

**BLOOMINGTON TRAFFIC COMMISSION  
AGENDA  
April 27, 2016  
5:30 P.M. – COUNCIL CHAMBERS**

- I. Call to Order
- II. Approval of Minutes – February 24, 2016
- III. Public Comment
- IV. Communications from Commission
  - A. City Projects Update
  - B. 3<sup>rd</sup> and Highland Intersection
- V. Reports from Staff –
- VI. Old Business –
  - A. No Parking Zone – W. Smith Avenue between N. Rogers Street and S. Jackson Street (north-side of street); and between S. Jackson Street and S. Fairview Street (south side of street)\*
- VII. New Business –
  - A. Bloomington Transit Driver’s Ideas for Route 6 (information only, no action required)

Traffic Inquiries – none

- VIII. Adjournment

*Next meeting – May 25, 2016*

*\*Action Requested/Public comment prior to any vote (limited to five minutes per speaker)*

**Auxiliary aids for people with disabilities are available upon request with adequate notice. Please call (812) 349-3429 or e-mail [human.rights@bloomington.in.gov](mailto:human.rights@bloomington.in.gov).**

**City of Bloomington Traffic Commission Minutes  
February 24, 2016 in the Council Chambers, City Hall**

*Traffic Commission minutes are transcribed in a summarized outline manner. Audio recordings of the meeting are available in the Planning and Transportation Department for reference. \*\*\*No digital recording for the 2/24/2016 was produced due to technical issues\*\*\**

**Attendance**

Traffic Commission: James Batcho, Andrew Cibor, Ryan Cobine, Markeus Farrand, Larry Haywood, Judi Maki, Abigail Pietsch, and Sarah Ryterband

Others in Attendance: Kylie Batcho, Rich Mordurgo, Janet Roberts, Fred Walkins, Doug Wissing, Neil Kopper (Staff), Nate Nickel (Staff), and Scott Robinson (Staff)

- I. Call to Order (~5:33 PM)**  
Elections of Chair and Vice Chair: Mr. Cibor nominated Sarah Ryterband and Ryan Cobine as the Chair and Vice Chair, respectively; Judy Maki seconded the motion. The motion passed 8-0.
- II. Approval of Minutes** – January 27, 2016. Ms. Ryterband motioned, and Mr. Cobine seconded, to approve the minutes. The motion passed 8-0.
- III. Communications from Commission** – Mr. Cibor thanked the Street Department for the great job they do maintaining the streets, especially on snowy days like today. Ms. Ryterband welcomed newly appointed members Markeus Farrand and Larry Haywood to the Commission.
- IV. Public Comment** - none.
- V. Reports from Staff**
  - A. 4<sup>th</sup> and Rogers Streets Intersection Improvement Project** – Mr. Kopper explained the intent of the project and opportunities to provide comments. A survey will close at the end of February and a meeting will be held in the spring to gather public feedback and highlight possible options.
  - B. Title 15 Updates** – none.
- VI. Old Business** - none
- VII. New Business** –
  - A. Resolution 16-01: Establishing Traffic Commission Rules and Procedures** - Mr. Robinson explained that by City code the Commission shall adopt rules and procedures. No current rules and procedures have been located, so staff is asking the Commission to adopt Resolution 16-01. Mr. Cibor mentioned that “traffic” includes pedestrians, bicyclists, busses,

as well as vehicles. He also said that safety is synonymous with “improving conditions”; both of which are implied within these rules. Mr. Cobine asked about the roll call vote and if these are typical for other Commissions. Ms. Maki asked about making future amendments. Robinson explained that staff worked with City legal staff and these generally follow standard rules and procedures. He also explained that the Commission may amend or adopt rules as necessary. Mr. Cobine motioned, and Ms. Pietsch seconded, to adopt Resolution 16-01. The motion passed 8-0.

- B. No Parking Zone – S. Gentry Street between 4<sup>th</sup> Street and W. Kirkwood Avenue (both sides of the street) -** Mr. Robinson explained that this is in response to Parking Enforcement concerns over lack of parking controls along Gentry Street. He spoke with two businesses along Gentry and both felt this proposal helps to address concerns. Mr. Farrand asked about access to the other drives and some members wondered about the area under the Hyatt’s awning. Mr. Robinson explained that with these changes access should be improved. Creating a loading zone under the awning may be counter to the 15-minute meter spaces that all hotels have in the downtown. The Street Department can restripe a line to help indicate the travel lane and the respective shoulder area under the awning to maintain traffic flow. Code changes can be made if access continues to be problematic. Mr. Cobine motioned, and Mr. Cibor seconded, to approve the recommendation and forward it to the City Council. The motion passed 8-0.
- C. No Parking Zone – W. Smith Avenue between N. Rogers Street and S. Jackson Street (north side of street); and between S. Jackson Street and S. Fairview Street (south side of the street) –** Mr. Nickel explained this request was also staff generated and is based upon access and safety concerns. He said the proposal is very similar to the existing conditions and regulations found on Prospect Street. It also reflects how Smith Avenue currently functions and this would codify the current conditions. Ms. Ryterband expressed concern over the lack of any specific problems or accidents and was unsure being proactive was needed in this instance. She also wanted to give the neighborhood association time to provide feedback and gather public comments. Mr. Wissing said six houses would be impacted from this proposal, the pavement does not extend to the full right-of-way width, there are encroachments along the street, and perhaps paint could be used to identify where parking could occur for each half block. Mr. Walkins said vehicles have side-swiped his camper while it was parked on the street. He has also received parking tickets for it before. The Commission wanted more information from staff on specific accidents or conflicts. They also wanted the neighborhood to have some time to consider the proposal and provide feedback. No formal action was taken on this request.

**D. Pedestrians – Title 15.060.050 amendments** – Mr. Robinson said that removing “jaywalking” and “pedestrians” from Title 15 would be more consistent with the state code. He explained that while this may seem counterintuitive towards pedestrian safety, this actually gives deference to pedestrians on locations to cross the street legally. Vehicles still must yield to pedestrians within crosswalks. Mr. Cibor mentioned that this was supported by the Bicycle and Pedestrian Safety Commission. Mr. Cobine motioned, and Mr. Haywood seconded, to approve these changes and forward it to City Council. The motion passed 8-0.

**VIII. Traffic Inquiries - none**

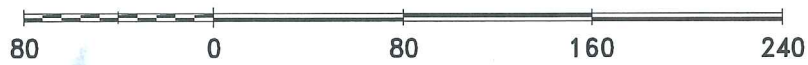
**IX. Adjournment (~6:45 PM)**  
*Next meeting – March 23, 2016*





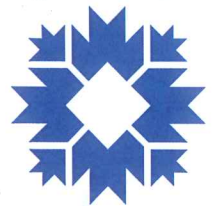
Intersection of E. 3rd Street and S. Highland Avenue

By: nickeln  
14 Apr 16



For reference only; map information NOT warranted.

City of Bloomington  
Planning & Transportation

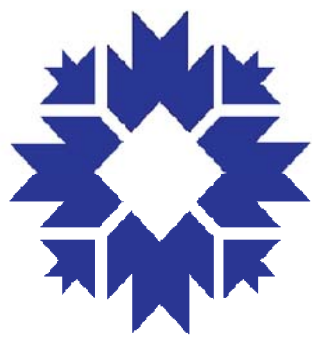


Scale: 1" = 80'





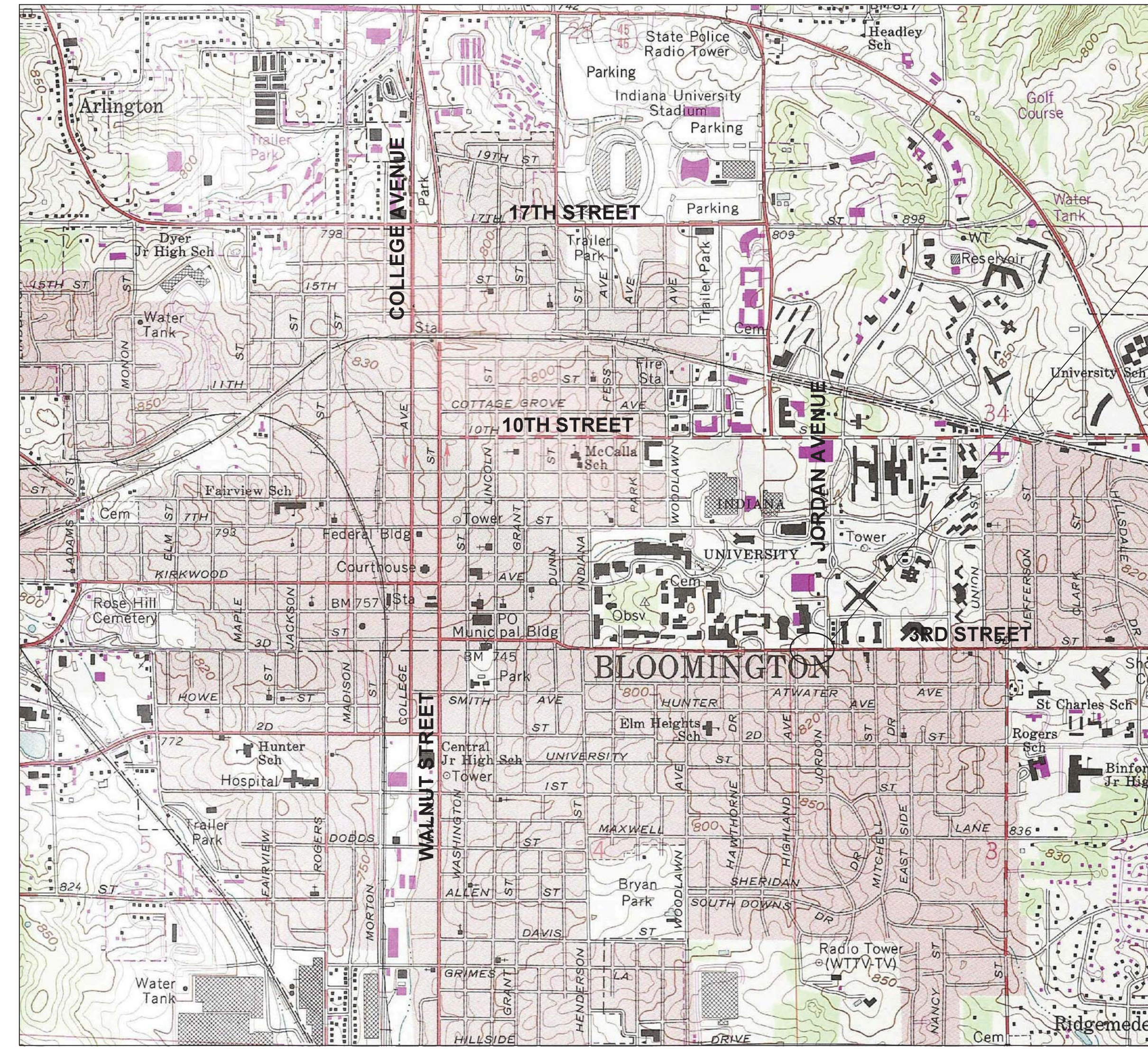
# CITY OF BLOOMINGTON



## IMPROVEMENTS ON SOUTH SIDE OF 3RD STREET BETWEEN HIGHLAND AVENUE AND JORDAN AVENUE

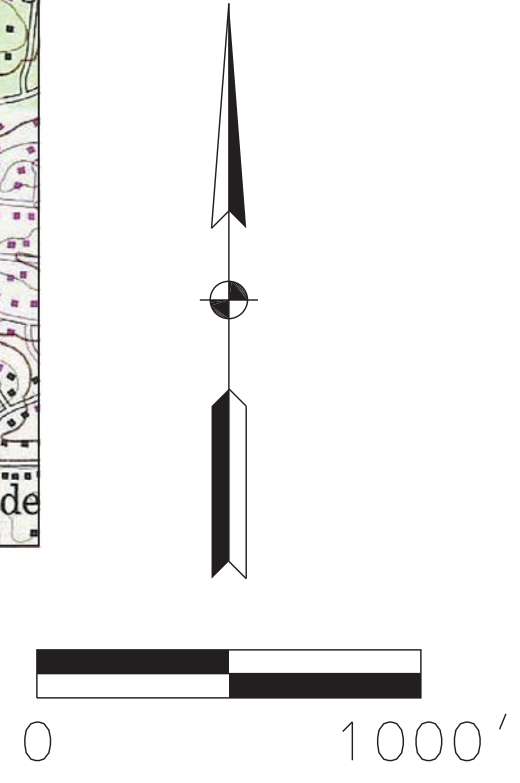
### INDEX OF DRAWINGS

- 1 TITLE SHEET
- 2 DETAILS AND STRUCTURE DATA
- 3-4 JORDAN AVENUE AND 3RD STREET  
SIDEWALK AND RAMP PLANS
- 5 MAINTENANCE OF TRAFFIC  
AND ACCESS PLAN




PROJECT LOCATION

**LOCATION MAP**  
BLOOMINGTON, INDIANA

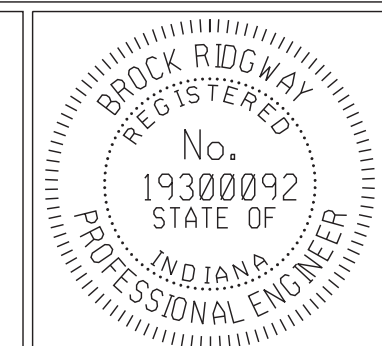


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PREPARED BY:



1321 Laurel Oak Drive  
Avon, Indiana 46123  
(317)370-9672



RECOMMENDED FOR APPROVAL	<i>B. Ridgway</i>	2/9/2016
DESIGNED:	BR	DRAWN:
CHECKED:	MT	CHECKED:
		SCS
		BR

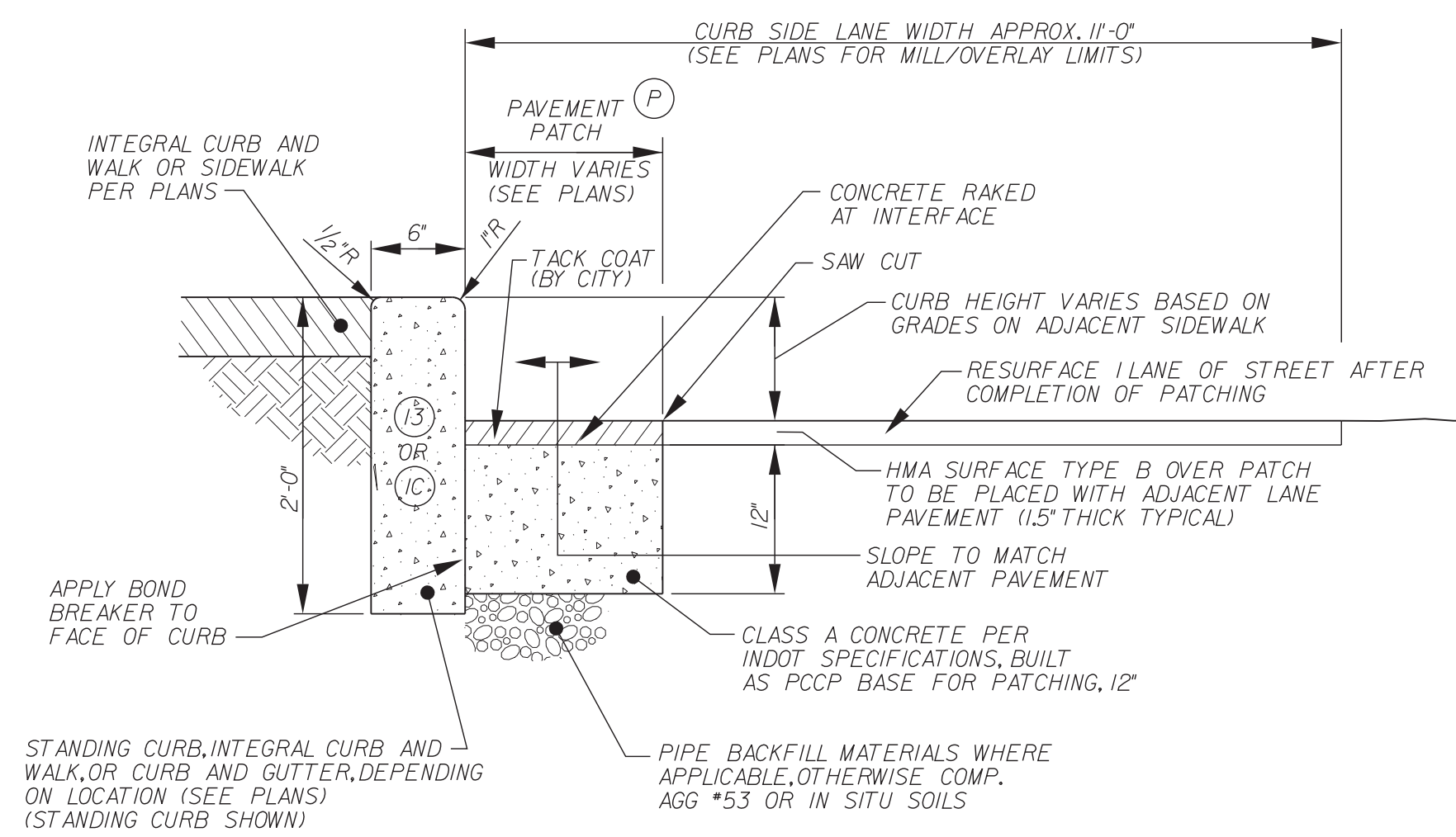
CITY OF BLOOMINGTON

JORDAN AVENUE AND 3RD STREET  
INTERSECTION IMPROVEMENTS

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	N/A
SURVEY BOOK	DESIGNATION
	N/A
CONTRACT	SHEETS
	1 of 5
	PROJECT
	3RD ST AND JORDAN AVE.

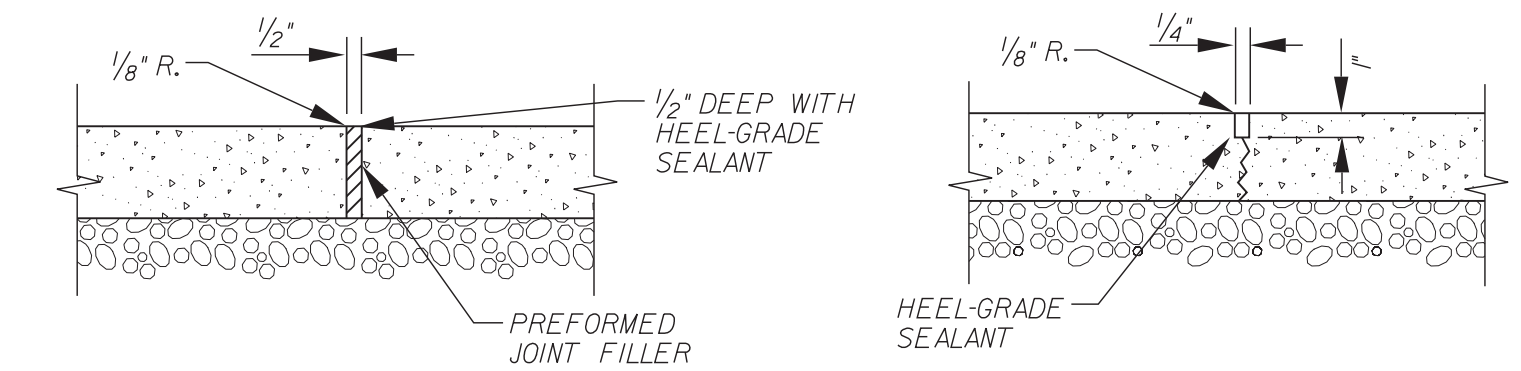
3RD STREET STORM AND SIDEWALK IMPROVEMENTS





**P PAVEMENT PATCH AND OVERLAY DETAIL**  
NTS

- NOTES:
1. AREA BEHIND CURBS TO RECEIVE TREATMENT AS SHOWN ON PLANS.
  2. REFER TO CURB JOINTS NOTES FOR CURB JOINTING.
  3. PAVEMENT PATCHING MAY NOT BE REQUIRED IF EXISTING PAVEMENT CAN BE REMOVED TO A CLEAN AND STRAIGHT EDGE AT FRONT OF PROPOSED CURB OR GUTTER. CITY APPROVAL IS REQUIRED.
  4. PAVEMENT PATCHING REQUIRED WHEREVER EXISTING PAVEMENT MUST BE REPAIRED OR REPLACED NEXT TO REQUIRED CURB, DRIVE OR SIDEWALK WORK.



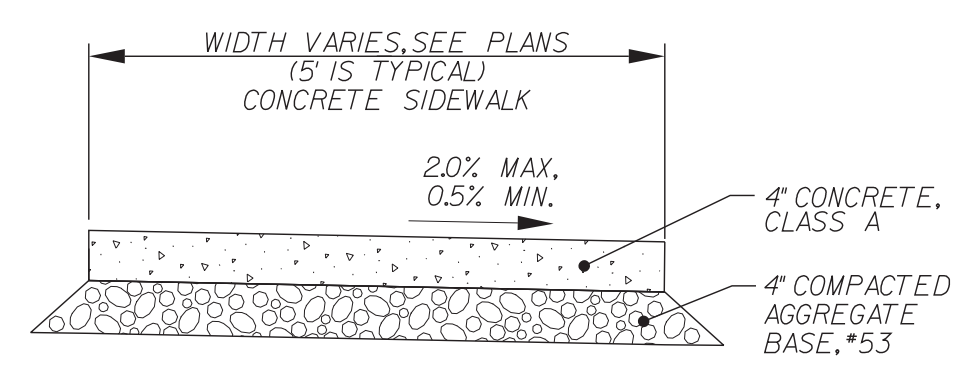
**9 EXPANSION JOINT**

EXPANSION JOINT MATERIAL SHALL BE "ZIP-STRIP" EXPANSION MATERIAL WITH REMOVABLE CAP BY GREENSTREAK OR APPROVED EQUAL.

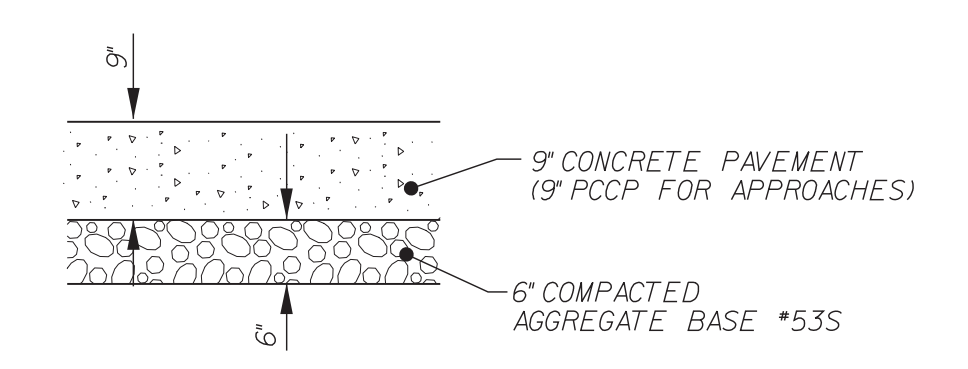
**JOINT IN CONCRETE SIDEWALKS**  
NTS

**CONTRACTION JOINT**

SIDEWALK AND CURB JOINTS ARE NOT FULLY SHOWN ON PLANS BUT ARE REQ'D. PER THE NOTES AND PROPER CONSTRUCTION PRACTICE. NEW PAVEMENTS NEXT TO BUILDINGS AND STRUCTURES SHALL BE ISOLATED WITH 0.5\"/>



**F TYPICAL SIDEWALK DETAIL**  
NTS



**C3 TYPICAL CLASS III DRIVE PAVEMENT**  
NTS

**SIDEWALK JOINT NOTES:**

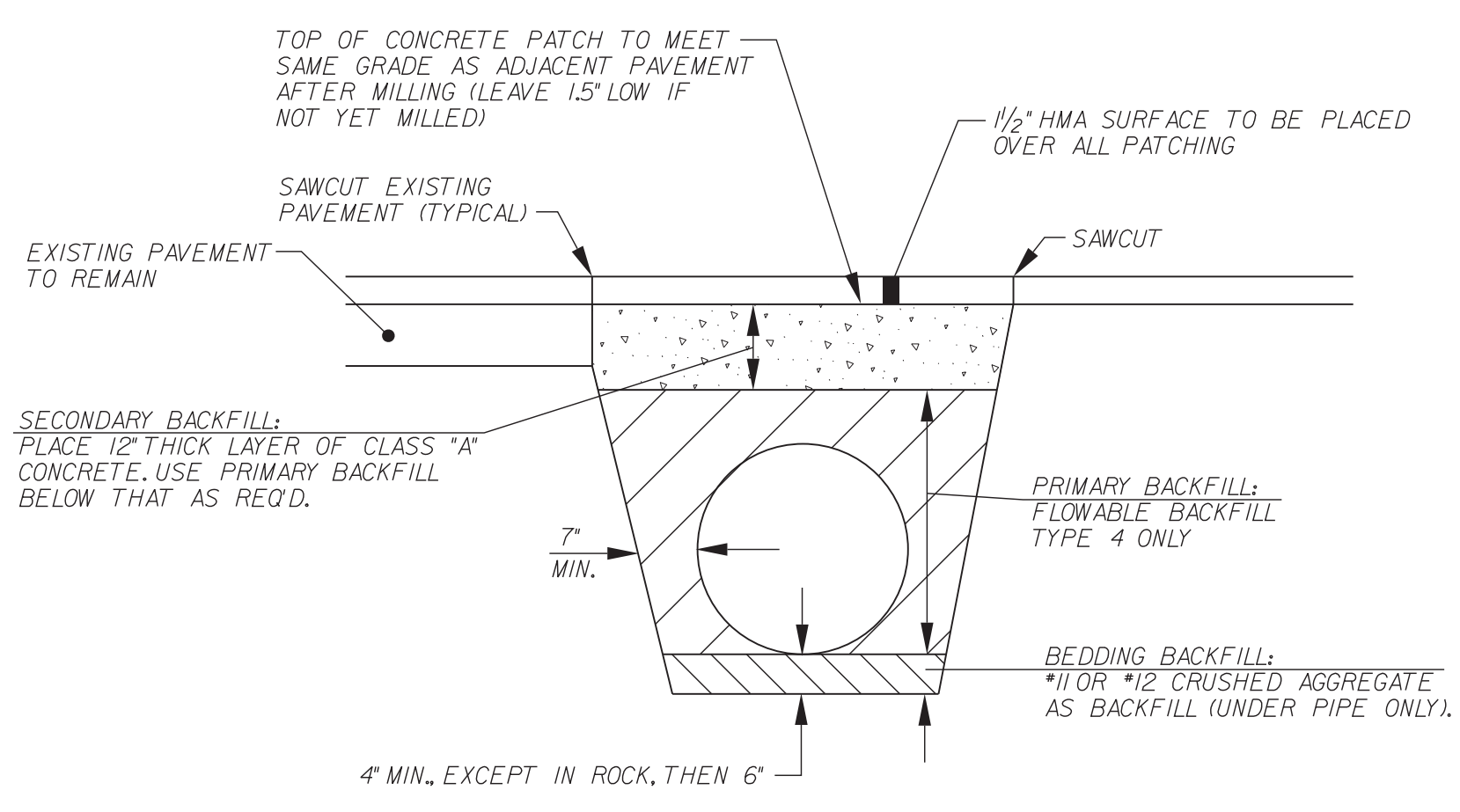
1. INSTALL 1/2\"/>
2. IN EACH SIDEWALK SEGMENT, TOOL CONSTRUCTION JOINTS AT EVEN SPACING BUT NO MORE THAN 6' MAXIMUM APART. INSTALL AN EXPANSION JOINT WITH 1/2\"/>

**CURB JOINTS NOTE:**

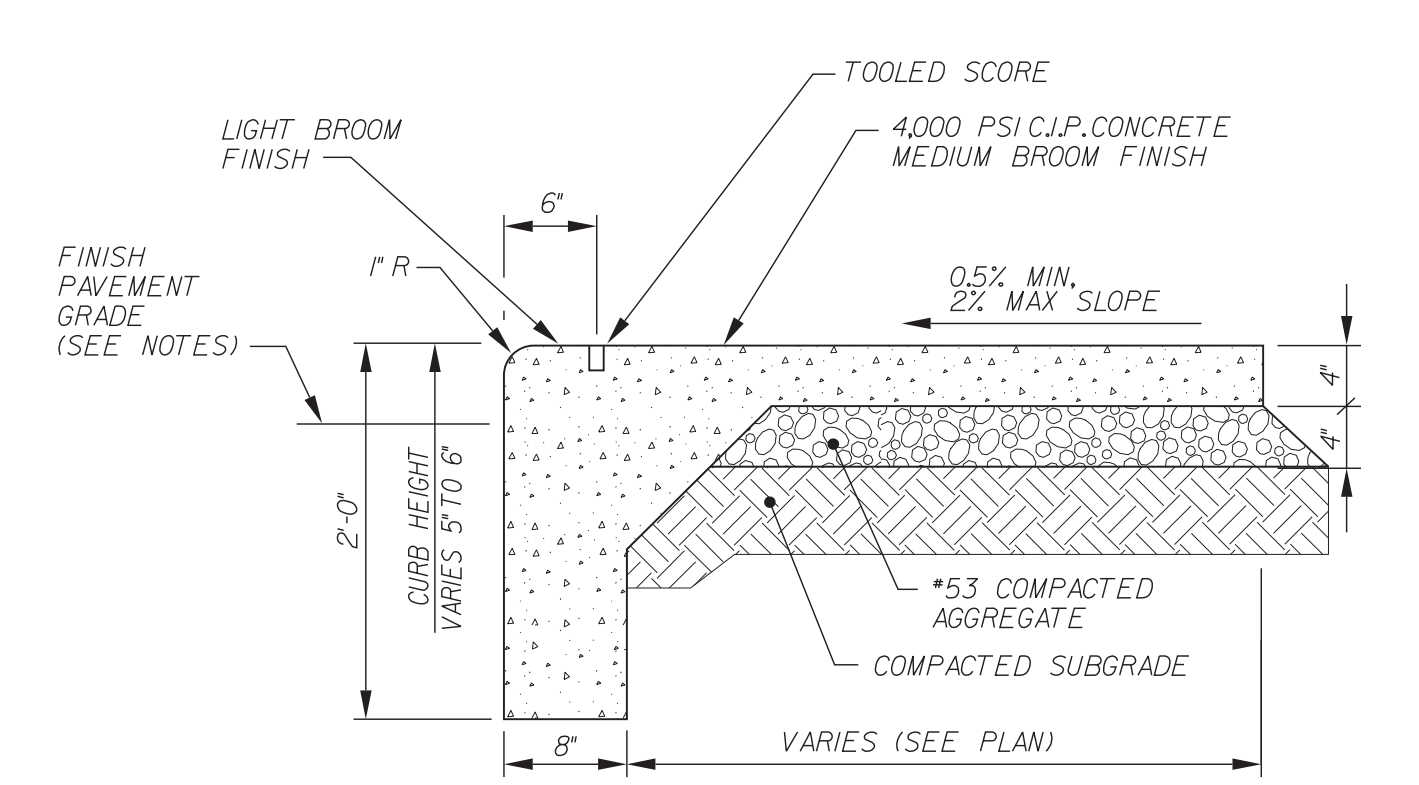
1. ALL CURBS SHALL RECEIVE A FULL DEPTH EXPANSION JOINT FORMED WITH 1/2\"/>
2. CURBS & COMBINED CURB AND GUTTER SHALL RECEIVE A TOOLED CONTRACTION JOINT AT 20' MAXIMUM SPACING.

**CURB RAMPS AND TRUNCATED DOMES:**

1. RAMPS TO BE CONSTRUCTED AT A MAXIMUM 12% SLOPE TO A LANDING AREA FOR ENTRY ONTO ROADWAY PER PLANS.
2. RAMPS ARE TO RECEIVE DETECTABLE WARNING PLATES. PER CITY STANDARDS, PLATES SHALL BE EAST JORDAN IRON WORKS, MODEL \*7005. INSTALL PER MANUFACTURER'S SPECIFICATIONS.
3. RAMPS SHALL BE IN GENERAL ACCORDANCE WITH INDOT RAMP TYPES A AND D, EXCEPT WIDTH AND LAYOUT SHALL BE PER PLANS, AND CITY REQUIRES IRON PLATES AS NOTED.

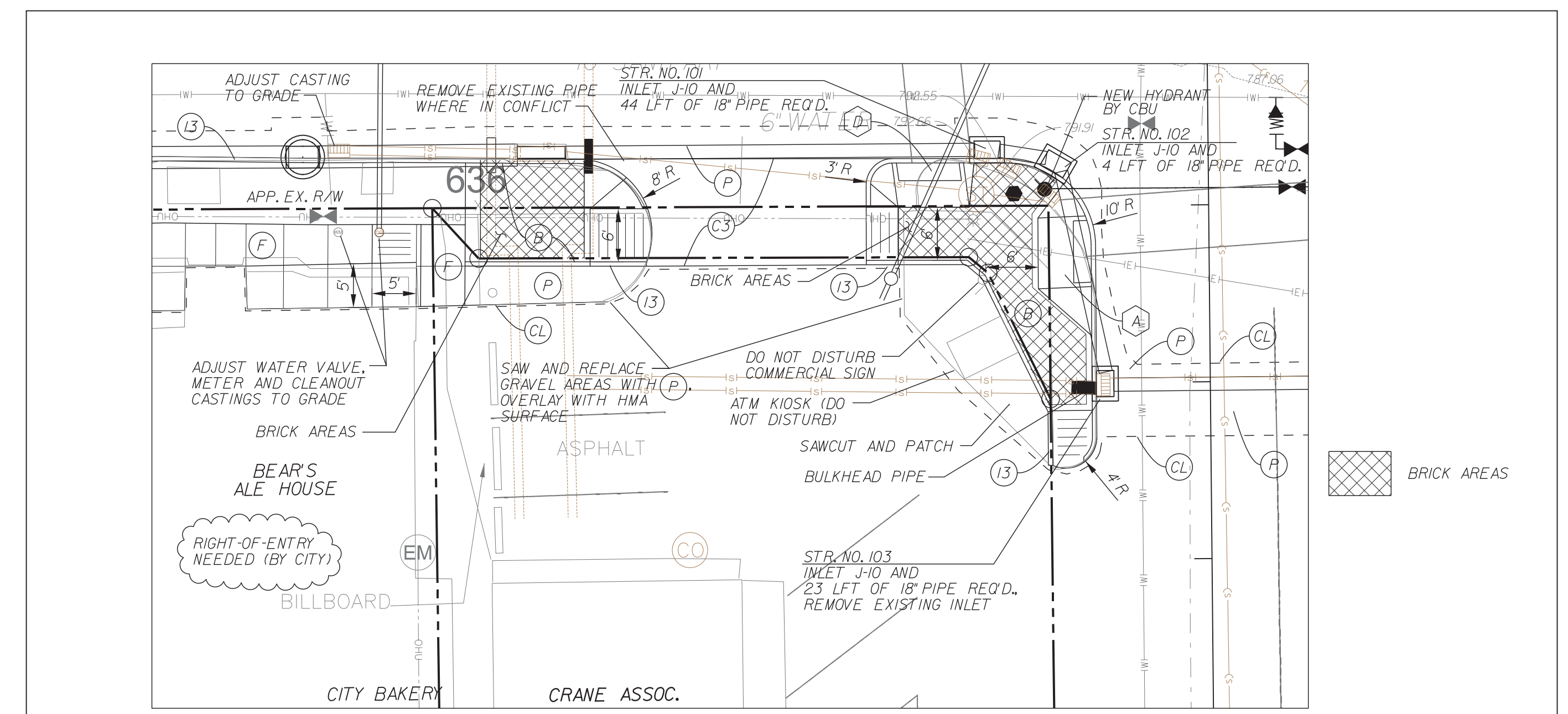


**PIPE BACKFILL AND PAVEMENT PATCHING**  
NTS

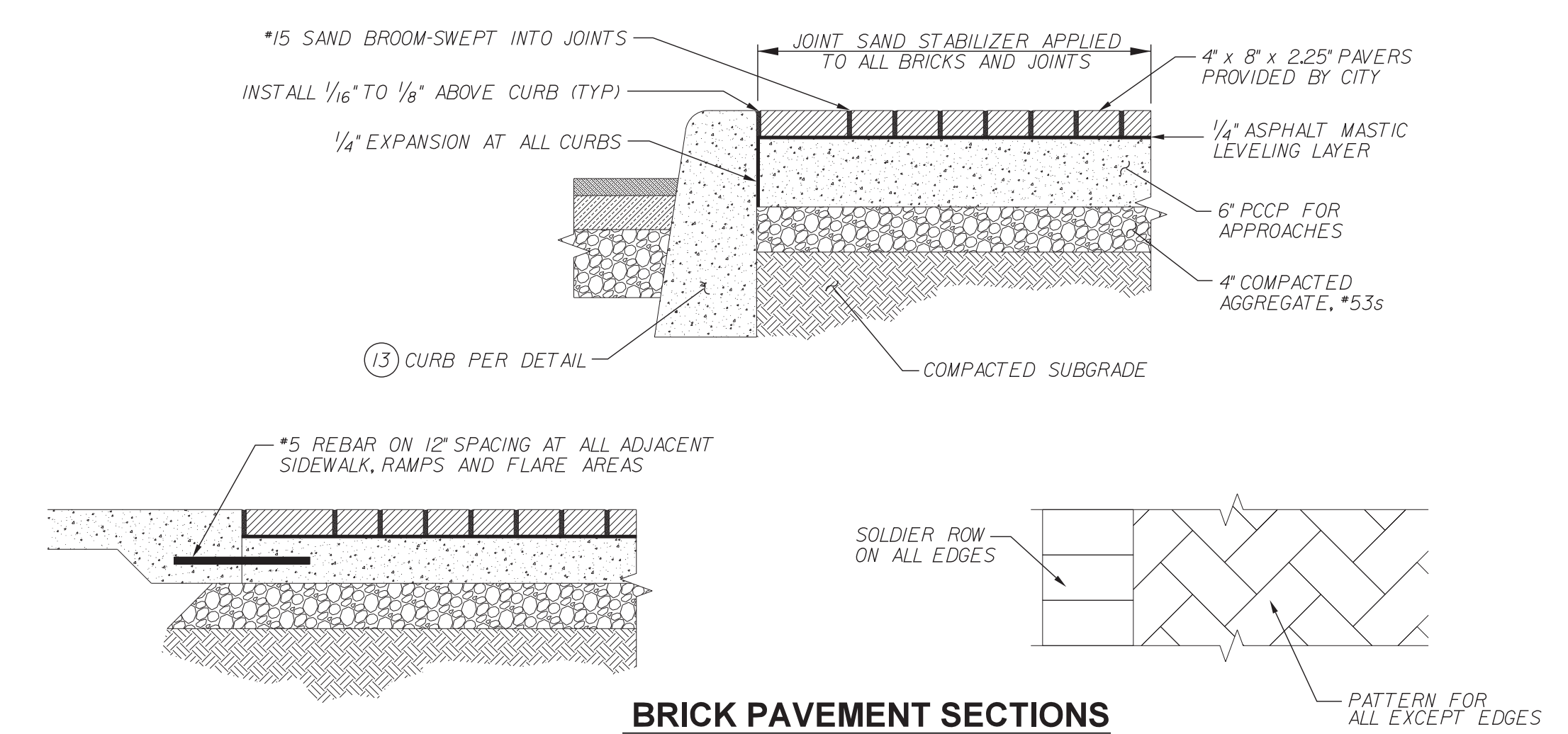


**IC INTEGRAL CURB AND WALK**  
NTS

- NOTES:
1. HAND FINISH CURB TO A 6\"/>
  2. THIS DETAIL MAY BE USED WHERE NEW CONCRETE WALKS ARE NEXT TO NEW CURBS.
  3. USE IN CONJUNCTION WITH PAVEMENT PATCH DETAIL.



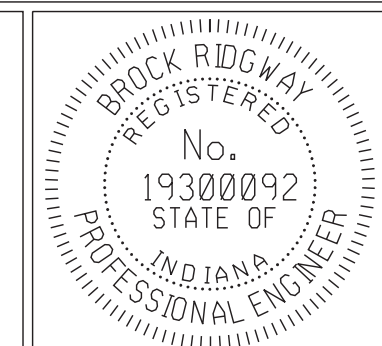
**SOUTHWEST CORNER AREA**



**B BRICK PAVEMENT SECTIONS**  
**SOUTHWEST CORNER AREA - BRICK PAVEMENT OVER CONCRETE IN SIDEWALK AREAS**

STRUCTURE NUMBER	SIZE	PIPE MATERIAL	MANHOLE, CATCH BASIN, INLET, OR SPECIALTY STRUCTURE	EJWV CASTING REQUIRED	FLOWLINE						REMARKS				
					LENGTH	SLOPE	COVER	UP-STREAM	DOWN-STREAM	CASTING		CONNECT TO STR.			
	in				ft	%	ft	ELEV.	ELEV.	ELEV.	CYD	CYD	CYD		
<b>STORM SEWERS</b>															
101	18	HDPE	INLET J	7030, M4 Grate, T1 Back	44	1.14	1.7	791.00	790.50	795.20	Box Culv.	0.4	1.6	13.6	Re-use existing hole in box culvert, enlarge as needed, reseal with concrete
102	18	HDPE	INLET J	7030, M4 Grate, T1 Back	4	2.50	1.4	791.20	791.10	795.10	101	N/A	0.2	1.1	
103	18	HDPE	INLET J	7030, M4 Grate, T1 Back	23	1.30	1.5	791.60	791.50	795.63	102	0.1	0.9	6.6	Bulkhead existing pipe to west with Concrete
104	18	HDPE	INLET J	7030, M4 Grate, T1 Back	25	1.20	1.2	792.00	791.70	795.67	103	N/A	0.9	6.3	
105	30	HDPE	MANHOLE D	7510, M3 Grate, T1 Back	11	0.91	0.5	792.55	792.45	796.40	106	N/A	0.6	3.6	Precast manhole to include formed box out for future pipe from south
106	30	HDPE	MANHOLE D	7510, M3 Grate, T1 Back	55	1.00	0.5	792.40	791.85	796.35	107	N/A	2.6	17.6	Precast manhole to include formed box out for future pipe from west
107	30	HDPE	MANHOLE D	7510, M3 Grate, T1 Back	65	1.00	0.8	791.80	791.15	796.10	108	N/A	3.4	24.1	
108	30	HDPE	MANHOLE D	7510, M3 Grate, T1 Back	62	1.00	1.3	791.10	790.48	795.80	109	N/A	3.2	27.8	
108A	6	PVC	DRAIN PIPE	NA	8	TBD	0.0	Exist	793.00	NA	108	0.1	0.2	0.9	Upstream invert must be field located. Adjust inverts if conflict with sanitary service
108B	6	PVC	DRAIN PIPE WITH CASTING	6204 or Approve equal	15	TBD	0.0	794.00	793.80	796.00	108	0.1	0.3	1.3	Adjust inverts if conflict with sanitary service
109	36	HDPE	MANHOLE E	V-4430 or Approved equal	63	1.00	1.3	790.43	789.80	795.50	110	N/A	3.7	33.4	
110	36	HDPE	MANHOLE E	7510, M3 Grate, T1 Back	23	0.87	1.5	789.75	789.55	795.30	Box Culv.	0.5	1.4	13.0	Sawcut new hole into existing box culvert, reseal with pint seal and concrete per CBU specifications
111	NA	NA	Existing Box Culvert	7511 or Approve equal	NA	NA	NA	NA	795.17	NA	NA	0	0	See Note #6 for Special Casting Replacement Requirements	
<b>TOTALS: Inlet J: 4 ea</b>					<b>Pipe, 18\"/&gt; </b>	<b>Pipe, 30\"/&gt; </b>	<b>Pipe, 6\"/&gt; </b>	<b>1.2</b>	<b>19.2</b>	<b>149.3</b>	<b>CYD</b>	<b>CYD</b>	<b>CYD</b>		

- NOTES:
1. ALL INLETS AND MANHOLES SHALL INCLUDE CAST IN PLACE TROUGH (BENCHING) THROUGH THE STRUCTURE, MANHOLES WITH INLET CASTINGS MAY BE SUBSTITUTED.
  2. ALL WORK ON INLETS AND MANHOLES, BOTH STORM AND SANITARY, SHALL BE PER CBU SPECIFICATIONS.
  3. CONTRACTOR MAY BE REQUIRED TO SCHEDULE A PRECONSTRUCTION MEETING WITH CBU.
  4. CONTRACTOR SHALL MEET CBU REQUIREMENTS FOR INSPECTIONS OF STORM STRUCTURES, AND OBTAIN WRITTEN CBU APPROVAL FOR THE WORK.
  5. WATER SERVICE REPLACEMENT WILL BE PERFORMED BY CBU AT CONTRACTOR REQUEST. SANITARY SERVICE REPLACEMENTS SHALL BE BY CONTRACTOR.
  6. THE EXISTING CASTING ON STR. 111 (EXISTING BOX CULVERT) IS TO BE REPLACED WITH A LARGER CASTING AND THE HOLE INTO THE BOX MUST BE MODIFIED TO PROVIDE A COMPATIBLE SEAT FOR THE FRAME. THE EXISTING HOLE IS TO BE WIDENED TO THE SOUTH TO ALLOW FOR EJWV 7510 WITH 7030 T1 BACK, M3 GRATE AND TO ALLOW FOR ALIGNMENT OF CURB BOX WITH PROPOSED CURB. NOTE THAT THIS WILL REQUIRE THE PARTIAL REMOVAL OF THE EXISTING CULVERT TOP TO A DEPTH OF APPROXIMATELY 6', BUT THAT IT IS DESIRED TO RETAIN AS MUCH OF THE EXISTING CULVERT TOP AS IS FEASIBLE. IT IS DESIRED THAT THE REMOVAL BE SLOPED TOWARD THE EXISTING HOLE TO ALLOW WATER FROM THE CASTING ABOVE, BUT TO RETAIN AS MUCH OF THE CULVERT TOP AS POSSIBLE. CBU APPROVAL OF THIS WORK, ONCE EXPOSED AND VERIFIED, IS REQUIRED PRIOR TO REMOVAL OF ANY PORTION OF THE CULVERT TOP.



RECOMMENDED FOR APPROVAL: *B. Ridgway* DESIGN ENGINEER DATE: 2/9/2016

DESIGNED: BR DRAWN: SCS

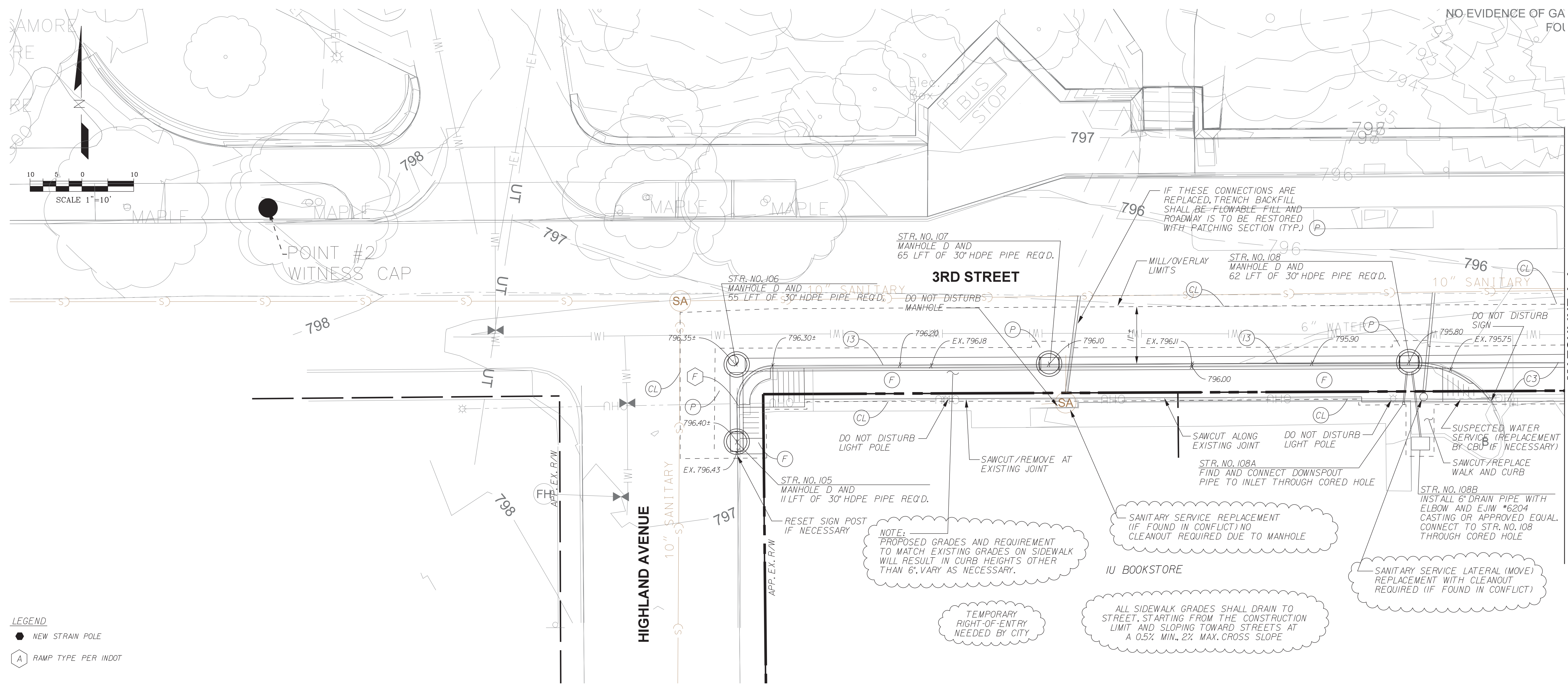
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CITY OF BLOOMINGTON

TYPICAL DETAILS AND RAMP STANDARD DETAILS

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	N/A
SURVEY BOOK	DESIGNATION
CONTRACT	SHEETS
	2 of 5
	PROJECT
	3RD ST AND JORDAN AVE.





**LEGEND**  
 ● NEW STRAIN POLE  
 A RAMP TYPE PER INDOT

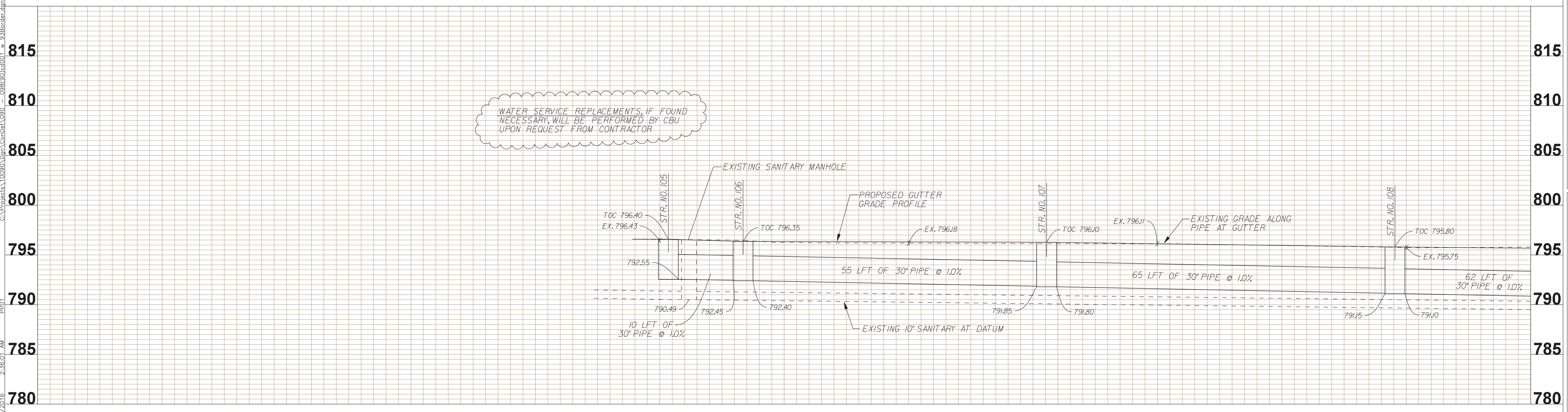
EXISTING UTILITIES SHOWN ON PLANS ARE APPROXIMATED IN ACCORDANCE WITH AVAILABLE RECORDS AND PHYSICAL EVIDENCE. OTHER UTILITIES MAY BE PRESENT. ACTUAL LOCATIONS AND ELEVATIONS ARE TO BE DETERMINED BY CONTRACTOR.

NOTE: PROPOSED GRADES AND REQUIREMENT TO MATCH EXISTING GRADES ON SIDEWALK WILL RESULT IN CURB HEIGHTS OTHER THAN 6". VARY AS NECESSARY.

TEMPORARY RIGHT-OF-ENTRY NEEDED BY CITY

ALL SIDEWALK GRADES SHALL DRAIN TO STREET, STARTING FROM THE CONSTRUCTION LIMIT AND SLOPING TOWARD STREETS AT A 0.5% MIN., 2% MAX. CROSS SLOPE

SANITARY SERVICE LATERAL (MOVE) REPLACEMENT WITH CLEANOUT REQUIRED (IF FOUND IN CONFLICT)



MATCHLINE A-A

REVISIONS	DATE	BY

PREPARED BY:  
  
 1321 Laurel Oak Drive  
 Avon, Indiana 46123  
 (317) 970-9672

REGISTERED PROFESSIONAL ENGINEER  
 No. 14300092  
 STATE OF INDIANA

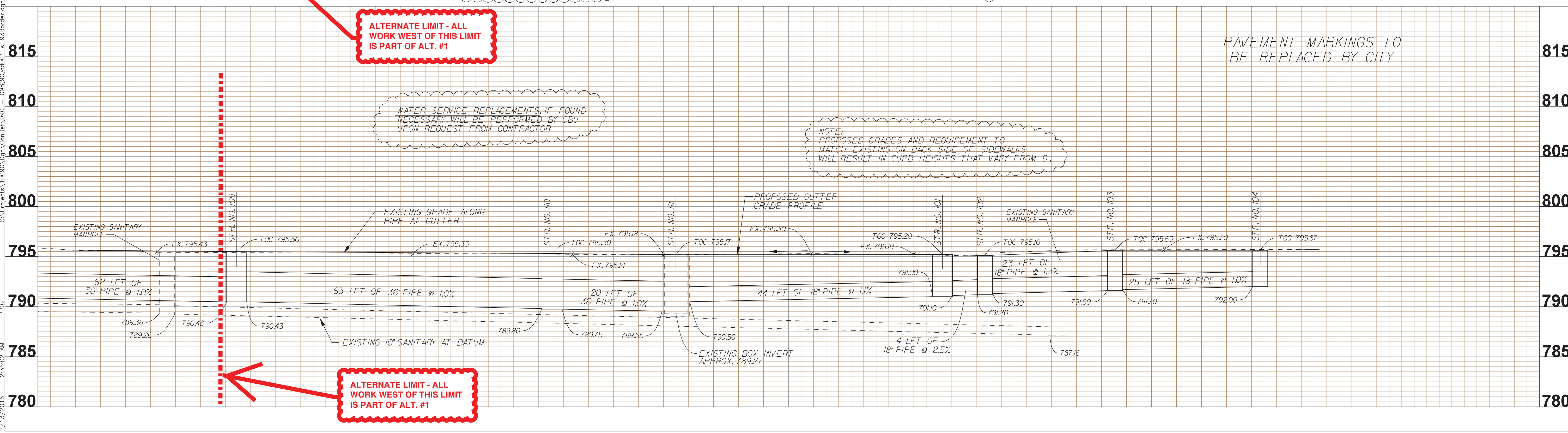
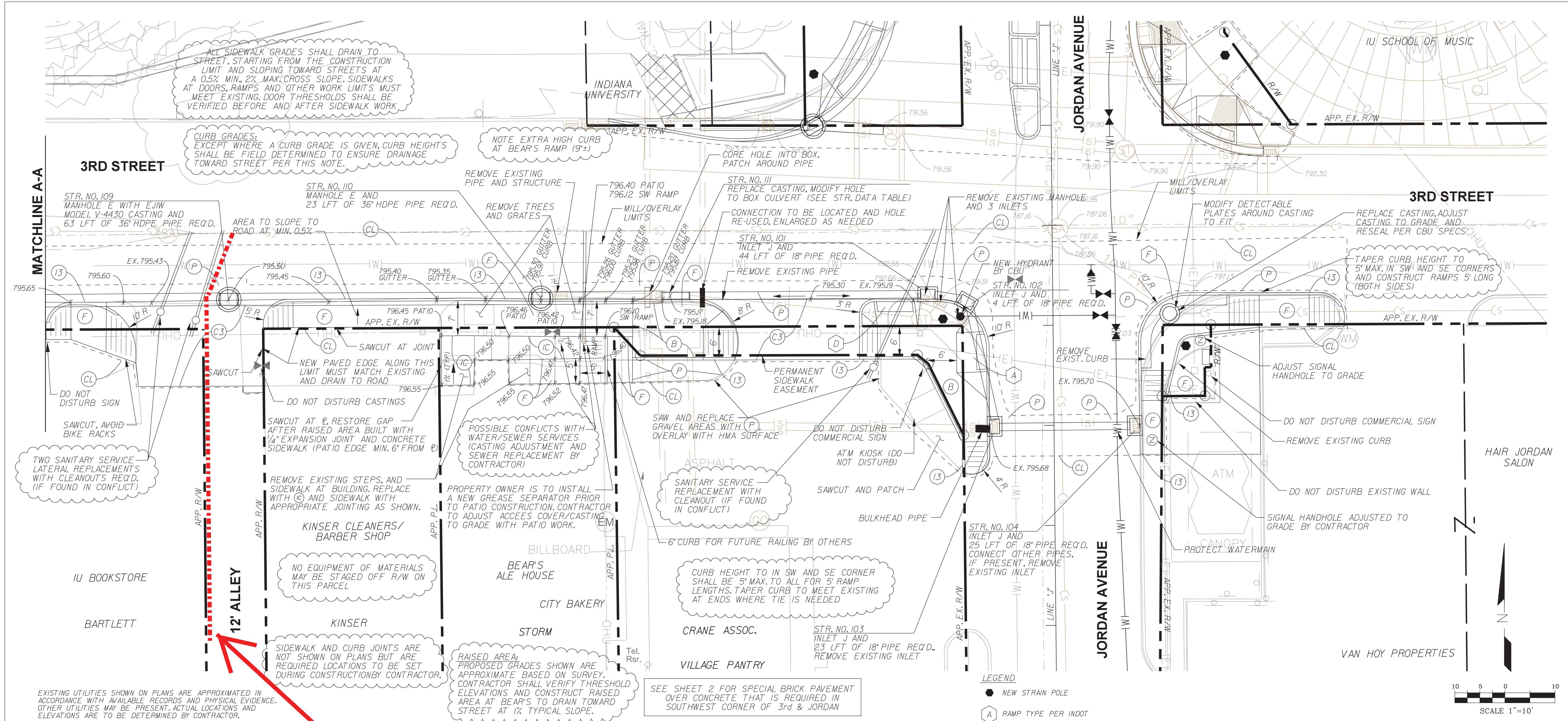
RECOMMENDED FOR APPROVAL	DATE	DESIGNED BY	DRAWN BY	CHECKED BY
<i>B. E. Eppinger</i>	2/19/2016	BR	BR	BR

CITY OF BLOOMINGTON  
 INTERSECTION IMPROVEMENTS AT  
 3RD STREET AND JORDAN AVENUE

CONSULTANT PROJECT NUMBER	ERCES 090
SHEET	3 OF 5

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REVISIONS	DATE	BY

PREPARED BY:	DESIGNED BY:	CHECKED BY:
<i>B. Blyden</i>	BR	BR

RECOMMENDED FOR APPROVAL	DATE
<i>B. Blyden</i>	2/19/2016

DESIGNED:	SCALE:
BR	SCS

CHECKED:	CHECKED:
BR	BR

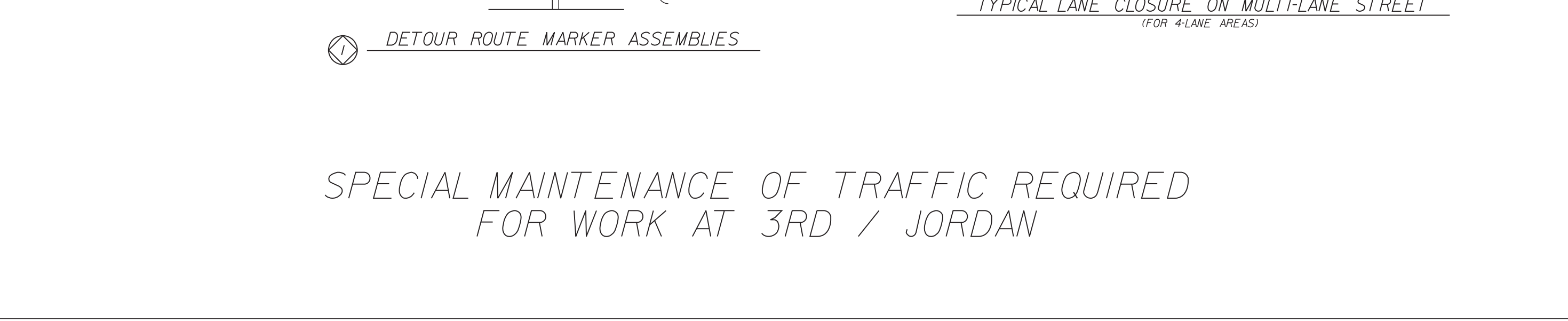
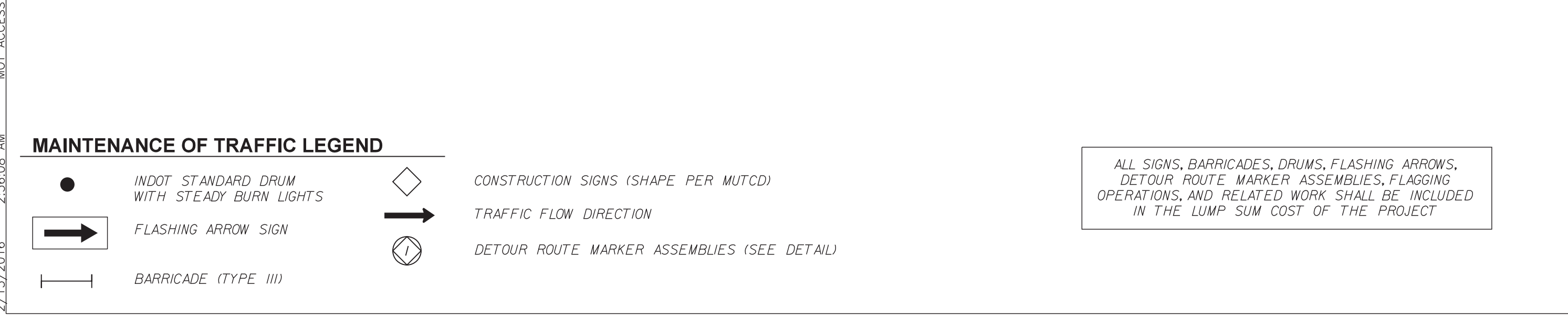
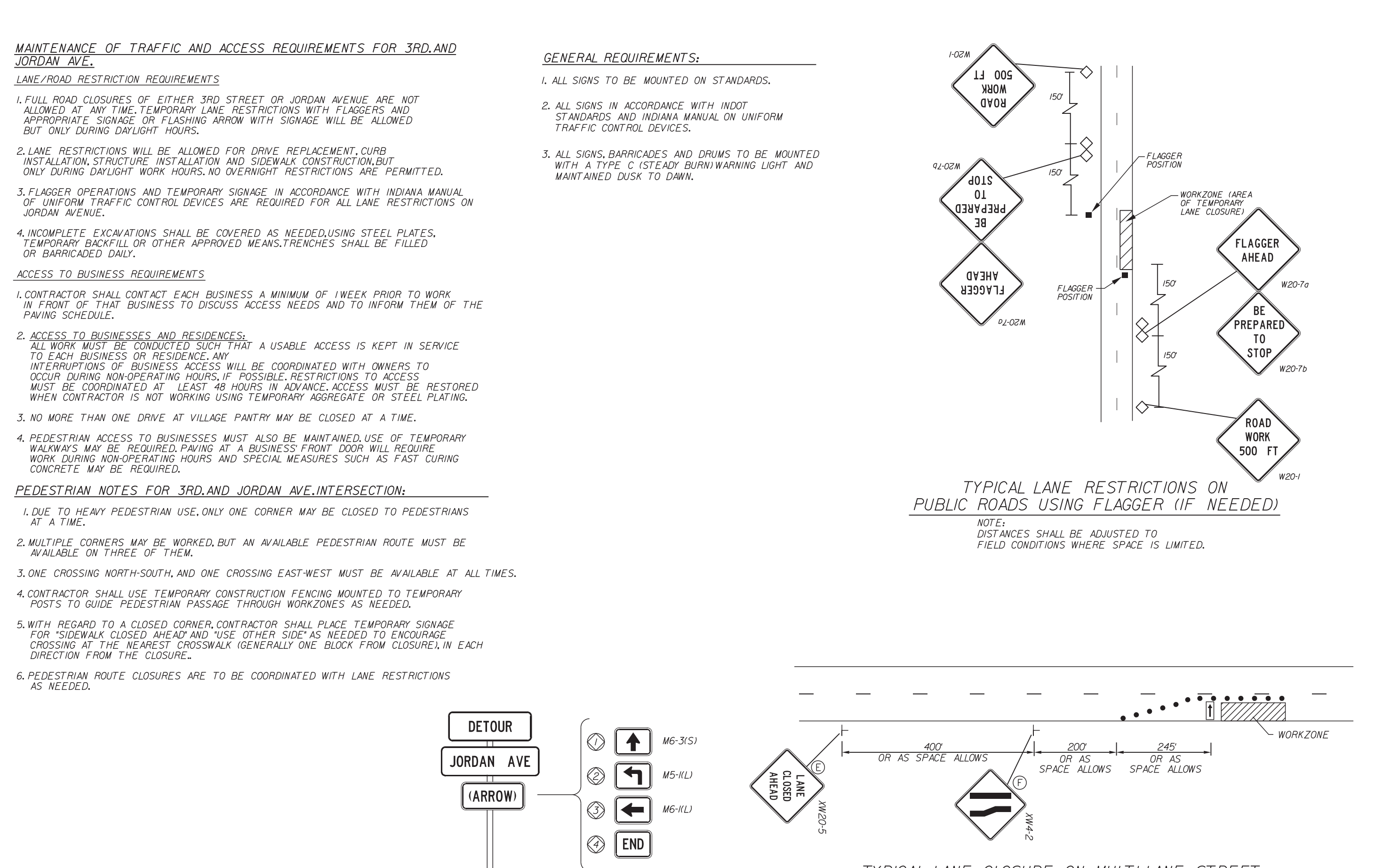
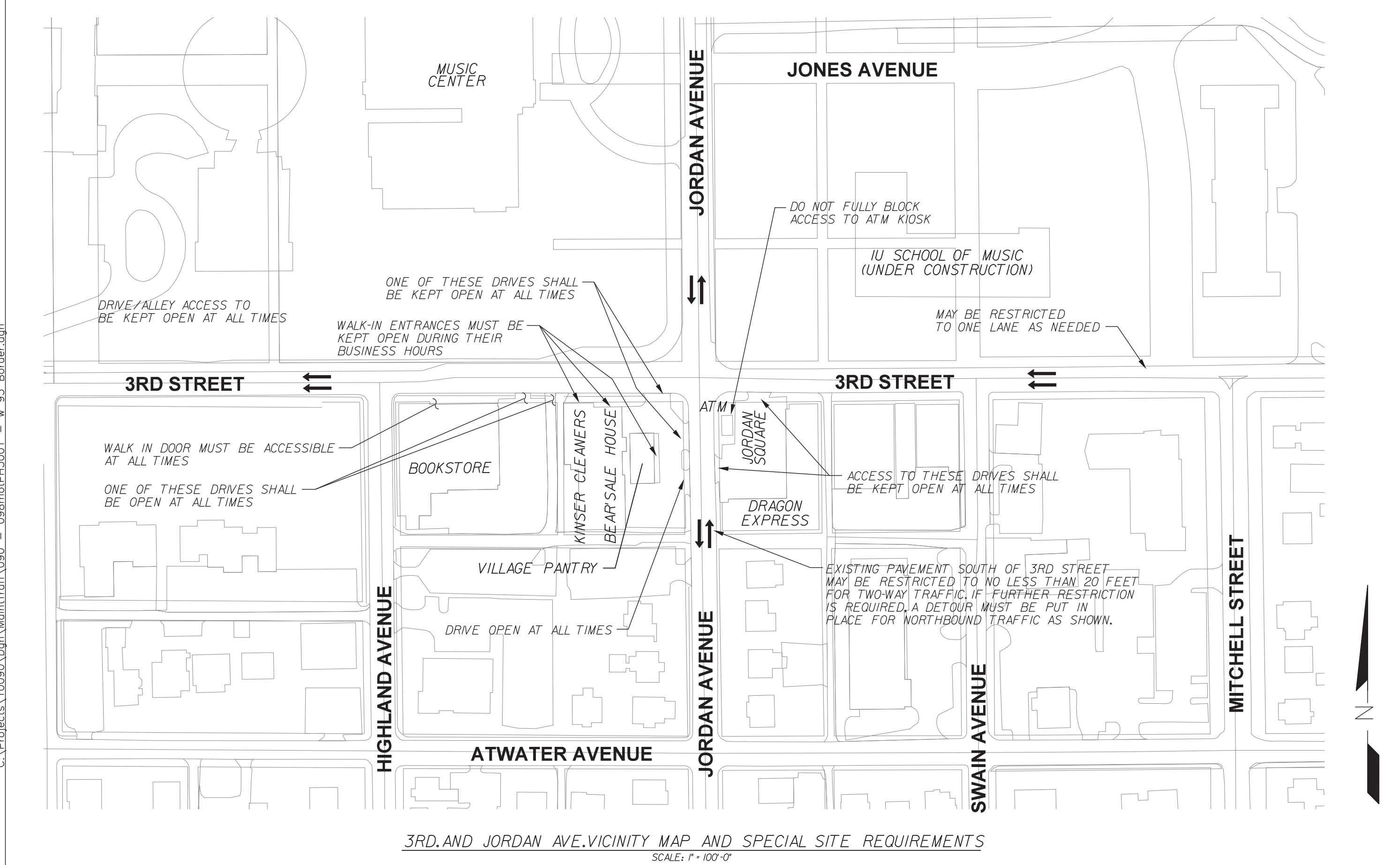
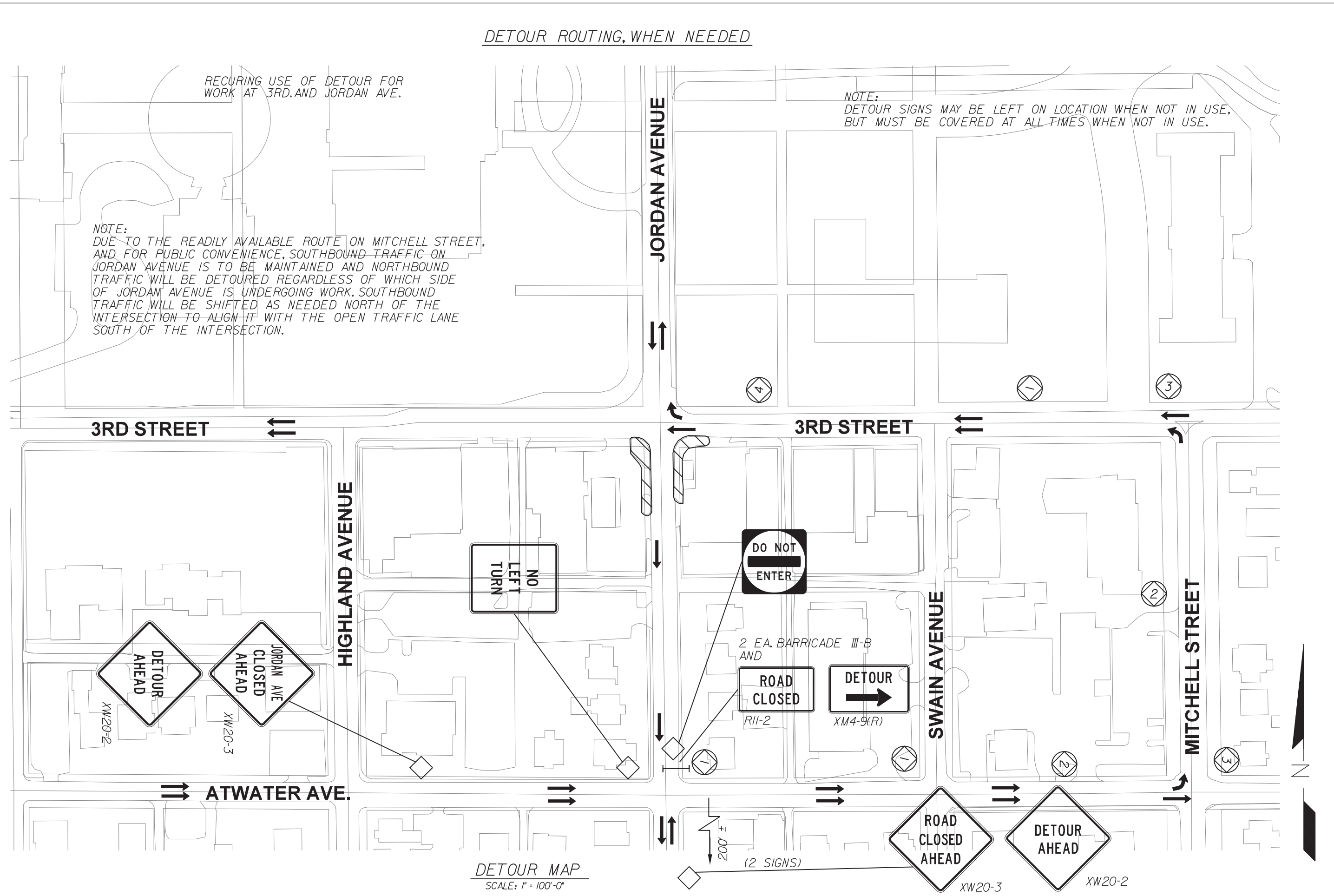
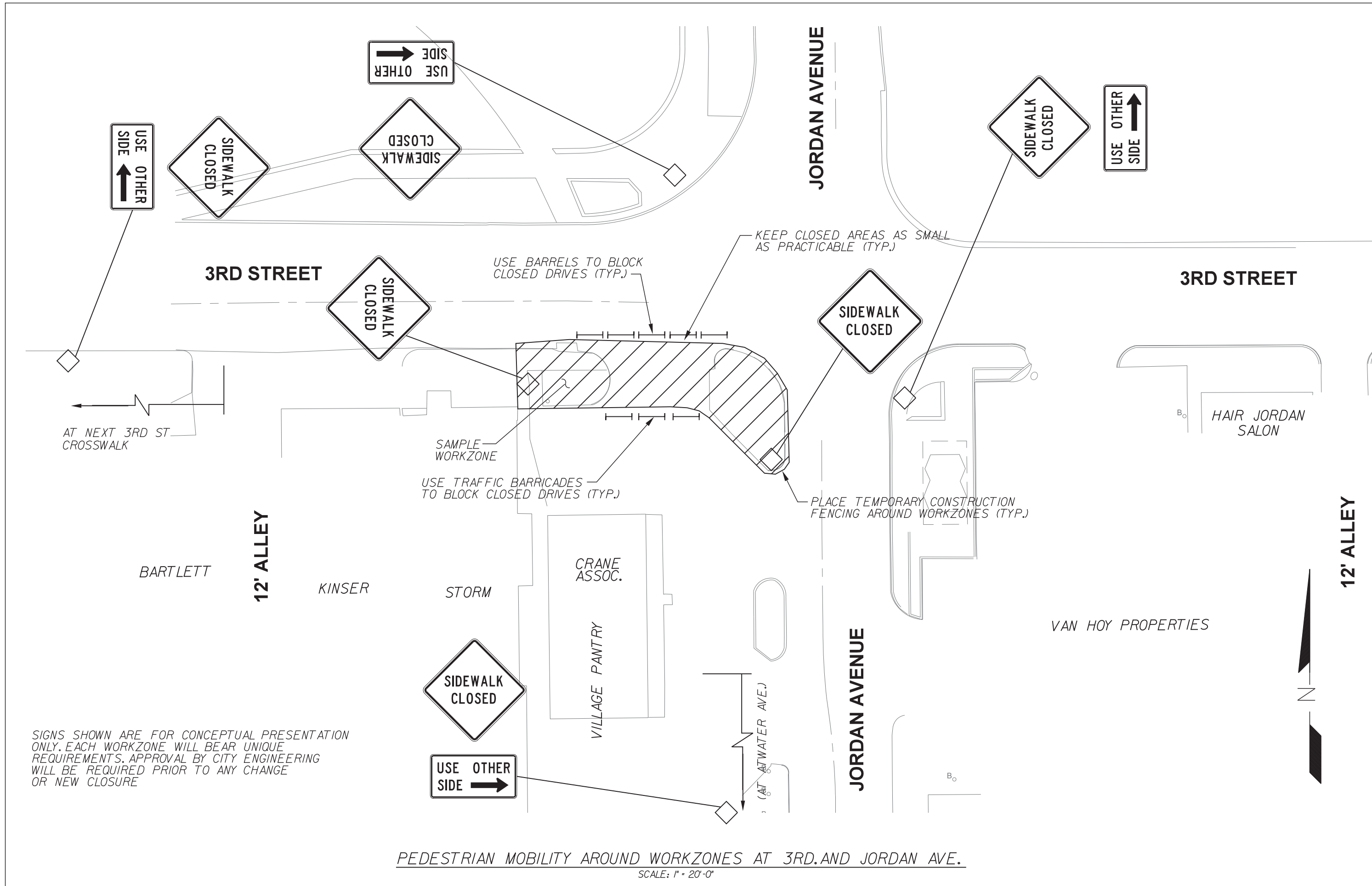
CONSULTANT PROJECT NUMBER	SHEET	OF
ERCES 090	4	5

CITY OF BLOOMINGTON

INTERSECTION IMPROVEMENTS AT 3RD STREET AND JORDAN AVENUE





2/13/2016 2:36:08 AM C:\Projects\10090\Draw\Main\Traffic\090 - 098.mxd\PH001 - w\_93\_Border.dgn MOT\_ACCESS

PREPARED BY:   
 1321 Laurel Oak Drive  
 Avon, Indiana 46123  
 (317) 970-9672

RECOMMENDED FOR APPROVAL:

CITY OF BLOOMINGTON  
 MAINTENANCE OF TRAFFIC AND ACCESS PLANS

CONSULTANT PROJECT NUMBER: ERGES 090  
 SHEET: 5 OF 5

DESIGNER: BR  
 DRAWN: SCS  
 CHECKED: BR

DATE: 2/19/2016





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## MEMORANDUM

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To: Traffic Commission

From: Nate Nickel, Sr. Long Range Planner

Date: April 20, 2016

Re: W. Smith Avenue – Create No Parking Zone on W. Smith Avenue between N. Rogers St. and S. Fairview St.

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### Background

This no parking zone proposal comes at the request of City staff and was first heard at the February 24, 2016 meeting. No formal action was taken by the Commission at that time and the proposal was forwarded to be heard at the next meeting.

Smith Avenue is a very narrow road with a width (approximately 18') that is more similar to an improved alley rather than a typical city street. Parking currently is not regulated on Smith Avenue between Rogers Street and Fairview Street. Parked vehicles can create conflicts for garbage trucks, snow plows and other vehicles that utilize this street. This proposal can help alleviate those conflicts. Prospect Street, which is located directly north of Smith Avenue, also has very narrow road width (approximately 16'). It currently has a no parking restriction in place on the south-side of the street, between Jackson Street and Madison Street.

This proposal will restrict parking at all times on the north-side of the street, between N. Rogers Street and Jackson Street. This is consistent with several existing parking spaces that are located along the south-side of the street, which will remain in place. Additionally, parking is to be restricted at all times on the south-side of the street between Jackson Street and Fairview Street. This too follows the existing parking patterns that have been observed along this part of Smith Avenue. Due to the placement of utility poles, landscaping features and some curbing, most vehicles park on the north-side of the street, so this restriction should not have a great impact on those that utilize on-street parking in this stretch of Smith Avenue.

At the February 24<sup>th</sup> meeting, the Commission asked that several issues about this request be addressed and reported on at the next meeting. These included 1) providing more information concerning specific accident data or traffic conflicts that have occurred along this section of Smith Avenue; and 2) soliciting input and thoughts from the Prospect Hill Neighborhood Association concerning this request.

The Street Department reported that, due to parked vehicles and narrow road with, their snow plow crews especially found this area of Smith Avenue extremely difficult to navigate during their operations. Additionally, Monroe County Community School Corporation (MCCSC) bus drivers have reported that they avoid this section of Smith Avenue because parked vehicles often make the travel lane too difficult to negotiate with their school buses. As a result, the MCCSC's busses use Jackson Street as a travel route instead of using Smith Avenue.

Regarding specific accident data, City staff researched a 10 year window (2006-2015) for this section of Smith Avenue. Listed on the following page is a summary of the vehicle crashes that were reported to the Bloomington Police Department; it should be noted that other additional accidents or incidents may have occurred, but if they were not formally reported, no record of them is available to City staff.



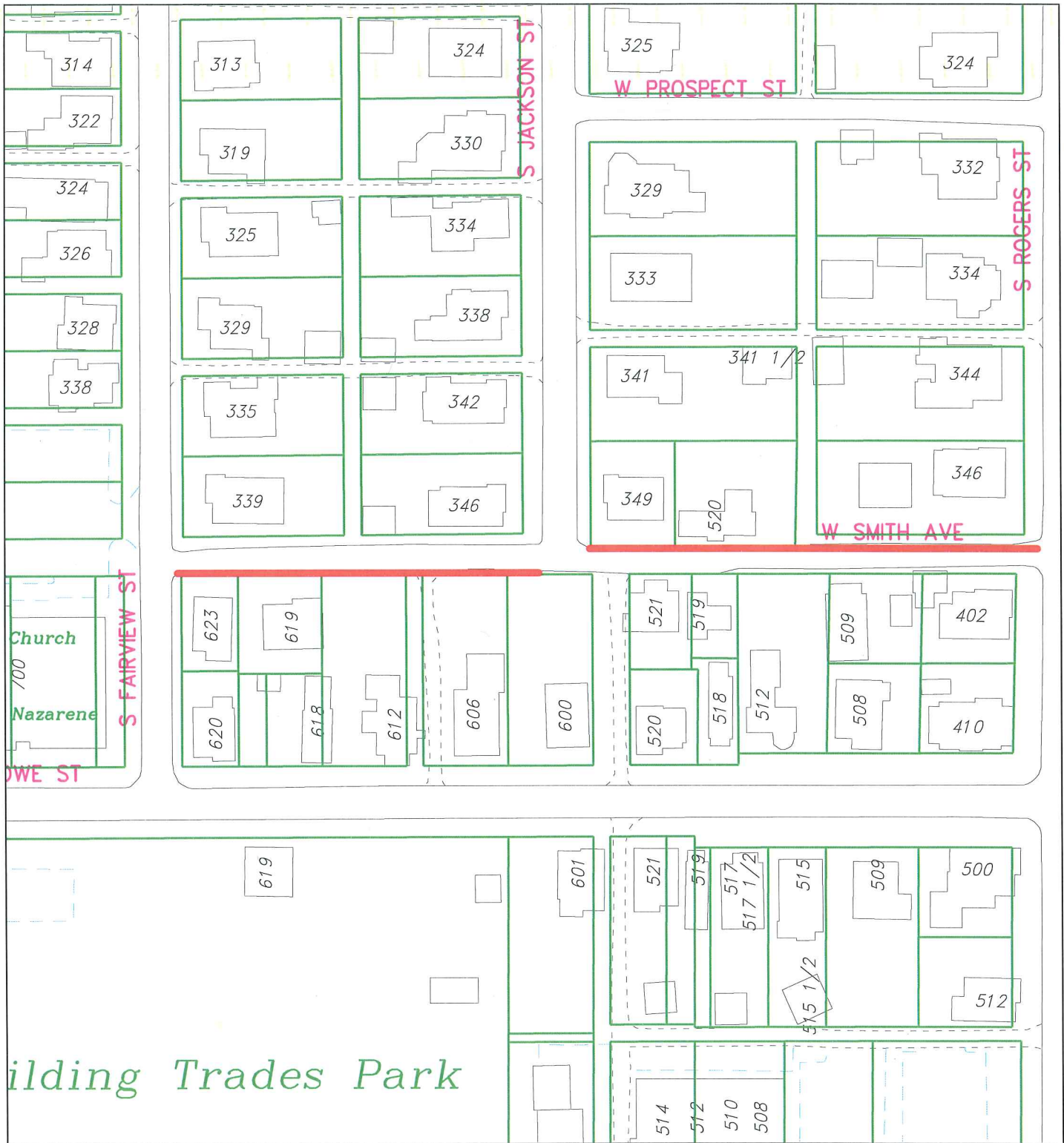
**Smith Avenue Crash Data (between Rogers St. and Fairview St.): 2007-2015**

- **2007** - crash with parked car on the north side of Smith, just west of Jackson (hit and run).
- **2009** - crash with parked car on the south side of Smith, just east of Jackson; hit by a car turning from southbound Jackson onto eastbound Smith.
- **2010** - crash with utility pole, turning from Smith onto Rogers (hit and run).
- **2012** - two car crash in Smith and Rogers intersection (hit and run).
- **2015** - car backed into a wooden shed at 520 W Smith (hit and run).

The Prospect Hill Neighborhood Association discussed this request in detail at their meeting held on March 7, 2016. A copy of their meeting minutes is included in this report for reference. Attendees at the meeting decided not to formally vote on the item as a neighborhood; instead, all interested members of the Prospect Hill Neighborhood Association were simply encouraged to voice their opinions at the Traffic Commission meeting. Additionally, individual residents submitted their own comments concerning this proposal to staff. These comments are also included for your review in this report.

**Recommendation:** Staff is seeking a recommendation from the Traffic Commission on this proposal.





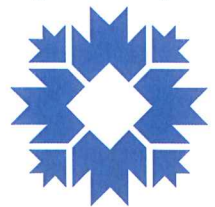
W. Smith Avenue: No Parking Zone

By: nickeln  
10 Feb 16



For reference only; map information NOT warranted.

City of Bloomington  
Planning & Transportation



Scale: 1" = 100'







*Smith Avenue at the intersection of Jackson Street, looking east towards Rogers Street.*



*Smith Avenue at the intersection of Jackson Street, looking west.*





*Smith Avenue looking east from S. Fairview Street.*



**PROSPECT HILL NEIGHBORHOOD ASSOCIATION (PHNA)**  
**MEETING MINUTES – REVISED AS OF 4/3/2016**  
**MONDAY, MARCH 7, 2016**  
**6:30 PM**

Rainbow Bakery, W. 4<sup>th</sup> Street

**Attendance:** Chris Abert, Leslie Abshier, Kate Braun, Sarah Cahillane, Lisa Dcrazenski, Darcie Fawcett, Dirk Fraser, Tom Gallasgher, Jeff Golden, Bill & Mary Beth Hansen, Don Harp, Becky Holtmann, Julia Karr, Natale Levin, Richard Lewis, Daniel Muller, Patrick & Glenda Murray, Brian OQuinn, Jaclyn Ray, Matt Tobey, David Wierhake, and Don Wise

**I. Introductions:** We introduced ourselves ala: Round Robin

**II. Approval of the Minutes and Treasurer’s Report:** The minutes of the February meeting were approved with no changes; Treasurer Richard Lewis reported that there is \$327.97 in the checking account. Jeff Golden moved acceptance, Glenda Murray seconded; passed with a voice vote.

**III. Pressing Issues and New Business:**

Smith Avenue Parking Proposal: Issue raised by City Staff at Feb 24 Traffic Commission meeting.

Doug Wissing reported that the Traffic Commission was proposing to post “No Parking” signs on the north side (500 block) and the south side of Smith Ave (600 block). They are doing this because it is a very narrow street and parking has not been regulated there. They are concerned for the ability of larger vehicles, trash trucks, snow plows and other vehicles to have safe clearances. Their proposal is to prohibit parking on the north side between Rogers and Jackson and prohibit parking on the south side between Jackson and Fairview. This generally follows the current practice on how residents park here.

Doug Wissing is proposing that the street be widened to 18 feet (to the easement line) adjacent to his property and that parking be allowed on the north side between Jackson and Rogers. He is also concerned about a net loss of parking for adjacent residents.

There was a general conversation about the pros and cons of the city proposal and the alternate. **Some of the opinions were as follows:** The current parking arrangement helps to slow traffic. ~~The worry that if~~ **If** all parking is on one side traffic would have a straight shot and people would drive faster. ~~Most of the adjacent residents want to keep the parking the way it is now which conforms to the city proposal.~~ **There were neighbors from three properties adjacent to the 500-600 block of W. Smith Ave. at the March 7, 2016 PHNA meeting: David Wierhake and Kate Braun from 520 W. Smith Ave., Douglas Wissing from 521 W. Smith Ave., and Julia Karr from 600 W. Howe St., whose back lot fronts the 600 block of W. Smith Ave.**

**Mr. Wierhake and Ms. Braun supported keeping the parking the way it is now, which conforms to the city proposal. Mr. Wissing supported a no-parking zone on the south side of the 500 and 600 blocks of W. Smith Ave. Ms. Karr answered a question about parking on the south side of the 600 block of W. Smith Ave. and stated cars typically do not park there, but did not express an opinion about parking on the 500 block of W. Smith Ave.”**

Effort should be made to provide as much parking as possible since this is a high density area. **Attendees decided not to vote on the item as a neighborhood, but those interested in the issue were encouraged to attend the next Traffic Commission meeting and make their concerns known.**



#### **IV. Old Business:**

4<sup>th</sup> and Rogers Streetscape proposal: Jackie Ray our Bike/Ped representative reported that the survey period for the 4<sup>th</sup> and Rogers streetscape improvements has been closed. The engineers were expecting about 50 responses; they got 273. Their report is expected by the end of April.

#### **V. Committee Reports**

A. Design Review Committee: No Report

B. Bike/Ped Safety Committee: No additional information

C. Park Ambassador Update: David Wierhake reported that he is still interested in getting a tennis court rebuilt on the paved area adjacent to the basket ball court. Parks and Rec have discouraged him on this. They said if a majority of the neighborhood residents petition they would install the court. David is going to talk to the Director regarding the survey. A few people at the meeting expressed interest in playing tennis if the court is built. The restrooms are being renovated. David reminded us that it is important for the neighborhood to take an active interest in the park so that Parks and Rec will make improvements and maintain the park to a higher standard. David wants to organize a park cleanup day; picking up trash, weeding, mulching trees, etc. Are there others that would like to work with David on this?

D. Hospital Site Reutilization Steering Committee: there have been no new meetings. We are still many years away from a hospital move.

E. HAND – No report

F. CONA: do we need a new CONA representative?

#### **VI. Odds and Ends From Around the Neighborhood:**

- A new art gallery is reported to be opening in Matt Murphy's building at the corner of 4<sup>th</sup> and Rogers.
- The Indiana Recovery Alliance has opened a new office in the old BMV building at the corner of 4<sup>th</sup> and Rogers (on the parking lot side). Sarah Cahillane said that they do drug abuse awareness outreach. The State of Indiana is in need of more services, but they are not putting any funding into them. The Alliance is a non-profit that does public education and harm reduction. Most of their work is done offsite. Their website is: indianarecoveryalliance.org
- Doug Wissing said that he has finished a new book "Indiana in Writing." There will be a book event sponsored by Bloom magazine March 29.
- Lotus is moving into the old fire station on Rogers St.
- Darcie reminded us to vote in the May 3<sup>rd</sup> primary election, she is running for Circuit Court Judge.

**VII. Adjournment:** The meeting was adjourned at approximately 7:30 pm.

Next Meeting will be Monday April 4<sup>th</sup> at 6:30 pm. at the Rainbow Bakery on W. 4<sup>th</sup> Street.

Respectfully submitted,

Patrick Murray, Minute Taker



## Public Input received by City staff

Dear Nate,

It has just come to my attention (since I was not at last Monday night's Prospect Hill Neighborhood Assn. meeting) that it was noted in last month's minutes that I (as a homeowner whose property backs up to the above-mentioned blocks) did not have a preference for what should or shouldn't be done with parking on Smith Ave. That is not true. I think parking should remain exactly as it is & I'm pretty sure I said so in the meeting.

There have been no problems re: parking on Smith in the 16+ years that I have lived at 600 W. Howe (back of property is on Smith Ave.) (except for the occasional handyman/construction vehicles that park improperly at times when working on adjacent properties). Anyway, I was told you are the person to let know that I am opposed to any changes or to additional signage in the 500 block. I am against tampering with the character of this alley/street (it really barely qualifies as a street in my opinion.) Smith Ave (at it currently is) has a charm that adds much to this little part of Prospect Hill. Signs and/or widening of the street would destroy that.

Sincerely,  
Julia Karr  
600 W. Howe St.  
[812-325-1181](tel:812-325-1181)



**Public Input received by City staff**

Scott Robinson &lt;robinsos@bloomington.in.gov&gt;

**Proposal to establish a No Parking Zone on W. Smith Avenue...**

1 message

Laurel Cornell &lt;cornell@indiana.edu&gt;

Tue, Mar 29, 2016 at 6:06 PM

To: "Robinson, Scott" &lt;robinsos@bloomington.in.gov&gt;, Chris Sturbaum &lt;sturbauc@bloomington.in.gov&gt;, Sarah Ryterband &lt;saryter@gmail.com&gt;

29 March 2016

Scott Robinson, Planning Services Manager, City of Bloomington  
City of Bloomington Traffic Commission  
Cc: Chris Sturbaum, Sarah Ryterband

Re: No Parking Zone --- W. Smith Avenue between S. Rogers Street and S. Fairview Street

Dear Traffic Commission,

My name is Laurel Cornell. I am a homeowner who lives at 402 S. Rogers Street. This is at the intersection of Smith and Rogers, on the east end of the road segment discussed in this proposal. I enter my house on Smith Street, and I have lived here for 15 years, so I am familiar with how we neighbors use Smith Street.

I was out of town from late January through March 12, 2016, so I did not have the opportunity to attend the February 24 meeting of the Traffic Commission. I prepared remarks for the March 23 Traffic Commission meeting, but it was cancelled.

I suggest that the city modify its proposal to establish a no-parking zone along Smith Street.

Smith Street is a unique kind of street. It is narrow and short, so neighbors treat it more like a public meeting space and park than like a thoroughfare. We ride our bikes or walk our dogs down the center of it. We stand on the street and chat. We garden alongside it to make it more inviting. Cats take naps in the middle of the street. If there were children living on Smith Street I am sure that they would be out drawing on it with chalk. Most of us have space for one car on our own lots. Most of us park our cars on Smith Street from time to time as the need arises --- for example, when someone comes to work on our houses. Smith Street is more like a multi-use street or a shared street than it is like a one dedicated to car traffic. In European road design this kind of street is called by the Dutch term "woonerf." (<http://www.nytimes.com/2013/04/28/automobiles/where-share-the-road-is-taken-literally.html>). Instead of simply laying out conventional parking spaces, I wish that Smith Street could serve as a demonstration project for INDOT for establishing multi-use streets like the woonerf in the State of Indiana. That is to say, neighbors should continue using the street as they currently do, as a multi-use street, without designation of parking zones.

However, a few of the houses on Smith Street do need parking spaces designated specifically for those addresses. One of the houses on Smith Street --- 519 --- does not have parking on its property because the façade of the house occupies the whole of the lot face. At 520, the façade does not occupy the whole of the lot face, but the house sits on a hill above the curb. 619 may also need a designated parking space. There is space for a driveway on the west side of the lot (in front of the shed), but a utility pole blocks the exit to the street. Provision of a parking space for this address is especially important because our historic preservation guidelines prohibit parking in front yards. As for 521, when I went out on the afternoon of March 23, 2016 to do field research for the Traffic Commission meeting that evening, 521 had parking in the rear. Since then the property owner has filled in that space with a garden.

Spreading out the necessary designated parking spaces along the street --- separated from each other --- would both address garbage truck and fire truck access and be an equitable and fair method of addressing this need for designated parking.

On a lighter note, I also have one suggestion. That is, let's rename this segment of Smith Street "Vasen Street," in



honor of the Lotus World Music and Arts Festival, and in tribute to the Swedish folk rock band, Vasen, that has been such a mainstay of it.

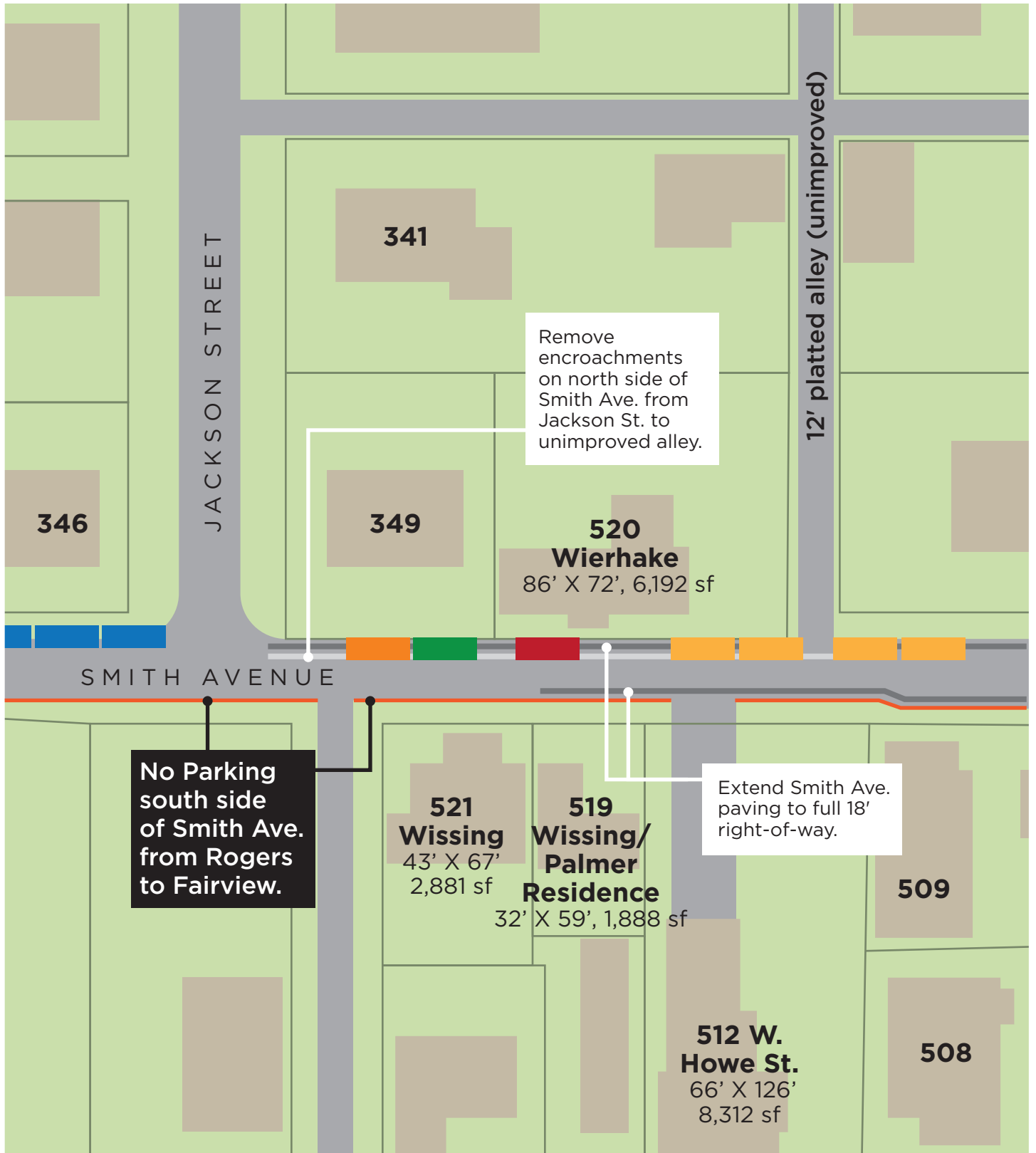
Thank you for your consideration.

Sincerely,  
Laurel Cornell  
402 S. Rogers Street  
Bloomington, IN 47403  
[812-219-2051](tel:812-219-2051)  
[cornell@indiana.edu](mailto:cornell@indiana.edu)



This proposal and map were submitted by Doug Wissing

# 500-600 Blocks W. Smith Ave. No-Parking Proposal



-  Current Wierhake 520 W. Smith Ave. private parking space
-  Current Palmer 519 W. Smith Ave. private parking space
-  Proposed Wissing 521 W. Smith Ave. private parking space
-  Three cars currently parking on the north side of the 600 block of Smith, beside 346 S. Jackson
-  Proposed parking



**Doug Wissing comments regarding proposed no-parking zone in 500-600 blocks W. Smith Ave.**

**The City of Bloomington Traffic Commission, March 23, 2016 meeting**

**Traffic Commissioners—**

**Parking and traffic safety on W. Smith Ave. from Rogers to Fairview will be improved with a no-parking zone on the south side of the 500 and 600 blocks of W. Smith Ave.**

- 1) **In the Prospect Hill neighborhood, excepting W. 3<sup>rd</sup>, every no-parking zone is on one side of the street.** This includes no-parking zones on 4<sup>th</sup> St., Prospect St., Howe St., Jackson St., Fairview, Maple, Euclid, Buckner and Davisson. All of these streets have no-parking zones on one side—not alternating blocks. This is the Prospect Hill neighborhood precedent and pattern. Having this one anomaly of two alternating blocks will confuse drivers and make the street less safe.
- 2) **The parking in the 500 block will be improved if the south-side is a no-parking zone because utility poles complicate parking.** The City staff recommends a no-parking zone on the south side of the 600 block because the utility poles adjacent to the street complicate parking. The 500 block has the same south-side utility pole issue.
- 3) **Residents have long parked on the north side of the 500 block of W. Smith Ave., and continue to do so.<sup>1</sup>**
- 4) **Removal of right-of-way encroachments from the north side of the 500 block of W. Smith Ave. will improve parking and traffic safety.<sup>2</sup>**
- 5) **Paving W. Smith Ave. to the full right-of-way and painting parking markers will improve parking and traffic safety.**

Political sages have long known it is a bad idea to make public policy based on a few personalities or transient conditions. Good public policy is best made to deliver long-term benefits to the greatest number. Parking and traffic safety along the 500-600 blocks of W. Smith Ave. will be improved by removing encroachments, paving to the full right-of-way width, and establishing no-parking along the south side of these two blocks. I urge the Traffic Commission to make this recommendation to the Common Council.

Respectfully,

Doug Wissing

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<sup>1</sup> For historic data, please see my 4 March 2016 email to the Traffic Commission with the attachment "Wierhake 2007-8 permit app," which includes a 2007 City engineering report with an aerial photo showing a car parked on the north side adjacent to 520 W. Smith Ave. See page 3. Page 11 of the attached docs shows another car parked on the north side of W. Smith. For recent data, see attached "Wierhake 2015 north side parking," which includes Nov. and Dec 2015 photos of cars also parked on the north side of the 500 block of W. Smith Ave.

<sup>2</sup> There are substantial ROW encroachments, including limestone blocks, rebar markers and vegetation beside 520 W. Smith Ave. and 349 W. Jackson. David Wierhake, the owner of 520 W. Smith Ave., stated in an August 4, 2015 email to City Planning that his front porch encroaches into the W. Smith Ave. right-of-way. See David - dwBrykalski - Wierhake [sellingideas@hotmail.com](mailto:sellingideas@hotmail.com) to [planning@bloomington.in.gov](mailto:planning@bloomington.in.gov), Tue, Aug 4, 2015 at 4:31 PM





Nate Nickel &lt;nickeln@bloomington.in.gov&gt;

**Public Input received by City staff**

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## Just for the record

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David — dwBrykalski — Wierhake <sellingideas@hotmail.com>  
To: Nate Nickel Traffic Commission <nickeln@bloomington.in.gov>

Wed, Apr 20, 2016 at 1:30 PM

Mr. Nickel:

Thank you for the heads-up on the upcoming TC meeting.

Yes, I will be there in person to comment sincerely and thoughtfully regarding what is a certain 'neighbor's' (landlord/property owner) attempt to not only disrupt traffic—both vehicle and pedestrian—but disrupt the peace and minds of individuals who actually live on Smith Ave.

You caught me off guard, not anticipating writing an 'official' statement, but here are a few key points:

1. I've lived at 520 W. Smith Avenue since 1985. There has NEVER been a parking problem on this alleyway. (Until a certain person's contractors parked their trucks at the T intersection of Jackson/Smith, which made it challenging for city services to get through e.g. garbage and recycling trucks.)
2. The character of Smith Avenue is as historic as the homes that sit along side. And, it supports foot traffic e.g. baby strollers and mothers/fathers, young children on bikes, dogs and cats, etc. It's like the B-Line with some occasional cars and trucks. (Did you know that Smith dead ends into the church parking lot? That's a two block 'run' from Rogers.)
3. Smith Avenue is a QUEUING STREET. It naturally slows traffic down.
4. When I arrived here in 1985, there were three PUBLIC parking spots directly across from my front door (519/521 W. Smith Ave). This was a simple pull-off of cinder rock/dust. When the present owner of those rental properties acquired the deeds, he managed to get the City to pave said inlet parking area with taxpayer money. Later he applied for a PRIVATE PARKING spot for his Tuscan Cottage (519), even though he did not RESIDE in the house (*15.36.010 Eligibility. "Any person **residing in a single household detached dwelling** in an area of the city zoned for residential purposes who owns an automobile may apply to the city controller for a permit for one parking space adjacent to such residential property. "*), but only rented it. My partner Kate Braun and I (we actually reside in our house) were approved for a PRIVATE PARKING spot this year next to the Tuscan Cottage tenant's spot. (They are nice folks and we consider them friends.) That left a single public space. But back in the summer/fall (prior to Kate and I getting a PP SPOT), the landlord of those properties felt inclined to park his spare pickup and Volvo sedan in the two public parking areas, give the keys to his workers so that they could move his vehicles in the morning, park their trucks in front of the rental properties, then repeat the process in the evening e.g. a vehicle game of chess. End result? Lock Kate and David out of any opportunity to park where they usually parked--public parking, my friends. Now, any kind and thoughtful neighbor who had some construction business coming up would have approached us and simply inquired, "Hey, Dave and Kate, do you think you'd mind not parking in front of these two houses from this date until this date, so my workers can easily navigate their tools and stuff?" Never happened. Meanwhile, Kate and I received PP spot, then the landlord in question manages to convince the City (Suzie Johnson & Co.) to erase the third parking spot at the alley next to Julia Karr's backyard, by extending the yellow curb to at least 12 feet--far more yellow than the usual 2-4 feet you see throughout this neighborhood's alleyways.



5. The petitioner in question just erased his backyard private parking area (521), filling it in with sod and a couple raised beds. I wish him luck with his plantings.

I have much, much more to share regarding the insanity of what was proposed by said petitioner—all his self-serving options on his lovely colored diagram. If one wants a Hyde Park-like street, then one should move to Hyde Park. Smith Avenue has a beauty all its own. Don't mess with paradise. Sticking up a sign that says NO PARKING ON THIS SIDE will do nothing except add to more sign clutter in Prospect Hill. (Said signs are on Prospect St. and do nothing to alter parking habits. Besides, rarely, and I mean RARELY, does anyone park on the north side of the 500 block of Smith. IF any sign must be planted on Smith Avenue, make it a SLOW DOWN CHILDREN AT PLAY. Matt and Selma's three-year-old daughter Enna is learning to cross the street and pay Kate and I a visit—on her own. That is the only signage I am willing to support. Keep Smith Avenue as is with banks of wildflowers and kind neighbors exchanging news of love and kindness. Bring back the 'missing' 3rd PUBLIC parking spot off the alleyway next to 521 and Julia's backyard—and keep it PUBLIC.

Hope this helps shed some light of truth on this matter. I look forward to meeting the entire membership of the Commission.

P.S. Visitors always have the option of parking on Rogers Street. Always spots available. And, it's only a half block away! (Ever spent time in Chicago? You'd be tickled pink to park a block or two away from your apartment or home.)

Warm Regards,

David

*"Comparison is the death of joy." — Mark Twain*

**dwBrykalski\***  
**Actor/Musician/Songwriter**

- **WEBSITE** featuring Monologues/Songs/Tunes: [dwBrykalski.com](http://dwBrykalski.com)
- **WATCH Trailer** of **NATHAN and the LUTHIER**: <http://www.youtube.com/watch?v=NLsdsJZotqU>

cell: 812.369.7787

e-mail: [sellingideas@hotmail.com](mailto:sellingideas@hotmail.com)

us mail: 520 W. Smith Ave. • Bloomington, Indiana 47403 USA

Actors' Equity Member

\*The official stage/performance 'handle' adopted by **David Wierhake** as a tribute to his Polish roots via his mother, *Irene Antoinette Brykalski (1925-2012)*.

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Date: Wed, 20 Apr 2016 12:05:10 -0400

Subject: Re: PHNA Minutes

From: [nickeln@bloomington.in.gov](mailto:nickeln@bloomington.in.gov)

To: [sellingideas@hotmail.com](mailto:sellingideas@hotmail.com)

CC: [robinsos@bloomington.in.gov](mailto:robinsos@bloomington.in.gov)

David,

I wanted to contact you regarding next week's Traffic Commission meeting. It appeared as though you may have had some specific comments or thoughts regarding Smith Avenue to share with the Traffic Commission. If that's the case, can you please send those to me by 4:00 PM this afternoon. That way, I can include them in

## Proposal to Traffic Commission

**Presented by: Marilyn Conn, President, AFSCME Local 613**

**Re: 7<sup>th</sup> Street Problems – Route 6**

I wish to address an issue that has long been a problem for our Route 6 buses.

The problem is the portion of the route that requires the buses to be driven down 7<sup>th</sup> street between College and Indiana.

Many accidents continue to occur in this stretch of 7<sup>th</sup> street because the lanes are too narrow to safely pass with the buses (and other large vehicles).

### Traffic Commission Study

I have a report attached here of a recent study done by the Traffic Commission, in conjunction with Bloomington Transit. This was a 2 part study:

1. **“in response to a pattern of property damage crashes by larger vehicles passing transit buses in the opposite direction”**
2. a means to improve transit accessibility for customers at several transit stops.

After recommending several changes in bus stop accessibility, the report goes on to say **“but this option did not adequately address the concern over crash hazards for vehicles.”**

### Our Proposal:

The drivers have proposed a solution that would cost the city nothing.

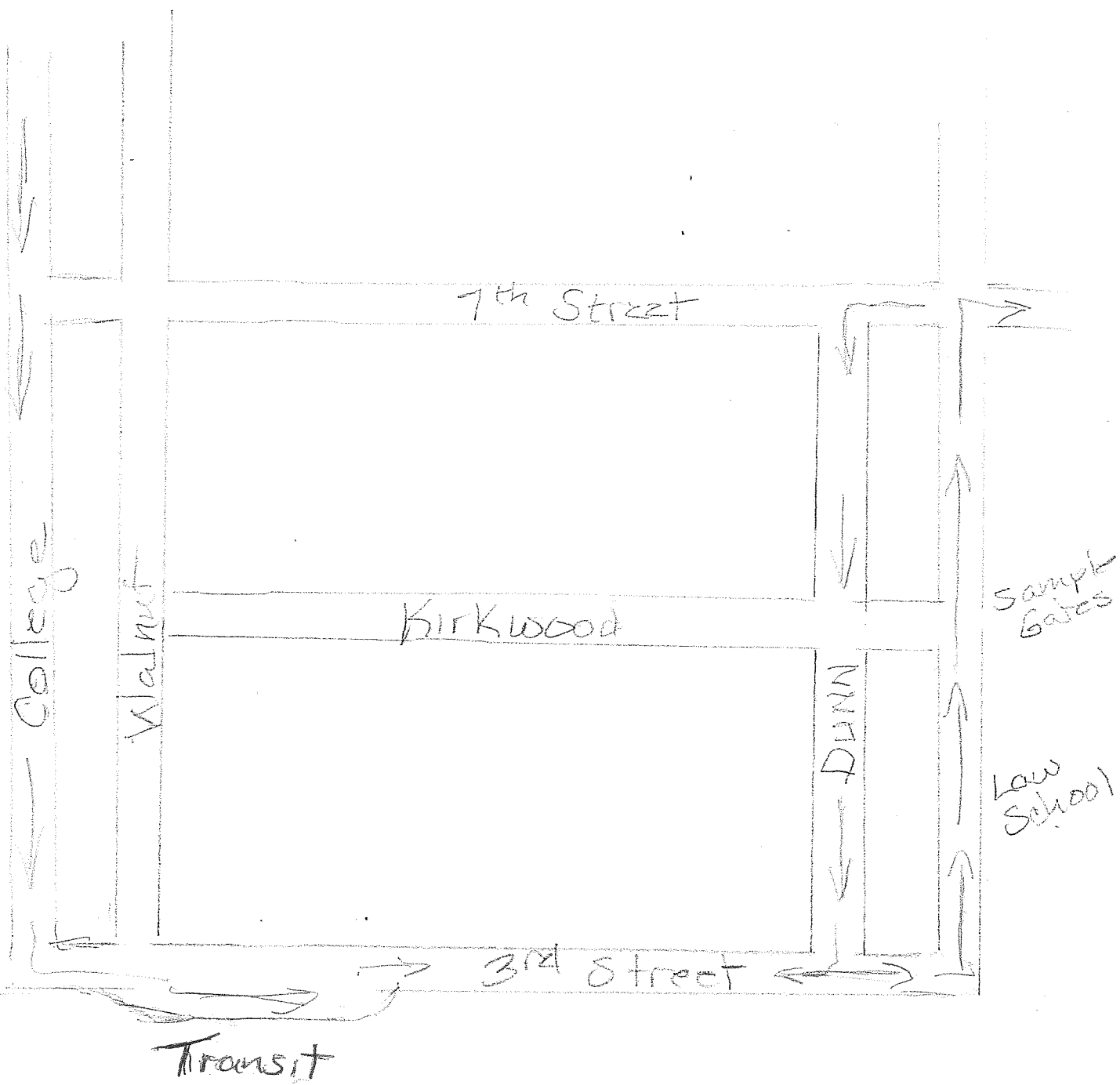
**Get the buses off of 7<sup>th</sup> street.**

The route change we propose would be as follows: (see attached map)

East Bound: South on College to 3<sup>rd</sup> (a stop at Transit) **Added Service**  
East on 3<sup>rd</sup> to Indiana  
North on Indiana (stops at Law School and Sample Gates) **Added Service**  
East on 7<sup>th</sup> (resume current route)

West Bound: West on 7<sup>th</sup> to Dunn  
South on Dunn to 3<sup>rd</sup>  
West on 3<sup>rd</sup> (bus stop at Walnut for Transit) **Added Service**  
North on Walnut (resume current route)







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## MEMORANDUM

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To: Traffic Commission  
From: Scott Robinson, Planning Services Manager  
Date: September 16, 2015  
Re: 7<sup>th</sup> Street Bloomington Transit Bus Zones – Bus Stop Modifications

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### Background

The City of Bloomington Planning and Transportation Department, in conjunction with Bloomington Transit, would like to formalize several transit stops along 7<sup>th</sup> Street, from Walnut Street to Indiana Avenue. This is in response to a pattern of property damage crashes by larger vehicles passing transit buses in the opposite direction as well as a means to improve transit accessibility for customers at several transit stops.

The creation of bus pull-off areas along this narrow section of 7<sup>th</sup> Street allows buses to better anticipate and avoid crash hazards from oncoming vehicles. Improves visibility, accessibility, and safety for transit riders getting on or off the bus. The creation of these bus zones will impact the location of several on-street metered parking spaces. Staff also evaluated curb extensions as an alternative option to improve accessibility, but this option did not adequately address the concern over crash hazards for vehicles. Below are photos of both existing transit stops and proposed.



*Left: 7<sup>th</sup> and Washington, south side, existing stop; Right: 7<sup>th</sup> and Grant, south side, existing stop.*





*Top Left: 7<sup>th</sup> and Grant looking east; Top Right: 7<sup>th</sup> and Grant looking west; Bottom Left: 7<sup>th</sup> and Dunn, south side, proposed stop; Bottom Right: 7<sup>th</sup> and Dunn, north side, existing stop.*





*Top Left: 7<sup>th</sup> and Grant, north side, existing stop (passenger waiting); Top Right: 7<sup>th</sup> and Grant, north side, proposed stop; Bottom Left: 7<sup>th</sup> and Lincoln, north side, proposed stop; Bottom Right: 7<sup>th</sup> and Washington, existing stop.*





*Left: 7<sup>th</sup> and Washington, looking east, existing stop; Right: 7<sup>th</sup> and Walnut, north side, existing stop.*

To accommodate this request, the Bloomington Municipal Code would need to be amended. Specifically, Title 15.32.110, Schedule P – Bus Zones would need to include the location for five bus zones. Staff also recommends moving one additional stop, east of Dunn Street, which is already within a no parking area and does not need to be included in Schedule P. Staff will work with Bloomington Transit on moving this stop closer to the intersection of Dunn Street and 7<sup>th</sup> Street, as well as with Parking Enforcement, on any impacts to metered parking spaces from bus zones. An illustration is included for reference on the six total bus stop locations, stops to be relocated, and possible impacts to metered parking spaces.

**Recommendation:** Staff recommends adding the following general locations to Title 15.32.110 Bus Zones, Schedule P: 7<sup>th</sup> Street and Walnut Street on the north side of 7<sup>th</sup> Street; 7<sup>th</sup> Street and Washington Street on the south side of 7<sup>th</sup> Street; 7<sup>th</sup> Street and Lincoln Street on the north side of 7<sup>th</sup> Street; 7<sup>th</sup> Street and Grant Street on the north side of 7<sup>th</sup> Street; and 7<sup>th</sup> Street and Dunn Street on the south side of 7<sup>th</sup> Street. If approved, a more detailed amendment will be prepared once this request is forwarded to the Common Council for their consideration. Bloomington Transit will also install concrete pads at locations where they are needed for these bus zones. These pads will provide improved access from the sidewalk to the curb for transit riders getting on and off buses.