighborhood streets in Bloomington represent a high percentage of the total road mileage in the area surrounding the downtown. Many of these streets are quite narrow in width in comparison to the neighborhood streets that were constructed in the past few decades. The cross-sections of the existing core neighborhood streets are considered an important element of the residents' living environment and often serve as a meeting place for residents. It should be noted that many of these streets feature cross-sections different from what is outlined in the following pages. In order to preserve neighborhood fabric, existing core neighborhood streets shall not be required to conform to the cross section standards that are being proposed for more suburban environments.

#### Typical Characteristics of a Existing Core Neighborhood Street:

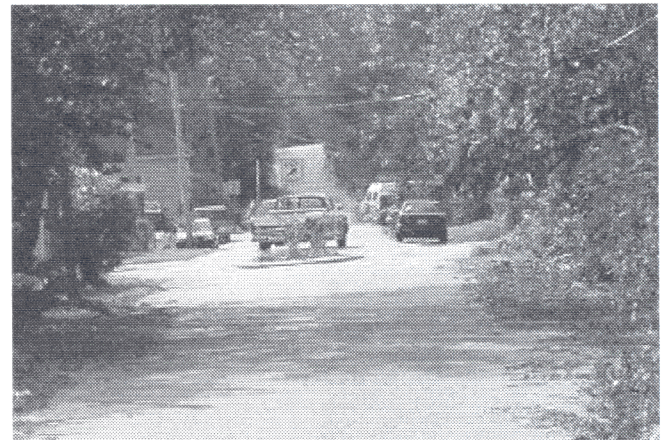
- Total right-of-way – varies
- Travel lane widths – typically 8 to 10 feet
- On-Street parking included
- Bike lanes – no lanes are included because of lower traffic volumes and speed
- Sidewalks and street trees vary; often there is not enough room for both elements

#### Priority For the Right-of-Way:

- Primary Priority Elements
  - On-Street parking
  - Residential access
  - Neighborhood preservation
- Secondary Priority Elements
  - Width of travel lanes
  - Sidewalks
- Examples of Traffic Management Features
  - On-street parking
  - Street trees
  - Narrower travel lanes
  - Reduced pedestrian crossing distances at intersections (using curb extensions and other measures if necessary)
  - Traffic circles



*Jackson Street at 3rd Street looking north*



*Waldron Street at 6th Street looking south*



*William Street at 9th Street looking south*





## Secondary Arterials

The typical cross section for a secondary arterial street is indicated in Figure 4. These two-lane divided streets in Bloomington typically carry between 10,000 to 20,000 vehicles per day (depending upon the number of traffic signals per mile). A median of 14 feet provides motorists with a left-turn lane into and out of the arterial street.

Due to higher traffic volumes, 4 foot bike lanes that interconnect with signed bike routes and neighborhood streets are required. In lieu of bike lanes, an 8 foot sidepath may be considered on one side of the street provided the street has a limited number of access points or is located in a low-density area.

### Typical Characteristics of a Secondary Arterial:

- Total right-of-way – 80 feet
- Median width – 14 feet or wider
- Travel lane widths – 12 feet
- Bike lanes – 4 foot lanes (an 8 foot sidepath may be substituted if appropriate)
- On-street parking not included (8 foot parking lane(s) may be added for higher density developments with limited on-site parking)
- Sidewalk, street trees and a utility area included

### Priority For the Right-of-Way:

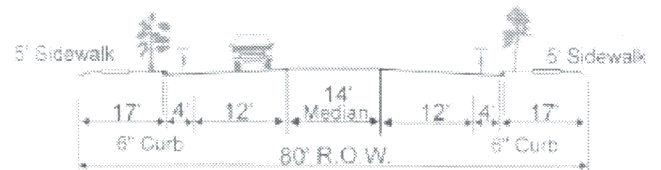
- Primary Priority Elements
  - Sidewalks with transit access
  - Tree plots
  - Bike lanes (or other bicycle facilities)
- Secondary Priority Elements
  - Width of travel lanes
  - Median
  - Left-turn lanes

### Traffic Management Features:

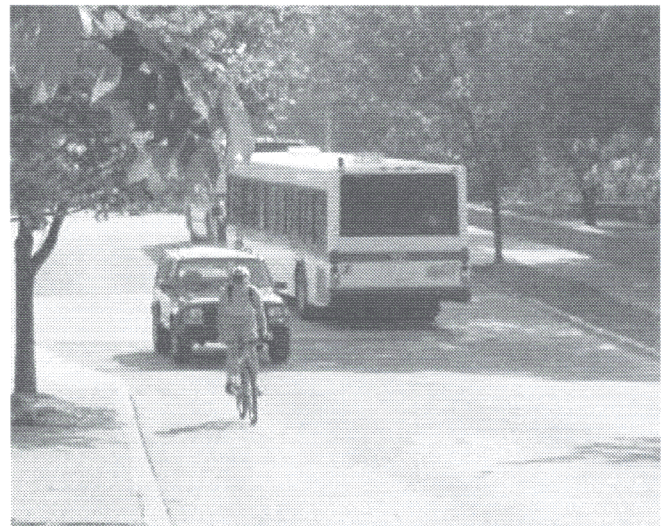
- Street trees and landscaped medians
- Narrower travel lanes
- Reduced pedestrian crossing distances at intersections
- Roundabouts
- Acceleration and deceleration lanes
- Wider turning radius at intersections and access points
- Synchronization of traffic signals



*Kinser Pike with the golf course to the left looking south*



*Figure 4: Typical Secondary Arterial Cross Section*

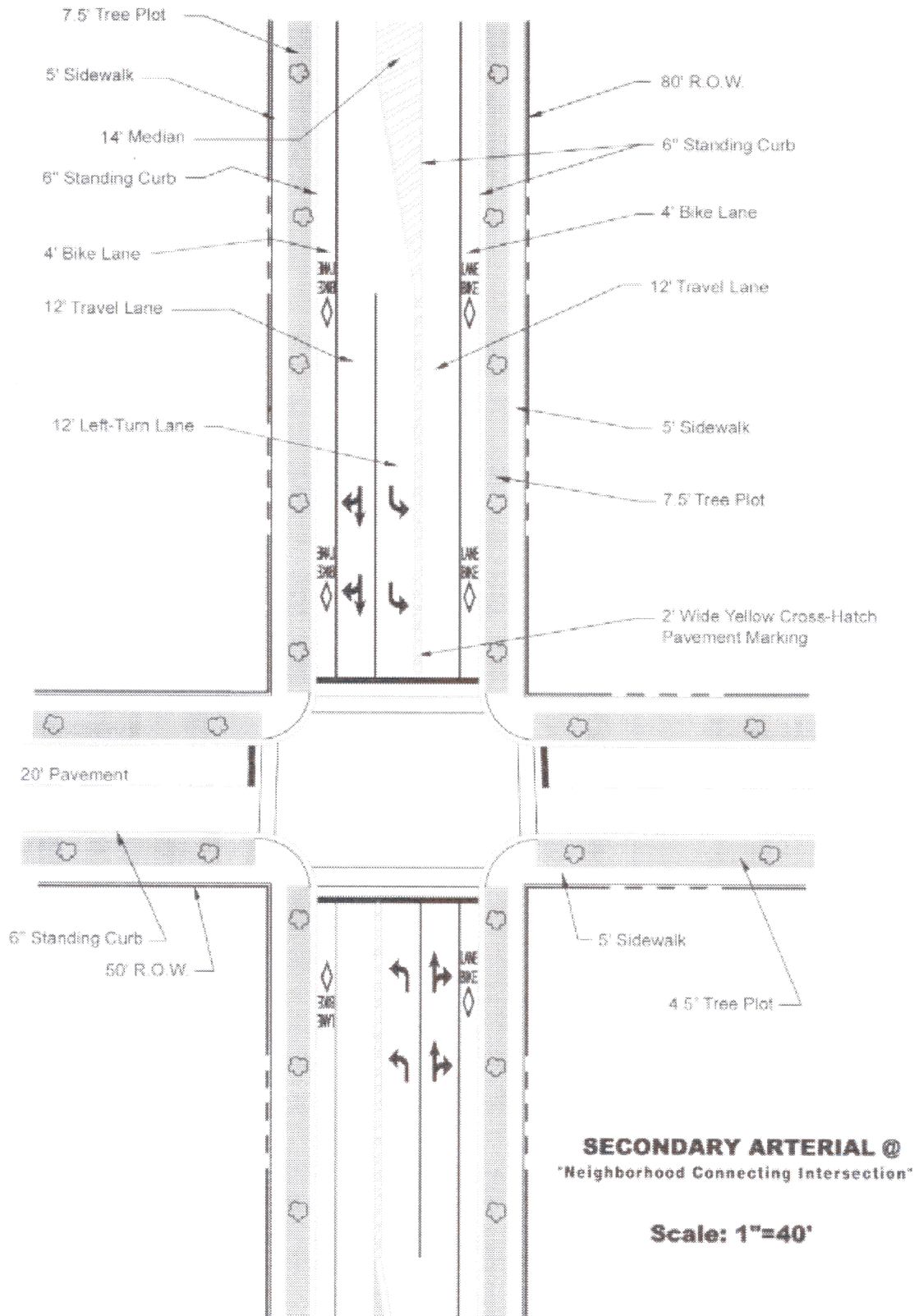


*10th Street at Fee Lane looking east*



## PART 5: Master Thoroughfare Plan

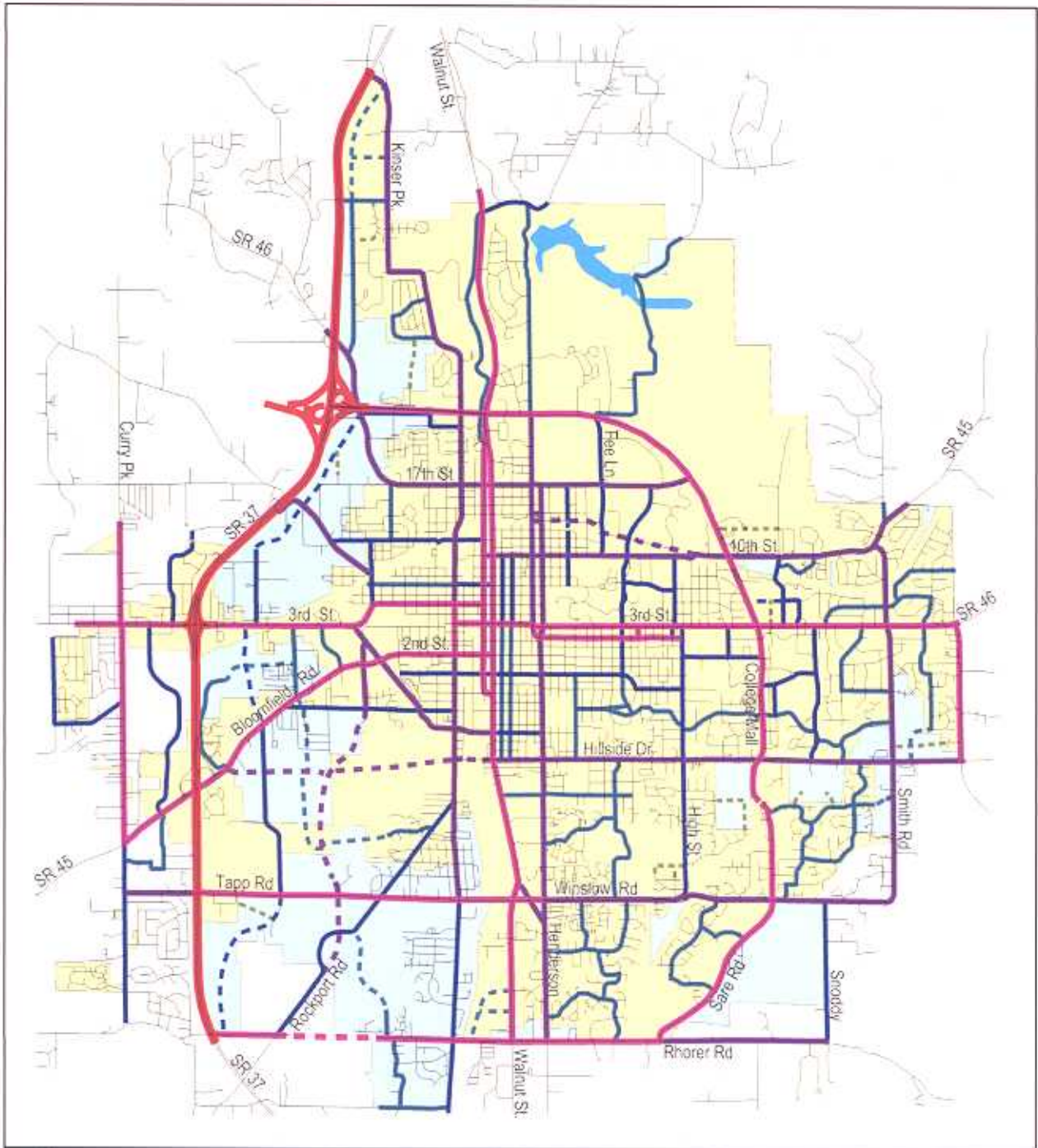
Shown here is a typical plan view of a secondary arterial intersecting with a neighborhood connecting street. This figure illustrates the treatment of an on-street bike lane as it passes through this type of intersection. This figure also illustrates the transition from a continuous median to a left-turn lane as the intersection is reached.







# PART 5: Master Thoroughfare Plan



## MAP LEGEND

- Freeway/Expressway
- Primary Arterial
- Secondary Arterial
- Primary Collector
- Secondary Collector
- Local Street
- Proposed Primary Arterial
- Proposed Secondary Arterial
- Proposed Primary Collector
- Proposed Secondary Collector
- Proposed Local Street
- Municipal Boundary
- Additional Planning Jurisdiction

## Master Thoroughfare Plan Map

Prepared by: Ground Rules, Inc

In the Council Chambers of the Showers City Hall on Wednesday, March 24, 2004 at 7:30 pm with Council President Diekhoff presiding over a Regular Session of the Common Council.

COMMON COUNCIL  
REGULAR SESSION  
March 24, 2004

Roll Call: Banach, Diekhoff, Ruff, Gaal, Rollo, Sturbaum, Volan, Sabbagh, Mayer

ROLL CALL

Council President Diekhoff gave the Agenda Summation.

AGENDA SUMMATION

The minutes of Special Session March 10, 2004 were approved by a voice vote.

APPROVAL OF MINUTES

Jason Banach offered Happy Birthday wishes to Chris Sturbaum.

MESSAGES FROM  
COUNCILMEMBERS

Banach offered, for the council's acceptance, a Conflict of Interest Statement concerning Resolution 04-07. He stated he was an employee of the Indiana University Real Estate Department and therefore represents the Landlord with respect to the leasing part of University property (501 North Morton) to the City of Bloomington Utilities. He said he would sit out that vote.

- Banach – Conflict of Interest Statement

It was moved and seconded that this Conflict of Interest be accepted.

Chris Sturbaum said this year held an opportunity for the community to set out its plans for the future. He said that while there would be elections nationally and county wide this year, it was important for the city to get past last year's election and urged elected officials and community members to work together for the best of the community. He said it would be best to work with sincere and honest people in deliberations.

David Sabbagh, having just attended a Park and Ride briefing, said he was a strong supporter of the program but admitted it needed modifications. He added that Indiana University should be involved in the process of transporting students to campus and urged the mayor to facilitate this collaboration. He stated that the portion of 7<sup>th</sup> Street at the heart of campus should be opened to bus traffic to facilitate this transportation and urged the involvement of the entire council.

Timothy Mayer thanked the street crews for sweeping the streets recently, and noted that the "green machine" was also seen sweeping sidewalks downtown. He acknowledged Engineering Services Manager Justin Wykoff's late night work on behalf of the city, and noted the coming spring.

He thanked Matt Pierce, State Representative for District 61, for keeping local elected officials updated on the actions of the state legislature. He said it was welcome.

Mark Kruzan, Mayor, addressed the recently reported news that the city faced a half million dollar budget shortfall for this fiscal year. He announced that while this would necessitate cuts in spending, no jobs would be lost and the city would not lay off anyone. He added that dipping into reserves would be a very distant second choice to solve the problem.

MESSAGE FROM THE MAYOR

Kruzan said that public disclosure, council input along with department heads and employee input would help in finding the solution to this problem. He thanked the public works department for already suggesting specific cuts in spending. He said we shouldn't lose our vision, but need to find one that is within our means.

Kruzan asked Susan Clark, Interim Controller, to give a more detailed explanation of the revenue shortfall. She listed numerous actions of



House Enrolled Act 1001, including the portion that changed the assessed valuation of the city. She said this was part of the tax restructuring in going to a market value based system as required by the supreme court. Clark explained that when this resulted in a lower Net Assessed Value for each of the townships within the city, the information came too late to change the already advertised tax rate, and thus the decrease of the city's revenue.

Clark said that the effects of the reduction in inventory assessments hit the city by surprise and that there would be more reductions in Pay 2007.

Sturbaum asked who benefited from this legislation, to which Clark said that no one particular property owner benefited more than another.

Volan asked when this decrease was first known, to which Clark said this was apparent only after the fact. Volan asked why the delay, to which Clark said she got the first bit of information about this at the end of January, at which time it was too late to react to the news. She added that the budget order was received on March 5, 2004, and that was when the actual numbers could be viewed by the city so that the impact could be quantified.

Rollo asked how this could be prevented in the future. Clark said that the city tries to stay in tune with the legislature but with the tax restructuring in 2002, this one was not on the radar screen.

Ruff thanked Clark for her reduction in levy spread sheets, especially with regards to all the city funds. He asked what flexibility there was in moving money in the funds, to which Clark said that there was some flexibility, but that state law required payback by the end of the year.

Diekhoff thanked Clark for her presentation.

Mayer, the council representative to the Utilities Service Board, reported that upon being questioned by the board regarding claims for cleaning services to the Utility department, the staff stepped up to do the cleaning themselves. He lauded this effort.

There was no public comment.

There were no appointments to boards or commissions.

It was moved and seconded that Resolution 04-04 be introduced and read by title and synopsis. Clerk Moore read the legislation and synopsis, stating that there was no committee recommendation. She also announced that the public comment portion of this item would serve as the legally advertised public hearing. It was moved and seconded that Resolution 04-04 be adopted.

Ron Walker, Economic Development Director, stated that this resolution would approve a five year tax abatement for real property improvements for Bloomington Paint and Wallpaper's new construction on the northwest corner of Walnut Street and West Grimes Lane. He described the two story brick and limestone building as having two businesses on the lower level (Bloomington Paint and Wallpaper and Bloomington Carpet Company) and six, 1000 square foot apartments on the second floor.

Walker said the total project is estimated at \$1,010,000 and would create two to four jobs based on the projected growth of this business. He noted that the petitioner worked very cooperatively with the Economic Development Commission and the mayor office to bring forth a quality project.

MESSAGE FROM THE MAYOR  
(cont'd)

COMMITTEE REPORTS

PUBLIC INPUT

BOARD AND COMMISSION  
APPOINTMENTS

LEGISLATION FOR SECOND  
READING

Resolution 04-04 To Confirm  
Resolution 04-03 Which Designated  
an Economic Revitalization Area,  
Approved a Statement of Benefits,  
and Authorized a Period of Tax  
Abatement - Re: 1116 South Walnut  
(Bloomington Paint and Wallpaper,  
Inc., Petitioner)

Mr. Knight, the petitioner, had no comments, and there were no public comments.

Resolution 04-04 (cont'd)

Mayer thanked the petitioner for bringing the project forward and said it looked like a good project. He wished them well in their new location.

Sturbaum said it was a good project and congratulated the petitioner for bringing such a good building forward and said he was glad the city could help.

Gaal, while noting the brevity of the discussion, noted that previous committee hearings on this resolution had served to flesh out the proposal

Rollo encouraged citizens to support local businesses such as this one.

Resolution 04-04 received a roll call vote of Ayes: 9, Nays: 0.

It was moved and seconded that Resolution 04-04 be introduced and read by title and synopsis. Clerk Moore read the legislation and synopsis, stating that there was no committee recommendation. It was moved and seconded that Resolution 04-04 be adopted.

Resolution 04-06 To Authorize Agents to Accept Pension Liability on behalf of the City of Bloomington

(Mary) Susan Clark, Interim Controller, explained that this housekeeping type of resolution would allow the city to comply with changes in PERF since 1999. She said the Transit Controller was also included in the agents designated to serve as PERF agents.

Resolution 04-04 received a roll call vote of Ayes: 9, Nays: 0.

It was moved and seconded that Resolution 04-07 be introduced and read by title and synopsis. Clerk Moore read the legislation and synopsis, stating that there was no committee recommendation. It was moved and seconded that Resolution 04-07 be adopted.

Resolution 04-07 To Determine the Need to Lease a Portion of 501 North Morton Street in Bloomington, Indiana

Vicki Renfrow, Legal Department Attorney, said that under state law, there were a number of steps that needed to be taken in order for a political subdivision to enter into a lease of real property. She said this transaction needed to have a petition signed by at least 50 taxpayers of the political subdivision of concern (available in the council office), and this was now completed. She said that the temporary leasing arrangement now needed the council's approval noting that this arrangement was necessary because of a fire at the South Henderson utilities building. She noted that the landlord, Indiana University, had been very helpful in the lease arrangements and noted also that a public hearing would take place at the Utilities Service Board meeting on April 5, 2004.

Volan asked if the department intended to return to the old location, to which Renfrow answered that the decision had not yet been made. She said if a long term arrangement would occur, this process would be repeated for a long term lease.

Volan said he wondered why the utilities department had been so far away from city hall in the past. James McNamara, Deputy Mayor, said there were some water lines ends and storage tanks that had been located at South Henderson thirty years and it made sense at the time to locate administration of the facility there. He also said that a committee was exploring the pros and cons of the location of utilities offices at this time and that Council member Mayer sat on that committee.

Volan asked about creating an opening between the IU and City Hall sections of the Showers building to which McNamara said it was feasible.

In answering more questions from Volan, McNamara said the short term lease would be for 11,000 square feet of space which was approximately one sixth of the office floor space.

Resolution 04-07 (cont'd)

Volan asked if there would be security issues having non-city offices nearby, to which McNamara said that in the long term there could be flexibility with the space configuration. He added that utilities had not expressed concerns about security risks, but that it should be considered with long term arrangements.

Volan questioned the desirability of having utilities and city hall close in proximity, to which McNamara said there was a diversity of opinions at this time. Volan asked how much of the office space at 501 N. Morton was currently rented. Banach, (who was in the back of the audience portion of the council chambers but who was not participating in the discussion because of his relationship with IU, the landlord in the arrangement) offered the information that 90% of the space had been spoken for in the next couple of months.

Mayer thanked Volan for his probing questions and said that there was a committee looking into the space needs for the Utilities department and that the north portion of the Showers building was an option for them. He said there were time, money and insurance settlement issues to be considered first.

Volan thanked IU for its support.

Resolution 04-07 received a roll call vote of Ayes: 8, Nays: 0 (Banach did not participate in the discussion or vote.)

There was no legislation for first reading.

LEGISLATION FOR FIRST READING

David R. Grubb made a statement about the relationship of Monroe County residents to City of Bloomington residents. He complained that Third Street had not been extended to the west side.

PUBLIC INPUT

The meeting was adjourned at 8:30 pm.

ADJOURNMENT

APPROVE:

ATTEST:

Michael Diekhoff, President  
Bloomington Common Council

Regina Moore, CLERK  
City of Bloomington

In the Council Chambers of the Showers City Hall on Wednesday, July 14, 2004 at 7:30 pm with Council President Diekhoff presiding over a Regular Session of the Common Council.

COMMON COUNCIL  
REGULAR SESSION  
July 14, 2004

Roll Call: Banach, Diekhoff, Ruff, Gaal, Rollo, Sturbaum, Volan, Sabbagh, Mayer

ROLL CALL

Council President Diekhoff gave the Agenda Summation

AGENDA SUMMATION

It was moved and seconded to postpone the approval of the minutes of February 18, 2004, April 28, 2004, and May 5, 2004. Mayer said he wanted more time to review the lengthy documents.

APPROVAL OF MINUTES

The motion was approved by a roll call vote of Ayes: 9, Nays: 0.

Jason Banach wished President Diekhoff a Happy Birthday. He noted he would be absent for the Budget Hearings next week from Monday to Wednesday because would be attending a meeting of the Coalition for University Regional Economy. He added that this conference dealt with how cities and universities could cooperate for the betterment of their communities.

MESSAGES FROM  
COUNCILMEMBERS

Chris Sturbaum mentioned changes in Bloomington and notes that the Growth Policies Plan recommended a professional downtown planner. He mentioned the concept of demolition delay adding that it was important to save what is best of Bloomington.

Chris Gaal encouraged people to see the film Fahrenheit 9/11.

Stephen Volan gave an update from the Solid Waste District Board saying that the district is waiting on a drafted contract from Hoosier Disposal that would negotiate the closure of the landfill and future management of solid waste disposal in Monroe County. He reported that a sub-committee would review that contract.

David Sabbagh congratulated Bloomington POPS for their performance on July 3<sup>rd</sup>.

Tim Mayer wished all freedom-loving people around the world "Happy Bastille Day."

Julie Ramey from The Parks and Recreation Department gave a brief presentation announcing National Parks and Recreation Month and outlined local recreational opportunities.

MESSAGE FROM THE MAYOR

There were no reports.

COMMITTEE REPORTS

George Brooks, citizen, responded to a guest column in the Herald Times about the Living Wage Initiative. He refuted its arguments by saying that what would make a person competitive in the workforce is not necessarily higher skills, but people who are willing to settle for the least amount of pay thus resulting in workers being paid not on their productivity level but their replaceability level.

PUBLIC INPUT

It was moved and seconded that the following appointments be made:

BOARD AND COMMISSION  
APPOINTMENTS

Deb Backhus	Environmental Commission
Ross Britain	Environmental Commission
Suzann Owen	Telecommunications Council



It was moved and seconded that the following reappointments be made:  
Jesse Stryker           Telecommunications Council  
Byron Bangert           Human Rights Commission  
Emily Bowman           Human Rights Commission

Board and Commission  
Appointments (cont'd)

All of the above were approved by voice votes.

There was no legislation for final action at this meeting.

LEGISLATION FOR SECOND  
READING

It was moved and seconded that the following legislation be introduced and read by title and synopsis only. Clerk Moore read the legislation by title and synopsis.

LEGISLATION FOR FIRST  
READING

Appropriation Ordinance 04-04 To Specially Appropriate from the Parking Enforcement Fund Expenditures Not Otherwise Appropriated (Appropriating Funds from the Parking Enforcement Fund for New Positions in the Parking Enforcement Division and the Purchase of Equipment)

Appropriation Ordinance 04-04

Ordinance 04-14 To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles and Traffic" – Re: Changes to Chapter 15.32 (Parking Controls), Chapter 15.37 (Residential Neighborhood Permit Parking), Chapter 15.38 (Public Employee Parking Permits), Chapter 15.40 (Municipal Parking Lots, Garages, and On-Street Metered Parking), Chapter 15.48 (Emergency Removal and Impoundment of Vehicles), and Chapter 15.64 (Traffic Violation Schedule)

Ordinance 04-14

Ordinance 04-15 To Amend the Ordinance Which Fixed the Salaries of Appointed Officers and Employees of the Civil City (Ordinance 03-20) For the Year 2004 – Re: Adding a Parking Enforcement Officer and Two Security Guards in the Parking Enforcement Division

Ordinance 04-15

Ordinance 04-16 To Amend the Ordinance Which Fixed the Salaries of Appointed Officers and Employees of the Civil City (Ordinance 03-20) For the Year 2004 – Re: Positions in the Clerk's Office and the Council Office

Ordinance 04-16

Ordinance 04-17 To Authorize Reestablishment of the Cumulative Capital Development Fund

Ordinance 04-17

There was no public input at this point in the meeting.

PUBLIC INPUT

The meeting was adjourned at 8:06 p.m.

ADJOURNMENT

APPROVE:

ATTEST:

Michael Diekhoff, President  
Bloomington Common Council

Regina Moore, CLERK  
City of Bloomington