

City of Bloomington Common Council

Legislative Packet

Wednesday, 31 July 2019

Regular Session and Committee of the Whole

Legislation and background material regarding Ordinance 19-09 [Regulating Motorized Scooters], please consult [03 April 2019 Legislative Packet](#). For previously proposed amendments, please consult [01 May 2019 Legislative Packet](#).

Legislation and background material regarding Ordinance 19-12, Ordinance 19-13, and Ordinance 19-14, along with new amendments to Ordinance 19-09 contained herein.

For a schedule of upcoming meetings of the Council and the City's boards and commissions, please consult the City's [Calendar](#).

Office of the Common Council
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<http://www.bloomington.in.gov/council>



LEGISLATIVE PACKET CONTENTS

Regular Session and Committee of the Whole: Wednesday, 31 July 2019, 6:30 pm

- Memo from Council Office
- Agenda for Regular Session and Committee of the Whole
- Notice: PS-LIT -- 29 July and 01 August both @ 6pm, Council Chambers

Second Readings

- **Ordinance 19-09** (Amending Title 15 - Shared-Use Scooters - *Postponed from 15 May 2019*)
 - Previously Adopted Amendments (Summarized further on in this memo)
 - Am 06a (Cm. Piedmont-Smith, Sponsor)
 - Am 07a (Cm. Piedmont-Smith, Sponsor)
 - *Note: This amendment was opposed by the Administration and shared-use scooter vendors and may be revisited on July 31st. Also, please see Am 15 (Cm. Piedmont-Smith) for an alternate approach to this issue.*
 - Am 08a (Cm. Piedmont-Smith, Sponsor)
 - Am 09 (Cm. Sandberg, Sponsor, at the request of the Administration)
 - Am 10 (Cm. Piedmont-Smith, Sponsor, at the request of the Administration)
 - Am 11 (Cm. Piedmont-Smith, Sponsor)
 - Am 12 (Cms. Sandberg, Granger, and Ruff)
 - Proposed for Introduction on July 31st (Summarized further on in this memo)
 - Am 13 (Cm. Piedmont-Smith, Sponsor)
 - Am 14 (Cm. Piedmont-Smith, Sponsor)
 - Am 15 (Cm. Piedmont-Smith, Sponsor)
 - Am 16 (Cm. Piedmont-Smith, Sponsor)
 - Am 17 (Cm. Piedmont-Smith, Sponsor)

First Readings

- **Ordinance 19-12** Amending Zoning Maps – New PUD for 1800 N. Walnut Street
 - Certificate of Action (Dated June 17th; Vote: 7 – 0);
 - Maps of Site and Surrounding Uses (including Future Land Use Map from the Comprehensive Plan)
 - Staff Report to the Council from Eric Greulich, Senior Zoning Planner (which largely repeats the Staff Report to the Plan Commission (below))
 - Plan Commission Materials
 - Staff Report to the Plan Commission (June 10th)

- Memo from the Environmental Commission (June 10th)
(Portion with Petitioner Materials)
- Letter to Plan Staff (April 8th)
- Petitioner Statement (undated)
- Letter Memorializing Commitment to Make Contribution to the Housing Development Fund (June 5th)
- Development Standards (reflecting action of Commission on June 10th)
- Traffic and Transit Discussion
 - Maps including Master Thoroughfare Plan and various Transit routes
- Site Plans
 - Overall, Enlarged, Utility & Landscape
 - Map of off-site sidewalk improvements
- Elevations
- Lot Coverage Map
- Cross-Sections of Building by Level
- Height and Massing
- External Renderings

Contact

Eric Greulich 812-349-3423, greulice@boomington.in.gov

- **Ordinance 19-13 – Amending Title 15 – No Parking Zones**
 - Memo from Cm Volan
 - BMC 15.32.080 – No parking zone with strike-out changes
 - Map of parking changes to Dunn Street
- **Ordinance 19-14 – Amending Title 2 – Appointments to Parking Commission**

Minutes

- **12 June 2019 – Regular Session**

MEETING ON WEDNESDAY, 31 JULY 2019, AT-A-GLANCE

REGULAR SESSION

Second Reading:

- **Ordinance 19-09** - To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles and Traffic” – Re: Amending Chapter 15.04 (Definitions), 15.56 (Bicycles, Skateboards, and Other Foot-Propelled Vehicles), 15.60 (Miscellaneous Traffic Rules), 15.64 (Traffic Violations Schedule) and Adding a New Chapter 15.58 (Motorized Scooters and Shared Use Motorized Scooters) to Provide for Regulation Governing Motorized Scooter, Shared- Use Motorized Scooters, and Shared-Use Motorized Scooter Operations.
→ for previous material please consult the:
[03 April 2019](#) Legislative Packet; and
[01 May 2019](#) Legislative Packet (for previously approved amendments)

First Reading:

- **Ordinance 19-12** - To Amend the City of Bloomington Zoning Maps by Rezoning 3.85 Acres of Property from Commercial Arterial (CA) to Planned Unit Development and to Approve a District Ordinance and Preliminary Plan - Re: 1800 N. Walnut Street (CDG Acquisitions, LLC, Petitioner)

- *Note: This ordinance brings forward a PUD from the Plan Commission for Council consideration. Under local code, upon introduction, the Council should entertain a Motion to Refer to the Land Use Committee (first) and if that fails, then to the Committee of the Whole. Please also note that both the President of the Council and Chair of the LUC agree that the first discussion should occur after the Regular Session on August 7th.*

- Ordinance 19-13 - To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles & Traffic” – Re: Amending Chapter 15.32.080 (No Parking Zones)
- Ordinance 19-14 - To Amend Title 2 (Administration and Personnel) of the Bloomington Municipal Code – Re: Amending Chapter 2.12.110 (Parking Commission) to Loosen Requirements for Appointments to the Parking Commission

→ Please see this packet for the legislation, materials, and summary.

COMMITTEE OF THE WHOLE

Discussion – Chair: Cm. Sturbaum

- Ordinance 19-13 - To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles & Traffic” – Re: Amending Chapter 15.32.080 (No Parking Zones)
- Ordinance 19-14 - To Amend Title 2 (Administration and Personnel) of the Bloomington

Second Readings and Resolutions – Summary of New Material

Item 1:

Ordinance 19-09 - Regulation Governing Motorized Scooter, Shared- Use Motorized Scooters, and Shared-Use Motorized Scooter Operations.

At the Regular Session next Wednesday, the Council is scheduled to take up Ord 19-09, regarding the regulation of motorized and shared-use motorized scooters. As cited above, previous discussion and actions were taken in April and May, and on May 15th the ordinance was postponed until July 31st.

The following outline lists amendments that were previously adopted followed by amendments that are proposed for introduction next week (all of which are included in this packet). There are seven amendments that have been adopted and five amendments to be introduced next week. All of the new amendments are sponsored by Cm. Piedmont-Smith, who worked on them during the Council Recess and met, at one point, with advocates from the accessibility community, with eye to incorporating some of their concerns. These lists indicate the sponsor and briefly summarize the legislation. Please see the comments which are indented and in italics for items that were particularly significant or may be in conflict with new amendments.

Previously Adopted Amendments

- Am 06a (Cm. Piedmont-Smith, Sponsor) – Extended boundaries of the dismount zone for bicycles, scooters & coasters (and was revised after April 17th to extend those boundaries further) – with map.

- Am 07a (Cm. Piedmont-Smith, Sponsor) – Prohibited the parking of scooters on all public sidewalks.
 - *Note: This amendment was opposed by the Administration and shared-use scooter vendors and may be revisited on July 31st. Also, please see Am 15 (Cm. Piedmont-Smith) for an alternate approach to this issue.*
- Am 08a (Cm. Piedmont-Smith, Sponsor) - Replaced the words “painted boxes” with “corrals,” which is where scooters and bicycles may park on the street, and provided that they be designed to prevent those vehicles from falling outside the boundaries of the corral.
- Am 09 (Cm. Sandberg, Sponsor, at the request of the Administration) – Removed requirement that the shared-use scooter vendors provide monthly reporting in addition to real-time reporting of certain data, and prohibited the release of trade secrets and personally identifiable information received by the City.
- Am 10 (Cm. Piedmont-Smith, Sponsor, at the request of the Administration) – Allowed discounts for shared-use scooters other than 50% as long as the discount was no less than that percentage of the straight price.
- Am 11 (Cm. Piedmont-Smith, Sponsor) – Corrected the date of the arrival of shared-use scooters as presented in the first whereas clause.
- Am 12 (Cms. Sandberg, Granger, and Ruff) – Removed prohibition against operating shared-use scooters after 10 pm and before 6 am.

Proposed for Introduction on July 31st

- Am 13 (Cm. Piedmont-Smith, Sponsor) – would require all persons operating a scooter to dismount when passing any pedestrians on sidewalks and dismount when overtaking those pedestrians with a visual impairment while on a multi-use path or trail, or within a crosswalk.
- Am 14 (Cm. Piedmont-Smith, Sponsor) – would broaden the requirement for shared-use scooter vendors to make helmets available locally so that they must be made available “at convenient locations throughout the community, with such locations determined by areas of highest shared-use motorized scooter usage.”
- Am 15 (Cm. Piedmont-Smith, Sponsor) – would limit scooter parking on sidewalks to bicycle parking facilities, including bike racks and bike hoops, while still emphasizing accessibility of the public right-of-way for all users; would change the term “bike rack” to “bicycle facility”; and, would request that the Administration install additional such facilities, with those on sidewalks placed parallel to the sidewalk.
 - *Note: This amendment offers another approach towards parking of scooters than adopted with Am 07a which would need to be reconciled with Am 07a.*
- Am 16 (Cm. Piedmont-Smith, Sponsor) – would specify the ADA when requiring compliance with all local, state and federal law, as requested by members of advocates from the accessibility community.
- Am 17 (Cm. Piedmont-Smith) – would shift the effective date of the ordinance from July 1st to September 1st (or to another date) to allow for implementation of these provisions.
 - *Note: The effective date may need to be adjusted in the even the ordinance passes and more time is needed for implementation.*

First Readings – Summaries

Item 1:

Ord 19-12: A Proposal to Rezone 3.85 Acres at 1800 North Walnut (Motel 6 Site) from Commercial Arterial (CA) to Planned Unit Development (PUD) – CDG Acquisitions, LLC - Summary

Ord 19-12 would rezone 3.85 acres at 1800 North Walnut (Motel 6 Site) from Commercial Limited (CA) to Planned Unit Development (PUD) and approve the associated District Ordinance¹ and Preliminary Plan.²

Procedural Posture. The petition was filed by CDG, LLC, of St. Louis MO, and heard by the Plan Commission on May 13th ³ and June 10th,⁴ and forwarded to the Council with a positive recommendation on June 17th by a vote of 7 – 0. As is stated near the end of this summary, the 90-days in which the Council has to act on this proposal will expire on September 16th.

The Site and Surrounding Uses. This “L”⁵ shaped site is 3.85 acres (which requires a waiver of the 5-acre minimum size for a PUD). The site is occupied by a motel that faces North Walnut on the west, which serves as the sole vehicular access. The land slopes upward to the east. It is surrounded by multi-family and commercial uses on the north, multi-family (Varsity Villa) uses to the east, hotel and multi-family uses to the south, and institutional uses (Miller Showers Park) to the west.

The Proposal. In the words of the Memo to the Council:

The petitioner proposes to create a Planned Unit Development in order to construct 2 buildings that would range in height from 5 to 6 stories. The proposal includes a minimum of 6,000 square feet of commercial space, 6,000 square feet of amenity space, and apartments on the upper floors and portions of the ground floors. The multifamily portion of the proposal includes a mix of studio units, one-bedroom units, two-bedroom units, and four-bedroom units for a possible total of 270 units and 820 bedrooms. The overall density is proposed at a maximum of 77 D.U.Es per acre. All of the apartments would be fully furnished. The building will also contain a structured parking garage accessed from the

¹ According to BMC 20.04.040 (c), “The PUD district ordinance shall indicate the land uses, development requirements, and other applicable specifications that shall govern the planned unit development.” The District Ordinance may only provide alternative standards to those set forth in Chapter 20.02 (Zoning Districts) and Chapter 20.05 (Development Standards). Where the District Ordinance is silent those specifications, the relevant provisions of the UDO apply. See also BMC 20.04.080 (Process – PUD district ordinance and preliminary plan).

² According to BMC 20.04.040(d), “The preliminary plan shall show the conceptual location of all proposed improvements.” See also BMC 20.04.080 (Process – PUD District ordinance and preliminary plan) (a)(2) and (c)(3) for the purpose and required content of the preliminary plan.

³ For the Plan Commission packet for that meeting go to: <https://bloomington.in.gov/boards/plan/meetings/2019> and select packet for that date; [Minutes have not yet been posted.](#)

⁴ For the Plan Commission packet for that meeting go to: <https://bloomington.in.gov/boards/plan/meetings/2019> and select packet for that date; [Minutes have not yet been posted.](#)

⁵ The front, vertical piece angles northwest along North Walnut and is about 300’ by 300’; behind this part of the site is a horizontal extension that runs east and is about 150’ wide and 300’ long.

south side of the building with 410 parking spaces. There would also be 8 parking spaces provided along the western front for the commercial space. The proposed parking ratio is a minimum 0.50 spaces per bedroom and a maximum of 0.75 spaces per bedroom.

According to the memo, between the first and second hearing, the Petitioner:

- Removed a roundabout entrance to the building along North Walnut;
- Added a pedestrian crossing to the Varsity Villa site;
- Increased the bicycle parking (which, according to our code must be covered) from 1 space per 6 bedrooms to 1 space per 4 bedrooms;
- Agreed to fill-in an off-site gap in the sidewalk along North Walnut in order to connect with 19th Street to the south;
- Submitted a revised district ordinance to “clean up some of the proposed standards”; and

Affordable Housing Contribution

- Agreed to contribute \$20,000 per bed for 15% of the total beds on the property to the Bloomington Housing Development Fund as part of zoning commitment, with 25% of the contribution paid upon receipt of the building permits for the project and the remainder paid upon receipt of the occupancy permit. At 820 beds, this amounts to about \$2.46 million.

Approach Towards this Summary. As has occasionally been done in the past, rather than review and summarize the materials provided in this packet, this summary will refer the reader to the Staff Report to the Council, which largely repeats the Staff Report, and the Petitioner materials. Key here are the:

- District Ordinance and Preliminary Plan⁶ - which are summarized in the Staff Report and consist of the Petitioner’s Statement; and
- PUD Considerations for Plan Commission and Council under the UDO – which are listed, discussed, and include findings adopted by the Plan Commission.
 - Since they mirror considerations by the Council, these findings might inform the Council’s perspective on these considerations.

Environmental Commission Memo. - Please also note that the Environmental Commission filed a report with recommendations and included opposition to:

- Review of the Final Plan by staff (which was requested by the Petitioner but neither

⁶ As set forth in the Memo, the Preliminary Plan (and District Ordinance) cover:

- Uses and Development Standards (largely those of the Commercial Arterial district);
- Residential Density (where there is an increase from 15 to 77 Units per Acre);
- Height and Bulk (where the building exceeds the 40’ limit by another 45’, but mitigates the impact through both horizontal and vertical modulation along with changes in material);
- Parking, Streetscape and Access (which includes 8 parking spaces for the retail next to North Walnut, a relocated drive to align with the street connection across the park and to be shared with property to the south, and a relocated sidewalk with shrubs in tree plot due to utilities);
- Bicycle Parking and Alternative Transportation (where, as mentioned above, the Petitioner will add a missing sidewalk link and increase bicycle storage, and not mentioned above, but more importantly, “must coordinate and contract with Bloomington Transit to provide any additional shuttle service that may be needed [and]... may not operate their own shuttle without” consent of Transit’);
- Architectural Materials (where the more rigorous Commercial Downtown (CD) standards will apply);
- Environmental Considerations (where “no known sensitive environmental features” were identified; and,
- Housing Diversity (where \$2.46 million will be contributed to the HDF).

- proposed by staff nor adopted by the Plan Commission);
- Allowance for 70% impervious surface area rather than the 60% authorized for CA zoning; and
- The possibility that the “Petitioner may rethink the commitment that 50% of all shrubs be evergreen.”

Comprehensive Plan – “Urban Corridor,” Neighborhood Residential, and Gateway North.

As spelled out in more detail at the end of this summary, in considering a PUD, decision-makers are to balance a number of factors, including (but not limited to) the extent to which the proposal is congruent with the Comprehensive Plan ([Link](#)) and surrounding uses. The Staff Report notes that the site overlaps two Land Use Classifications (Urban Corridor and Neighborhood) and one Focus Area (Gateway North). It concludes that the Urban Corridor and Gateway north designations are more applicable to this site and cites two Policies as particularly relevant:

- Policy 4.4.4 – discouraging new student-oriented housing the downtown and nearby areas until more balance is achieved there (paraphrase – p 55) and Policy 5.3.4 – redirecting such development “toward more appropriate locations closely proximate to the IU campus, that already contain a relatively high percentage of student-oriented housing units and are within easy walking distance to the campus and have direct access to university provided parking as well as the university transit system.” (p 64)

After noting other policies reflected in this petition, the Staff Report concludes:

The development of this parcel will add mixed uses and student oriented development that is located outside of the Downtown, within 10 minutes to IU facilities, adjacent to nearby goods and services, and that is directly on a main commercial thoroughfare. The petitioner is providing several connections and off-site improvements to facilitate pedestrian access to the development. The Plan Commission feels that the proposed Preliminary Plan is consistent with the intent and development guidance of the Comprehensive Plan for this area.

Planned Unit Development Considerations and Staff Report Recommendation: Under local code,⁷ both the Plan Commission and Council are to consider as many of a list of criteria that may be relevant to a specific proposal. The Staff Report sets forth the criteria and the findings adopted by the Plan Commission in each regard. Citing the proximity to the IU campus, other high-density student housing, and related goods and services, and the “unique architectural design”, the Staff Report concluded with the paragraph in support of this petition:

The Plan Commission found that the proposed PUD offers a unique architectural design and a range of benefits and features for the tenants that will also benefit the community by providing student housing that is adjacent to other student housing in a location that is well served, along a major arterial road, and proximate to IU’s campus. This petition accomplishes many of the goals of the Comprehensive Plan already outlined in this report, including providing high density student housing in an area that is appropriate and located outside of the Downtown. Several elements of the proposal satisfy the high design criteria outlined in the Comprehensive Plan and attention has been paid to facilitating

⁷ BMC 20.04.080(h)

pedestrian access to the site by providing off-site improvements to increase pedestrian access. The unique architectural design will compliment this area and provide a high quality building along the Urban Corridor.

Conditions of Approval. The staff recommendation also proposed five conditions of approval that were all adopted by the Plan Commission and included:

1. Review of the Final Development Plan by the Plan Commission;
2. Requirement that the Petitioner “coordinate with Bloomington Transit on the incorporation of a shuttle pick-p and potential shuttle service”;
3. Installation of sidewalk connection along North Walnut to 19th Street;
4. Recording the zoning commitment regarding the contribution to the HDF within 90 days of Council approval; and
5. Requirement for a bus shelter along the frontage of this property.

Council Review

As mentioned in the beginning paragraphs of this summary, the Council has until September 16th to act on this proposal and its failure to act by that time would lead to the project going into effect at that time.

In reviewing a PUD proposal, the Council’s review is guided by both local code and State statute. Both are reviewed below. In reviewing a PUD, Council must have a rational basis for its decision, but otherwise has wide discretion.

Bloomington Municipal Code (BMC)

BMC 20.04.080 directs that, in its review of a PUD, the Council shall consider as many of the following criteria as may be relevant to a specific PUD proposal ⁸:

- The extent to which the PUD meets the requirement of 20.04, Planned Unit Development Districts.
- The extent to which the proposed preliminary plan departs from the UDO provisions otherwise applicable to the property (including but not limited to, the density, dimension, bulk, use, required improvements, and construction and design standards and the reasons why such departures are or are not deemed to be in the public interest.)
- The extent to which the PUD meets the purpose of the UDO, the Comprehensive Plan, and other adopted planning policy documents.
- The physical design of the PUD and the extent to which it makes adequate provision for public services; provides adequate control over vehicular traffic; provides for and protects designated common open space; and furthers the amenities of light and air, recreation and visual enjoyment.
- Relationship and compatibility of the PUD to adjacent properties and neighborhood, and whether the PUD would substantially interfere with the use or diminish the value of adjacent properties and neighborhoods.

⁸ Amendments to a PUD are considered in the same manner as the creation of a new PUD. BMC 20.04.080(j)(1).

- The desirability of the proposed preliminary plan to the city's physical development, tax base and economic well-being.
- The proposal will not cause undue traffic congestion, and can be adequately served by existing or programmed public facilities and services.
- The proposal preserves significant ecological, natural, historical and architectural resources.
- The proposal will not be injurious to the public health, safety, and general welfare.
- The proposal is an effective and unified treatment of the development possibilities on the PUD site.

Local code also provides that permitted uses in a PUD are subject to the discretion and approval of the Plan Commission and the Council. Permitted uses are determined in consideration of the Comprehensive Plan, existing zoning, land uses contiguous to the area being rezoned and the development standards outlined in the UDO. BMC 20.04.030.

Indiana Code

Indiana Code § 36-7-4-603 directs that the legislative body “shall pay reasonable regard” to the following:

- the Comprehensive Plan (see above for the Memo’s perspective on congruence with this document);
- current conditions and the character of current structures and uses in each district;
- the most desirable use for which the land in each district is adapted;
- the conservation of property values throughout the jurisdiction; *and*
- responsible development and growth. (I.C. § 36-7-4-603)

Importantly, these are factors that a legislative body must *consider* when making a zone map change decisions. However, nothing in statute requires that the Council find absolute conformity with each of the factors outlined above. Instead, the Council is to take into consideration the entire constellation of the criteria, balancing the statutory factors.⁹

When adopting or amending a PUD district ordinance, State law provides that the Council may adopt or reject the proposal and may exercise any powers provided under State law. Those powers include:

- Imposing reasonable conditions;
- Conditioning issuance of an improvement location permit on the furnishing of a bond or a satisfactorily written assurance guaranteeing the timely completion of a proposed public improvement;
- Allowing or requiring the owner of real property to make written commitments (I.C. § 36-7-4-1512).

⁹ Notably, Indiana courts have made clear that municipalities have wide latitude in approving in PUDs and need not always comply with its comprehensive plan. Instead, comprehensive plans are guides to community development, rather than instruments of land-use control. *Borsuk v. Town of St. John*, 820 N.E. 2d 118 (2005).

Item 2:

Ordinance 19-13 - To Amend Title 15 of the Bloomington Municipal Code Entitled "Vehicles & Traffic" – Re: Amending Chapter 15.32.080 (No Parking Zones)

Ordinance 19-13 makes changes to Title 15 by removing redundant text regarding parking on Walnut Grove and Forrest Avenue and increases parking availability in Zone 4 and 5, and Dunn Street to ease supply pressure in these areas between Tenth and Seventh Streets.

Item 3:

Ordinance 19-14 - To Amend Title 2 (Administration and Personnel) of the Bloomington Municipal Code – Re: Amending Chapter 2.12.110 (Parking Commission) to Loosen Requirements for Appointments to the Parking Commission

Ordinance 19-14 makes changes to the Parking Commission's qualifications for voting membership. Sponsored by Cm. Volan, these changes are intended to loosen the requirements for appointees in light of difficulties filling vacancies. This causes problems, which were apparent on July 25th, when a quorum was not attained and the meeting was cancelled. The changes remove the requirements for certain appointments to the Parking Commission to either reside or work within a Schedule U - On Street Metered Parking zone or neighborhood zone, and set the geographic qualification to residency within the City Limits.

**NOTICE AND AGENDA
BLOOMINGTON COMMON COUNCIL
REGULAR SESSION AND COMMITTEE OF THE WHOLE
6:30 P.M., WEDNESDAY, 31 JULY 2019
COUNCIL CHAMBERS
SHOWERS BUILDING, 401 N. MORTON ST.**

REGULAR SESSION

I. ROLL CALL

II. AGENDA SUMMATION

III. APPROVAL OF MINUTES 12 June 2019 – Regular Session

IV. REPORTS (A maximum of twenty minutes is set aside for each part of this section.)

- 1. Councilmembers**
- 2. The Mayor and City Offices**
- 3. Council Committees**
- 4. Public***

V. APPOINTMENTS TO BOARDS AND COMMISSIONS

VI. LEGISLATION FOR SECOND READING AND RESOLUTIONS

1. Ordinance 19-09 To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles and Traffic” – Re: Amending Chapter 15.04 (Definitions), 15.56 (Bicycles, Skateboards, and Other Foot-Propelled Vehicles), 15.60 (Miscellaneous Traffic Rules), 15.64 (Traffic Violations Schedule) and Adding a New Chapter 15.58 (Motorized Scooters and Shared Use Motorized Scooters) to Provide for Regulation Governing Motorized Scooter, Shared- Use Motorized Scooters, and Shared-Use Motorized Scooter Operations.

Regular Session Action (5/15/19):

Postponement to July 31st 8 – 0 – 0

Regular Session Action (5/1/19):

Meeting recessed until May 15th 8 – 0 – 0

Am-01a Adopt: 3 – 6 – 0 (*FAILED*)

Am-06a Adopt: 6 – 2 – 1

Am-07a Adopt: 8 – 0 – 1

Am-08a Adopt: 5 – 4 – 0

Am-03 Adopt: 2 – 6 – 0 (*FAILED*)

Am-12 Adopt: 6 – 1 – 1

Regular Session Action (4/17/19):

Postponement to May 1st 8 – 0 – 0

Am-11 Adopt: 8 – 0 – 0

Am-10 Adopt: 8 – 0 – 0

Am-09 Adopt: 7 – 0 – 1

Note: Am 01, 03-08 was discussed but no vote was taken

Committee Recommendation (4/10/2019):

Do Pass 3 – 0 – 6

VII. LEGISLATION FOR FIRST READING

1. Ordinance 19-12 To Amend the City of Bloomington Zoning Maps by Rezoning 3.85 Acres of Property from Commercial Arterial (CA) to Planned Unit Development and to Approve a District Ordinance and Preliminary Plan - Re: 1800 N. Walnut Street (CDG Acquisitions, LLC, Petitioner)
2. Ordinance 19-13 To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles & Traffic” – Re: Amending 15.32.080 (No Parking Zones)
3. Ordinance 19-14 To Amend Title 2 (Administration and Personnel) of the Bloomington Municipal Code – Re: Amending Chapter 2.12.110 (Parking Commission) to Loosen Requirements for Appointments to the Parking Commission

* Members of the public may speak on matters of community concern not listed on the agenda at one of the two *Reports from the Public* opportunities. Citizens may speak at one of these periods, but not both. Speakers are allowed five minutes; this time allotment may be reduced by the presiding officer if numerous people wish to speak.

VIII. ADDITIONAL PUBLIC COMMENT* (A maximum of twenty-five minutes is set aside for this section.)

IX. COUNCIL SCHEDULE

X. ADJOURNMENT

(To be immediately followed by a)

COMMITTEE OF THE WHOLE

Chair: Cm. Chris Sturbaum

1. Ordinance 19-13 To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles & Traffic”
– Re: Amending 15.32.080 (No Parking Zones)

Sponsor: Councilmember Volan, District VI

2. Ordinance 19-14 To Amend Title 2 (Administration and Personnel) of the Bloomington Municipal Code
– Re: Amending Chapter 2.12.110 (Parking Commission) to Loosen Requirements for Appointments to the Parking Commission

Sponsor: Councilmember Volan, District VI

* Members of the public may speak on matters of community concern not listed on the agenda at one of the two *Reports from the Public* opportunities. Citizens may speak at one of these periods, but not both. Speakers are allowed five minutes; this time allotment may be reduced by the presiding officer if numerous people wish to speak.

THE MONROE COUNTY LOCAL INCOME TAX COUNCIL
(TAX COUNCIL)

NOTICE

THE
PUBLIC SAFETY LOCAL INCOME TAX COMMITTEE
(PS LIT COMMITTEE) OF THE TAX COUNCIL

WILL MEET AS FOLLOWS:

MONDAY, 29 JULY 2019, 6PM
AND
THURSDAY, 1 AUGUST 2019, 6PM
COUNCIL CHAMBERS
(SUITE 115)
CITY HALL - SHOWERS BUILDING
401 NORTH MORTON STREET
BLOOMINGTON, IN, 47404

THE TAX COUNCIL SERVES AS THE “ADOPTING BODY” IN REGARD TO CERTAIN LOCAL INCOME TAX RATES PER IC 6-3.6 ET AL. IT IS COMPRISED OF FOUR MEMBERS - THE: BLOOMINGTON COMMON COUNCIL, ELLETTSVILLE TOWN COUNCIL, MONROE COUNTY COUNCIL, AND STINESVILLE TOWN COUNCIL. REPRESENTATIVES OF THE MEMBERS SIT ON THE PS LIT COMMITTEE, WHICH WILL MEET AS INDICATED ABOVE TO: RECOMMEND A TAX RATE FOR THE PUBLIC SAFETY ANSWERING POINT [PSAP]; RECEIVE A REPORT FROM THE MONROE COUNTY COUNCIL AND RECOMMEND ALLOCATIONS TO QUALIFIED SERVICE PROVIDERS [QSP] UNDER IC § 6-3.6-6-8(c); AND, MAKE RELATED RECOMMENDATIONS TO THE TAX COUNCIL.

PURSUANT TO INDIANA OPEN DOOR LAW (IC 5-14-1.5), THIS PROVIDES NOTICE THAT THIS MEETING WILL OCCUR AND IS OPEN FOR THE PUBLIC TO ATTEND, OBSERVE, AND RECORD WHAT TRANSPIRES.

Member	Address	Phone / Email
Bloomington Common Council	401 N. Morton St. (Room 110) P.O. Box 100 Bloomington, IN 47402	812-349-3409 / council@bloomington.in.gov
Ellettsville Town Council	1150 W. Guy McCown Drive P.O. Box 8 Ellettsville, IN 47429	812-876-3860 / clerktreasurer@ellettsville.in.us
Monroe County Council	100 W. Kirkwood Ave (Room 306) Bloomington IN 47404 -5140	812-349-7312 / counciloffice@co.monroe.in.us
Stinesville Town Council	P.O. Box 66 Stinesville, IN 47464	812-876-8303 / stinesville@bluemarble.net

Ordinance 19-09

To Amend Title 15 of the Bloomington Municipal Code Entitled “Vehicles and Traffic” – Re: Amending Chapter 15.04 (Definitions), 15.56 (Bicycles, Skateboards, and Other Foot-Propelled Vehicles), 15.60 (Miscellaneous Traffic Rules), 15.64 (Traffic Violations Schedule) and Adding a New Chapter 15.58 (Motorized Scooters and Shared Use Motorized Scooters) to Provide for Regulation Governing Motorized Scooter, Shared- Use Motorized Scooters, and Shared-Use Motorized Scooter Operations.

Previously Adopted Amendments

Amendments to be Introduced

Previously Adopted Amendments

- Am 06a -- Piedmont-Smith (including Map)
- Am 07a -- Piedmont-Smith
- Am 08a -- Piedmont-Smith
- Am 09 -- Sandberg on behalf of the City
Administration
- Am 10 -- Piedmont-Smith on behalf of the City
Administration
- Am 11 -- Piedmont-Smith
- Am 12 -- Sandberg, Granger, and Ruff

***** Amendment Form *****

Ordinance #: 19-09

Amendment #: 06a (revised subsequent to discussion)

Submitted By: Councilmember Piedmont-Smith

Date: 17 April 2019 (discussed); 01 May 2019 (introduced in revised form)

Proposed Amendment:

1. Ordinance 19-09, shall be amended by revising SECTION 1, §15.04.055 “Dismount Zone” to expand the boundaries of the zone such that the entire provision shall now read as follows:

15.04.055 Dismount zone.

“Dismount zone” means those sidewalks and crosswalks where motorized scooters, shared-use motorized scooters, coasters, or bicycles may not be ridden and where users must (i) dismount and walk their devices, (ii) operate their devices in a bicycle lane, or (iii) operate their devices in the roadway. Dismount zones are located within the crosswalks and on the sidewalks on either side of the street along the following streets:

Fourth Street from Indiana Avenue to the B-Line Trail

Kirkwood Avenue from Indiana Avenue to the B-Line Trail.

Sixth Street from Indiana Avenue to the B-Line Trail.

Indiana Avenue from Fourth Street to Seventh Street.

Dunn Street from Fourth Street to Seventh Street.

Grant Street from Fourth Street to Seventh Street.

Lincoln Street from Fourth Street to Seventh Street.

Washington Street from Fourth Street to Seventh Street.

Walnut Street from Fourth Street to Seventh Street.

College Avenue from Fourth Street to Seventh Street

2. Ordinance 19-09 shall be amended by adding a new SECTION 9 and by re-numbering subsequent sections. The new section shall read as follows:

SECTION 9. Subsection 15.56.020(a)(7) shall be revised to expand the dismount zone such that the entire provision shall read as follows:

(7) Operating a bicycle on the sidewalks and within the crosswalks along the following streets is hereby declared a public nuisance and is therefore prohibited. Bicyclists in these areas shall dismount their bicycles. Signage shall be in place prior to enforcement of this requirement and shall, when practicable, be in the form of markings on the sidewalk.

STREETS:

Fourth Street from Indiana Avenue to the B-Line Trail

Kirkwood Avenue from Indiana Avenue to the B-Line Trail.

Sixth Street from Indiana Avenue to the B-Line Trail.

Indiana Avenue from Fourth Street to Seventh Street.

Dunn Street from Fourth Street to Seventh Street.

Grant Street from Fourth Street to Seventh Street.

Lincoln Street from Fourth Street to Seventh Street.

Washington Street from Fourth Street to Seventh Street.

Walnut Street from Fourth Street to Seventh Street.

College Avenue from Fourth Street to Seventh Street

3. Ordinance 19-09 shall be amended by adding a new SECTION 10 and by re-numbering subsequent sections. The new section shall read as follows:

SECTION 10. Subsection 15.56.025(a)(7) shall be revised to expand the dismount zone such that the entire provision shall read as follows:

(7) Operating a coaster on the sidewalks and within the crosswalks along the following streets is hereby declared a public nuisance and is therefore prohibited. A person operating a coaster in these areas shall dismount their coaster. Signage shall be in place prior to enforcement of this requirement and shall, when practicable, be in the form of markings on the sidewalk.

STREETS:

Fourth Street from Indiana Avenue to the B-Line Trail

Kirkwood Avenue from Indiana Avenue to the B-Line Trail.

Sixth Street from Indiana Avenue to the B-Line Trail.

Indiana Avenue from Fourth Street to Seventh Street.

Dunn Street from Fourth Street to Seventh Street.

Grant Street from Fourth Street to Seventh Street.

Lincoln Street from Fourth Street to Seventh Street.

Washington Street from Fourth Street to Seventh Street.

Walnut Street from Fourth Street to Seventh Street.

College Avenue from Fourth Street to Seventh Street

PARKS:

Waldron, Hill and Buskirk Park Fountain Circle

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and extends the boundaries of the dismount zone for scooters, bicycles, and coasters. *See* attached map.

Note that this amendment and associated map were revised subsequent to discussion on 17 April 2019 to add north-south streets (Indiana Ave., Dunn St., Grant St., Lincoln St., and Washington St.) to the dismount zone.

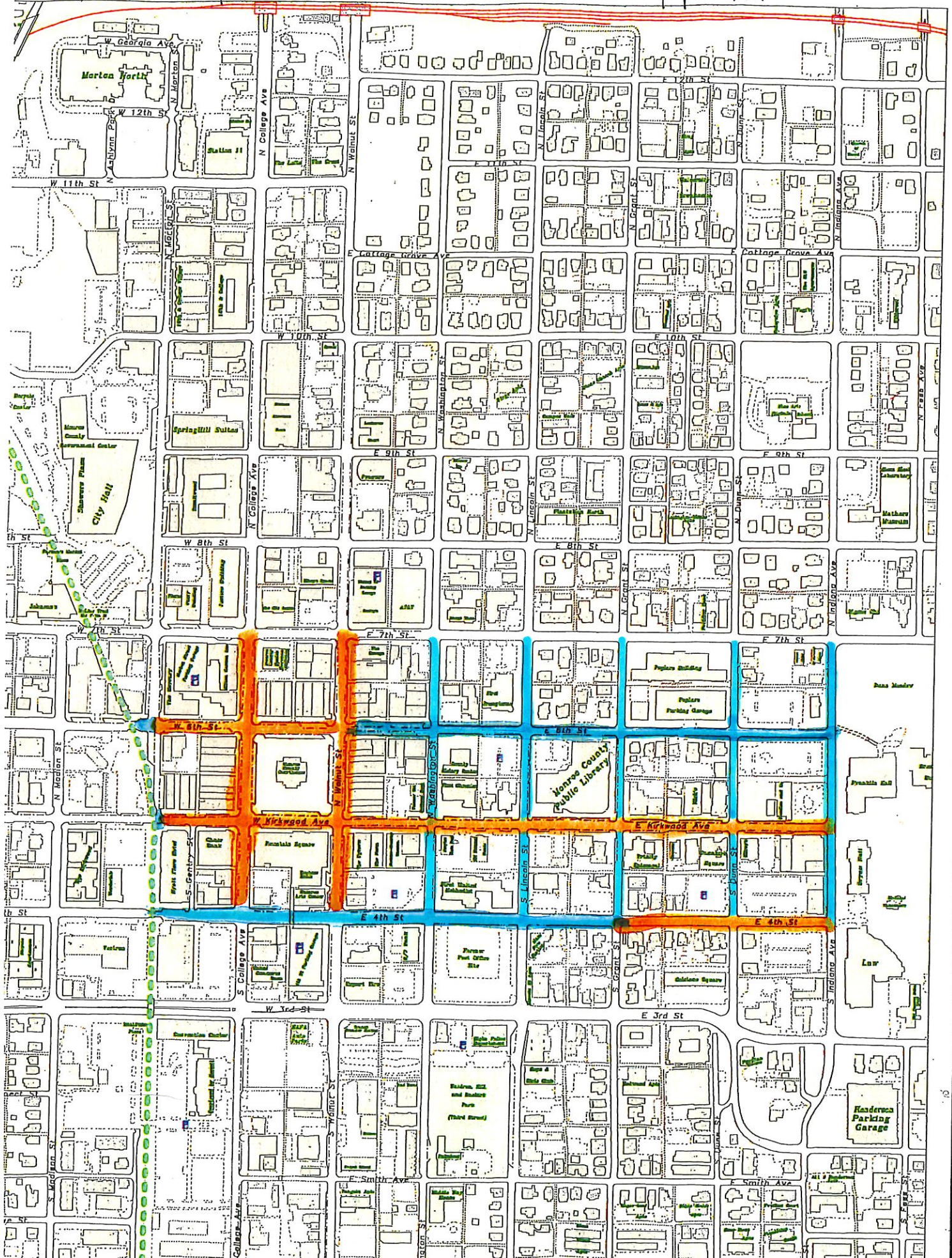
04/10/19 Committee Action: None

04/17/19 Regular Session Action: *Discussed, but no vote taken.*

05/01/19 Regular Session Action: ADOPTED, 6-2 (Ruff, Chopra)-1 (Volan)

(May 1, 2019)

proposed extinction



***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 07a (revised subsequent to discussion)
Submitted By: Councilmember Piedmont-Smith
Date: 17 April 2019 (discussed); 01 May 2019 (introduced in revised form)

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3 to delete subsection §15.58.050(d) in its entirety and to replace it with the following:

(d) Motorized scooters shall not be permitted to be parked on any public sidewalk.

2. Ordinance 19-09 shall be amended by revising the next-to-last sentence of the synopsis to eliminate the reference to permissible parking at bike racks within dismount zones, such that the sentence shall read as follows:

However, motorized scooter parking is prohibited on any public sidewalk; instead, proximate parking is provided at painted boxes.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and prohibits motorized scooter parking on any public sidewalk. This amendment is made in the interest of maximizing sidewalk accessibility. Notably, motorized scooters may be parked proximate to sidewalks at designated painted boxes on the street.

Note that this amendment was revised subsequent to discussion on 17 April 2019 to extend the prohibition on scooter parking to all public sidewalks, not just those sidewalks in the dismount zone.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: *Discussed, but no vote taken.*
05/01/19 Regular Session Action: ADOPTED, 8-0-1 (Sims)

(May 1, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 08a (revised subsequent to discussion)
Submitted By: Councilmember Piedmont-Smith
Date: 17 April 2019 (discussed); 01 May 2019 (introduced in revised form)

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3, §15.58.050 (c) to replace the words “painted boxes” with “corrals” and to make clear that such corrals shall be designed to prevent scooters and bicycles from falling outside of the corral borders. The entire subsection shall read as follows:

(c) Motorized scooters may be parked on the street within corrals that are specifically designated for scooters and bicycles. Corrals shall be designed to prevent scooters and bicycles from falling outside of the boundaries of the corral.

2. Ordinance 19-09 shall be amended by modifying SECTION 3, §15.58.050 (g) to replace the words “painted boxes” with “corrals.”

3. Ordinance 19-09 shall be amended by revising the next-to-last sentence of the synopsis to delete the words “painted boxes” and replace those words with “corrals.”

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and replaces the words “painted boxes” with “corrals” to describe the on-street parking areas designated for scooter and bicycle parking. The amendment also provides that corrals shall be designed to prevent scooters and bicycles from falling outside of the boundaries of the corral.

Note that this amendment was revised subsequent to discussion on 17 April 2019 to switch the term “scooter corral” with the more general term “corral” as these designated parking spaces are intended for use by both scooters and bicycles. The amendment also clarifies that the corrals shall be designed to prevent both scooters and bicycles from falling outside of its designated boundaries.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: *Discussed, but no vote taken.*
05/01/19 Regular Session Action: ADOPTED, 5-4

(May 1, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 09
Submitted By: Councilmember Sandberg on behalf of the City Administration
Date: 17 April 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3 to delete subsection (b) of 15.58.110 “Data sharing” in its entirety and to re-letter the remaining subsections of 15.58.110 accordingly.
2. Ordinance 19-09 shall be amended by modifying SECTION 3 to add the following sentence to the end of subsection (a)(3) of 15.58.110 “Data sharing”:

However, the City shall not publish or make available to the public at large any information that qualifies as a trade secret under Indiana Code § 24-2-3, et. seq. (the Uniform Trade Secrets Act) or any information that qualifies as personally identifiable information as that term is defined at 2 CFR § 200.79.

Synopsis

This amendment is sponsored by Councilmember Sandberg and removes a requirement for shared-use motorized scooter operators to provide periodic monthly reports in addition to real-time reporting in MDS format. In addition, this amendment clarifies that while the City may publish real-time information received from shared-use motorized scooter operators, it may not publish or make available to the public at large any information that qualifies as a trade secret or as personally identifiable information.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: Passed 8-0-0

(April 17, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 10
Submitted By: Councilmember Piedmont-Smith on behalf of the City Administration
Date: 17 April 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3 to add the following sentence to the end of subsection (a) of 15.58.130 "Affordability and accessibility":

However, a shared-use motorized scooter operator may utilize a different method for computing a price discount provided that, in every instance, the overall cost of the ride to the eligible user is fifty percent (50%) or less than the cost of the ride that would have been charged to a user who was not eligible for the discount.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and allows shared-use motorized scooter operators to provide discounts other than a straight 50% price discount as long as the consequence of the discount is at least equivalent to a 50% straight price discount.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: PASSED, 8-0-0

(April 17, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 11
Submitted By: Councilmember Piedmont-Smith
Date: 17 April 2019

Proposed Amendment:

1. Ordinance 19-09, first “Whereas” clause shall be amended to correct the date of the arrival of e- scooters in Bloomington such that the first “Whereas” clause shall read as follows:

WHEREAS, during September 2018, with little or no notice, shared-use motorized scooters (or e-scooters) arrived in Bloomington; and

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and corrects the date of the arrival of scooters in Bloomington in the first “Whereas” clause of the ordinance.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: PASSED, 8-0-0

(April 17, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 12
Submitted By: Councilmembers Sandberg, Granger, and Ruff
Date: 01 May 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3 to delete subsection (i) of 15.58.090, regarding the permissible hours of deployment. Subsection 15.58.090(i) shall be deleted in its entirety and the subsequent subsections shall be re-lettered.

Synopsis

This amendment is sponsored by Councilmembers Sandberg, Granger, and Ruff and deletes a subsection of the ordinance providing that shared-use motorized scooters shall not be made available to the public between the hours of 10:01 PM and 5:59 AM. Sponsors are eliminating this provision in the interest of making shared-use motorized scooters available to low-income residents working service-industry jobs, second- or third-shift jobs, or other jobs with non-traditional hours.

Note: Synopsis was corrected to clarify the period of the prohibition on July 26, 2019.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: None
05/01/19 Regular Session Action: ADOPTED, 6-1(Rollo)-1 (Sturbaum)

(July 26, 2019)

Amendments to be Introduced

- Am 13 -- Piedmont-Smith
- Am 14 -- Piedmont-Smith
- Am 15 -- Piedmont-Smith
- Am 16 -- Piedmont-Smith
- Am 17 -- Piedmont-Smith

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 13
Submitted By: Councilmember Piedmont-Smith
Date: 05 July 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3, 15.58.060(b)(2) and 15.58.060(b)(3) such that those provisions shall read as follows:

(b) (2) A person who is operating a motorized scooter on a sidewalk and who is passing a pedestrian traveling on the same facility shall stop, dismount and pass on foot, or exit the facility.

(b)(3) A person operating a motorized scooter upon a multiuse path, multiuse trail, or within a crosswalk, before overtaking a person with a visual impairment who is carrying a white cane or who is guided by a service animal, shall dismount and pass on foot, if necessary to avoid startling, inconveniencing or colliding with the person.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and would require all persons operating a scooter on a sidewalk to dismount when passing a pedestrian and would require persons operating a scooter on a multiuse path, multiuse trail, or within a crosswalk to dismount before overtaking a person with a visual impairment.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: None
05/01/19 Regular Session Action: None
05/15/19 Regular Session Action: None
07/31/19 Regular Session Action: *Pending*

(July 26, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 14
Submitted By: Councilmember Piedmont-Smith
Date: 05 July 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3, 15.58.120(e) such that the provision shall read as follows:

(e) Shared-use motorized scooter operators shall make helmets available to shared-use motorized scooter users. Such helmets shall be available year-round, at no charge, and shall be made available at convenient locations throughout the community, with such locations determined by areas of highest shared-use motorized scooter usage.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and modifies the language requiring that scooter companies make helmets available to users by shifting the requirement that helmets be made available “at a local location” to a requirement that helmets must be available “at convenient locations throughout the community, with such locations determined by areas of highest shared-use motorized scooter usage.”

04/10/19 Committee Action: None
04/17/19 Regular Session Action: None
05/01/19 Regular Session Action: None
05/15/19 Regular Session Action: None
07/31/19 Regular Session Action: *Pending*

(July 26, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 15
Submitted By: Councilmember Piedmont-Smith
Date: 05 July 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3, Subsections 15.58.050 (b) and 15.58.050 (d) such that those provisions shall read as follows:

(b) Motorized scooters may be parked at any bicycle parking facility located in the public right-of-way.

(d) Motorized scooter parking is permitted on sidewalks with the following limitations.

- (1) Motorized scooters shall be parked only at bicycle parking facilities.
- (2) Motorized scooters shall be parked in an upright position.
- (3) Motorized scooters shall be parked so as to leave a clear straight pathway at least fifty-four (54) inches wide.
- (4) Under no circumstance shall motorized scooters be parked so as to obstruct:
 - (A) Any parking space, including any parking access aisle;
 - (B) Any loading zone;
 - (C) Any curb ramp;
 - (D) Any public transportation infrastructure including, but not limited to, bus shelters, bus stop signs, or passenger waiting areas;
 - (E) Any driveway;
 - (F) Any entrance or exit from any building;
 - (G) Any fire hydrant, emergency call box, or other emergency facility;
 - (H) Any parking meter;
 - (I) Any utility pole or utility box;

- (J) Any street furniture or newsrack;
- (K) Any commercial window display; or
- (L) A crosswalk button.

(5) Under no circumstance shall motorized scooters be parked so as to impede accessibility under the Americans with Disabilities Act (ADA), so as to impede access to accessible parking zones, or so as to impede access to parking spaces designated for persons with disabilities.

2. Ordinance 19-09 shall be amended by modifying the seventh “Whereas” clause such that the clause reads as follows:

WHEREAS, it is necessary to ensure that shared-use motorized scooters are parked in a manner that does not clutter the public right-of-way or obstruct access for pedestrians, cars, bicycles, or buses. For that reason, the Council requests that the Administration install additional bicycle parking facilities in the roadway as space allows and install additional bike hoops parallel to the sidewalk as space allows, in the interest of effectuating the intent of this ordinance; and

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and restricts scooter parking on sidewalks only to bicycle parking facilities, including bike racks and bike hoops, while still emphasizing accessibility of the public right-of-way for all users. The amendment shifts the term “bike rack” to “bicycle parking facility” to more accurately reflect the term used in other places of the Bloomington Municipal Code, namely Title 20. The amendment also modifies the seventh “Whereas” clause to request that the Administration install additional bicycle facilities in the street and additional bike hoops parallel to the sidewalk in the interest of effectuating the intent of this change.

04/10/19 Committee Action:	None
04/17/19 Regular Session Action:	None
05/01/19 Regular Session Action:	None
05/15/19 Regular Session Action:	None
07/31/19 Regular Session Action:	<i>Pending</i>

July 26, 2019

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 16
Submitted By: Councilmember Piedmont-Smith
Date: 05 July 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 3, 15.58.150(c) such that the provision shall read as follows:

(c) The shared-use motorized scooter operator has violated any federal law including, but not limited to, the Americans with Disabilities Act (ADA) or federal regulation; any state law or regulation; or any local law or regulation.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith at the request of advocates from the accessibility community. The amendment makes clear that the Americans with Disabilities Act (ADA) is one of the federal laws whose violation would trigger revocation of a shared-use motorized scooter operator's license.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: None
05/01/19 Regular Session Action: None
05/15/19 Regular Session Action: None
07/31/19 Regular Session Action: *Pending*

(July 26, 2019)

***** Amendment Form *****

Ordinance #: 19-09
Amendment #: 17
Submitted By: Councilmember Piedmont-Smith
Date: 08 July 2019

Proposed Amendment:

1. Ordinance 19-09 shall be amended by modifying SECTION 10, the effective date of the ordinance, such that the Section shall read as follows:

SECTION 10. This ordinance shall be in full force and effect beginning **September 1, 2019**.

Note: This date may need to be pushed out further depending on how soon the Administration is positioned to implement the provisions of the ordinance, as amended.

Synopsis

This amendment is sponsored by Councilmember Piedmont-Smith and shifts the effective date of the ordinance from July 1, 2019 to **September 1, 2019**.

Note: The September date may need to be changed depending on the timeframe for implementation.

04/10/19 Committee Action: None
04/17/19 Regular Session Action: None
05/01/19 Regular Session Action: None
05/15/19 Regular Session Action: None
07/31/19 Regular Session Action: *Pending*

(July 26, 2019)

ORDINANCE 19-12

**TO AMEND THE CITY OF BLOOMINGTON ZONING MAPS BY REZONING 3.85
ACRES OF PROPERTY FROM COMMERCIAL ARTERIAL (CA) TO PLANNED
UNIT DEVELOPMENT AND TO APPROVE A DISTRICT ORDINANCE AND
PRELIMINARY PLAN**

**- Re: 1800 N. Walnut Street
(CDG Acquisitions, LLC, Petitioner)**

WHEREAS, Ordinance 06-24, which repealed and replaced Title 20 of the Bloomington Municipal Code entitled, "Zoning", including the incorporated zoning maps, and incorporated Title 19 of the Bloomington Municipal Code, entitled "Subdivisions", went into effect on February 12, 2007; and

WHEREAS, the Plan Commission has considered this case, PUD-17-19, and recommended that the petitioner, CDG Acquisitions LLC, be granted an approval to rezone 3.85 acres from Commercial Arterial (CA) to Planned Unit Development (PUD) and to approve a PUD District Ordinance and preliminary plan to allow a mixed-use development. The Plan Commission thereby requests that the Common Council consider this petition;

WHEREAS, the Plan Commission therefore requests that the Common Council consider this petition;

NOW, THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. Through the authority of IC 36-7-4 and pursuant to Chapter 20.04 of the Bloomington Municipal Code, the district ordinance and preliminary plan shall be approved for the PUD on the property located at 1800 N. Walnut Street. The property is further described as follows:

A PART OF THE SOUTHWEST QUARTER OF SECTION 28, TOWNSHIP 9 NORTH, RANGE 1 WEST, MONROE COUNTY, INDIANA DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT THAT IS 1,183.00 FEET NORTH 01 DEGREES 26 MINUTES WEST AND 22.00 FEET SOUTH 88 DEGREES 49 MINUTES WEST OF THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SAID SECTION 28, SAID POINT OF BEGINNING BEING ON THE WEST RIGHT-OF-WAY OF DUNN STREET; THENCE SOUTH 88 DEGREES 49 MINUTES WEST OVER AND ALONG THE NORTH LINE OF MILLER COURTS ADDITION FOR A DISTANCE OF 929.73 FEET; THENCE NORTH 00 DEGREES 03 MINUTES 19 SECONDS EAST FOR A DISTANCE OF 375.70 FEET TO A ½ INCH IRON ROD (FOUND) AT THE REAL POINT OF BEGINNING; THENCE SOUTH 88 DEGREES 32 MINUTES WEST FOR A DISTANCE OF 644.29 FEET TO A ¼ INCH PIPE (FOUND) ON THE EAST RIGHT-OF-WAY OF INDIANA STATE ROAD 37 (NORTH WALNUT STREET); THENCE NORTH 10 DEGREES 21 MINUTES 53 SECONDS WEST OVER AND ALONG SAID EAST RIGHT-OF- WAY FOR A DISTANCE OF 323.12 FEET TO A 5/8 INCH IRON ROD (FOUND); THENCE LEAVING SAID RIGHT-OF-WAY NORTH 88 DEGREES 27 MINUTES EAST FOR A DISTANCE OF 390.38 FEET TO A 5/8 INCH IRON ROD (SET); THENCE SOUTH 01 DEGREES 34 MINUTES EAST FOR A DISTANCE OF 177.74 FEET TO A 5/8 INCH IRON ROD (SET); THENCE NORTH 88 DEGREES 25 MINUTES 41 SECONDS EAST FOR A DISTANCE OF 358.99 FEET TO A ½ INCH IRON ROD THENCE SOUTH 00 DEGREES 00 MINUTES 35 SECONDS EAST FOR A DISTANCE OF 142.77 FEET TO A 5/8 INCH IRON ROD (SET); THENCE SOUTH 88 DEGREES 32 MINUTES 04 SECONDS WEST FOR A DISTANCE OF 51.77 FEET TO THE REAL POINT OF BEGINNING.

Subject to any and all easements, agreements, and restrictions of record.

SECTION 2. This District Ordinance and the Preliminary Plan shall be approved as attached hereto and made a part thereof.

SECTION 3. If any section, sentence or provision of this ordinance, or the application thereof to any person or circumstance shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions, or applications of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

SECTION 4. This ordinance shall be in full force and effect from and after its passage by the Common Council and approval by the Mayor.

PASSED AND ADOPTED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2019.

DAVE ROLLO, President
Bloomington Common Council

ATTEST:

NICOLE BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2019.

NICOLE BOLDEN, Clerk
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2019.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

Ordinance 19-12 would rezone 3.85 acres from Commercial Arterial (CA) to Planned Unit Development (PUD) and approve the associated District Ordinance and Preliminary Plan to allow the construction of a mixed-use development.

****ORDINANCE CERTIFICATION****

In accordance with IC 36-7-4-605 I hereby certify that the attached Ordinance Number 19-12 is a true and complete copy of Plan Commission Case Number PUD-17-19 which was given a recommendation of approval by a vote of 7 Ayes, 0 Nays, and 0 Abstentions by the Bloomington City Plan Commission at a public hearing held on June 10, 2019.

Date: June 17, 2019



Terri Porter, Secretary
Plan Commission

Received by the Common Council Office this 17th day of June, 2019.



Nicole Bolden, City Clerk

Appropriation
Ordinance #

Fiscal Impact
Statement
Ordinance #

Resolution #

Type of Legislation:

Appropriation
Budget Transfer
Salary Change

End of Program
New Program
Bonding

Penal Ordinance
Grant Approval
Administrative
Change
Short-Term Borrowing
Other

Zoning Change
New Fees

Investments
Annexation

If the legislation directly affects City funds, the following must be completed by the City Controller:

Cause of Request:

Planned Expenditure
Unforeseen Need

Emergency
Other

Funds Affected by Request:

Fund(s) Affected

Fund Balance as of January 1

\$

\$

Revenue to Date

\$

\$

Revenue Expected for Rest of year

\$

\$

Appropriations to Date

\$

\$

Unappropriated Balance

\$

\$

Effect of Proposed Legislation (+/-
)

\$

\$

Projected Balance

\$

\$

Signature of Controller

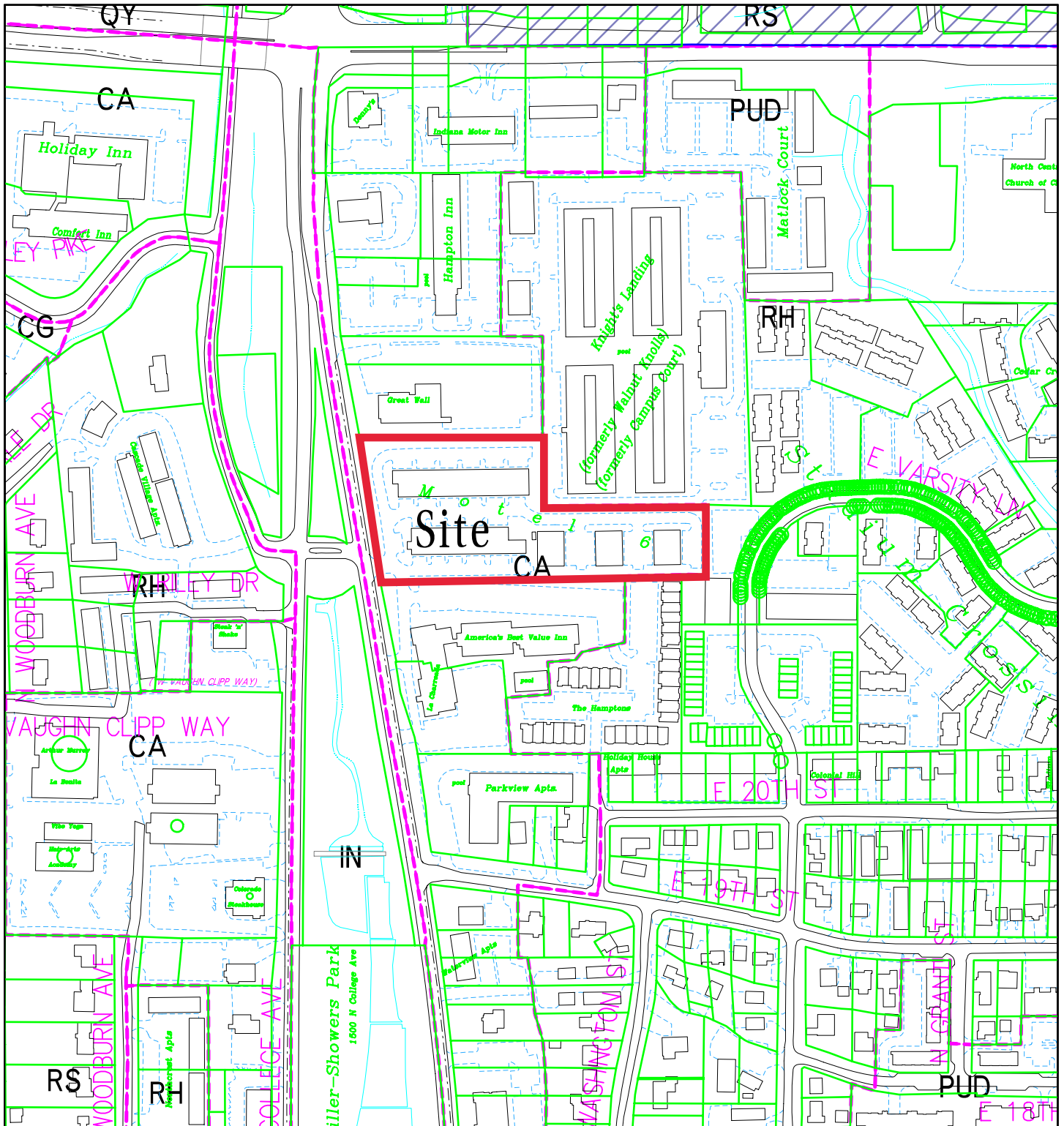
Will the legislation have a major impact on existing City appropriations, fiscal liability or revenues?

Yes

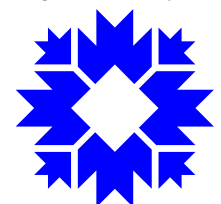
No

If the legislation will not have a major fiscal impact, explain briefly the reason for your conclusion.

If the legislation will have a major fiscal impact, explain briefly what the effect on City costs and revenues will be and include factors which could lead to significant additional expenditures in the future. Be as specific as possible. (Continue on second sheet if necessary.)



City of Bloomington
Planning & Transportation

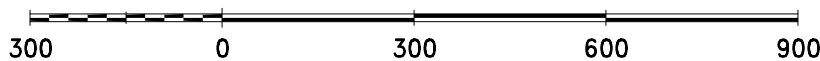


Scale: 1" = 300'

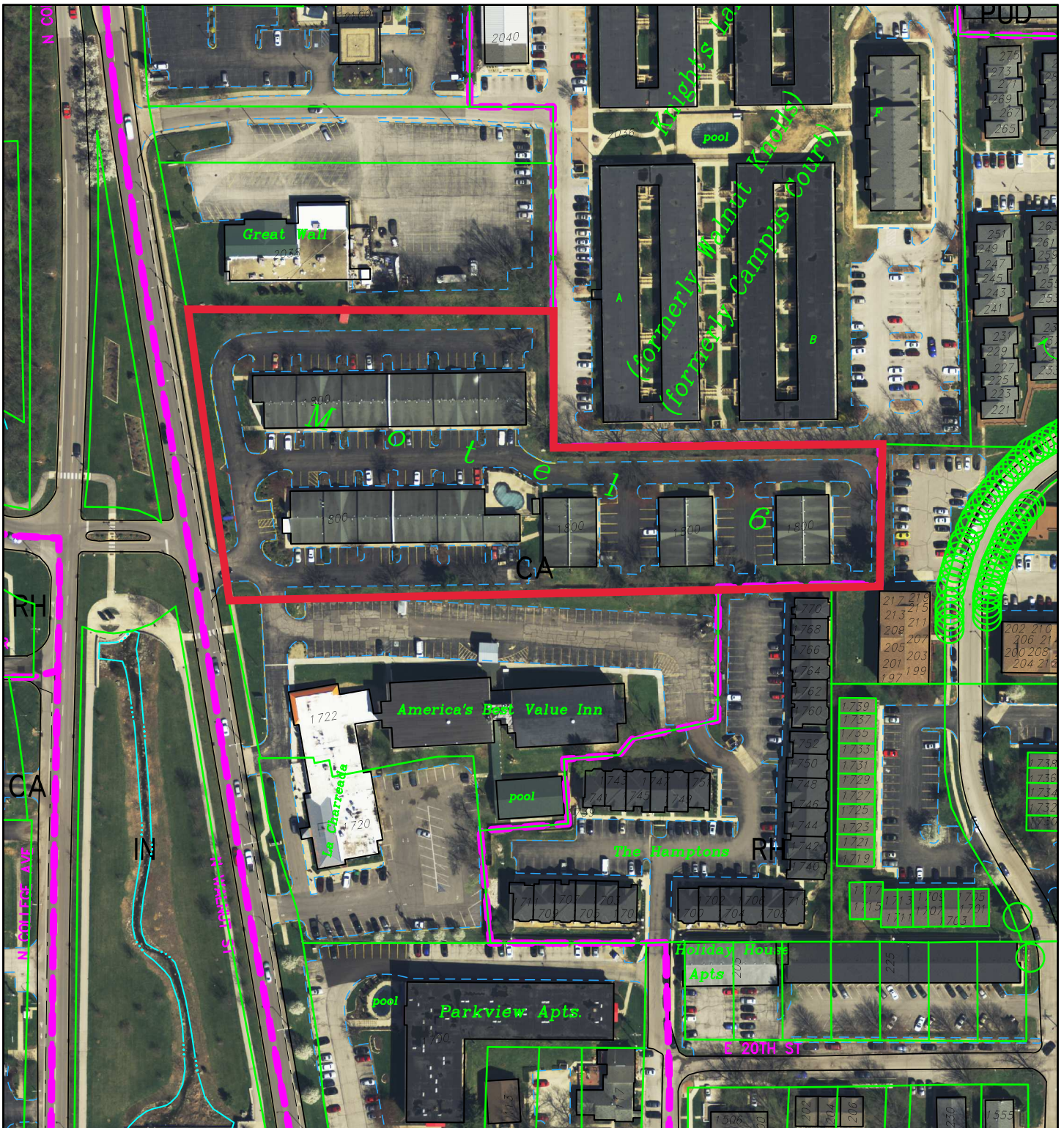
N

By: greulice

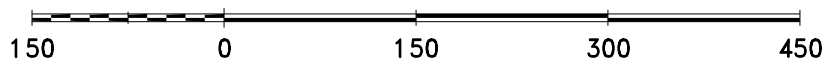
22 Mar 19



For reference only; map information NOT warranted.

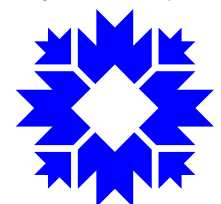


By: greulice
22 Mar 19



For reference only; map information NOT warranted.

City of Bloomington
Planning & Transportation



Scale: 1" = 150'

This is not a zoning map. The zoning map will be included in the Unified Development Ordinance to follow from this Comprehensive Master Plan.



Interdepartmental Memo

To: Members of the Common Council
From: Eric Greulich, Senior Zoning Planner
Subject: PUD-17-19
Date: June 17, 2019

Attached are the staff report, maps, petitioner's statement, and exhibits which pertain to Plan Commission case PUD-17-19. The Plan Commission heard this petition at the June 10, 2019 hearing and voted 7-0 to send this petition to the Common Council with a positive recommendation.

The Plan Commission report for that hearing is included below.

REQUEST: The petitioner is requesting a rezone from Commercial Arterial (CA) to Planned Unit Development (PUD) and approval of a Preliminary Plan and District Ordinance. Also requested is a waiver from the required 5 acre minimum for a Planned Unit Development and delegation of final plan approval to staff.

BACKGROUND:

Area:	3.85 acres
Current Zoning:	Commercial Arterial
Comp Plan Designation:	Neighborhood Residential
Existing Land Use:	Motel
Proposed Land Use:	Dwelling, Multi-Family / Commercial
Surrounding Uses:	North – Commercial/Dwelling, Multi-Family West – Miller Showers Park East – Multi-Family, Dwelling South – Hotel/Multi-Family, Dwelling

REPORT: The property is located at 1800 N. Walnut Street and is zoned Commercial Arterial (CA). The 3.85 acre property is currently developed with a series of buildings for a motel. Surrounding zoning includes Commercial Arterial (CA) and Residential High Density Multifamily (RH) to the north and south, Residential High Density Multifamily (RH) to the east, and Institutional (IN) to the west. The surrounding properties have been developed with a mix of high density multi-family residences and commercial spaces. This property fronts directly on N. Walnut Street. There are no environmental constraints on this property.

The petitioner proposes to create a Planned Unit Development in order to construct 2 buildings that would range in height from 5 to 6 stories. The proposal includes a minimum of 6,000 square feet of commercial space, 6,000 square feet of amenity space, and apartments on the upper floors and portions of the ground floors. The multifamily portion of the proposal includes a mix of studio units, one-bedroom units, two-bedroom units, and four-bedroom units for a possible total of 270 units and 820 bedrooms. The overall density is proposed at a maximum of 77 D.U.Es per acre. All of the apartments would be fully furnished. The building will also contain a structured parking garage accessed from the south side of the building with 410 parking spaces. There would also be 8 parking spaces provided along the western front for the commercial space. The proposed parking ratio is a minimum 0.50 spaces per bedroom and a maximum of 0.75 spaces per bedroom.

In response to comments from the Plan Commission at the first hearing, the petitioner revised the proposed entrance into the site from Walnut Street to remove the proposed roundabout, provided a pedestrian crossing to the Varsity Villas site to the east, increased the number of proposed bicycle parking spaces to one space per every 4 bedrooms, and proposed an off-site sidewalk extension to complete a missing sidewalk segment along Walnut Street south of this location to connect to 19th Street. A revised district ordinance was also been submitted to clean up some of the proposed standards.

The petitioner has proposed a contribution to be placed in the Housing Development Fund to be used to provide affordable housing units elsewhere within the City. Details of this have been outlined in the attached commitment, this would be incorporated into a zoning commitment for the property. As a result of the changes listed above, the Plan Commission voted to forward this petition with a favorable recommendation since they believed this petition will adequately achieve the goals of the Comprehensive Plan and benefit the community as a whole. The incorporation of affordable housing with this project has been an important aspect.

COMPREHENSIVE PLAN: This property is designated as *Urban Corridor* and *Neighborhood Residential* and is also within an area identified as *Gateway North*. The Comprehensive Plan identifies several characteristics and provides land use guidance for the *Neighborhood Residential* district. However, most of the area within this land use district involves the typical suburban residential neighborhoods, so a lot of the guidance is aimed at those areas and does not precisely fit the proposed redevelopment of this particular location. This petition site, as well as a majority of the area surrounding it, is unique in that it has been built out with a mix of commercial and high-density, student oriented housing rather than the typical suburban neighborhoods found elsewhere in the City within this land use designation. For the *Urban Corridor* district the Comprehensive Plan states that this district is designed to transform strip retail and commercial corridors along major roadways into a more urban mixed-use district. It identifies *Urban Corridor* as having excellent access to major roadways, utilities, and other services like transit, fire, and police services. The Comprehensive Plan identifies this area as built out with single story buildings and large surface parking lots that need to be reimaged in a more dense, mixed-use district. The Comprehensive Plan does provide guidance regarding the intent within the *Neighborhood Residential* and *Urban Corridor* areas relative to this proposed development as well as offer guidance for land use approvals:

- **Policy 4.4.4:** Until such time as a reasonable balance of different housing types is achieved in the Downtown and nearby areas, strongly discourage new student-oriented housing developments in these areas.
- **Policy 5.3.4:** Redirect new student-oriented housing developments away from the Downtown and nearby areas, and toward more appropriate locations closely proximate to the IU campus that already contain a relatively high percentage of student-oriented housing units, are within easy walking distance to the campus, and have direct access to university-provided parking as well as the university transit system.
- Optimize street, bicycle, and pedestrian connectivity to adjacent neighborhoods and other 20-minute walking destinations.
- In new development or redevelopment projects, utilities should be placed underground if feasible and located so as to minimize potential conflicts with trees and other landscaping features.

- Buildings should be developed with minimal street setbacks, with parking located behind the building, and with an emphasis on minimizing pedestrian obstacles to accessing businesses.
- Development and redevelopment within the district is particularly suited to high-density residential and mixed residential/commercial use and taller building heights, with the possibility of three or four-story buildings.
- Access to public transit service is an important component of the Urban Corridor district.
- Affordable housing units are an important component of the Urban Corridor district.
- To increase pedestrian and transit accessibility, street cuts should be limited as much as possible to reduce interruptions of the streetscape, tree plots, and sidewalks.
- *Gateway North* - Due to its close proximity to Indiana University, the character of this Focus Area can support the diversification of housing types, including, but not limited to, multifamily residential and commercial uses — such as student housing and service facilities.
 - Emphasis should be on architectural and site design characteristics that establish Gateway North branding.
 - Streetscape development from a multimodal standpoint should be highly emphasized on the primary facades and walkways of new developments.

The development of this parcel will add mixed uses and student oriented development that is located outside of the Downtown, within 10 minutes to IU facilities, adjacent to nearby goods and services, and that is directly on a main commercial thoroughfare. The petitioner is providing several connections and off-site improvements to facilitate pedestrian access to the development. The Plan Commission feels that the proposed Preliminary Plan is consistent with the intent and development guidance of the Comprehensive Plan for this area.

PRELIMINARY PLAN:

Uses/Development Standards: The petitioner is proposing to utilize the Commercial Downtown (CD) zoning district for the development standards for this project, with some modifications. The proposed list of uses are those of the Commercial Arterial (CA) zoning district. The deviations from the CD district include requesting to allow first-floor residential uses, increased density, setbacks, building height, and signage. The petitioner has modified the proposed development standards to clean up ambiguity from the first hearing.

Residential Density: The maximum residential density allowed in the CA district is 15 units per acre, which is the densest by-right development allowed in the UDO outside of the downtown. The petitioner is proposing a maximum of 77 units per acre for the PUD. The petitioner is proposing a possible number of 270 units with 820 bedrooms. The Comprehensive Plan encourages higher density along the *Urban Corridor* areas when appropriately designed and calls for medium- to high-density multifamily residential in the *Neighborhood Residential*, but also. This specific area has been heavily developed with various commercial buildings, large hotels, and high density student oriented housing.

Height and Bulk: The petitioners are proposing two buildings that would vary from 5 to 6 stories, with a maximum proposed height of 85 feet. There is a substantial amount of elevation change across the property with 30' of grade change from the east side of the site to the west side. The petitioner has incorporated a varying amount of façade modulation, building heights, and different materials to visually break up the building massing and provide visual interest. The Plan

Commission found that the proposed massing is adequately mitigated by the proposed modulations and articulations. The petitioner has included massing models with the submitted elevations and floor plans to show the building massing in relation to surrounding buildings.

Parking, Streetscape, and Access: A minimum number of 410 structured parking spaces are proposed in a garage that would be located in the middle portion of the building. There would also be 8 parking spaces provided along the front for the 6,000 square foot commercial space. The proposed parking ratio for the residential component is a minimum 0.50 spaces per bedroom and a maximum of 0.75 spaces per bedroom.

The petitioner will be relocating the existing sidewalk along Walnut Street to provide a 5' wide tree plot. However, due to an existing underground gas line, street trees may not be possible in the tree plot. The petitioner is proposing to install a dense planting of shrubs within the tree plot instead and will plant street trees behind the sidewalk if they cannot fit in the tree plot, to provide shade.

There is one access point for this project that also is shared with the adjacent property to the south. The petitioner will be adjusting the location of the shared access point to align with the cross connection across College/Walnut to the west.

Bicycle Parking and Alternative Transportation: The development has 270 units and 820 proposed bedrooms. The UDO requires one bicycle parking space for every 6 bedrooms. Since the project is larger than 20,000 square feet, all bicycle parking spaces must be covered. The petitioner has modified their proposed bicycle parking number to increase the number of bicycle parking spaces provided to provide a minimum of one bicycle parking space for every 4 bedrooms, for a total of 205 covered spaces. One quarter of those spaces will need to be long-term storage. Four spaces will be needed in front of the commercial space.

Bloomington Transit has indicated that a bus shelter is desired at this location and should therefore be included with the site plan. Bloomington Transit has also indicated difficulties with managing space at bus stations throughout their service area and in order to alleviate congestion at bus stops, the petitioner must coordinate and contract with Bloomington Transit to provide any additional shuttle service that may be needed. The petition may not operate their own shuttle without Bloomington Transit agreement.

The petitioner has also committed to completing a segment of missing sidewalk south of this site. The segment of sidewalk would extend south along the property occupied by Parkview Apartments and would extend east along 19th Street. This would complete a sidewalk system along Walnut Street and extend it east to connect to existing sidewalks along 19th Street. This off-site improvement is not required, but is being done to facilitate pedestrian access to the site and to the IU bus stop by the Stadium.

Architecture/Materials: The petitioner has utilized the Commercial Downtown zoning district for architecture standards. The proposed building includes a wide range of materials including brick veneer, cementitious panels, cementitious lap siding, and board and batten panels. Modulation and articulation have been shown both horizontally and vertically along with different building heights. Two courtyard areas are provided in the eastern building that will feature pedestrian entrances.

Environmental Considerations: The petition site is currently developed with a motel use with

several buildings and parking areas. There are no known sensitive environmental features.

Housing Diversity: The petitioner has voluntarily committed to providing a contribution to the City's Housing Development Fund to assist the City in promoting diverse and affordable housing within the community. Their commitment is outlined in a letter included with this packet.

20.04.080(h) Planned Unit Development Considerations

In 20.04.080(h) The UDO outlines that in their consideration of a PUD District Ordinance and Preliminary Plan, the Plan Commission and Common Council shall consider as many of the following as may be relevant to the specific proposal. The following list shall not be construed as providing a prioritization of the items on the list. Each item shall be considered individually as it applies to the specific Planning Unit Development proposal.

- (1) The extent to which the proposed Preliminary Plan meets the requirements, standards, and stated purpose of Chapter 20.04: Planned Unit Development Districts.

Section 20.04.010 of the UDO, states that the purpose of the planned unit development (PUD) is to encourage flexibility in the development of land in order to promote its most appropriate use; to improve the design, character and quality of new developments; to encourage a harmonious and appropriate mixture of uses; to facilitate the adequate and economic provision of streets, utilities, and city services; to preserve the natural, environmental and scenic features of the site; to encourage and provide a mechanism for arranging improvements on sites so as to preserve desirable features; and to mitigate the problems which may be presented by specific site conditions. It is anticipated that planned unit developments will offer one or more of the following advantages:

- (a) Implement the guiding principles and land use policies of the Comprehensive Plan; specifically reflect the policies of the Comprehensive Plan specific to the neighborhood in which the planned unit development is to be located;
 - (b) Buffer land uses proposed for the PUD so as to minimize any adverse impact which new development may have on surrounding properties; additionally provide buffers and transitions of density within the PUD itself to distinguish between different land use areas;
 - (c) Enhance the appearance of neighborhoods by conserving areas of natural beauty, and natural green spaces;
 - (d) Counteract urban monotony and congestion on streets;
 - (e) Promote architecture that is compatible with the surroundings;
 - (f) Promote and protect the environmental integrity of the site and its surroundings and provide suitable design responses to the specific environmental constraints of the site and surrounding area; and
 - (g) Provide a public benefit that would not occur without deviation from the standards of the Unified Development Ordinance.
- (2) The extent to which the proposed Preliminary Plan departs from the Unified Development Ordinance provisions otherwise applicable to the subject property, including but not limited to, the density, dimension, bulk, use, required improvements, and construction and design standards and the reasons why such departures are or are not deemed to be in the public interest.

ADOPTED FINDING: The proposed deviations from the UDO that are outlined in the PUD District Ordinance are necessary to further the purpose of the PUD which is to provide an innovative building that is appropriately designed for this area. The Petitioner has attempted to address deviations related to increased building height through modulation, change in building height, and recessing sections of the building. These architectural elements also help break up the massing of the building that is a result of the increased density and building size.

- (3) The extent to which the Planned Unit Development meets the purposes of this Unified Development Ordinance, the Comprehensive Plan, and any other adopted planning objectives of the City. Any specific benefits shall be specifically cited.

ADOPTED FINDING: This petition does further many of the goals of the UDO and the Comprehensive Plan. Specifically it addresses the goals of diversifying the location of student oriented housing located outside of the Downtown, provides increased density around existing goods and services, as well as redevelops an underdeveloped property along a primary urban corridor. The proposed location is adjacent to goods and services, within walking distance to IU facilities, and is surrounded by student oriented housing. The buildings provides many features and amenities for the tenants, and the project provides a contribution toward the affordable housing needs of the community as well.

- (4) The physical design of the Planned Unit Development and the extent to which it:
- a. Makes adequate provision for public services;
 - b. Provides adequate control over vehicular traffic;
 - c. Provides for and protects designated common open space; and
 - d. Furthers the amenities of light and air, recreation and visual enjoyment.

ADOPTED FINDING: The PUD provides adequate public services by improving an existing misaligned drive cut across Walnut Street as well as provides an off-site improvement to construct a missing sidewalk segment south of this site to connect to 19th Street. There will be only 2 drive cuts on Walnut Street with this petition that will serve this property and the property to the south. Common open space has been provided on this site and there is also a public park immediately to the west of this property. Modulation has been provided throughout the building to provide light and air throughout the property and building.

- (5) The relationship and compatibility of the proposed Preliminary Plan to the adjacent properties and neighborhood, and whether the proposed Preliminary Plan would substantially interfere with the use or diminish the value of adjacent properties and neighborhoods.

ADOPTED FINDING: This site is surrounded by high density multifamily residences and commercial uses. While the density proposed on this site is higher than surrounding properties, this type of dense infill development is encouraged by the Comprehensive Plan when surrounded by appropriate infrastructure and goods and services. In addition, it is located immediately adjacent to several grocery stores and shopping areas, as well as is on a Bloomington Transit bus route. In addition, the petitioner has worked with the adjacent property owner to provide a pedestrian connection through

the Varsity Villas site to the east to improve pedestrian access to the IU bus stop at the Stadium.

- (6) The desirability of the proposed Preliminary Plan to the City's physical development, tax base and economic well-being.

ADOPTED FINDING: The provision of an estimated 820 bedrooms units and commercial space along a major corridor will increase the tax base to the City and provide a unique building along our Gateway. In addition, the construction of the building will benefit the local workforce and adjacent commercial businesses.

- (7) The proposal will not cause undue traffic congestion, and can be adequately served by existing or programmed public facilities and services.

ADOPTED FINDING: This site will be accessed directly from Walnut Street and no connections through adjacent residential neighborhoods will be required. Bloomington Transit provides bus service to this property. There are existing utilities to this area in place to support this development.

- (8) The proposal preserves significant ecological, natural, historical and architectural resources.

ADOPTED FINDING: There are no known significant ecological, natural, historical or architectural resources on this site.

- (9) The proposal will not be injurious to the public health, safety, and general welfare.

ADOPTED FINDING: At this time the Plan Commission did not identify any negative impacts to the public health, safety, or general welfare of the community.

- (10) The proposal is an effective and unified treatment of the development possibilities on the PUD site.

ADOPTED FINDING: The establishment of a PUD for this property allows a unique development that would not otherwise be accomplished within an existing zoning district and under the UDO guidelines. The creation of this PUD allows the necessary deviations from the UDO requirements to allow the construction of unique buildings in order to accomplish one of the goals of the Comprehensive Plan in terms of providing an appropriate location for student oriented housing that is not located within the Downtown.

CONCLUSION: The Plan Commission found that the proposed PUD offers a unique architectural design and a range of benefits and features for the tenants that will also benefit the community by providing student housing that is adjacent to other student housing in a location that is well served, along a major arterial road, and proximate to IU's campus. This petition accomplishes many of the goals of the Comprehensive Plan already outlined in this report, including providing high density student housing in an area that is appropriate and located outside of the Downtown. Several elements of the proposal satisfy the high design criteria outlined in the Comprehensive Plan and attention has been paid to facilitating pedestrian access to the site by providing off-site

improvements to increase pedestrian access. The unique architectural design will compliment this area and provide a high quality building along the Urban Corridor.

RECOMMENDATION: The Plan Commission voted 7-0 to forward this petition to the Common Council with a favorable recommendation and the following conditions:

1. Final development plan approval must be reviewed by the Plan Commission.
2. The petitioner must coordinate with Bloomington Transit on the incorporation of a shuttle pick-up and potential shuttle service.
3. The petitioner will provide the sidewalk connection along Walnut Street that connects to 19th Street.
4. A zoning commitment regarding the contribution to the Housing Development Fund must be recorded within 90 days of Council approval.
5. A bus shelter is required along this property frontage.

**BLOOMINGTON PLAN COMMISSION
STAFF REPORT
Location: 1800 N. Walnut St**

**CASE #: PUD-17-19
DATE: June 10, 2019**

PETITIONER: CDG Acquisitions, LLC
7711 Bonhomme Ave., St. Louis, MO

CONSULTANTS: Smith Brehob & Associates, Inc.
453 S. Clarizz Blvd., Bloomington

REQUEST: The petitioner is requesting a rezone from Commercial Arterial (CA) to Planned Unit Development (PUD) and approval of a Preliminary Plan and District Ordinance. Also requested is a waiver from the required 5 acre minimum for a Planned Unit Development and delegation of final plan approval to staff.

BACKGROUND:

Area: 3.85 acres
Current Zoning: Commercial Arterial
Comp Plan Designation: Neighborhood Residential
Existing Land Use: Motel
Proposed Land Use: Dwelling, Multi-Family / Commercial
Surrounding Uses: North – Commercial/Dwelling, Multi-Family
West – Miller Showers Park
East – Multi-Family, Dwelling
South – Hotel/Multi-Family, Dwelling

CHANGES SINCE SECOND HEARING: At the first hearing staff, the Plan Commission discussed numerous potential issues including-

- Is the height and massing appropriate for this area?
- Have enough environmentally sustainable development practices been included?
- Is a higher bicycle parking standard appropriate?
- Has enough information been presented for the Plan Commission to feel comfortable delegating final plan approval to staff?

Since the first hearing, the petitioner has revised the proposed entrance into the site from Walnut Street to remove the proposed roundabout, provided a pedestrian crossing to the Varsity Villas site to the east, increased the number of proposed bicycle parking spaces to one space per every 4 bedrooms, and proposed an off-site sidewalk extension to complete a missing sidewalk segment along Walnut Street south of this location to connect to 19th Street. A revised district ordinance has also been submitted to clean up some of the proposed standards.

The petitioner has proposed a contribution to be placed in the Housing Development Fund to be used to provide affordable housing units elsewhere within the City. Details of this have been outlined in the attached commitment, this would be incorporated into a zoning commitment for the property. As a result of the changes listed above, the Department believes this petition will

adequately achieve the goals of the Comprehensive Plan and benefit the community as a whole. The incorporation of affordable housing with this project has been an important aspect.

REPORT: The property is located at 1800 N. Walnut Street and is zoned Commercial Arterial (CA). The 3.85 acre property is currently developed with a series of buildings for a motel. Surrounding zoning includes Commercial Arterial (CA) and Residential High Density Multifamily (RH) to the north and south, Residential High Density Multifamily (RH) to the east, and Institutional (IN) to the west. The surrounding properties have been developed with a mix of high density multi-family residences and commercial spaces. This property fronts directly on N. Walnut Street. There are no environmental constraints on this property.

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Environmental Considerations: The petition site is currently developed with a motel use with several buildings and parking areas. There are no known sensitive environmental features.

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20.04.080(h) Planned Unit Development Considerations

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- (g) Provide a public benefit that would not occur without deviation from the standards of the Unified Development Ordinance.
- (2) The extent to which the proposed Preliminary Plan departs from the Unified Development Ordinance provisions otherwise applicable to the subject property, including but not limited to, the density, dimension, bulk, use, required improvements, and construction and design standards and the reasons why such departures are or are not deemed to be in the public interest.

PROPOSED FINDING: The proposed deviations from the UDO that are outlined in the PUD District Ordinance are necessary to further the purpose of the PUD which is to provide an innovative building that is appropriately designed for this area. The Petitioner has attempted to address deviations related to increased building height through modulation, change in building height, and recessing sections of the building. These architectural elements also help break up the massing of the building that is a result of the increased density and building size.

- (3) The extent to which the Planned Unit Development meets the purposes of this Unified Development Ordinance, the Comprehensive Plan, and any other adopted planning objectives of the City. Any specific benefits shall be specifically cited.

PROPOSED FINDING: This petition does further many of the goals of the UDO and the Comprehensive Plan. Specifically it addresses the goals of diversifying the location of student oriented housing located outside of the Downtown, provides increased density around existing goods and services, as well as redevelops an underdeveloped property along a primary urban corridor. The proposed location is adjacent to goods and services, within walking distance to IU facilities, and is surrounded by student oriented housing. The buildings provides many features and amenities for the tenants, and the project provides a contribution toward the affordable housing needs of the community as well.

- (4) The physical design of the Planned Unit Development and the extent to which it:
 - a. Makes adequate provision for public services;
 - b. Provides adequate control over vehicular traffic;
 - c. Provides for and protects designated common open space; and
 - d. Furthers the amenities of light and air, recreation and visual enjoyment.

PROPOSED FINDING: The PUD provides adequate public services by improving an existing misaligned drive cut across Walnut Street as well as provides an off-site improvement to construct a missing sidewalk segment south of this site to connect to 19th Street. There will be only 2 drive cuts on Walnut Street with this petition that will serve this property and the property to the south. Common open space has been provided on this site and there is also a public park immediately to the west of this property. Modulation has been provided throughout the building to provide light and air throughout the property and building.

- (5) The relationship and compatibility of the proposed Preliminary Plan to the adjacent properties and neighborhood, and whether the proposed Preliminary Plan would substantially interfere with the use or diminish the value of adjacent properties and neighborhoods.

PROPOSED FINDING: This site is surrounded by high density multifamily residences and commercial uses. While the density proposed on this site is higher than surrounding properties, this type of dense infill development is encouraged by the Comprehensive Plan when surrounded by appropriate infrastructure and goods and services. In addition, it is located immediately adjacent to several grocery stores and shopping areas, as well as is on a Bloomington Transit bus route. In addition, the petitioner has worked with the adjacent property owner to provide a pedestrian connection through the Varsity Villas site to the east to improve pedestrian access to the IU bus stop at the Stadium.

- (6) The desirability of the proposed Preliminary Plan to the City's physical development, tax base and economic well-being.

PROPOSED FINDING: The provision of an estimated 820 bedrooms units and commercial space along a major corridor will increase the tax base to the City and provide a unique building along our Gateway. In addition, the construction of the building will benefit the local workforce and adjacent commercial businesses.

- (7) The proposal will not cause undue traffic congestion, and can be adequately served by existing or programmed public facilities and services.

PROPOSED FINDING: This site will be accessed directly from Walnut Street and no connections through adjacent residential neighborhoods will be required. Bloomington Transit provides bus service to this property. There are existing utilities to this area in place to support this development.

- (8) The proposal preserves significant ecological, natural, historical and architectural resources.

PROPOSED FINDING: There are no known significant ecological, natural, historical or architectural resources on this site.

- (9) The proposal will not be injurious to the public health, safety, and general welfare.

PROPOSED FINDING: At this time the Department does not identify any negative impacts to the public health, safety, or general welfare of the community.

- (10) The proposal is an effective and unified treatment of the development possibilities on the PUD site.

PROPOSED FINDING: The establishment of a PUD for this property allows a unique development that would not otherwise be accomplished within an existing zoning district and under the UDO guidelines. The creation of this PUD allows the necessary deviations from the UDO requirements to allow the construction of unique buildings in order to accomplish one of the goals of the Comprehensive Plan in terms of providing an appropriate location for student oriented housing that is not located within the Downtown.

CONCLUSION: The proposed PUD offers a unique architectural design and a range of benefits and features for the tenants that will also benefit the community by providing student housing that is adjacent to other student housing in a location that is well served, along a major arterial road, and proximate to IU's campus. This petition accomplishes many of the goals of the Comprehensive Plan already outlined in this report, including providing high density student housing in an area that is appropriate and located outside of the Downtown. Several elements of the proposal satisfy the high design criteria outlined in the Comprehensive Plan and attention has been paid to facilitating pedestrian access to the site by providing off-site improvements to increase pedestrian access. The unique architectural design will compliment this area and provide a high quality building along the Urban Corridor.

RECOMMENDATION: The Planning and Transportation Department recommends that the Plan Commission forward this petition to the Common Council with a favorable recommendation and the following conditions:

1. Final development plan approval must be reviewed by the Plan Commission.
2. The petitioner must coordinate with Bloomington Transit on the incorporation of a shuttle pick-up and potential shuttle service.
3. The petitioner will provide the sidewalk connection along Walnut Street that connects to 19th Street.
4. A zoning commitment regarding the contribution to the Housing Development Fund must be recorded within 90 days of Council approval.
5. A bus shelter is required along this property frontage.



City of Bloomington
Bloomington Environmental Commission

MEMORANDUM

Date: June 10, 2019
To: Bloomington Plan Commission
From: Bloomington Environmental Commission
Subject: PUD-17-19: North Walnut Planned Unit Development
CDG Acquisitions LLC student apartments (formerly Motel 6)
1800 N. Walnut Street

The purpose of this memo is to convey the environmental concerns and recommendations of the Environmental Commission (EC) with the hope that action will be taken to enhance its environment-enriching attributes. The EC reviewed the petition and offers the following comments and requests for your consideration, as it did for the first hearing.

This request does not include details that would be necessary for a Site Plan because the request is for changing the property from a Residential Core (RC) zoning district to a Planned Unit Development (PUD). The purpose of a PUD is to encourage flexibility in development; however, the EC does not agree with all of the requests being made in the proposed PUD District Ordinance.

1.) STAFF REVIEW

If the rezone to a PUD is granted, the EC does not believe that the future development plans should be delegated to staff for review. This petition is too large with too many variables to bypass Plan Commission and public review.

2.) FOLLOW UDO ENVIRONMENTAL STANDARDS

The EC believes that any PUD District Ordinance should not reduce the environmental protection requirements to less than the minimum Unified Development Ordinance (UDO) standards. A number of years ago staff and citizens of Bloomington worked tirelessly to craft the development regulations we now find in the Bloomington Municipal Code. These standards went through a public process and were vetted by the citizenry and voted on by our lawmakers. Although it's time to update those regulations, the trend in Bloomington had been to strengthen its environmental standards, not weaken them.

The 2018 Comprehensive Plan, Chapter 3 Environment, states explicitly that a goal for Bloomington is to limit the amount of impervious surface coverage in new developments and increase greenspace and tree planting. According to Stand4Forests, a single mature tree can sequester up to 48 pounds of carbon

per year. We cannot afford to think vegetation is only important outside the urban environment; every public, residential, or commercial yard must contribute its small part to cumulatively protect and expand flora and faunal environments in Bloomington.

The new Consolidated Draft of the UDO contains some regulation changes that will certainly be promulgated, and just as certainly, some that will not. We cannot yet begin using rules from the draft as if they have been vetted by other bodies and the public. Therefore, the EC is not considering what might, in the future, get passed for this location.

3.) IMPERVIOUS SURFACE COVERAGE

The Petitioner is requesting that the District Ordinance allow up to 70% impervious surface coverage on the site. The EC believes that is too much. The current zoning is Commercial Arterial (CA), which allows a maximum of 60%. (The Consolidated Draft also allows a maximum of 60% impervious coverage in this district, which is called Mixed Use Corridor (MC).) The EC believes the impervious surface should remain at a maximum of 60% of the site.

4.) MAXIMUM DENSITY AND HEIGHT

The maximum density currently allowed in a CA zoning district is 15 units per acre. The request is to allow up to 75 units per acre in the District Ordinance. The EC has no issue with granting a variance for some additional density or increase in building height, but not at the expense of green space. We all are well aware that urban sprawl is not desirable and that Bloomington tries to limit it to the extent it can. However, we cannot pave so much of our city that it feels more like New York City than like Bloomington.

5.) LANDSCAPE PLAN

The EC appreciates that the District Ordinance calls for only native plants to be used; however, the Petitioner may rethink the commitment that 50% of all shrubs be evergreen. There are very few native evergreen shrubs, and this rule would limit biodiversity to the extent of being undesirable.

Commented [T1]:



"Providing professional land planning, design, surveying and approval processing for a quality environment"

Stephen L. Smith, P.E., P.L.S.
Steven A. Brehob, B.S.CNT.
Todd M. Borgman, P.L.S.
Don J. Kocarek, R.L.A.
Katherine E. Stein, P.E.

April 8, 2019

Eric Greulich
City of Bloomington Planning and Transportation
401 N. Morton Street
Bloomington, IN. 47404

RE: 1800 North Walnut Planned Unit Development

Dear Eric,

On behalf of our client, CDG Acquisitions, LLC we respectfully request to be placed on the May 13th agenda for the City of Bloomington Plan Commission for consideration of a rezone petition for the property located at 1800 N. Walnut Street.

CDG Acquisitions seeks to rezone the property from CA (Commercial Arterial) to a Planned Unit Development to permit the development of a purpose-built student housing project with a ground floor commercial component. Details of the project are contained in the attached petitioner's statement, development materials and proposed Planned Unit Development Ordinance. We also request that if successful in the rezone petition that future development plans be delegated to staff for review and approval. Furthermore, we request a waiver of the minimum 5 acre requirement for a PUD zoning designation.

We look forward to working with staff, the Administration, Plan Commission and Council on this project located within a targeted student housing area.

Sincerely,

Steven A. Brehob
Smith Brehob & Associates, Inc.

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PETITIONER’S STATEMENT

Detailed elements of the proposal are contained in the proposed Planned Unit Development Ordinance. Additional information and highlights are discussed below:

Location – The site is located at 1800 N. Walnut Street. The 3.85-acre site is currently in use as a Motel 6. This location is a highly visible site in Bloomington and is located along a gateway entry corridor.

Proposed Use – The site is underutilized in its present configuration. It is located within an area that is overwhelmingly student oriented. It is adjacent to the stadium and park and ride, walkable to the north edge of campus, has good pedestrian access, is convenient to shopping and services, and is on a bus route. The 2018 Comprehensive Plan recommends this type of location for new multifamily housing catered largely to IU students. As such, the proposed use is a purpose-built student housing development with commercial and active uses on the ground floor fronting N. Walnut. The current zoning of CA (Commercial Arterial) would permit student housing but it’s resultant density would be an underutilization of the site. CDG seeks to rezone the site to Planned Unit Development.

Density – The proposed density for the site is to allow for up to seventy-five units per acre. Dwelling Unit Equivalency (DUE’s) would be used. Ground floor commercial space totaling 6,400 sf will be included in the development with associated parking for the commercial use in front of the building.

Architectural style – A modern architectural style building is proposed on site. There are no adjacent historical buildings or architectural themes or character to draw context from. Adjacent buildings are a mix of commercial styles from the late 70’s to early 90’s. Accordingly, a new modern style building provides the opportunity to make a positive, exciting statement along the entry corridor.

Site Constraints – There is a significant elevation change across the site from Walnut Street back towards the east with Walnut Street being the lowest point. Some adjacent sites on the east side of Walnut (Hampton Inn, Denny’s) sit substantially above the roadway. To fit on the site, the proposed building will have to transition up in levels from west to east. There is also an existing 20” water line crossing the site that will have to be considered in the redevelopment of the site. Shallow bedrock also drives the design.



“Providing professional land planning, design, surveying and approval processing for a quality environment”

Stephen L. Smith, P.E., P.L.S.

Steven A. Brehob, BS.CNT.

Todd M. Borgman, P.L.S.

Don J. Kocarek, R.L.A.

Katherine E. Stein, P.E.

Transportation – The site’s entry drive will be relocated to the north to line up with the existing median crossover from College Avenue through the Miller Showers Park. Aligning the drive with the crossover will provide for a significant improvement in vehicular access to and egress from the site. The proposed building will wrap a parking garage structure. Based on previous projects completed by CDG and the sites superior access to pedestrian facilities, the park and ride at the stadium and transit service, a parking ratio of 50% has been proposed and is appropriate for this location. CDG is also exploring a partnership arrangement with Bloomington Transit for additional bus or shuttle service.

Sustainability – The proposed development will incorporate energy saving features and devices based on National Green Building Standard (NGBS) scoring and provide a scorecard detailing the energy conservation and green building practices. Redevelopment will result in a reduction of existing impervious surface area on site. Water quality enhancement will be included in the site’s storm water collection network, where none currently exists, improving water quality of the discharge from the site. Landscaping will consist of plant material native to Indiana and will be selected based on sustainability without the need for irrigation.

Diverse Housing – CDG is working with the Administration to meet the City’s need for diversified housing.

Terri Porter
Director of Planning & Transportation
City of Bloomington
401 N Morton St Suite 130
Bloomington IN 47404

Date: June 5, 2019

RE: 1800 N. Walnut Street, CDG Acquisitions, LLC

Dear Ms. Porter:

This correspondence is to memorialize our commitment to the City of Bloomington concerning a proposed planned unit development located at 1800 N. Walnut Street ("Project").

CDG Acquisitions, LLC ("CDG") recognizes the importance of affordable housing within the community and is pleased to work with the City of Bloomington in furtherance of the Mayor's affordable housing initiative. In that regard, CDG will commit a contribution ("Contribution") in the amount of \$20,000.00 per bed for 15% of the total beds on the property located at 1800 N. Walnut Street to the City of Bloomington Housing Development Fund. CDG will memorialize this contribution as part of a zoning commitment that will be filed with the Monroe County Recorder's Office. CDG will pay 25% of the total contribution upon receipt of building permits for the Project. The remainder of the commitment will be paid upon receipt of an occupancy permit for the property. In the event CDG does not receive all building permits, CDG shall not be responsible for the Contribution.

The Commitment and Contribution are made voluntarily pursuant to Indiana Code Section 36-1-24.2-4.

Lastly, we appreciate the City of Bloomington's support of our Project and participation within the community.

Respectfully Submitted,

A handwritten signature in dark ink, appearing to read 'Brandt Stiles', with a long, sweeping horizontal line extending to the right.

Brandt Stiles
CDG Acquisitions LLC

cc: Alex Crowley
Doris Sims
Larry D. Allen

1800 N. Walnut Planned Unit Development—Development standards.

(a) Density and Intensity Standards.

(1) Maximum residential density: seventy-five units per acre.

(A) Dwelling unit equivalents:

Four-bedroom unit = one and one-half units;

Three-bedroom unit = one unit;

Two-bedroom unit with less than nine hundred fifty square feet = 0.66 of a unit;

One-bedroom unit with less than seven hundred square feet = 0.25 of a unit;

Efficiency or studio unit with less than five hundred fifty square feet = 0.20 of a unit.

(2) Maximum impervious surface coverage: seventy percent.

(b) Height Standards.

(1) Minimum structure height: twenty feet.

(2) Maximum structure height: The vertical dimension from the top of the roof deck and the adjacent ground grade shall not exceed 85'. Parapets, chimneys, vents, mechanical equipment, utility service structures and other architectural features shall not be included in the measurement of vertical dimensions.

(c) Parking Standards.

(1) Minimum Surface Parking Setback.

(A) Front yard: parking and drive aisles may be permitted between the building and the right of way line, but must be set back a minimum of 10';

(B) Side yard: seven feet;

(C) Rear yard: seven feet.

(2) Residential parking standards: Minimum of 0.50 spaces per bedroom and maximum of 0.75 spaces per bedroom.

(3) Nonresidential parking standards: one space per every one thousand square feet.

(4) Compact parking spaces: Up to 50% of the residential parking spaces may be compact. The minimum dimensions for compact spaces are 7'6" x 15'.

(5) Bicycle parking standards: minimum of one space per every four bedrooms.

(d) Building Setback Standards. Building setback standards are:

(1) Build-to line: n/a;

(2) Maximum front setback: one hundred feet from the existing public right-of-way;

(3) Minimum front setback: forty feet;

(4) Minimum side setback: seven feet;

(5) Minimum rear setback: ten feet.

(e) Ground Floor Nonresidential Uses.

(1) The building will consist of no less than 12,000 square feet of nonresidential uses on the ground floor, including, no less than 6,000 square feet of retail/commercial uses.

(2) Unless stated otherwise in this Planned Unit Development, all uses permitted in the Commercial Arterial zoning district shall be permitted.

1800 N. Walnut Planned Unit Development—Architectural standards.

These architectural standards shall apply to new building construction and building additions. Where an addition is made to an existing building, the architectural standards shall apply only to the new construction. Interior remodeling of existing structures shall not cause the exterior of the building to be subject to the architectural standards.

(a) Site Plan.

(1) Building Orientation and Entrances.

(A) For all buildings, any facade facing a public street shall be considered a primary facade.

(B) A minimum of one pedestrian entrance shall be provided for any primary facade which contains at least sixty-six feet of frontage facing a public street. No primary pedestrian entrance shall be located on a building facade adjacent to an alley.

(C) At least one pedestrian entrance to a building shall be constructed at an elevation that is within three feet of the adjacent sidewalk elevation.

(2) Lighting.

(A) Pedestrian scaled lighting shall be provided as approved by the board of public works and shall meet the following standards:

(i) Height: pedestrian scaled street lights shall be less than fifteen feet high.

(B) All exterior building lighting shall comply with Chapter 20.05, LG: Lighting Standards.

(4) Mechanical Equipment and Service Areas. Utility service boxes, telecommunication devices, cables, conduits, vents, turbines, flues, chillers and fans, trash receptacles, dumpsters, service bays and recycling storage areas shall be screened from public view by incorporating the following design standards:

(A) Locate mechanical equipment and service areas at the rear of the building, along an alley facade or on the building rooftop;

(B) Mechanical equipment and service areas shall be screened using either architectural screen walls, screening devices or landscaping; and

(C) Mechanical equipment located on a building rooftop shall be set back from the building edge a sufficient distance to screen the equipment from view from the adjacent streets.

(b) Architectural Character.

(1) Void-to-Solid Percentage.

(A) First Floor (Building Base). Transparent glass areas shall comprise a minimum of forty percent of the wall/facade area of the first floor facade/elevation facing N. Walnut.

(B) Upper Stories (Building Middle). Transparent glass or facade openings shall comprise a minimum of fifteen percent of the wall/facade area of each floor above the first floor facade along each street, but shall not exceed seventy percent of the wall/facade area of each floor above the first floor facade facing a street.

(2) Windows.

(A) All windows shall be transparent and shall not make use of dark tinting or reflective glass.

(3) Materials. The following materials are not permitted as primary exterior finish materials:

(i) Smooth-faced or split-faced cement block;

(ii) Vinyl;

(iii) Highly reflective materials;

(iv) EIFS;

(v) Precast concrete.

(4) Materials. The following materials are not permitted as secondary exterior finish materials:

(i) Vinyl;

(ii) Highly reflective materials;

(iii) EIFS.

(5) Entrance Detailing. The primary pedestrian entrance for a building shall incorporate two or more of the following architectural design features:

(A) Recessed entry (minimum of four feet);

(B) Ornamental paving and integral landscape planters;

(C) Canopy or awning;

(D) Portico;

(E) Prominent building address, building name and enhanced entryway exterior lighting;

(F) Public art display, the size of which shall be adequate to be clearly viewed by pedestrians using the adjoining sidewalk;

(G) Raised corniced entryway parapet (may exceed building height three feet) or a gable;

(H) Landscaped patio area or front porch.

(c) Mass, Scale and Form.

(1) Building Facade Modulation. Facade modulation is required and shall be incorporated through recessing and through banding and/or articulation of exterior materials or change of materials by incorporating repeating patterns, textures and/or colors used on exterior facade materials.

(A) Building facades along each street shall utilize a maximum facade width interval of eighty feet and a minimum facade width interval of twenty feet for a facade module.

(B) The building facade module shall be offset by a minimum depth (projecting or recessing) of two feet.

(C) All vertical offsets expressed on the facade shall be a minimum of two feet.

(3) Building height step back: n/a.

1800 N. Walnut Planned Unit Development - Landscape Standards

General Landscaping Standards

(a) Plant Material Type

- (1) All plant material shall be plant material native to Indiana as listed in the plant material table of the City of Bloomington Unified Development Ordinance.

Streetscape

- (b) Streetscape - This standards section applies to lawn areas within or abutting the public right-of-way where street trees would typically be planted but cannot due to the presence of existing utility conflicts that preclude street tree installation.

(1) Shrubs

- (A) Number. A minimum of one shrub shall be planted per five feet of property that abuts a public right-of-way.
- (B) Type. A minimum of fifty percent of the required shrubs shall be evergreen.
- (C) Vision Clearance. Shrubs shall be planted outside the vision clearance triangle as defined in the City of Bloomington Unified Development Ordinance. Shrubs shall be located a minimum of ten feet from a driveway cut, traffic control sign, or street light, and a minimum of three feet from a fire hydrant.
- (D) Responsibility. Maintenance of the streetscape shall be the responsibility of the property owner.

General Parking

- (a) Parking Lot Perimeter Plantings. Parking lots shall be screened from streets and adjacent uses using a combination of plant materials, decorative fences, decorative walls, and/or earthen berms. Parking lots with four or more spaces shall have the following perimeter planting:

(1) Trees.

- (A) Number. Parking lot perimeter areas shall contain one tree per four parking spaces.
- (B) Type. A minimum of seventy-five percent of the required trees shall be large, canopy trees.
- (C) Location. Trees shall be planted within ten feet of the parking lot edge.

(2) Shrubs.

- (A) Number. Parking lot perimeter areas shall contain three shrubs per one parking space.
- (B) Type. A minimum of fifty percent of the shrubs shall be evergreen.
- (C) Location. Shrubs shall be planted within five feet of the parking lot edge.

(D) Height. Shrubs planted in parking lot perimeter areas shall be selected from species that grow to a minimum height of four feet.

Multifamily Residential

(a) Interior Plantings. The following standards apply:

(1) Trees.

(A) Number. Any areas of a site not covered by a structure, parking lot or required buffer yard shall be planted with a minimum of fourteen large canopy trees, five evergreen trees, and five medium or small canopy trees per acre.

(2) Shrubs.

(A) Number. Lots shall be planted with a minimum of thirty-six shrubs per acre.

(B) Type. A minimum of fifty percent of the required shrubs shall be evergreen.

(C) Substitution. One ornamental tree may be substituted for every four shrubs; however, substitution shall not exceed fifty percent of the required shrubs.

(D) Foundation Plantings. Shrubs and ornamental trees along foundation walls of structures shall be planted no closer than two feet and eight feet respectively from the foundation wall.

1800 N. Walnut Planned Unit Development – Signage Standards

(a) Wall Signs.

(1) The following standards shall apply to wall signs for individual use by the primary development.

(A) The cumulative square footage of all wall signs shall not exceed one and one-half square feet per lineal foot of primary façade.

(B) Maximum of two wall signs is allowed.

(C) Limits. No use shall be limited to less than thirty square feet of wall signage and no use shall be permitted to exceed three hundred twenty square feet of aggregate wall signage.

(D) Maximum Projection. Except an awning sign, no part of a wall sign shall project more than twelve inches from the wall or face of the building to which it is attached.

(E) Illumination. Sign may be internally or indirectly illuminated.

(b) Freestanding Signs.

(1) Number.

(A) One freestanding sign is permitted.

(2) Area.

(A) The freestanding sign shall not exceed forty-five square feet.

(3) Height.

(A) The maximum sign height shall be fifteen feet.

(c) Projecting Signs.

(1) Maximum of two projecting signs is allowed.

(2) Projecting Signs shall not exceed 60 square feet.

(3) Projecting signs may be internally illuminated

(4) Sign may not project more than 5' from the building façade.

(d) Secondary Retail Tenants.

(1) The following standards shall apply to wall signs for individual use by the retail tenants.

(A) The cumulative square footage of all wall signs shall not exceed one and one-half square feet per lineal foot of primary façade.

(B) One wall sign is allowed for each tenant space.

(C) Limits. No use shall be limited to less than thirty square feet of wall signage and no use shall be permitted to exceed sixty square feet of aggregate wall signage.

(D) Maximum Projection. Except an awning sign, no part of a wall sign shall project more than twelve inches from the wall or face of the building to which it is attached.

(E) Illumination. Sign may be internally or indirectly illuminated.

(2) Window Graphics.

(A) Each retail tenant is allowed up to 25% coverage vinyl window graphics.

1800 N. Walnut Planned Unit Development - Green Development Standards

In order to promote energy conservation and green building techniques, the applicant aims to incorporate energy saving features and devices based on ICC/ASHRAE 700-2015 National Green Building Standard (NGBS). The applicant will provide a summary checklist which will include, but not be limited to, the NGBS items listed below. Additionally the project will include stormwater management, heat island mitigation, support of multi-modal transportation, including electric car charging stations, use of native

landscape vegetation and permeable hardscape surfaces, use of Energy Star appliances, energy efficient mechanical systems, water-saving plumbing fixtures, user controls for thermal comfort and lighting, high reflectance roofing materials, bicycle storage facilities, on-site recycling for occupant refuse, furnished units, energy efficient lighting and building envelope insulation that meets or exceeds applicable Energy Code requirements.

1) Lot Design, Preparation and Development

- a) A lot is selected within five miles (8,046 m) of a mass transit station with provisions for parking.
- b) Walkways, street crossings, and entrances designed to promote pedestrian activity are provided. New buildings are connected to existing sidewalks and areas of development.
- c) Dedicated bicycle parking and racks are indicated on the site plan and constructed for mixed-use and multifamily buildings: Minimum of 1 bicycle parking space per 2 residential units
- d) At least 75% of total length of the utilities on the lot are designed to use one or more alternative means: Placement of utilities under paved surfaces instead of yards
- e) Permeable materials are used for driveways, parking areas, walkways, patios, and recreational surfaces and the like according to the following percentages: less than 25 percent
- f) Off-street parking areas are shared or driveways are shared.
- g) In a multifamily project, parking capacity does not exceed the local minimum requirements.
- h) Structured parking is utilized to reduce the footprint of surface parking areas. Greater than 75 percent.
- i) Water permeable surfaces, including vegetative paving systems, are utilized to reduce the footprint of impervious surface driveways, fire lanes, streets or parking areas. 25 percent to 75 percent.
- j) Hardscape: Not less than 50 percent of the surface area of the hardscape on the lot meets one or a combination of the following methods. Permeable hardscaping: Permeable hardscaping materials are installed.
- k) The lot contains a mixed-use building.

2) Quality of Construction Materials and Waste

- a) Finished floor area of a dwelling unit is limited. Finished floor area is calculated in accordance with ANSI Z765 for single family and ANSI/BOMA Z65.4 for multifamily buildings. Only the finished floor area for stories above grade plane is included in the calculation. Less than or equal to 1,500 square feet (139 m²)
- b) Structural systems are designed or construction techniques are implemented that reduce and optimize material usage. Minimum structural member or element sizes necessary for strength and stiffness in accordance with advanced framing techniques or structural design standards are selected.
- c) Stories above grade are stacked, such as in 1½-story, 2-story, or greater structures. The area of the upper story is a minimum of 50 percent of the area of the story below based on areas with a minimum ceiling height of 7 feet (2,134 mm).
- d) Capillary breaks A capillary break and vapor retarder are installed at concrete slabs in accordance with ICC IRC Sections R506.2.2 and R506.2.3 or ICC IBC Sections 1907 and 1805.4.1.
- e) Building materials with visible mold are not installed or are cleaned or encapsulated prior to concealment and closing.

- f) Insulation in cavities is dry in accordance with manufacturer's instructions when enclosed (e.g., with drywall).
 - g) The moisture content of lumber is sampled to ensure it does not exceed 19 percent prior to the surface and/or cavity enclosure.
 - h) Where required by the ICC, IRC, or IBC, a water-resistive barrier and/or drainage plane system is installed behind exterior veneer and/or siding.
 - i) Flashing is provided as follows to minimize water entry into wall and roof assemblies and to direct water to exterior surfaces or exterior water-resistive barriers for drainage.
 - j) All window and door head and jamb flashing is either self-adhered flashing complying with AAMA 711-13 or liquid applied flashing complying with AAMA 714-15 and installed in accordance with fenestration or flashing manufacturer's installation instructions.
 - k) Through-wall flashing is installed at transitions between wall cladding materials or wall construction types.
 - l) Architectural features that increase the potential for water intrusion are avoided: No roof configurations that create horizontal valleys in roof design.
 - m) A minimum of 90 percent of roof surfaces, not used for roof penetrations and associated equipment, on-site renewable energy systems such as photovoltaics or solar thermal energy collectors, or rooftop decks, amenities and walkways, are constructed of one or more of the following: Minimum initial SRI of 78 for low-sloped roof (a slope less than 2:12) and a minimum initial SRI of 29 for a steep-sloped roof (a slope equal to or greater than 2:12).
 - n) A gutter and downspout system or splash blocks and effective grading are provided to carry water a minimum of 5 feet (1524 mm) away from perimeter foundation walls.
 - o) Water is directed to drains or swales to ensure drainage away from the structure.
- 3) Energy Efficiency
- a) The building shall comply with Section 702 (Performance Path), Section 703 (Prescriptive Path), or Section 704 (HERS Index Target Path).
 - b) Space heating and cooling system is sized according to heating and cooling loads calculated using ACCA Manual J, or equivalent.
 - c) Duct system is sized and designed in accordance with ACCA Manual D or equivalent.
 - d) Windows, skylights and sliding glass doors have an air infiltration rate of no more than 0.3 cfm per square foot (1.5 L/s/m²), and swinging doors no more than 0.5 cfm per square foot (2.6 L/s/m²)
 - e) Lighting efficacy in dwelling units is in accordance with one of the following: Lighting power density, measured in watts/square foot, is 1.1 or less.
 - f) Photo or motion sensors are installed on 75 percent of outdoor lighting fixtures to control lighting.
 - g) In a multifamily building, occupancy controls are installed to automatically reduce light levels in garages and parking structures when the space is unoccupied.
 - h) Structured parking garages are designed to require no mechanical ventilation for fresh air requirements.
- 4) Indoor and Outdoor Water Use
- a) Energy Star or equivalent water-conserving appliances are installed for dishwasher and washing machine.

- b) The total maximum combined flow rate of all showerheads controlled by a single valve at any point in time in a shower compartment is 1.6 to less than 2.5 gpm.
- c) Water-efficient lavatory faucets with a maximum flow rate of 1.5 gpm (5.68 L/m), tested at 60 psi (414 kPa) in accordance with ASME A112.18.1, are installed: all lavatory faucets in the dwelling unit(s).
- d) A water closet is installed with an effective flush volume of 1.28 gallons (4.85 L) or less and meets the flush performance criteria when tested in accordance with ASME A112.19.2/CSA B45.1 or ASME A112.19.14 as applicable.

5) Pollutant Source Control

- a) Air handling equipment or return ducts are not located in the garage, unless placed in isolated, air-sealed mechanical rooms with an outside air source.
- b) Wall-to-wall carpeting is not installed adjacent to water closets and bathing fixtures.
- c) Site-applied interior architectural coatings, which are inside the water proofing envelope, are in accordance with one or more of the following: Zero VOC as determined by EPA Method 24 (VOC content is below the detection limit for the method)
- d) A minimum of 85 percent of site-applied adhesives and sealants located inside the waterproofing envelope are in accordance with one of the following, as applicable. The emission levels are in accordance with CDPH/EHLB Standard Method v1.1. Emission levels are determined by a laboratory accredited to ISO/IEC 17025 and the CDPH/EHLB Standard Method v1.1 is in its scope of accreditation.
- e) Emissions of 85 percent of wall, ceiling, and floor insulation materials are in accordance with the emission levels of CDPH/EHLB Standard Method v1.1.
- f) Environmental tobacco smoke is minimized by one or more of the following: All interior common areas of a multifamily building are designated as non-smoking areas with posted signage and exterior smoking areas of a multifamily building are designated with posted signage and located a minimum of 25 feet from entries, outdoor air intakes, and operable windows.
- g) Verify there are no moisture, mold, and dust issues per 602.1.7.1(3), 901.4-901.11, ASTM D7338 Section 6.3, and ASTM D7338 Section 7.4.3.

Traffic Discussion
1800 N. Walnut Site (Motel 6)
Collegiate Development Group

Introduction

CDG is proposing to redevelop the existing Motel 6 property located at 1800 N. Walnut Street into a student housing complex with a total of 820 beds and a parking garage capable of parking 417 vehicles. The adjacent roadways providing access to the site, North Walnut and North College are classified as primary arterial streets.

Summary

The primary destination of the residents of the project will be the IU campus. The predominate modes of campus access will be bus service, and bicycle and pedestrian access. Vehicular access is limited due to the lack of parking available on campus. Most students will take the IU Campus bus, Bloomington Transit, walk, bicycle or utilize available scooters.

Transit - IU Campus Bus has a stop at the stadium. Bloomington Transit has a bus stop immediately adjacent to the site on Walnut and two routes that provide service to campus areas (6 Campus Shuttle and 1 North).

Pedestrian Access, Bicycle and Scooter – 19th Street provides direct access to the IU Campus Bus stop at the stadium. Though there is no dedicated bike lane or continuous sidewalks along 19th between Walnut and Dunn, the roadway is a low volume road. Sidewalks are available from the site to 19th Street with the completion of a section across the Parkview Apartments site. Dedicated bike lanes are marked along Walnut and College. Plans are currently in process for completion of a multi-use path along 17th Street connecting east to the bike and pedestrian facilities along Woodlawn.

Vehicular – Projects with anticipated higher volumes of traffic generation should be located along arterial streets. This site meets that criteria. Though there are 417 parking spaces provided, the parking garage on site will be predominately used for vehicle storage. Residents may use vehicles during evening and weekend times but use during AM peaks of traffic on the adjacent roadways will be very limited. Delays on adjacent roadways are typically caused by an opposing left turn movement into a site. Since the site is located on a one-way pair roadway, left turns into the site will not occur. Access to the site will be limited to a right-in from northbound Walnut or straight utilizing the median cut-through from southbound College to Walnut. Similarly, vehicles exiting the site will utilize a right-out to go northbound on Walnut or straight utilizing the median cut-through to travel southbound on College. Delays are not anticipated as there are no opposing movements required to enter the site.

Travel Characteristics






The nature of student occupancy, the location convenient to campus and IU and City transit make the traffic patterns generated by this complex different than a typical market rate or work force apartment complex. From other studies in the area and of student-oriented housing complexes even further from campus that we have completed, we have learned that:

- Most students use the IU shuttle located at the stadium to get to and from campus
- Students in this complex walk to the shuttle stop at the stadium or take Bloomington Transit
- The neighborhood of which this complex is a part is predominately students with similar travel patterns
- The neighborhood is within easy walking distance to many IU facilities and to services making walking a second transportation method.
- Bicycling is another reasonable option for travel to campus.
- Many residents park their cars in the lots and complex garages, but they don't drive them for their daily trip to campus, resulting in a very low trip generation rate.

The factors described above made it clear that the typical trip generation rates for apartments in the ITE Trip Generation manual and database do not accurately apply to student-oriented housing. The expectation and what we have quantified from other studies (2018 Fountain Residential proposal) is a lower generation rate and a different distribution through the day. Traffic distribution from a student-oriented project do not coincide with AM and PM traffic peaks on adjacent roadways.

Exhibits

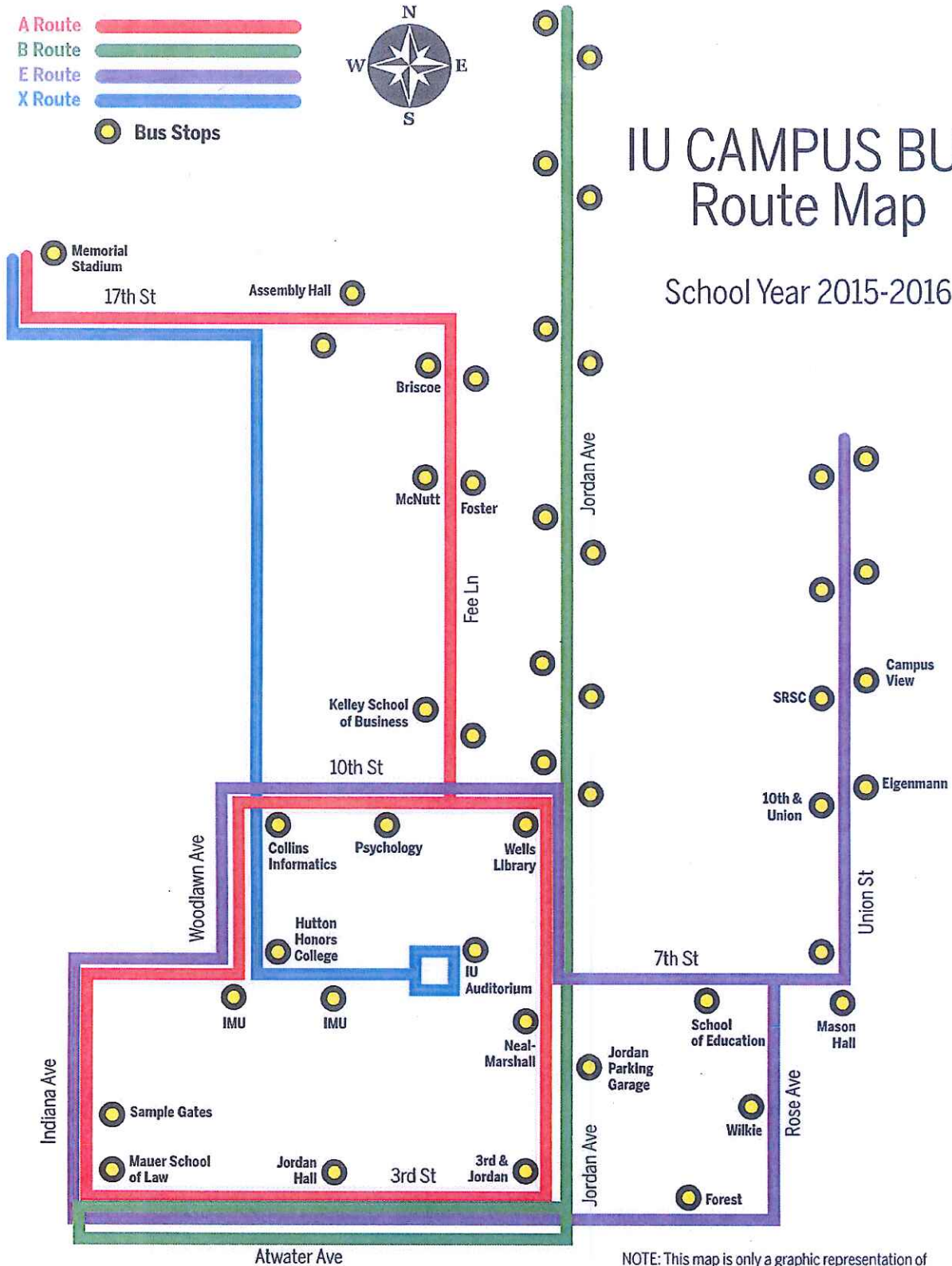
City Master Thoroughfare Plan
IU Campus Bus Route Map
BT 1 North Route Map and Schedule
BT 6 Campus Shuttle Route Map

- A Route 
- B Route 
- E Route 
- X Route 
-  Bus Stops



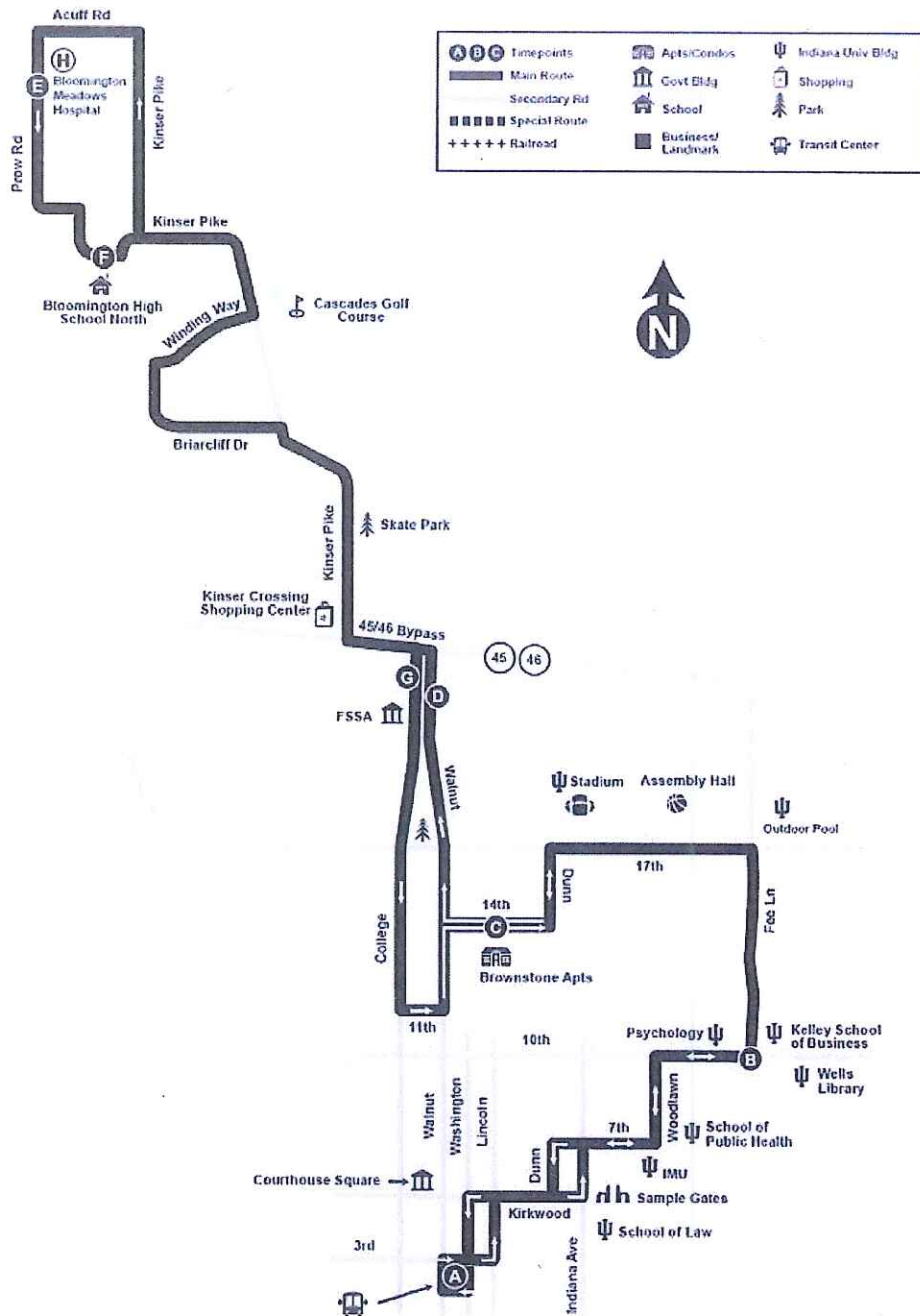
IU CAMPUS BUS Route Map

School Year 2015-2016



NOTE: This map is only a graphic representation of the routes and the bus stops and subject to change.

1 North - Fee Lane / BHS North



Route Particulars

*** At 5:30pm the bus does not travel beyond North High School. This trip will not serve Rosewood Drive, Acuff Road, Prow Road, and Meadows Hospital. Route 1 does not operate on Sundays. Shaded area does not operate on Saturdays. On IU athletic event Saturdays call Bloomington Transit at 336-RIDE for operating information.



Map and Schedule

Download the PDF brochure containing the Map and Schedules for this route.

(1 North Fee Lane - BHS North)

[Return to Top](#)

[Return to Top](#)

English



ROUTE



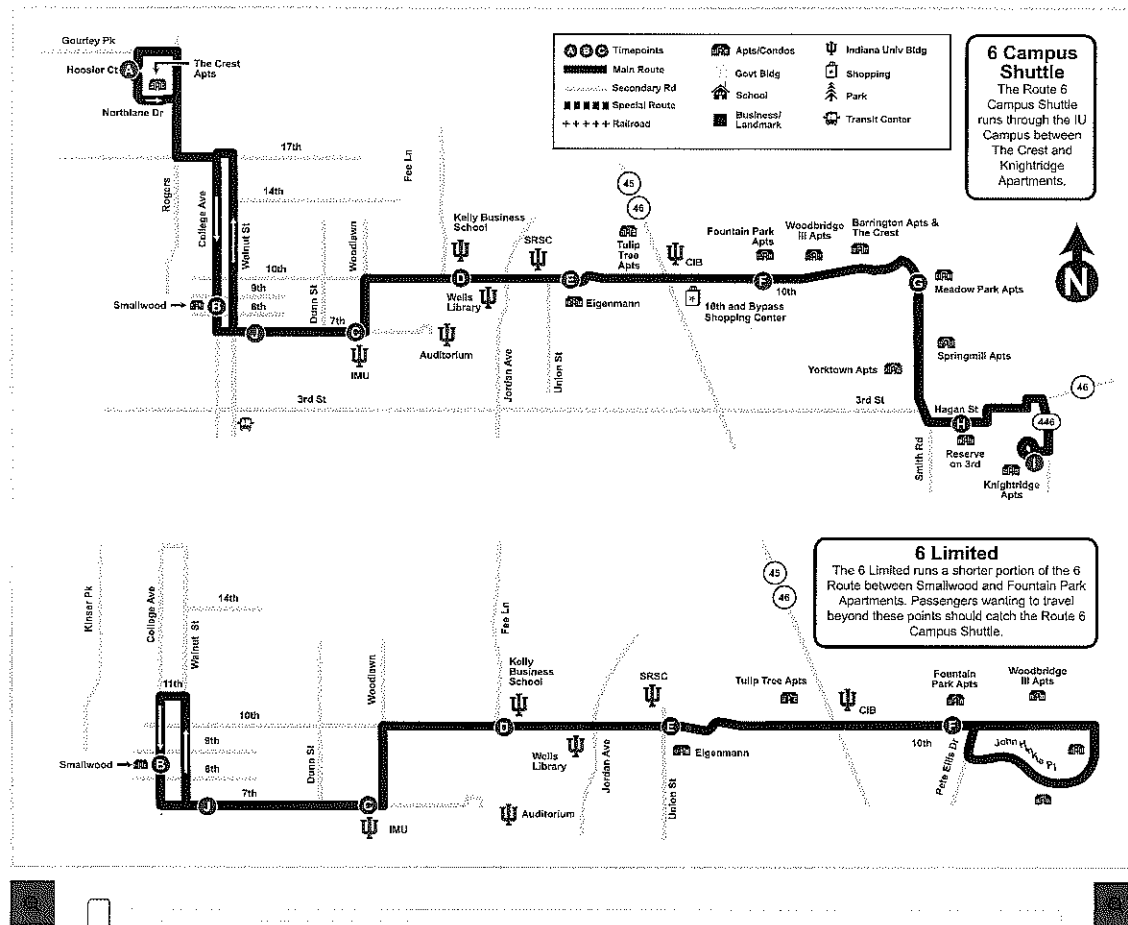
SCHEDULE



ROUTE LIST

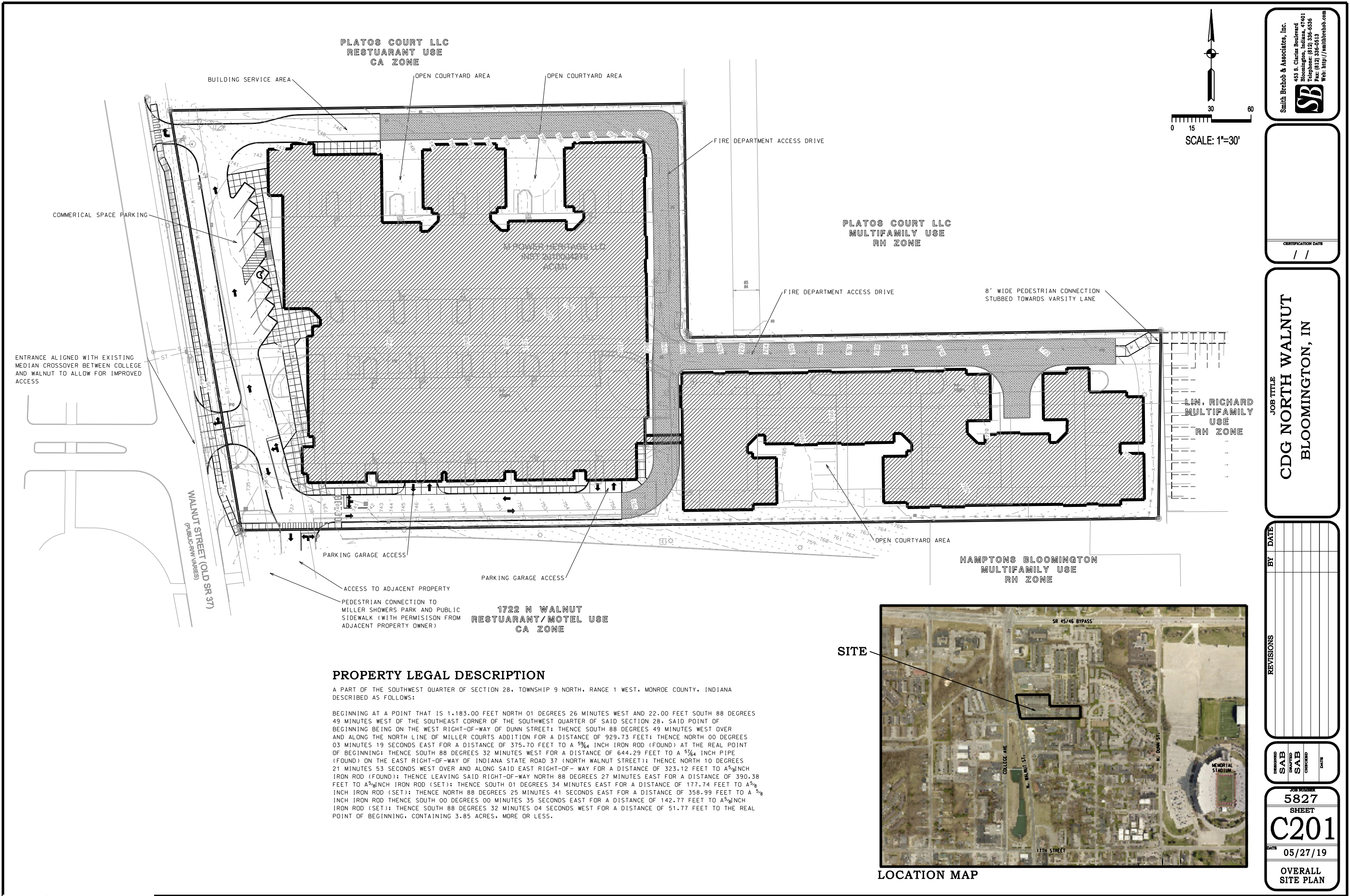
6 CAMPUS SHUTTLE & 6 LIMITED

FROM MAY 5 TO AUGUST 24 THESE ROUTES WILL BE OPERATING THE SUMMER SCHEDULE. Two routes operate for 6. The campus shuttle, which serves many off campus apartments, and the 6 Limited, which is a shortened version of the campus shuttle.



Shuttle: If you live on N. Walnut or N. College this is the route to take to get to the 10th St area of campus as well as the Memorial Union. Route 6 is also the best way to get to campus along 10th St from many off-campus apartment complexes such as Fountain Park, Woodbridge I-II-III, Meadow Park, Yorktown, Reserve on Third, Knightbridge, and The Arch. This route also provides great service to and from Eigenmann and other residence halls on 10th St. Route 6 is a good way to get to the Business School, Wells Library, Geology & Psychology buildings. During the IU fall & spring semesters runs every 20 minutes on both the east and west sides of the route, Monday through Friday during the day. During IU semester weeknights this bus runs every 60 minutes up to 12:30 a.m. Also runs every 60 minutes on Saturday, Sunday, and on weekdays during the summer months and break periods.

Limited: This route is very similar to the 6 Campus Shuttle, however, it only runs a shortened version of the 6 Campus Shuttle. The western end of the 6 Limited is Smallwood Plaza. The eastern end of the 6 Limited is Fountain Park apartments on 10th Street. The 6 Limited generally runs between trips of the 6 Campus Shuttle. It does not operate during IU's winter, spring, fall, and Thanksgiving breaks.



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Bloomington, Indiana, 47401
Telephone: (812) 336-6836
Fax: (812) 336-0513
Web: <http://smithbrubbs.com>

CERTIFICATION DATE
/ /

JOB TITLE
CDG NORTH WALNUT
BLOOMINGTON, IN

BY DATE

REVISIONS

DESIGNED
SAB

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SAB

DATE

JOB NUMBER
5827

SHEET
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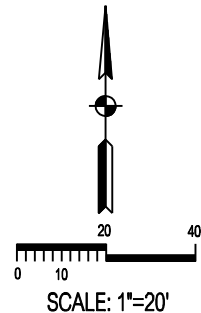
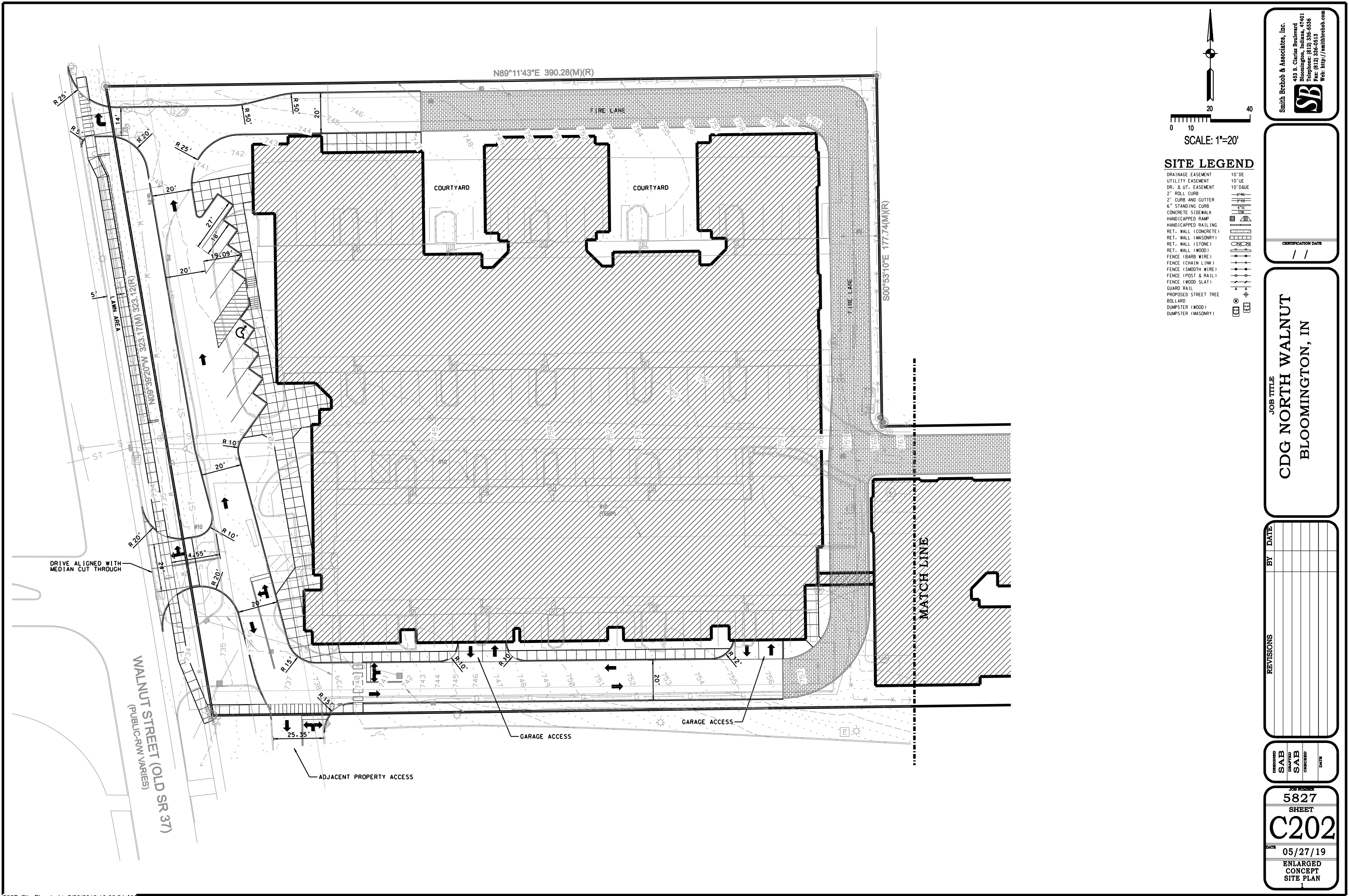
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OVERALL
SITE PLAN

PROPERTY LEGAL DESCRIPTION

A PART OF THE SOUTHWEST QUARTER OF SECTION 28, TOWNSHIP 9 NORTH, RANGE 1 WEST, MONROE COUNTY, INDIANA DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT THAT IS 1,183.00 FEET NORTH 01 DEGREES 26 MINUTES WEST AND 22.00 FEET SOUTH 88 DEGREES 49 MINUTES WEST OF THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SAID SECTION 28, SAID POINT OF BEGINNING BEING ON THE WEST RIGHT-OF-WAY OF DUNN STREET; THENCE SOUTH 88 DEGREES 49 MINUTES WEST OVER AND ALONG THE NORTH LINE OF MILLER COURTS ADDITION FOR A DISTANCE OF 929.73 FEET; THENCE NORTH 00 DEGREES 03 MINUTES 19 SECONDS EAST FOR A DISTANCE OF 375.70 FEET TO A 5/64 INCH IRON ROD (FOUND) AT THE REAL POINT OF BEGINNING; THENCE SOUTH 88 DEGREES 32 MINUTES WEST FOR A DISTANCE OF 644.29 FEET TO A 5/64 INCH PIPE (FOUND) ON THE EAST RIGHT-OF-WAY OF INDIANA STATE ROAD 37 (NORTH WALNUT STREET); THENCE NORTH 10 DEGREES 21 MINUTES 53 SECONDS WEST OVER AND ALONG SAID EAST RIGHT-OF-WAY FOR A DISTANCE OF 323.12 FEET TO A 5/8 INCH IRON ROD (FOUND); THENCE LEAVING SAID RIGHT-OF-WAY NORTH 88 DEGREES 27 MINUTES EAST FOR A DISTANCE OF 390.38 FEET TO A 5/8 INCH IRON ROD (SET); THENCE SOUTH 01 DEGREES 34 MINUTES EAST FOR A DISTANCE OF 177.74 FEET TO A 5/8 INCH IRON ROD (SET); THENCE NORTH 88 DEGREES 25 MINUTES 41 SECONDS EAST FOR A DISTANCE OF 358.99 FEET TO A 5/8 INCH IRON ROD THENCE SOUTH 00 DEGREES 00 MINUTES 35 SECONDS EAST FOR A DISTANCE OF 142.77 FEET TO A 5/8 INCH IRON ROD (SET); THENCE SOUTH 88 DEGREES 32 MINUTES 04 SECONDS WEST FOR A DISTANCE OF 51.77 FEET TO THE REAL POINT OF BEGINNING, CONTAINING 3.85 ACRES, MORE OR LESS.



SITE LEGEND

DRAINAGE EASEMENT	10' DE
UTILITY EASEMENT	10' UE
DR. & UT. EASEMENT	10' DAUE
2' ROLL CURB	2' RC
2' CURB AND GUTTER	2' CG
6" STANDING CURB	6" SC
CONCRETE SIDEWALK	CS
HANDICAPPED RAMP	HR
HANDICAPPED RAILING	HR
RET. WALL (CONCRETE)	RWC
RET. WALL (MASONRY)	RWM
RET. WALL (STONE)	RWS
RET. WALL (WOOD)	RWD
FENCE (BARB WIRE)	FBW
FENCE (CHAIN LINK)	FL
FENCE (SMOOTH WIRE)	FSW
FENCE (POST & RAIL)	FR
FENCE (WOOD SLAT)	FRS
GUARD RAIL	GR
PROPOSED STREET TREE	PST
BOLLARD	B
DUMPSTER (WOOD)	DW
DUMPSTER (MASONRY)	DM

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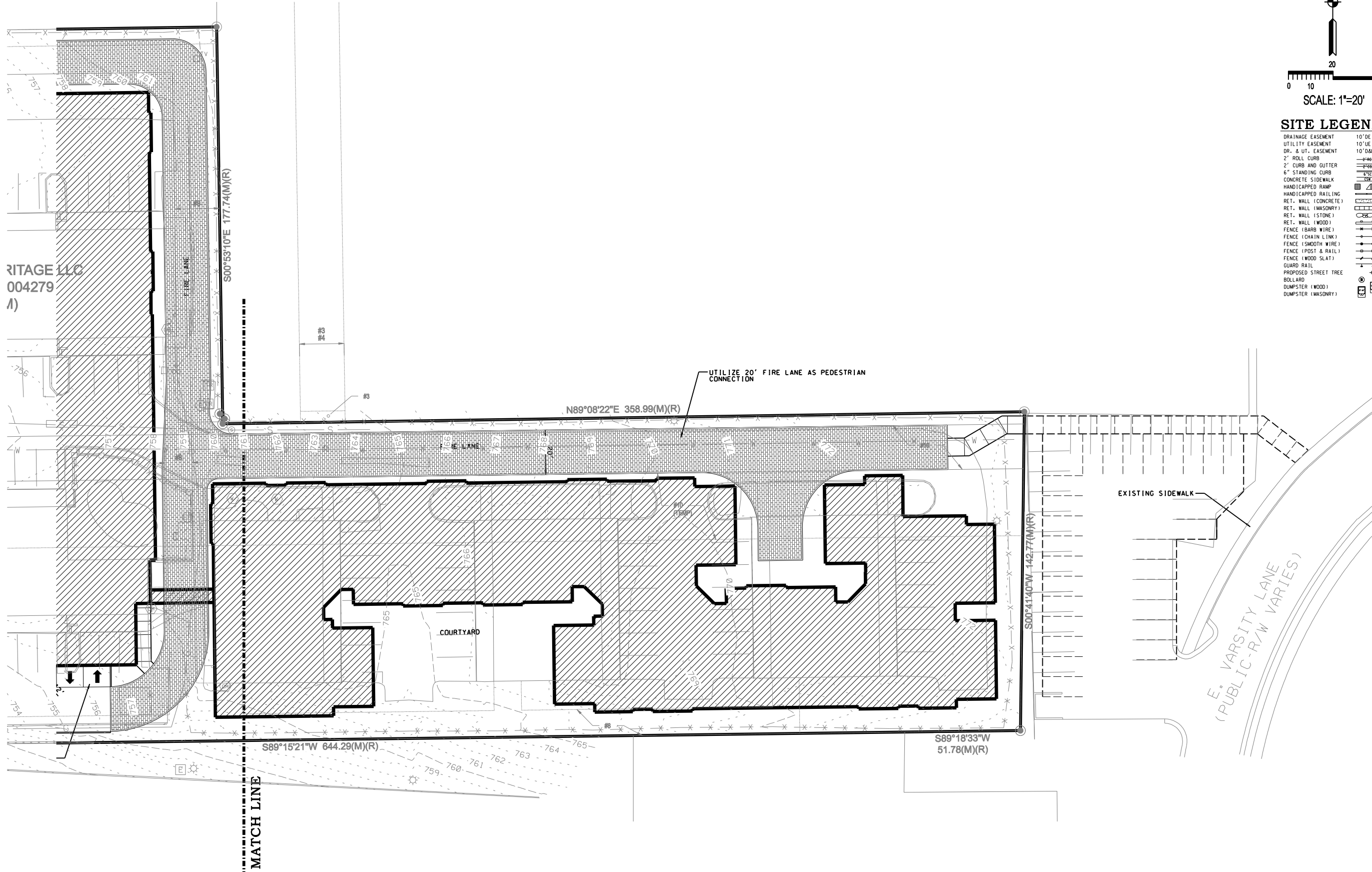
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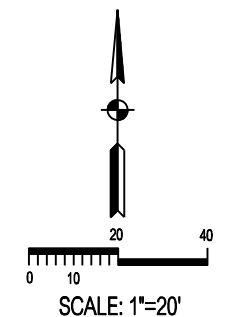
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RITAGE LLC
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SITE LEGEND

DRAINAGE EASEMENT	10' DE
UTILITY EASEMENT	10' UE
DR. & UT. EASEMENT	10' D&UE
2" ROLL CURB	
2" CURB AND GUTTER	
6" STANDING CURB	
CONCRETE SIDEWALK	
HANDICAPPED RAMP	
HANDICAPPED RAILING	
RET. WALL (CONCRETE)	
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RET. WALL (STONE)	
RET. WALL (WOOD)	
FENCE (BARB WIRE)	
FENCE (CHAIN LINK)	
FENCE (SMOOTH WIRE)	
FENCE (POST & RAIL)	
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GUARD RAIL	
PROPOSED STREET TREE	
BOLLARD	
DUMPSTER (WOOD)	
DUMPSTER (MASONRY)	

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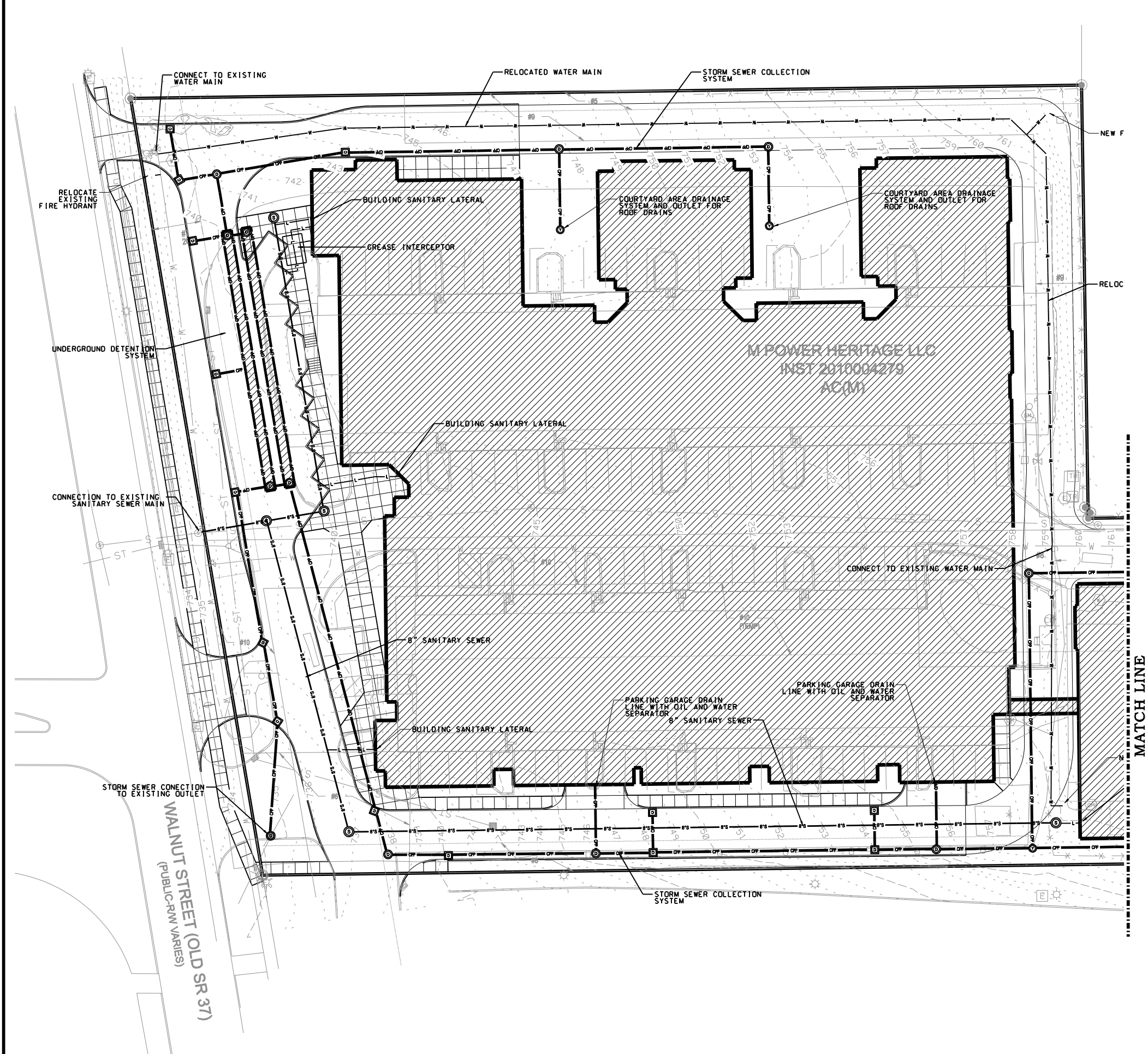
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UTILITY NOTES

- 1) ALL PROJECTS WILL REQUIRE A PRE-CONSTRUCTION MEETING WITH THE CITY OF BLOOMINGTON UTILITIES PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR AND/OR DEVELOPER MUST CONTACT THE UTILITIES TECHNICIAN AT (812) 349-3633 TO SCHEDULE THE MEETING.
- 2) CONTRACTOR SHALL NOTIFY THE CITY OF BLOOMINGTON UTILITIES ENGINEERING DEPARTMENT ONE (1) WORKING DAY PRIOR TO CONSTRUCTION OF ANY WATER, STORM OR SANITARY SEWER UTILITY WORK. A CBU INSPECTOR MUST HAVE NOTICE SO WORK CAN BE INSPECTED, DOCUMENTED, AND A PROPER AS-BUILT MADE. WHEN A CONTRACTOR WORKS ON WEEKENDS, A CBU DESIGNATED HOLIDAY, OR BEYOND NORMAL CBU WORK HOURS, THE CONTRACTOR WILL PAY FOR THE INSPECTOR'S OVERTIME. FOR CBU WORK HOURS AND HOLIDAY INFORMATION, PLEASE CONTACT THE CITY OF BLOOMINGTON UTILITIES ENGINEERING DEPARTMENT AT (812)349-3660.
- 3) SEE SPECIFICATIONS FOR SIZES OF WATER SERVICE LINES AND SEWER LATERALS NOT SPECIFICALLY NOTED ON THE PLANS.
- 4) MS-1 IF SHOWN ON THE PLANS, = MINIMUM SEWER ELEVATION. IT INDICATES THE LOWEST FLOOR ELEVATION THAT WILL ALLOW GRAVITY SEWER SERVICE WITHOUT A SPECIAL BACKWATER VALVE. ANY FLOOR ELEVATION THAT WILL BE SERVED BY GRAVITY SEWER MUST BE ABOVE THE RIM ELEVATION OF THE UPSTREAM SANITARY MANHOLE. IF NOT A BACKWATER VALVE MUST BE INSTALLED ACCORDING TO SEC. 409(A) OF THE UNIFORM PLUMBING CODE. SEE SPECIFICATIONS PACKET FOR MORE DETAIL.
- 5) ON ALL EXISTING SANITARY MAINS, WYES SHALL BE CUT AND SLEEVED IN PLACE BY CITY OF BLOOMINGTON UTILITIES PERSONNEL WITH CITY OF BLOOMINGTON UTILITIES FURNISHING ALL MATERIAL, EQUIPMENT, AND LABOR NECESSARY FOR INSTALLATION. DEVELOPER SHALL PROVIDE ALL NECESSARY EXCAVATION, SHORING, BACKFILL, AND SURFACE REPAIR. PLEASE CONTACT NANCY AXSON AT (812) 349-3689 FOR MORE INFORMATION.
- 6) WHEN CONNECTING A NEW PIPE TO AN EXISTING MANHOLE, THE MANHOLE SHALL BE CORE-DRILLED. PIPE SHALL BE CONNECTED TO THE MANHOLE BY EITHER A FLEXIBLE BOOT OR A 1" OR 2" FLEXIBLE CONNECTOR OR APPROVED EQUAL. TABLE AND TROUGH SHALL BE MODIFIED AS NECESSARY TO DIRECT THE FLOW FROM THE NEW PIPE. INVERT OF CONNECTION SHALL BE NO MORE THAN ONE FOOT HIGHER THAN THE INVERT OUT FOR THIS STRUCTURE.
- 7) IN ACCORDANCE WITH SECTION 4-5.2.1.5.1, OF THE CBU CONSTRUCTION SPECIFICATIONS ALL SEWER LATERALS SHALL HAVE A CLEAN-OUT AT LEAST EVERY 90 FEET. ALL CLEAN-OUTS, WEATHER IN GRASSY AREAS OR IN PAVEMENT, SHALL BE SUB-SURFACE AND PROTECTED BY A SUITABLE METAL CASTING SUCH AS EAST JORDAN CATALOGUE NO. 2975 OR NEENAH CATALOGUE NO. R-1974-A. IN GRASSY AREAS, THE CASTING SHALL BE PROVIDED WITH A CIRCULAR CONCRETE COLLAR FLUSH WITH THE TOP OF THE CASTING AND THE GROUND SURFACE. THE COLLAR SHALL BE MINIMUM 6" THICK AND SHALL EXTEND AT LEAST 8" BEYOND THE OUTSIDE OF THE CASTING ON ALL SIDES. IN PAVEMENT, THE TOP OF THE CASTING SHALL BE FLUSH WITH THE SURROUNDING PAVEMENT. TOP OF CLEAN-OUT SHALL BE NO MORE THAN 3" BELOW THE TOP OF THE CASTING. A #10 INSULATED SOLID COPPER LOCATOR WIRE SHALL BE WRAPPED AROUND ALL NON-METALLIC PIPES SO THAT ONE REVOLUTION IS MADE AT LEAST EVERY PIPE JOINT. SPLICES ARE TO BE MADE WITH AN APPROVED CONNECTOR, AND ARE TO BE SUITABLY PROTECTED AGAINST CORROSION. THE WIRE IS TO BE BROUGHT TO THE SURFACE WITH A CLEAN-OUT IN A CASTING. ALSO SEE THE CBU CONSTRUCTION SPECIFICATIONS FOR THE "STANDARD SANITARY LATERAL CLEAN-OUT DETAIL #19".
- 8) WHEREVER C900 PIPE IS USED FOR SEWER, ALL WYES SHALL BE HARCO, SIZED FOR C900 ON THE RUN AND SDR-35 ON THE BRANCH. TRANSITION FROM C900 TO SDR-35 PIPE SHALL BE MADE BY USE OF A HARCO C900 TO SDR-35 ADAPTER WYES.
- 9) ALL D.I.P. USED FOR SANITARY SEWER SHALL HAVE CERAMIC EPOXY LINING, MINIMUM THICKNESS 40 MILS, AND SHALL BE PROTECTO 401, AS MANUFACTURED BY INDURON PROTECTIVE COATINGS. WYES FOR D.I.P. SHALL BE HARCO D.I.P. TO SDR-35 ADAPTER WYES.
- 10) THE OWNERSHIP OF THE WATER AND SANITARY SEWER MAINS IN THIS DEVELOPMENT WILL BE OWNED AND MAINTAINED BY THE CITY OF BLOOMINGTON UTILITIES. OWNERSHIP WILL TAKE EFFECT AFTER FINAL WALK-THROUGH, WHEN EASEMENT ARE RECORDED AND FINAL ACCEPTANCE IS GIVEN. ANY EXCEPTIONS SHALL BE INDICATED WITH SYMBOLS ON THE PLANS, ADDRESSED BY LETTER AND APPROVED IN WRITING BY THE UTILITIES ENGINEER.
- 11) A PERMANENT INDICATING CONTROL VALVE SHALL BE INSTALLED ON THE FIRE LINE 12" ABOVE THE FLOOR AT THE TERMINATION POINT. THIS VALVE WILL BE USED TO HYDROSTATIC PRESSURE TEST AGAINST AND WILL REMAIN AS A PART OF THE SYSTEM, ONCE ALL TESTING IS COMPLETE, THE LINE WILL NOT BE DISMANTLED FOR CONNECTION TO THE FIRE SUPPRESSION SYSTEM. PLEASE CONTACT NANCY AXSON (812-349-3689) FOR ADDITIONAL INFORMATION.
- 12) ALL DUCTILE IRON PIPE (DIP) WILL REQUIRE POLYETHYLENE ENCASEMENT SLEEVES AND SHALL BE 8-MIL LINEAR LOW-DENSITY (LLD) POLYETHYLENE ENCASEMENT OR 4-MIL HIGH-DENSITY CROSS-LAMINATED (HDCL). POLYETHYLENE ENCASEMENT MATERIAL, INCLUSIVE OF VALVES AND FITTINGS, THE MATERIAL SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH ANSI/AWWA C-105/A21.5, USING PLASTIC TIE STRAPS OR CIRCUMFERENTIAL OR ADHESIVE TAPE PROVIDING THE PIPE WITH A SECURE PROTECTIVE ENCLASURE.
- 13) BACKFLOW PREVENTER CONDUIT REQUIREMENTS:
 - RADIO READ EQUIPMENT MUST BE PLACED ON THE OUTSIDE OF THE BUILDING.
 - CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE RADIO HEAD DEVICE FROM CBU.
 - CONTRACTOR IS RESPONSIBLE FOR INSTALLING CONDUIT AND WIRING FROM THE METER LOCATION, THROUGH THE BUILDING WALL, AND ATTACHING THE DEVICE TO THE WALL AT A POINT HIGH ENOUGH TO KEEP IT OUT OF REACH FROM TAMPERING AND DAMAGE, YET ACCESSIBLE WITH A LADDER.
 - CBU WILL MAKE THE WIRING CONNECTIONS AT THE TWO END POINTS.

UTILITY LEGEND

- WATER IRRIGATION LINE
- DOMESTIC WATER SERVICE
- FIRE WATER SERVICE
- WATER MAIN
- WATER SERVICE LINE
- WATER VAULT
- WATER HANDHOLE
- WATER MANHOLE
- WATER METER
- FIRE HYDRANT
- FLUSH HYDRANT
- WATER MASTER METER
- WATER AIR RELEASE VALVE
- WATER END CAP
- WATER TANK
- SANITARY MAIN LINE
- SANITARY FORCE MAIN
- LOW PRESSURE SEWER
- SANITARY LATERAL
- SANITARY MANHOLE
- SANITARY CLEANOUT
- SANITARY VAULT METER
- SANITARY METER
- SANITARY LIFT STATION
- SINGLE FLUSHING CONN.
- DOUBLE FLUSHING CONN.
- LOW PRESSURE SEWER PUMP
- L.P.S. AIR RELEASE VALVE
- F.W. AIR RELEASE VALVE
- GREASE TRAP
- SEPTIC TANK
- MONITORING WELL
- STORM PIPE
- ROOF DRAIN PIPE
- UNDERDRAIN PIPE
- STORM MANHOLE
- STORM CURB INLET
- STORM CATCH BASIN
- STORM YARD INLET
- STORM DOWNSPOUT
- STORM TRENCH DRAIN
- STORM PIPE END SECTION
- PROPOSED GAS LINE
- GAS MANHOLE
- GAS VAULT METER
- GAS METER
- GAS PLANT
- GAS SHUTOFF VALVE
- GAS MARKER
- CABLE TV
- OVERHEAD ELECTRIC
- UNDERGROUND FIBER OPTIC
- UNDERGROUND TELEPHONE
- ELECTRIC VAULT
- ELECTRIC MANHOLE
- ELECTRIC TRANS. PAD
- ELECTRIC TRANS. VAULT
- ELECTRIC HANDHOLE
- ELECTRIC POWER POLE
- PHONE RISER
- PHONE VAULT
- PHONE MANHOLE

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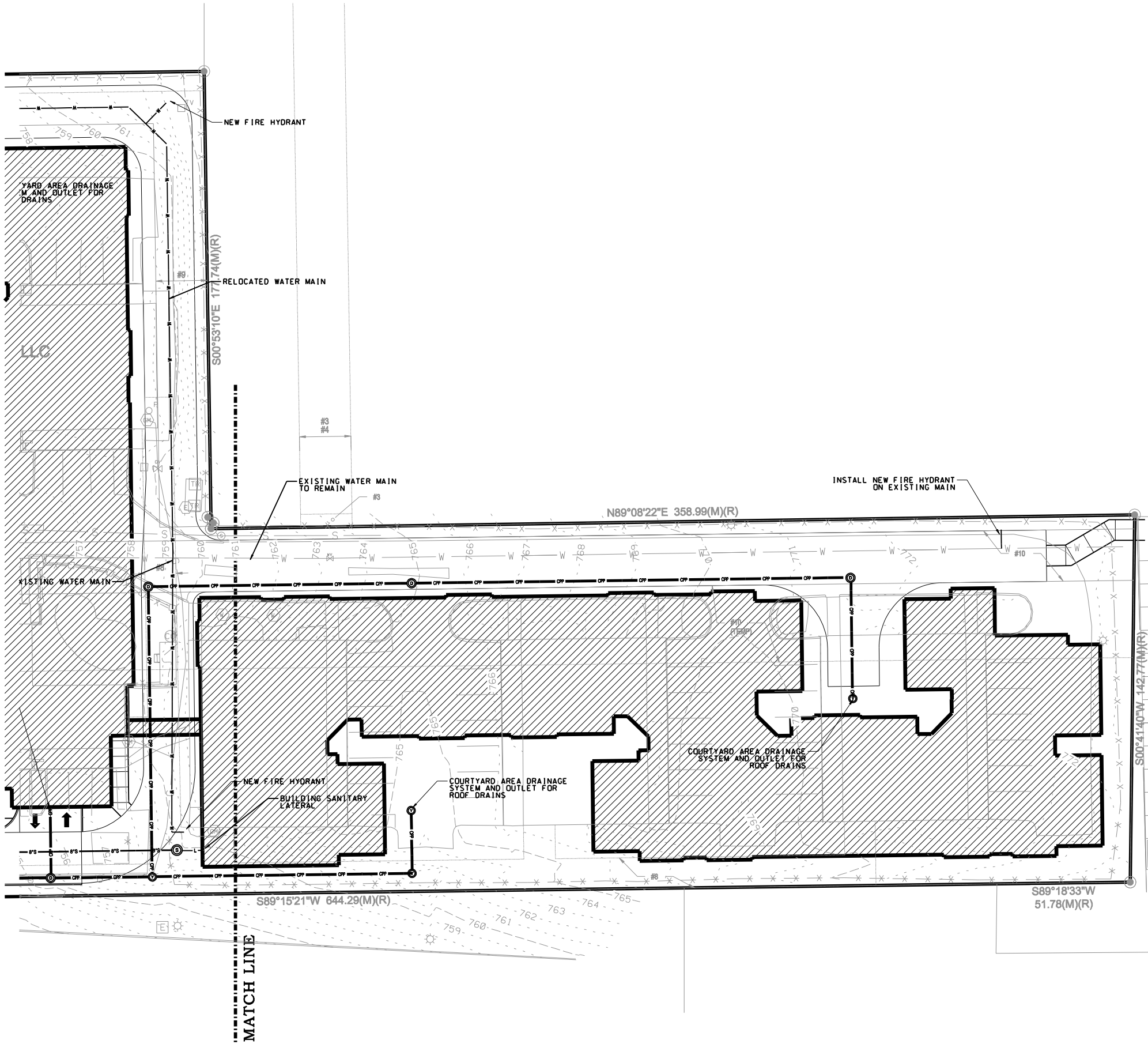
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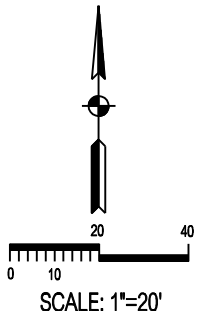
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- 2) CONTRACTOR SHALL NOTIFY THE CITY OF BLOOMINGTON UTILITIES ENGINEERING DEPARTMENT ONE (1) WORKING DAY PRIOR TO CONSTRUCTION OF ANY WATER, STORM OR SANITARY SEWER UTILITY WORK. A CBU INSPECTOR MUST HAVE NOTICE SO WORK CAN BE INSPECTED, DOCUMENTED, AND A PROPER AS-BUILT MADE. WHEN A CONTRACTOR WORKS ON WEEKENDS, A CBU DESIGNATED HOLIDAY, OR BEYOND NORMAL CBU WORK HOURS, THE CONTRACTOR WILL PAY FOR THE INSPECTOR'S OVERTIME. FOR CBU WORK HOURS AND HOLIDAY INFORMATION, PLEASE CONTACT THE CITY OF BLOOMINGTON UTILITIES ENGINEERING DEPARTMENT AT (812)349-3660.
- 3) SEE SPECIFICATIONS FOR SIZES OF WATER SERVICE LINES AND SEWER LATERALS NOT SPECIFICALLY NOTED ON THE PLANS.
- 4) MS+4 IF SHOWN ON THE PLANS, = MINIMUM SEWER ELEVATION. IT INDICATES THE LOWEST FLOOR ELEVATION THAT WILL ALLOW GRAVITY SEWER SERVICE WITHOUT A SPECIAL BACKWATER VALVE. ANY FLOOR ELEVATION THAT WILL BE SERVED BY GRAVITY SEWER MUST BE ABOVE THE RIM ELEVATION OF THE UPSTREAM SANITARY MANHOLE. IF NOT A BACKWATER VALVE MUST BE INSTALLED ACCORDING TO SEC. 409(A) OF THE UNIFORM PLUMBING CODE. SEE SPECIFICATIONS PACKET FOR MORE DETAIL.
- 5) ON ALL EXISTING SANITARY MAINS, WYES SHALL BE CUT AND SLEEVED IN PLACE BY CITY OF BLOOMINGTON UTILITIES PERSONNEL WITH CITY OF BLOOMINGTON UTILITIES FURNISHING ALL MATERIAL, EQUIPMENT, AND LABOR NECESSARY FOR INSTALLATION. DEVELOPER SHALL PROVIDE ALL NECESSARY EXCAVATION, SHORING, BACKFILL, AND SURFACE REPAIR. PLEASE CONTACT NANCY AXSON AT (812) 349-3689 FOR MORE INFORMATION.
- 6) WHEN CONNECTING A NEW PIPE TO AN EXISTING MANHOLE, THE MANHOLE SHALL BE CORE-DRILLED. PIPE SHALL BE CONNECTED TO THE MANHOLE BY EITHER A FLEXIBLE BOOT KOR-N-SEAL, 1 OR 2 FLEXIBLE CONNECTOR OR APPROVED EQUAL. TABLE AND TROUGH SHALL BE MODIFIED AS NECESSARY TO DIRECT THE FLOW FROM THE NEW PIPE. INVERT OF CONNECTION SHALL BE NO MORE THAN ONE FOOT HIGHER THAN THE INVERT OUT FOR THIS STRUCTURE. IN ACCORDANCE WITH SECTION 4.5.2.1.5.1, OF THE CBU CONSTRUCTION SPECIFICATIONS ALL SEWER LATERALS SHALL HAVE A CLEAN-OUT AT LEAST EVERY 90 FEET. ALL CLEAN-OUTS, WEATHER IN GRASSY AREAS OR IN PAVEMENT, SHALL BE SUB-SURFACE AND PROTECTED BY A SUITABLE METAL CASTING SUCH AS EAST JORDAN CATALOGUE NO. 2975 OR MEEHAN CATALOGUE NO. R-1974-A. IN GRASSY AREAS, THE CASTING SHALL BE PROVIDED WITH A CIRCULAR CONCRETE COLLAR FLUSH WITH THE TOP OF THE CASTING AND THE GROUND SURFACE. THE COLLAR SHALL BE MINIMUM 6" THICK AND SHALL EXTEND AT LEAST 8" BEYOND THE OUTSIDE OF THE CASTING ON ALL SIDES. IN PAVEMENT, THE TOP OF THE CASTING SHALL BE FLUSH WITH THE SURROUNDING PAVEMENT. TOP OF CLEAN-OUT SHALL BE NO MORE THAN 3" BELOW THE TOP OF THE CASTING. A #10 INSULATED SOLID COPPER LOCATOR WIRE SHALL BE WRAPPED AROUND ALL NON-METALLIC PIPES SO THAT ONE REVOLUTION IS MADE AT LEAST EVERY PIPE JOINT. SPLICES ARE TO BE MADE WITH AN APPROVED CONNECTOR, AND ARE TO BE SUITABLY PROTECTED AGAINST CORROSION. THE WIRE IS TO BE BROUGHT TO THE SURFACE WITH A CLEAN-OUT IN A CASTING. ALSO SEE THE CBU CONSTRUCTION SPECIFICATIONS FOR THE "STANDARD SANITARY LATERAL CLEAN-OUT DETAIL #19".
- 7) WHEREVER C900 PIPE IS USED FOR SEWER, ALL WYES SHALL BE HARCO, SIZED FOR C900 ON THE RUN AND SDR-35 ON THE BRANCH. TRANSITION FROM C900 TO SDR-35 PIPE SHALL BE MADE BY USE OF A HARCO C900 TO SDR-35 ADAPTER WYES.
- 8) ALL D.I.P. USED FOR SANITARY SEWER SHALL HAVE CERAMIC EPOXY LINING, MINIMUM THICKNESS 40 MILS, AND SHALL BE PROTECTO 401, AS MANUFACTURED BY INDOURON PROTECTIVE COATINGS. WYES FOR D.I.P. SHALL BE HARCO D.I.P. TO SDR-35 ADAPTER WYES.
- 9) THE OWNERSHIP OF THE WATER AND SANITARY SEWER MAINS IN THIS DEVELOPMENT WILL BE OWNED AND MAINTAINED BY THE CITY OF BLOOMINGTON UTILITIES. OWNERSHIP WILL TAKE EFFECT AFTER FINAL WALK-THROUGH. WHEN EASEMENT ARE RECORDED AND FINAL ACCEPTANCE IS GIVEN. ANY EXCEPTIONS SHALL BE INDICATED WITH SYMBOL ON THE PLANS, ADDRESSED BY LETTER AND APPROVED IN WRITING BY THE UTILITIES ENGINEER.
- 10) A PERMANENT INDICATING CONTROL VALVE SHALL BE INSTALLED ON THE FIRE LINE 12" ABOVE THE FLOOR AT THE TERMINATION POINT. THIS VALVE WILL BE USED TO HYDROSTATIC PRESSURE TEST AGAINST AND WILL REMAIN AS A PART OF THE SYSTEM, ONCE ALL TESTING IS COMPLETE. THE LINE WILL NOT BE DISMANTLED FOR CONNECTION TO THE FIRE SUPPRESSION SYSTEM. PLEASE CONTACT NANCY AXSON (812-349-3689) FOR ADDITIONAL INFORMATION.
- 11) ALL DUCTILE IRON PIPE (DIP) WILL REQUIRE POLYETHYLENE ENCASEMENT SLEEVES AND SHALL BE 8-MIL LINEAR LOW-DENSITY (LLD) POLYETHYLENE ENCASEMENT OR 4-MIL HIGH-DENSITY CROSS-LAMINATED (HDL) POLYETHYLENE ENCASEMENT MATERIAL, INCLUSIVE OF VALVES AND FITTINGS. THE MATERIAL SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH ANSI/AWWA C-105/A21.5, USING PLASTIC TIE STRAPS OR CIRCUMFERENTIAL OR ADHESIVE TAPE PROVIDING THE PIPE WITH A SECURE PROTECTIVE ENCLOSURE.
- 12) BACKFLOW PREVENTER CONDUIT REQUIREMENTS:
 - RADIO READ EQUIPMENT MUST BE PLACED ON THE OUTSIDE OF THE BUILDING.
 - CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE RADIO HEAD DEVICE FROM CBU.
 - CONTRACTOR IS RESPONSIBLE FOR INSTALLING CONDUIT AND WIRING FROM THE METER LOCATION, THROUGH THE BUILDING WALL, AND ATTACHING THE DEVICE TO THE WALL AT A POINT HIGH ENOUGH TO KEEP IT OUT OF REACH FROM TAMPERING AND DAMAGE, YET ACCESSIBLE WITH A LADDER.
 - CBU WILL MAKE THE WIRING CONNECTIONS AT THE TWO END POINTS.



UTILITY LEGEND

- WATER IRRIGATION LINE
- DOMESTIC WATER SERVICE
- FIRE WATER SERVICE
- WATER MAIN
- WATER SERVICE LINE
- WATER VAULT
- WATER MANHOLE
- WATER METER
- WATER VALVE
- FIRE HYDRANT
- FLUSH HYDRANT
- WATER MASTER METER
- WATER AIR RELEASE VALVE
- WATER END CAP
- WATER TANK
- SANITARY MAIN LINE
- SANITARY FORCE MAIN
- LOW PRESSURE SEWER
- SANITARY LATERAL
- SANITARY MANHOLE
- SANITARY CLEANOUT
- SANITARY VAULT METER
- SANITARY METER
- SANITARY LIFT STATION
- SINGLE FLUSHING CONN.
- DOUBLE FLUSHING CONN.
- LOW PRESSURE SEWER PUMP
- L.P.S., AIR RELEASE VALVE
- F.W., AIR RELEASE VALVE
- GREASE TRAP
- SEPTIC TANK
- MONITORING WELL
- STORM PIPE
- ROOF DRAIN PIPE
- UNDERDRAIN PIPE
- STORM MANHOLE
- STORM CURB INLET
- STORM CATCH BASIN
- STORM YARD INLET
- STORM DOWNSPOUT
- STORM TRENCH DRAIN
- STORM PIPE END SECTION
- PROPOSED GAS LINE
- GAS MANHOLE
- GAS VAULT METER
- GAS METER
- GAS PLANT
- GAS SHUTOFF VALVE
- GAS MARKER
- CABLE TV
- OVERHEAD ELECTRIC
- OVERHEAD TELEPHONE
- UNDERGROUND FIBER OPTIC
- UNDERGROUND TELEPHONE
- ELECTRIC VAULT
- ELECTRIC MANHOLE
- ELECTRIC TRANS. PAD
- ELECTRIC TRANS. VAULT
- ELECTRIC HANDHOLE
- ELECTRIC POWER POLE
- PHONE RISER
- PHONE VAULT
- PHONE MANHOLE

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Bloomington, Indiana, 47401
Telephone: (812) 336-6536
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Web: http://smithbrubaker.com

SB

CERTIFICATION DATE

/ /

JOB TITLE

CDG NORTH WALNUT
BLOOMINGTON, IN

REVISIONS	BY	DATE

DRAWN	CHECKED	DATE
SAB	SAB	

JOB NUMBER

5827

SHEET

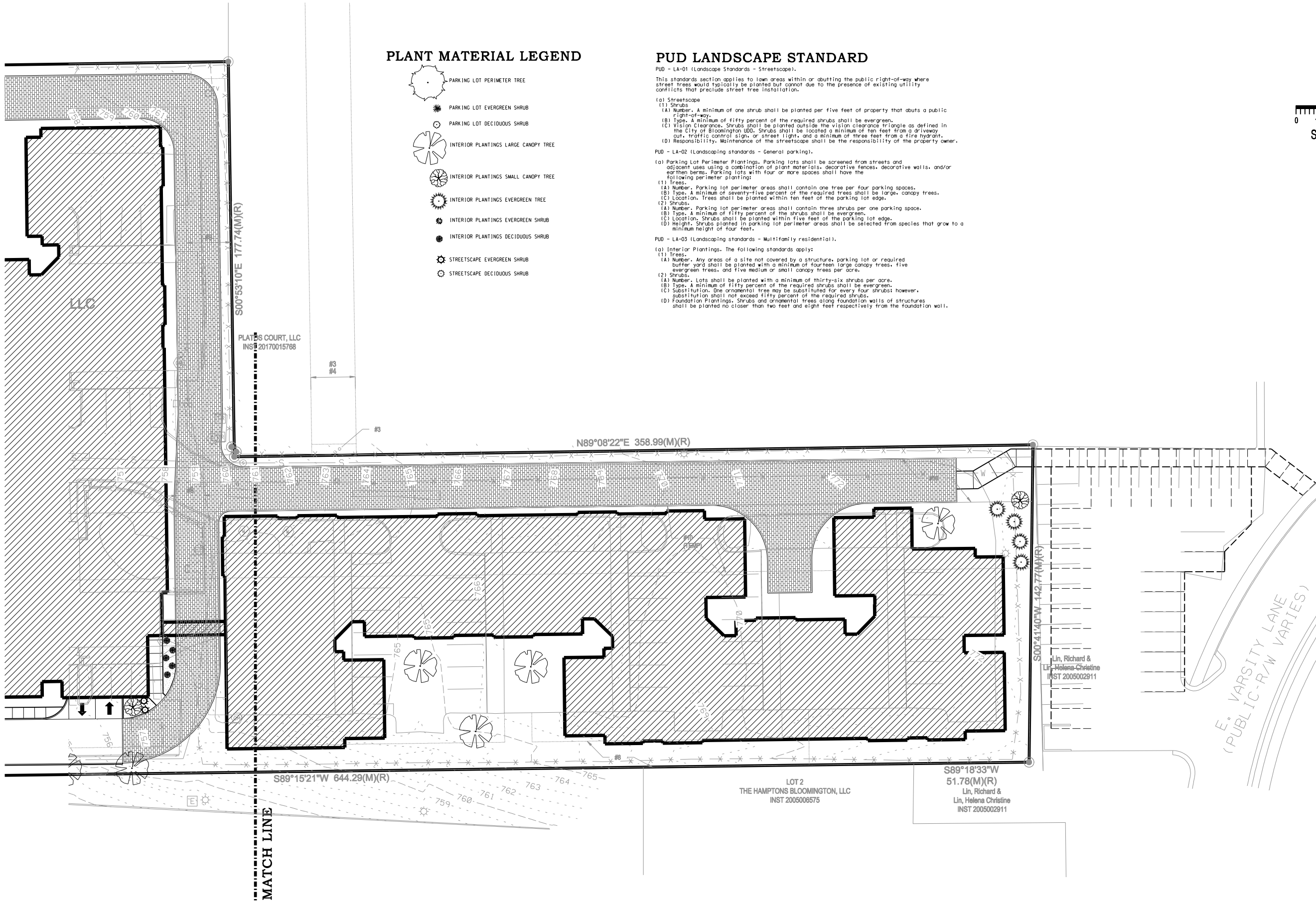
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DATE

05/27/19

ENLARGED
CONCEPT
UTILITY PLAN

2



PLANT MATERIAL LEGEND

- PARKING LOT PERIMETER TREE
- PARKING LOT EVERGREEN SHRUB
- PARKING LOT DECIDUOUS SHRUB
- INTERIOR PLANTINGS LARGE CANOPY TREE
- INTERIOR PLANTINGS SMALL CANOPY TREE
- INTERIOR PLANTINGS EVERGREEN TREE
- INTERIOR PLANTINGS EVERGREEN SHRUB
- INTERIOR PLANTINGS DECIDUOUS SHRUB
- STREETSCAPE EVERGREEN SHRUB
- STREETSCAPE DECIDUOUS SHRUB

PUD LANDSCAPE STANDARD

PUD - LA-01 (Landscape Standards - Streetscape).

This standards section applies to lawn areas within or abutting the public right-of-way where street trees would typically be planted but cannot due to the presence of existing utility conflicts that preclude street tree installation.

(a) Streetscape

(1) Shrubs

(A) Number. A minimum of one shrub shall be planted per five feet of property that abuts a public right-of-way.

(B) Type. A minimum of fifty percent of the required shrubs shall be evergreen.

(C) Vision Clearance. Shrubs shall be planted outside the vision clearance triangle as defined in the City of Bloomington UDO. Shrubs shall be located a minimum of ten feet from a driveway cut, traffic control sign, or street light, and a minimum of three feet from a fire hydrant.

(D) Responsibility. Maintenance of the streetscape shall be the responsibility of the property owner.

PUD - LA-02 (Landscaping standards - General parking).

(a) Parking Lot Perimeter Plantings. Parking lots shall be screened from streets and adjacent uses using a combination of plant materials, decorative fences, decorative walls, and/or earthen berms. Parking lots with four or more spaces shall have the following perimeter planting:

(1) Trees.

(A) Number. Parking lot perimeter areas shall contain one tree per four parking spaces.

(B) Type. A minimum of seventy-five percent of the required trees shall be large, canopy trees.

(C) Location. Trees shall be planted within ten feet of the parking lot edge.

(2) Shrubs.

(A) Number. Parking lot perimeter areas shall contain three shrubs per one parking space.

(B) Type. A minimum of fifty percent of the shrubs shall be evergreen.

(C) Location. Shrubs shall be planted within five feet of the parking lot edge.

(D) Height. Shrubs planted in parking lot perimeter areas shall be selected from species that grow to a minimum height of four feet.

PUD - LA-03 (Landscaping standards - Multifamily residential).

(a) Interior Plantings. The following standards apply:

(1) Trees.

(A) Number. Any areas of a site not covered by a structure, parking lot or required buffer yard shall be planted with a minimum of fourteen large canopy trees, five evergreen trees, and five medium or small canopy trees per acre.

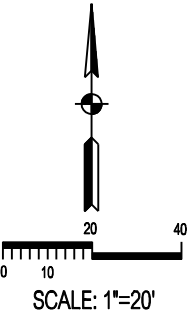
(2) Shrubs.

(A) Number. Lots shall be planted with a minimum of thirty-six shrubs per acre.

(B) Type. A minimum of fifty percent of the required shrubs shall be evergreen.

(C) Substitution. One ornamental tree may be substituted for every four shrubs; however, substitution shall not exceed fifty percent of the required shrubs.

(D) Foundation Plantings. Shrubs and ornamental trees along foundation walls of structures shall be planted no closer than two feet and eight feet respectively from the foundation wall.



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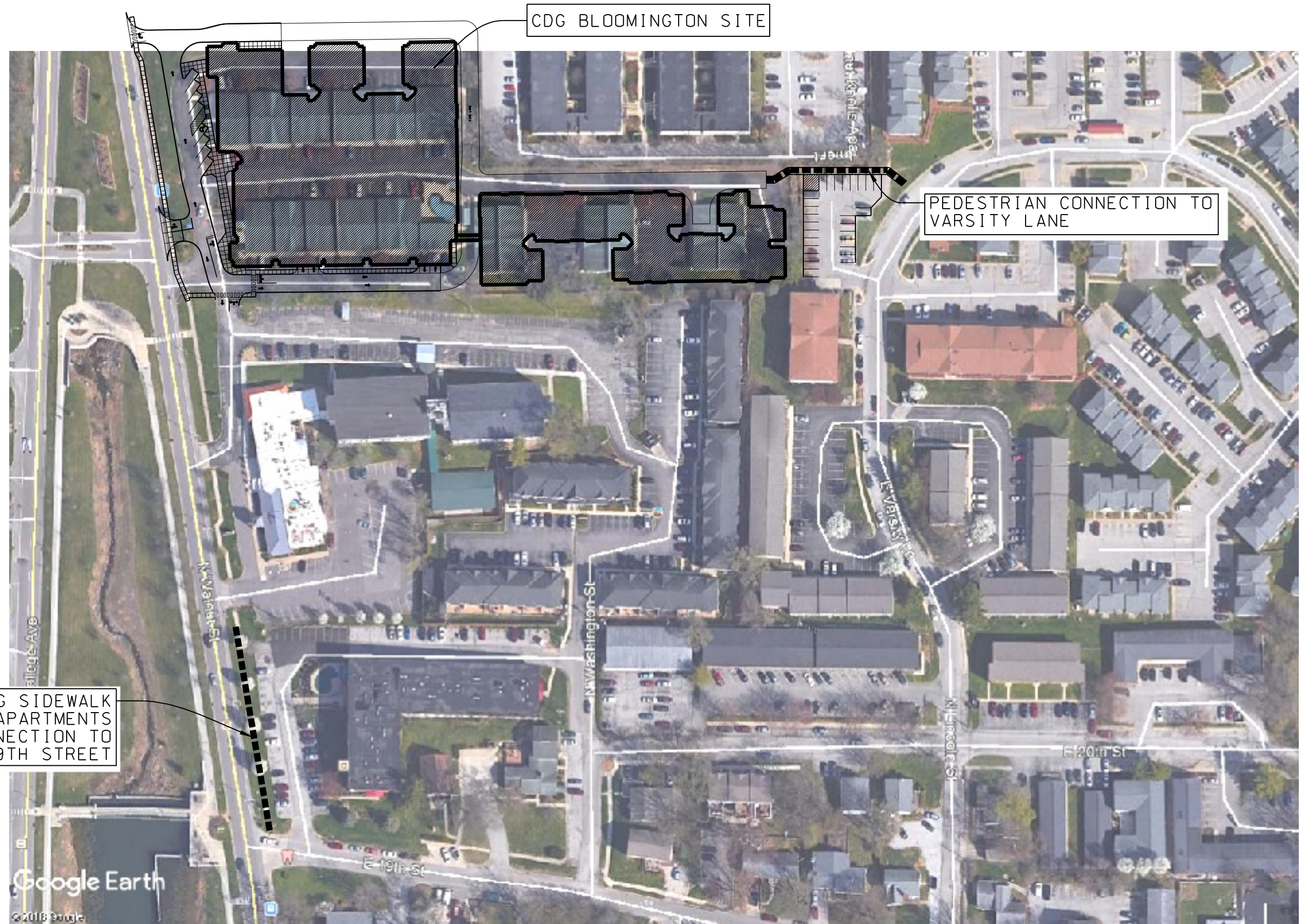
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BLOOMINGTON, IN

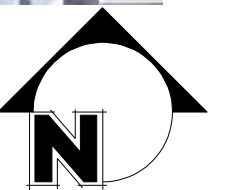
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DESIGNED	DRAWN	CHECKED	DATE
SAB	SAB	SAB	

JOB NUMBER
5827
SHEET
C207
DATE
05/27/19
ENLARGED
CONCEPT
LANDSCAPE
PLAN 2



CDG BLOOMINGTON OFFSITE PEDESTRIAN CONNECTIONS

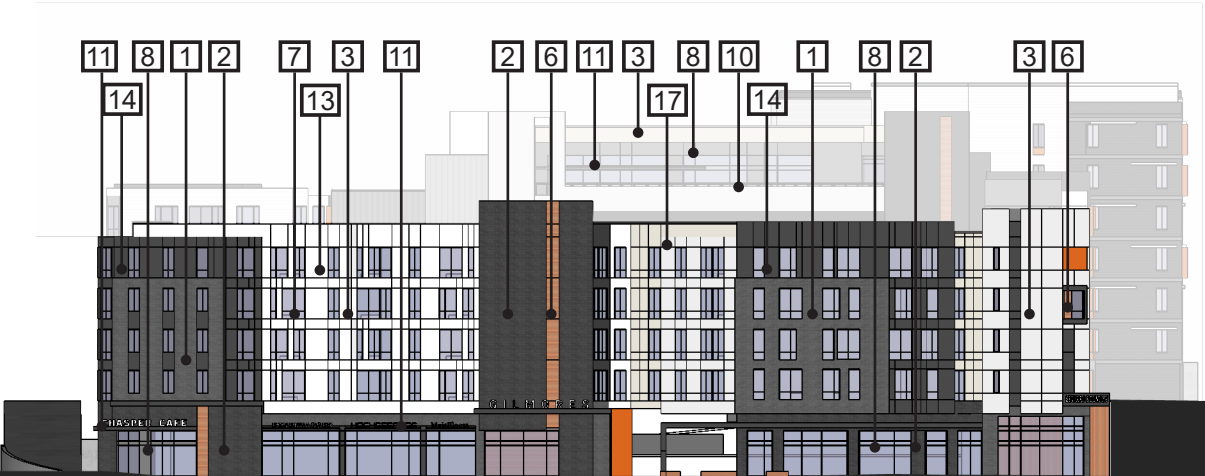


ELEVATIONS

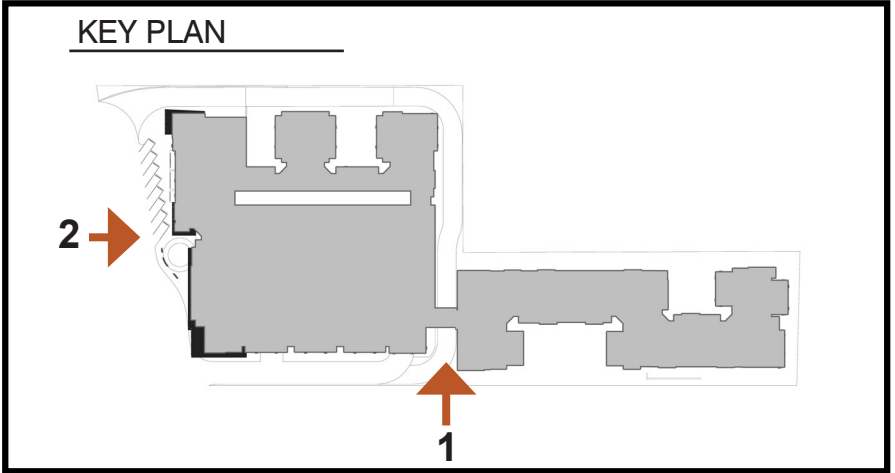
FINISH SCHEDULE					
1	BRICK VENEER 1 - LIGHT	4	CEMENTITIOUS LAP SIDING	7	VINYL WINDOWS
2	BRICK VENEER 2 - DARK	5	BOARD AND BATTEN	8	ALUMINUM STOREFRONT
3	CEMENTITIOUS PANEL	6	WOODTONE	9	BALCONY W/ MTL. PNL. RAILING
				10	MESH RAILING
				11	MTL. CANOPY
				12	TRIM BOARD
				13	PT-1
				14	PT-2
				15	PT-3
				16	PT-4
				17	PT-5
				18	PT-6



1 ELEVATION FACING SOUTH
SCALE: 1"=50'



2 ELEVATION FACING EAST
SCALE: 1"=50'



05/28/2019

ELEVATIONS

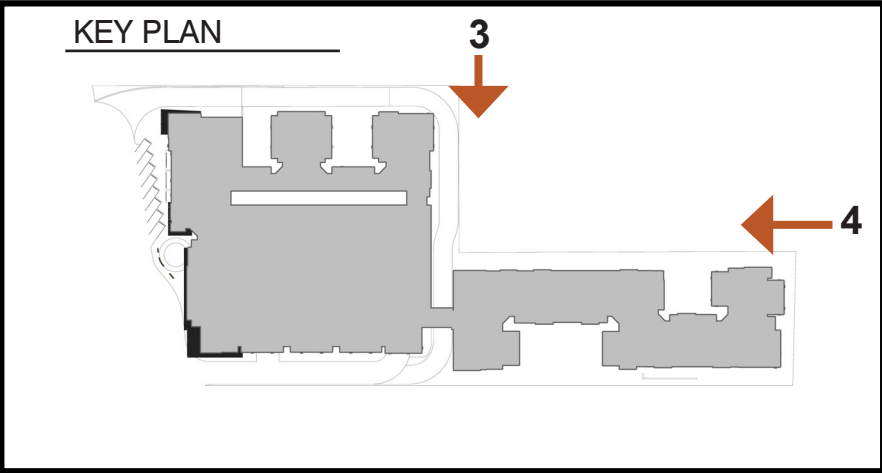
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				11	MTL. CANOPY
				12	TRIM BOARD
				13	PT-1
				14	PT-2
				15	PT-3
				16	PT-4
				17	PT-5
				18	PT-6



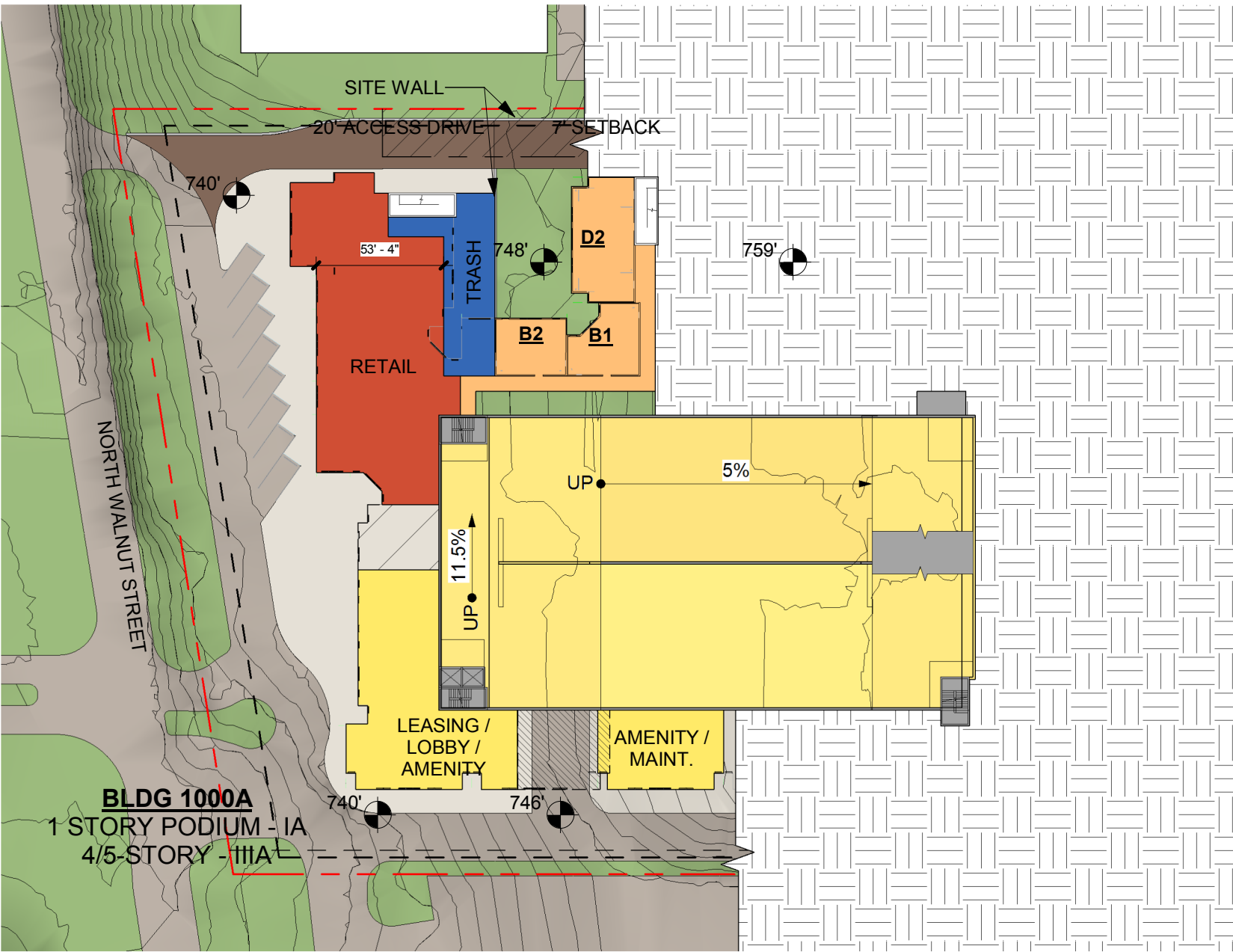
3 ELEVATION FACING NORTH
SCALE: 1"=50'



4 ELEVATION FACING WEST
SCALE: 1"=50'



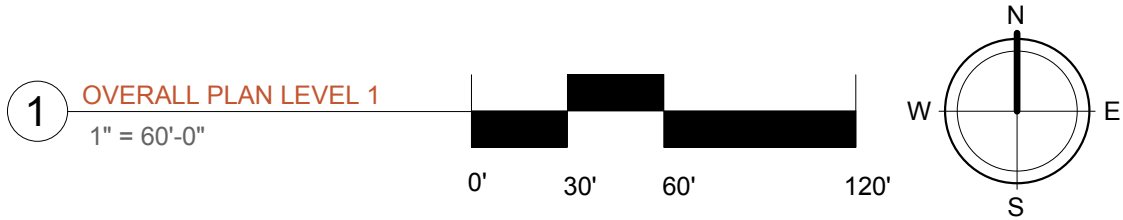
05/28/2019



N WALNUT BLOOMINGTON		5/28/2019
LOT COVERAGE CALCS		
ZONING	PARCEL ZONING OVERLAY ZONING	3.85 ACRES 167,844 SF
TOTAL RESIDENTIAL GROSS SF		330,600 SF
TOTAL AMENITY / LEASING / SERVICE		16,080 SF
RETAIL GROSS SF		6,700 SF
TOTAL SF		353,380 SF
81.67% RESIDENTIAL EFFICIENCY		270,007 SF
1,000 SF AVG. - TOTAL UNITS		270 UNITS
329 SF AVG. - TOTAL BEDS		820 BEDS
PARKING REQUIRED		TOTAL
RETAIL*		7 SP
RESIDENTIAL***		410 SP
TOTAL		417 SP
BICYCLE PARKING		
REQUIRED (1 PER 4 BEDS)		205 SP
PROVIDED		205 SP

* RETAIL PARKING 1.00 SP/ 1,000 SF
*** RESIDENTIAL PARKING AVG 0.50 SP/ 1.00 BED

UNIT MIX		4/29/2019	
UNIT TYPE	DESCRIPTION	TOTAL UNITS	TOTAL BEDS
M	MICRO STUDIO	34	34
1B	1 BEDROOM / 1 BATH	2	2
2B	2 BEDROOM / 2 BATH	76	152
4B	4 BEDROOM / 4 BATH	158	632
TOTAL UNITS		270	820



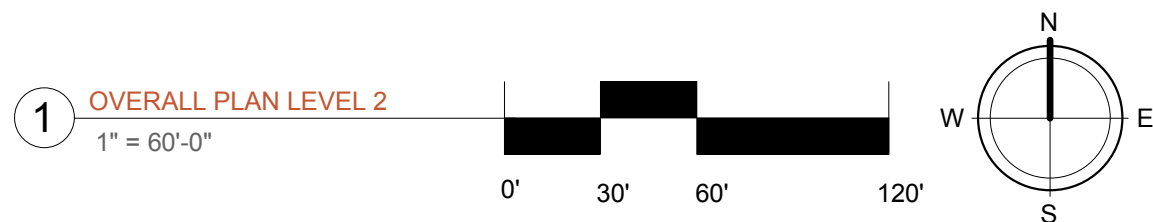
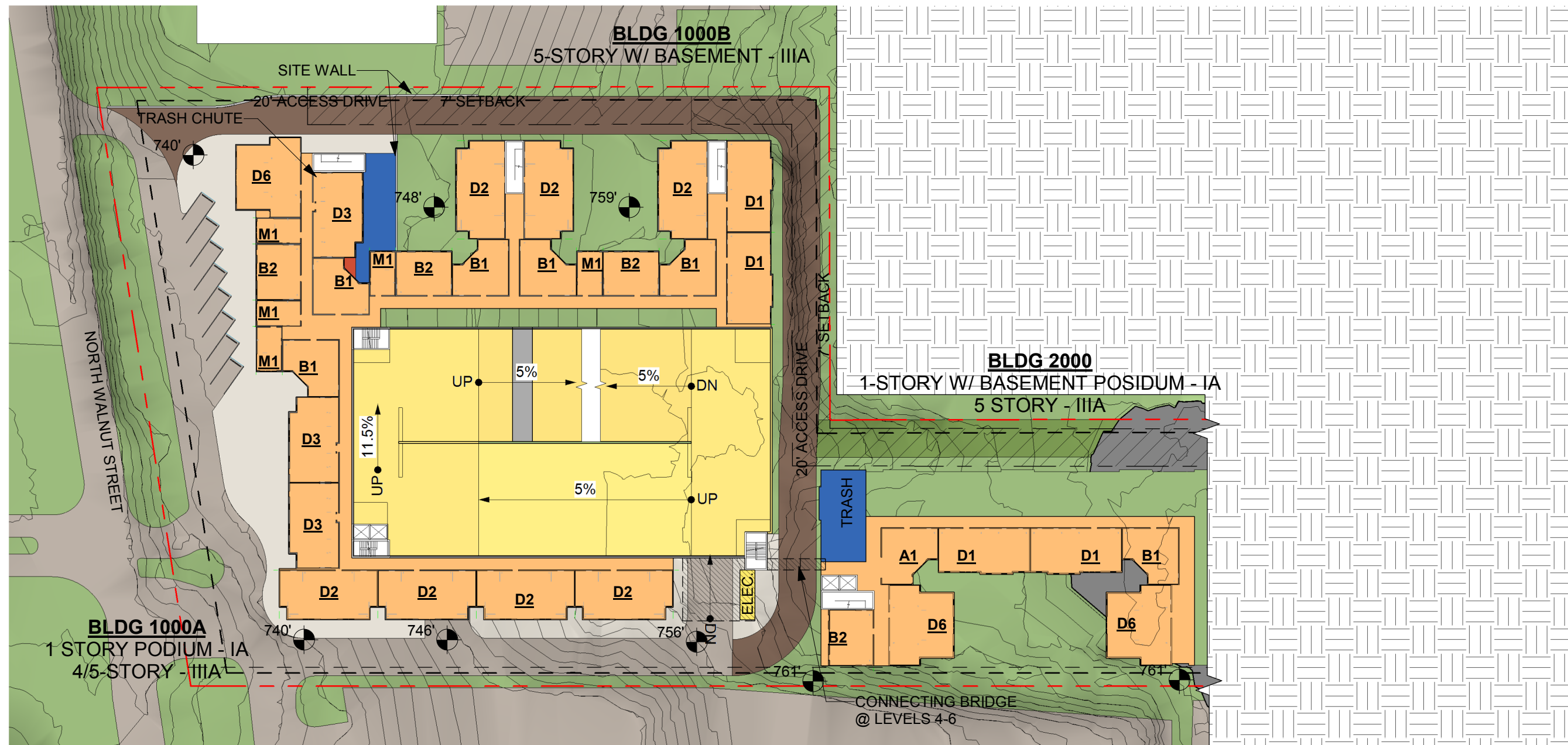
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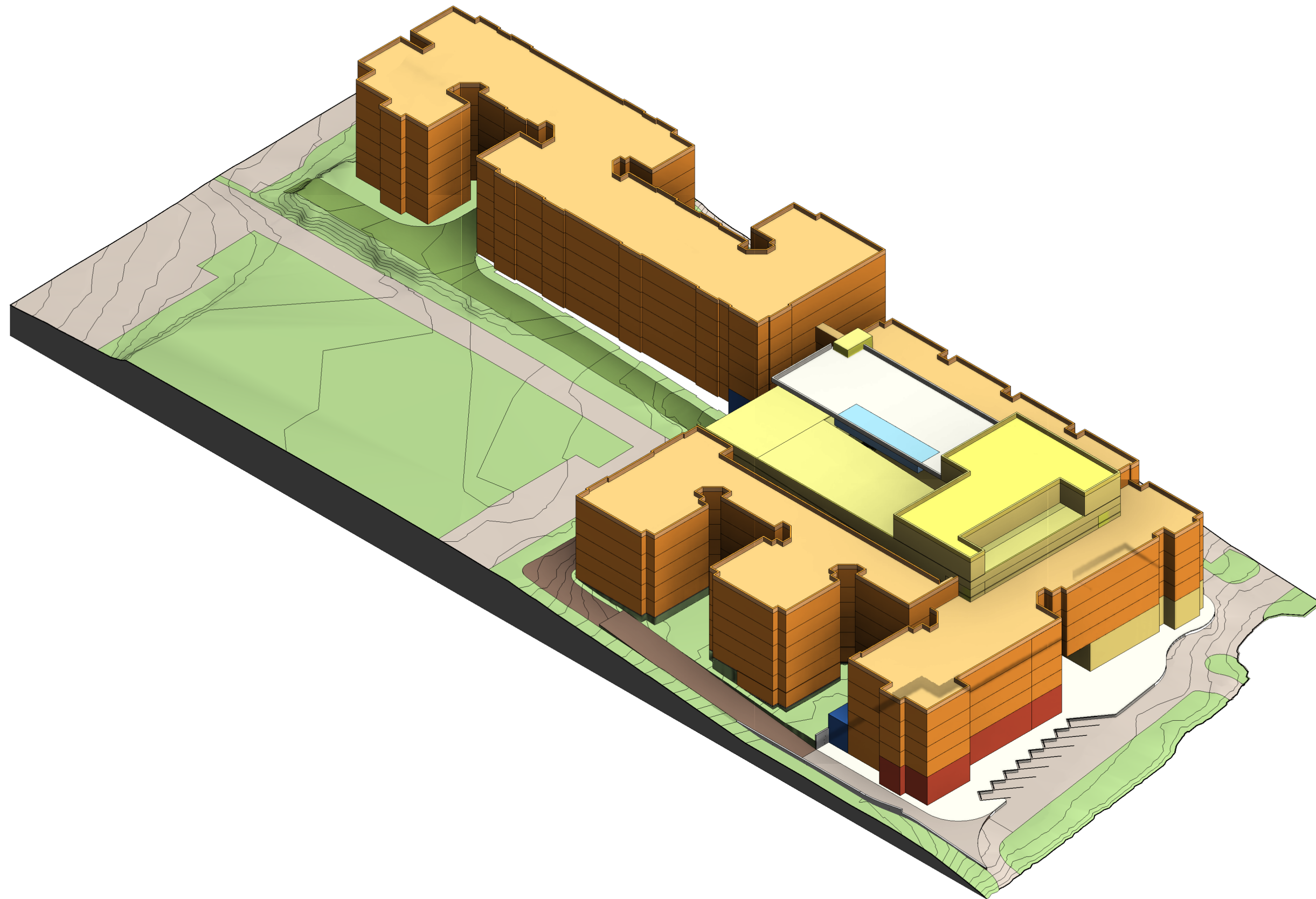


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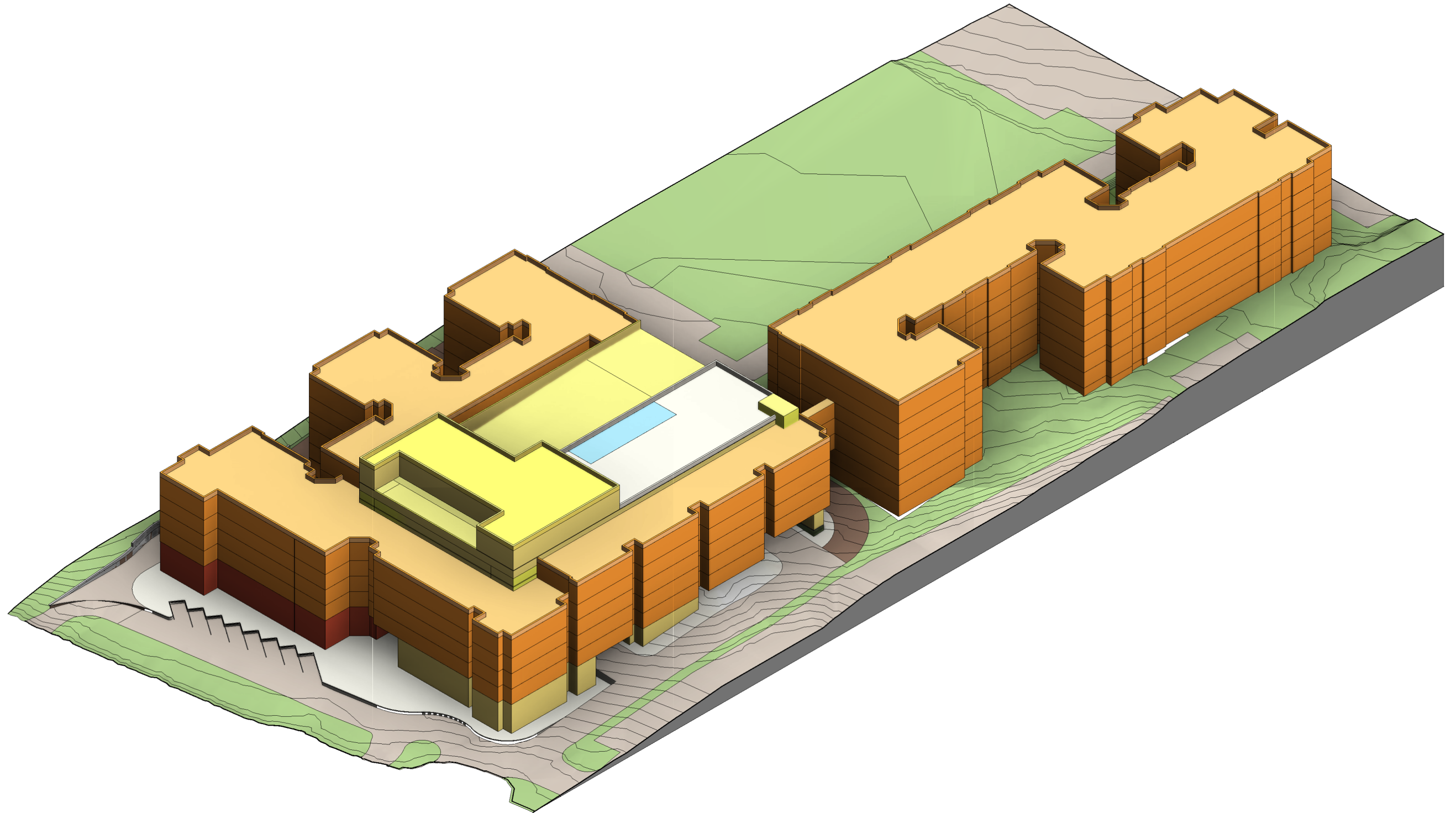
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May 24, 2019



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May 24, 2019



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ORDINANCE 19-13

**TO AMEND TITLE 15 OF THE BLOOMINGTON MUNICIPAL CODE ENTITLED
“VEHICLES & TRAFFIC”
Re: Amending Chapter 15.32.080 (No Parking Zones)**

WHEREAS, redundant text regarding parking on Walnut Grove and Forrest Avenues near Indiana University was not eliminated in Ordinance 18-26 and needs to be removed from the code; and

WHEREAS, parking is in great demand in the new Zones 4 and 5, and Dunn Street between Tenth and Seventh Streets is relatively wide and has room to add to the neighborhood parking supply, which serves the University Village Overlay as well as the residences within the zones;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. Section 15.32.080, Schedule M "No Parking Zones" shall be amended to delete the following street segments:

Street	From	To	Side of Street	Time of Restrict.
Dunn Street	Ninth Street	Tenth Street	East	Any Time
Dunn Street	Third Street	Ninth Street	West	Any Time
Forrest Avenue	Tenth Street	1 st Alley N. of Eleventh Street	East	Any Time
Walnut Grove	Tenth Street	Seventeenth Street	East/West	Any Time

SECTION 2. Section 15.32.080, Schedule M "No Parking Zones" shall be amended to add the following street segments:

Street	From	To	Side of Street	Time of Restrict.
Dunn Street	Third Street	Sixth Street	West	Any Time

SECTION 3. If any section, sentence or provision of this resolution, or the application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions or applications of this resolution which can be given effect without the invalid provision or application, and to this end the provisions of this resolution are declared to be severable.

PASSED by the Common Council of the City of Bloomington, Monroe County, Indiana upon this _____ day of _____, 2019

DAVE ROLLO, President
Bloomington Common Council

ATTEST:

NICOLE H. BOLDEN, Clerk
City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon
this _____ day of _____, 2019.

NICOLE BOLDEN, Clerk
City of Bloomington

SIGNED and APPROVED by me upon this _____ day of _____, 2019.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

This ordinance eliminates redundant or inapplicable no-parking code and creates new parking spaces on Dunn Street north of Seventh.

MEMO

Date: July 26, 2019

From: Steve Volan, Councilmember, District 6

Re: Ordinances 19-13 and -14, regarding “fixes” to parking

INTRODUCTION

These ordinances make minor changes to parking regulations. One anticipates the addition of parking to Dunn St, and fixes errors in code to reflect reality on the ground. The other removes restrictions on some seats on the Parking Commission to make them easier to fill.

ORD 19-13

This ordinance amends Title 15 to create new parking spaces on Dunn Street north of Seventh, and to eliminate redundant or inapplicable no-parking code. First, a word about what’s not in it.

The original section 1 was to modify the new Zone 6 to reasonably accommodate the Windfall Dancers organization by removing certain blocks from the new zone. I have spoken with Kay Olges of the not-for-profit arts group Windfall Dancers for several months regarding the challenge that the new Garden Hill Neighborhood Zone (GHNZ) would pose to their operations. Their headquarters, a building which they own, is located on the southern edge of the zone; the new restrictions would hamper their many different types of events: classes held during the day and evening, events on evenings and weekends. Windfall has a parking lot with 13 spaces. All other off-street parking nearby is private and subject to tow. Because the zone would be operational most of the week and overnight, and there are no meters anywhere nearby, there is little to no opportunity for more than 13 cars there at a time. Since many of their events attract 50 people or more, they consider the zone an existential threat to their operations.

To that end, the original Section 1 removed the blocks immediately to the west, north and east from restriction. The Garden Hill Neighborhood Association opposed this change, having fought long and hard to control parking in their neighborhood. Windfall, meanwhile, is open to using a system of temporary permits instead; Michelle Wahl, the city’s new Parking Services Director, is confident that they would be able to accommodate Windfall’s needs in that way. (The section also eliminated several blocks in the new zone already marked as No Parking, but there is no harm to the new zone overlaying those unparked blocks.)

The current Section 1 anticipates great demand for parking in the new Zones 4 and 5. Dunn Street between Tenth and Seventh Streets is relatively wide and can be a speedway. It has room to add to the neighborhood parking supply, which serves the University Village Overlay as well as the residences within the zones. Adding parking here would increase the supply while slowing down speeding car traffic.

Section 2 eliminates redundant code regarding parking on Walnut Grove and Forrest Avenues near Indiana University which was not eliminated in Ordinance 18-26.

ORD 19-14

This ordinance amends Title 2 to loosen requirements for certain seats on the Parking Commission which have proven difficult to fill or keep filled. The Commission has in its first two and a half years never had nine members present; it currently has three vacancies. Seats that require a merchant to be in the zones regulated by parking meters, or require a resident to live in either the meter zone or one of the neighborhood zones, would have their residency requirement expanded to anywhere within the city limits.

#

15.32.080 - No parking zones.

The streets or parts of streets described in Schedule M, attached hereto and made a part hereof, are designated as no parking zones. When appropriate signs are erected, no person shall park a vehicle upon any of the streets or parts of streets at the times designated in the schedule.

SCHEDULE M				
NO PARKING ZONES				
Street	From	To	Side of Street	Time of Restrict.
Adams Street	Kirkwood Avenue	Eleventh Street	East/West	Any Time
Alice Avenue	Grant Street	Dunn Street	North/South	Any Time
Allen Street	Bloomfield Road	300' East of Bloomfield Road	North/South	Any Time
Allen Street	550' W. of Fairview	Rogers Street	South	Any Time
Allen Street	Rogers Street	Madison Street	North/South	Any Time
Allen Street	Walnut Street	Lincoln Street	North	Any Time
Arbor Ridge Way	Sudbury Drive	End of City Street	North/South	Any Time
Arden Drive	Drive at 2310 E. Arden Drive	Drive as 2314 E. Arden Drive	South	Any Time
Atwater Avenue	Third Street	High Street	North/South	Any Time
Ballantine Road	Atwater Avenue	50' N. of Atwater Ave.	East/West	Any Time
Ballantine Road	Atwater Avenue	50' S. of Atwater Ave.	East/West	Any Time

Ballantine Road	First Street	Third Street	East	Any Time
Ballantine Road	Third Street	50' S. of Third Street	East/West	Any Time
Blair Avenue	Eleventh Street	50' N. of 11th Street	West	Any Time
Briar Cliff	Kingsley	Kinser Park	North	Any Time
Brookdale Drive	N. Kinser Pike	Termination of City Street	Both	Any Time
Brown Avenue	Coolidge Drive	Graham Drive	East	Any Time
Browncliff Street	Matlock Road	Termination of City Street	Both	Any Time
Bryan Avenue	Third Street	Fourth Street	East	Any Time
Bryan Avenue	Fifth Street	End of Bryan Ave N. of Seventh Street	East	Any Time
Bryan Avenue	Third Street	End of Bryan Ave N. of Seventh Street	West	Any Time
Buckner Street	Smith Road	Third Street	West	Any Time
Canada Drive	The Stands Road	364' East	South	Any Time
Clark Street	Third Street	Seventh Street	West	Any Time
Clifton Avenue	Atwater Avenue	Hunter Avenue	West	Any Time
Clifton Avenue	Third Street	Atwater Avenue	West	Any Time
College Avenue	65' South of Ninth Street	Ninth Street	West	Any Time
College Avenue	212' North of Tenth Street	2nd Alley North of Tenth Street	West	Any Time

College Avenue	500' S. of 14th St.	Fourteenth Street	East/West	Any Time
College Avenue	Dodds Street	Fourth Street	West	Any Time
College Avenue	Dodds Street	Second Street	East	Any Time
College Avenue	Eighth Street	80' North of Eighth Street	West	Any Time
College Avenue	Ninth Street	50' South of Ninth Street	West	Any Time
College Avenue	Second Street	1st Alley to the north	East	Any Time
College Avenue	Seventeenth Street	Cascade Road	West	Any Time
College Avenue	Seventh Street	2nd Alley N. of 10th St.	East	Any Time
College Mall Road	Second Street	Third Street	East/West	Any Time
Coolidge Drive	Ford Avenue	Brown Avenue	North	Any Time
Cory Lane	Third Street	660' South of Third Street	East/West	Any Time
Cottage Grove	Monroe Street	Diamond Street	South	Any Time
Cottage Grove Avenue	1st Drive West of Forrest Avenue	Walnut Grove	North	Any Time
Cottage Grove Avenue	1st Alley West of Lincoln Street	60' West of 1st Alley W. of Lincoln Street	South	Any Time
Cottage Grove Avenue	Lincoln Street	Forrest Avenue	South	Any Time
Cottage Grove Avenue	Walnut Street	Lincoln Street	North	Any Time
Covenanter Street	450' East of Auto	1200' East of Auto	West	Any Time

	Mall Road	Mall Road		
Davis Street	End of Davis Street West of Rogers Street	Rogers Street	North	Any Time
Davison Street	Smith Avenue	Third Street	West	Any Time
Diamond Street	West Cottage Grove	Eleventh Street	West	Any Time
Dixie Street	End of Dixie Street West of Fairview Street	Rogers Street	South	Any Time
Dodds Street	Rogers Street	Morton Street	North	Any Time
Driscoll Street	Rogers Street	Madison Street	North/South	Any Time
Driscoll Street	Walnut Street	Washington Street	North	Any Time
Dunn Street	11th Street	Railroad Tracks North of 12th St.	East	Any Time
Dunn Street	Alley North of Tenth Street	Cottage Grove	West	Any Time
Dunn Street	Cottage Grove	Eleventh Street	East	Any Time
Dunn Street	Eleventh Street	Twelfth Street	West	Any Time
Dunn Street	Grimes Lane	Hillside Drive	West	Any Time
Dunn Street	Lakewood Drive	Cascade Road	East/West	Any Time
Dunn Street	Ninth Street	Tenth Street	East	Any Time
Dunn Street	North Property line at 330 South Dunn Street	South Property line at 334 South Dunn Street	West	Any Time

Dunn Street	Second Street	50' North of Smith Avenue	West	Any Time
Dunn Street	Seventeenth Street	SR 45—SR 46	East	Any Time
Dunn Street	Smith Avenue	Third Street	East	Any Time
Dunn Street	Tenth Street	Alley North of Tenth Street	East	Any Time
Dunn Street	Third Street	Ninth Street	West	Any Time
Dunn Street	Thirteenth Street	Saville Street	West	Any Time
<u>Dunn Street</u>	<u>Third Street</u>	<u>Sixth Street</u>	<u>West</u>	<u>Any Time</u>
East/West alley between Fourth Street & Kirkwood Avenue and Dunn & Indiana	66' E. of Dunn Street	132' E. of Dunn Street	South	Any Time
Eastgate Lane	450' E. & S. of SR 46	Termination of City St.	East/North	Any Time
Eastgate Lane	SR 46	170' E. & S. of SR 46	North/East	Any Time
Eastgate Lane	SR 46	Termination of City St.	West/South	Any Time
Eastside Drive	First Street	Second Street	West	Any Time
Eighteenth Street	150' east of Lincoln Street	Lincoln Street	North/South	Any Time
Eighteenth Street	Grant Street	Lincoln Street	Both	Any Time
Eighth Street	247' East of Rogers Street	Entrance to Lot 11	South	Any Time
Eighth Street	247' East of Rogers	Entrance to Lot 13	North	Any Time

	Street			
Eighth Street	End of Eighth Street West of Elm Street	Fairview Street	South	Any Time
Eighth Street	Fountain Drive	Spring Street	North	Any Time
Eighth Street	Jefferson Street	Overhill Drive	Both	Any Time
Eleventh Street	College Avenue	Walnut Street	North	Any Time
Eleventh Street	Forrest Avenue	Walnut Grove Street	North	Any Time
Eleventh Street	Indiana Avenue	Walnut Grove Street	South	Any Time
Eleventh Street	Monroe Street	Walnut Street	South	Any Time
Eleventh Street	Morton Street	40' east of Morton Street	North	Any Time
Eleventh Street	Summit Street	Monroe Street	North	Any Time
Eleventh Street	Washington Street	Indiana Avenue	North	Any Time
Eleventh Street	Woodlawn Avenue	Forrest Avenue	North	Any Time
Elm Street	Sixth Street	Eighth Street	West	Any Time
Euclid Street	Second Street	Third Street	West	Any Time
Faculty Avenue	Atwater Avenue	50' N. of Atwater	East	Any Time
Faculty Avenue	Atwater Avenue	50' S. of Atwater	East	Any Time
Faculty Avenue	Hunter Avenue	Third Street	West	Any Time
Faculty Avenue	Third Street	50' S. of Third Street	East	Any Time
Fairfield Drive	Franklin Road	100' North of	East/West	Any Time

		Holiday Drive		
Fairview Street	Dodds Street	50' North of Dodds Street	West	Any Time
Fairview Street	Eighth Street	Ninth Street	West	Any Time
Fairview Street	Eleventh Street	Thirteenth Street	East	Any Time
Fairview Street	Howe Street	Third Street	West	Any Time
Fairview Street	Ninth Street	Tenth Street	West	Any Time
Fairview Street	Seventh Street	Eighth Street	East	Any Time
Fairview Street	Sixth Street	Kirkwood Avenue	East	Any Time
Fairview Street	Tenth Street	Eleventh Street	West	Any Time
Fairview Street	Wylie Street	130' South of Wylie Street	West	Any Time
Fee Lane	Seventeenth Street	SR 45 - SR 46	East/West	Any Time
Fee Lane	Tenth Street	Seventeenth Street	East/West	Any Time
Fess Avenue	90' S. of Atwater	Third Street	East	Any Time
Fess Avenue	Cottage Grove Avenue	Eleventh Street	West	Any Time
Fess Avenue	First Street	University Street	West	Any Time
Fess Avenue	Hunter Avenue	Third Street	West	Any Time
Fess Avenue	Maxwell Lane	First Street	East	Any Time
Fess Avenue	Seventh Street	Twelfth Street	East	Any Time
Fess Avenue	Thirteenth	Seventeenth Street	East/West	Any Time

Fess Avenue	Twelfth Street	Thirteenth Street	Both	Any Time
Fess Avenue	University Street	Hunter Avenue	East	Any Time
Fifteenth Street	College Avenue	Walnut Street	North	Any Time
Fifteenth Street	Walnut Street	Dunn Street	South	Any Time
Fifteenth Street	Woodlawn Avenue	Forrest Avenue	South	Any Time
Fifteenth Street	Forrest Avenue	Alley West of Forrest Avenue	North	Any Time
Fifth Street	Hillsdale Drive	Overhill Drive	North	Any Time
Fifth Street	Overhill Drive	East to Dead End	North	Any Time
Fifth Street	Union Street	Hillsdale Drive	Both	Any Time
First Street	208' East of Mitchell Street	High Street	North	Any Time
First Street	Henderson Street	Fess Avenue	South	Any Time
First Street	High Street	Dead-End approximately 160 feet east of High Street	North	Any Time
First Street	Mitchell Street	177' East of Mitchell Street	North	Any Time
First Street	Walker Street	Morton Street	North/South	Any Time
First Street	Walnut Street	Jordan Avenue	North	Any Time
Forrest Avenue	Tenth Street	1st Alley N. of Eleventh Street	East	Any Time
Forrest Avenue	Tenth Street	Dead End	East/West	Any Time

Forrest Avenue	Thirteenth Street	57' N. of 15th Street	Both	Any Time
Fourteenth Street	College Avenue	Walnut Street	South	Any Time
Fourteenth Street	Fess Avenue	Woodlawn Avenue	South	Any Time
Fourteenth Street	Forrest Avenue	Walnut Grove	North/South	Any Time
Fourteenth Street	Indiana Avenue	Woodlawn Avenue	North	Any Time
Fourteenth Street	Madison Street	150' E. of Walnut St.	North	Any Time
Fourteenth Street	Walnut Street	Indiana Avenue	South	Any Time
Fourteenth Street	Woodlawn Avenue	Forrest Avenue	North	Any Time
Fourth Street	66' W. of Morton Street	Morton Street	South	Any Time
Fourth Street	210' E. of Morton Street	College Avenue	South	Any Time
Fourth Street	280' west of Hillsdale Drive	Hillsdale Drive	Both	Any Time
Fourth Street	Elm Street	Jackson Street	North	Any Time
Fourth Street	Lincoln Street	100' W. of Grant St.	North	Any Time
Fourth Street	Roosevelt Street	Clark Street	Both	Any Time
Fourth Street	Union Street	Jefferson Street	Both	Any Time
Franklin Road	Third Street	End of Franklin Road	West	Any Time
Franklin Road	Third Street	North Corporation Lane	North/South	Any Time
Fritz Drive	Saville Avenue	End of city street	East	Any Time

Frontage Road	Wynnedale Drive	330' West of Wynnedale Drive	South	5:00 p.m. to 8:00 a.m. Monday - Friday
Gentry Street	Fourth Street	Kirkwood Avenue	East	Any Time
Gentry Street	Fourth Street	75' south of Kirkwood Avenue	West	Any Time
Glandore Drive	Marquis Drive	End of Street	West	Any Time
Grant Street	Alice Street	210' South of Tenth Street	East	Any Time
Grant Street	165' North of Seventh Street	Alice Street	West	Any Time
Grant Street	210' South of Tenth Street	Tenth Street	West	Any Time
Grant Street	Driscoll Street	Wilson Street	East	Any Time
Grant Street	Eighteenth Street	Nineteenth Street	Both	Any Time
Grant Street	Eighteenth Street	Nineteenth Street	East	Any Time
Grant Street	Grimes Lane	Driscoll Street	West	Any Time
Grant Street	Tenth Street	Twelfth Street	East	Any Time
Grant Street	Twelfth Street	North to Dead End	West	Any Time
Grant Street	University Street	165' North of Seventh Street	East	Any Time
Grant Street	Wilson Street	Hillside Drive	West	Any Time
Grimes Lane	Palmer Avenue	Henderson Street	North	Any Time
Grimes Lane	Rogers Street	Palmer Avenue	North	Any Time

Grimes Lane	Woodlawn Avenue	100' East of Henderson Street	South	Any Time
Grimes Lane	Woodlawn Avenue	Henderson Street	North	Any Time
Harold Street	Eighth Street	Ninth Street	East/West	Any Time
Hawthorne Drive	70' S. of Atwater	70' N. of Atwater Ave.	East/West	Any Time
Hawthorne Drive	75' S. Hunter Avenue	Hunter Avenue	East	Any Time
Hawthorne Drive	Third Street	105' South of Third Street	East	Any Time
Hawthorne Drive	Third St.	Atwater Ave.	West	Any Time
Hawthorne Drive	University Street	Second Street	East	Any Time
Headley Road	Matlock Road	Hinkle Road	East/West	Any Time
Heather Drive	Laurel Court	Grasstree Court	North	Any Time
Heather Drive	Walnut Street Pike	Laurel Court	North	Any Time
Henderson Street	Davis Street	Atwater Avenue	West	Any Time
Henderson Street	Davis Street	Third Street	East	Any Time
Henderson Street	Driscoll Drive	Wilson Drive	East	Any Time
Henderson Street	Grimes Lane	Driscoll Drive	East/West	Any Time
Henderson Street	Winslow Road	Miller Drive	East/West	Any Time
High Street	Atwater Avenue	Third Street	East	Any Time
Highland Avenue	50' S. of Atwater	50' N. of Atwater	West	Any Time

Highland Avenue	50' S. of Atwater	Third Street	East	Any Time
Highland Avenue	Hillside Drive	Thorton	East	Any Time
Highland Avenue	Third Street	105' South of Third Street	West	Any Time
Hillsdale Court	Hillsdale Drive	End of Cul-de-Sac	North/South	Any Time
Hillsdale Drive	Eighth Street	Dead End North of Eighth Street	East	Any Time
Hillside Drive	Park Avenue	Woodlawn Avenue	South	Any Time
Hillside Drive	Rogers Street	Madison Street	North/South	Any Time
Hillside Drive	Walnut Street	Henderson Street	North/South	Any Time
Hillside Drive	Walnut Street	Monon Street	South	Any Time
Hillside Drive	Walnut Street	The first alley west of Walnut Street	North	Any Time
Hillside Drive	Woodlawn Avenue	100' E. of Huntington Drive	South	Any Time
Hillside Drive	Woodlawn Avenue	Huntington Drive	North	Any Time
Hinkle Road	.37 miles North of Headley Road	.47 miles North of Headley Road	East	Any Time
Hinkle Road	Headley Road	Corporate Limits	East/West	Any Time
Howe Street	Walker Street	Madison Street	South	Any Time
Hunter Avenue	Henderson Street	Hawthorne Street	South	Any Time
Hunter Avenue	Jordan Avenue	Mitchell Street	Both	Any Time
Huntington Drive	Hillside Drive	Miller Drive	East	Any Time

Illinois Street	Twelfth Street	Fifteenth Street	West	Any Time
Indiana Avenue	12th Street	First alley to the north	East	Any Time
Indiana Avenue	50' S. of Fourth St.	Fourth Street	West	Any Time
Indiana Avenue	Cottage Grove Avenue	Fifteenth Street	East	9 a.m. to midnight on Saturdays when an I.U. football game is at home
Indiana Avenue	Eighth Street	50' S. of Eighth St.	West	Any Time
Indiana Avenue	Fifteenth Street	Seventeenth Street	East	Any Time
Indiana Avenue	Henderson Street	80' N. of Cottage Grove	East	Any Time
Indiana Avenue	Henderson Street	140' N. of Third St.	West	Any Time
Indiana Avenue	Kirkwood Avenue	Seventh Street	West	Any Time
Indiana Avenue	Ninth Street	Seventeenth Street	West	Any Time
Indiana Avenue	Thirteenth Street	85' North of Thirteenth Street	East	Any Time
Jackson Street	Eighth Street	Ninth Street	West	Any Time
Jackson Street	Kirkwood Avenue	1st Alley N. of Kirkwood Avenue	Both	Any Time
Jackson Street	Ninth Street	40' South of Ninth Street	East	Any Time
Jackson Street	Smith Avenue	Kirkwood Avenue	West	Any Time
Jackson Street	Thirteenth Street	Fifteenth Street	West	Any Time

Jefferson Street	Seventh Street	Tenth Street	East	Any Time
Jefferson Street	Third Street	Fourth Street	West	Any Time
John Hinkle Place	Tenth Street	Pete Ellis Drive	Both	Any Time
Jordan Avenue	1st Alley S. of 3rd	Third Street	East	Any Time
Jordan Avenue	20' S. of the first alley N. of Second Street	The first alley N. of Second Street	East	Any Time
Jordan Avenue	650' N. of 3rd Street	Seventeenth Street	East	Any Time
Jordan Avenue	Atwater Avenue	58' North of Atwater Avenue	East	Any Time
Jordan Avenue	Atwater Avenue	Tenth Street	West	Any Time
Jordan Avenue	Second Street	Atwater Avenue	West	Any Time
Jordan Avenue	Second Street	First Street	West	Any Time
Kennwood	Madison Street	Woodburn	North/South	Any Time
Kingston Drive	Third Street	Longview Avenue	East/West	Any Time
Kinser Pike	Bloomington High School North Entrance	Acuff Road	East/West	Any Time
Kinser Pike	Briarcliff Drive	Main Entrance to	East/North	Any Time
Kirkwood Avenue	Adams Street	100' E. of Rogers St.	South	Any Time
Kirkwood Avenue	Adams Street	Elm Street	North	Any Time
Kirkwood Avenue	Indiana Avenue	135' West of Indiana Avenue	North	Any Time

Kirkwood Avenue	Indiana Avenue	143' West of Indiana Avenue	South	Any Time
Kirkwood Avenue	Rogers Street	1st Alley West of Rogers Street	North	Any Time
Landmark Avenue	Bloomfield Road	Third Street	East	Any Time
Larkspur Lane	Allen Street	Woodhill Drive	East	Any Time
Laurel Court	Heather Drive	Heather Drive	Odd Address Side	Any Time
Liberty Drive	215' South of Third Street	Corporate Boundary	West	Any Time
Liberty Drive	Third Street	150' South of Third Street	West	Any Time
Liberty Drive	Third Street	Corporate Boundary	East	Any Time
Lincoln Avenue	50' S. of Sixth Street	Sixth Street	West	Any Time
Lincoln Street	1st Alley North of University Street	Second Street	East	Any Time
Lincoln Street	50' S. of crosswalk	Crosswalk bet. Smith & Third	Both	Any Time
Lincoln Street	82' S. of First Street	First Street	East	Any Time
Lincoln Street	110' South of Twentieth Street	Twentieth Street	West	Any Time
Lincoln Street	351' North of Seventeenth Street	Nineteenth Street	East	Any Time
Lincoln Street	Dodds Street	50' South of Dodds	Both	Any Time

		St.		
Lincoln Street	First Street	Second Street	West	Any Time
Lincoln Street	Nineteenth Street	Twentieth Street	East	Any Time
Lincoln Street	S. curbline on Dodds	50' South	West	Any Time
Lincoln Street	Seventeenth Street	72' North of Seventeenth Street	East	Any Time
Lincoln Street	Seventeenth Street	Nineteenth Street	West	Any Time
Lincoln Street	Third Street	Fourth Street	West	Any Time
Lincoln Street	Twelfth Street	North to Dead End	West	Any Time
Lincoln Street	University Street	2 parking spaces South	East	Any Time
Lincoln Street	Fourteenth Street	Fifteenth Street	East	Any Time
Lismore Drive	Seventeenth Street	Marquis Drive	West	Any Time
Longview Street	Pete Ellis Drive	Glenwood Avenue West	North/South	Any Time
Madison Street	186' North of Sixth Street	Seventh Street	East	Any Time
Madison Street	End of Madison South of Allen Street	Dodds Street	East	Any Time
Madison Street	Fifteenth Street	Fourteenth Street	West	Any Time
Madison Street	Second Street	Fourth Street	East	Any Time
Madison Street	Sixth Street	1st Alley North of	West	2:00 p.m. to 7:00

		Sixth Street		p.m. on Tuesdays from June through September
Madison Street	Sixth Street	142' North of Sixth Street	East	Any Time
Madison Street	Third Street	85' N. of Third Street	West	Any Time
Madison Street	Thirteenth Street	Seventeenth Street	East	Any Time
Maple Street	Eleventh Street	Fifteenth Street	East	Any Time
Maple Street	Second Street	Ninth Street	West	Any Time
Maple Street	Thirteenth Street	Fifteenth Street	West	Any Time
Marquis Drive	Crescent Road	End of Street	South	Any Time
Martha Street	SR 45/46	Dead End	West/South	Any Time
Matlock Road	SR 45/46	Headley Road	North/South	Any Time
Maxwell Lane	Henderson Street	Manor Road	North	Any Time
Maxwell Street	Hillside Drive	160' South of Hillside Drive	East/West	Any Time
Miller Drive	Henderson Street	Highland Avenue	Both	Any Time
Mitchell Street	Hunter Avenue	Atwater Avenue	East	Any Time
Mitchell Street	East Maxwell Lane	Southdowns Drive	East/West	Any Time
Moravec Way	Diamond Street	40' South of the B- Line Trail	Both	Any Time
Moravec Way	Southwest corner of 977 Moravec	East property line of 936 Moravec Way	East/South	Any time

	Way Property Line			
Moravec Way	South Sidewalk Ramp Adjacent to 917 Moravec Way	North Sidewalk Ramp Adjacent to 917 Moravec Way	West	Any Time
Morningside Drive	East Third Street (East Entrance)	675 feet North of East Third Street	East	Any Time
Morton Street	100' South of Eighth Street	280' North of Eighth Street	West	Any Time
Morton Street	Drive at 400' N. Martin	15' South	East	Any Time
Morton Street	First Bumpout North of Tenth Street	Tenth Street	West	Any Time
Morton Street	First Street	Second Street	West	Any Time
Morton Street	Grimes Lane	Dodds Street	East/West	Any Time
Morton Street	Second Street	Smith Avenue	West	Any Time
Morton Street	Tenth Street	1st Alley South of Tenth Street	East	Any Time
Nineteenth Street	140' West of Washington Street	Washington Street	South	Any Time
Nineteenth Street	Lincoln Street	Dunn Street	North	Any Time
Nineteenth Street	Walnut Street	135' East of Walnut Street	South	Any Time
Ninth Street	126' East of Fess Street	110' West of Park Street	North/South	Any Time
Ninth Street	College Avenue	Morton Street	North	Any Time

Ninth Street	College Avenue	Walnut Street	South	Any Time
Ninth Street	Elm Street	Fairview Street	North	Any Time
Ninth Street	Fairview Street	Jackson Street	North	Any Time
Ninth Street	Jackson Street	40' West of Jackson Street	South	Any Time
Ninth Street	Park Avenue	Woodlawn Avenue	North	Any Time
North/South alley bet. 1st Street & University and Lincoln & Grant	First Street	70' N. of 1st Alley North of 1st Street	East	Any Time
Olive Street	Thornton Road	Miller Drive	East/West	Any Time
Park Avenue	First Street	Atwater Avenue	East	Any Time
Park Avenue	Grimes Lane	End of Park Avenue	West	Any Time
Park Avenue	Seventh Street	Twelfth Street	East	Any Time
Park Ridge Road	Third Street	Morningside Drive	East	Any Time
Park Ridge Road	Third Street	Saratoga Drive	West	Any Time
Petal Court	Sunflower Drive	End of Cul-de-Sac	North/South	Any Time
Pete Ellis Drive	Third Street	Bike Route	East/West	Any Time
Pine Street	Kirkwood Avenue	Sixth Street	West	Any Time
Pine Street	Sixth Street	Seventh Street	East	Any Time
Prospect Street	Jackson Street	Madison Street	South	Any Time
Prow Avenue	Ninth Street	Tenth Street	West	Any Time
Rogers Street	90' South of Eighth	95' North of Eighth	West	Any Time

	Street	Street		
Rogers Street	250' S. of 2nd St.	150' N. of 2nd Street	East	Any Time
Rogers Street	300' South of Hillside Drive	75' North of Sixth Street	West	Any Time
Rogers Street	Fourth Street	Kirkwood Avenue	East	Any Time
Rogers Street	Hillside Drive	Patterson Drive	East	Any Time
Rogers Street	Third Street	Thirteenth Street	East	Any Time
Roosevelt Street	Fourth Street	Fifth Street	East/West	Any Time
Roosevelt Street	Third Street	288' North of Third Street	East	Any Time
Roosevelt Street	Third Street	295' North of Third Street	West	Any Time
Rose Avenue	Atwater Avenue	Third Street	East	Any Time
Rose Avenue	Hunter Avenue	Atwater Avenue	East	Any Time
Saville Avenue	Dunn Street	Fritz Drive	South	Any Time
Second Street	93' West of Washington St.	Washington Street	South	Any Time
Second Street	Eastside Drive	Jordan Avenue	North	Any Time
Second Street	Fess Avenue	Park Avenue	South	Any Time
Second Street	Grant Avenue	Henderson Street	North	Any Time
Second Street	Henderson Street	Fess Avenue	North	Any Time
Second Street	High Street	Rose Avenue	North	Any Time

Second Street	High Street	Woodcrest Drive	North/South	Any Time
Second Street	Jordan Avenue	High Street	South	Any Time
Second Street	Park Avenue	Ballantine Road	North	Any Time
Second Street	Park Street	54' E. of Park Street	South	Any Time
Second Street	Walker Street	Walnut Street	South	Any Time
Second Street	Walker Street	Washington Street	North	Any Time
Second Street	Washington Street	Grant Street	South	Any Time
Seventeenth Street	2nd Driveway East of Fee Lane	SR 45/46 Bypass	North	Any Time
Seventeenth Street	Fee Lane	1st Driveway East of Fee Lane	North	Any Time
Seventeenth Street	Walnut Street	Fee Lane	North	Any Time
Seventeenth Street	Walnut Street	SR 45/46 Bypass	South	Any Time
Seventh Street	Oak Street	103' East of Oak Street	North	Any Time
Seventh Street	187' East of Oak Street	Elm Street	North	Any Time
Seventh Street	114' East of Elm Street	307' East of Elm Street	North	Any Time
Seventh Street	Waldron Street	112' East of Waldron Street	North	Any Time
Seventh Street	173' East of Waldron Street	324' East of Waldron Street	North	Any Time
Seventh Street	90' East of Oak	243' East of Oak	South	Any Time

	Street	Street		
Seventh Street	Elm Street	125' East of Elm Street	South	Any Time
Seventh Street	292' East of Elm Street	Waldron Street	South	Any Time
Seventh Street	74' East of Waldron Street	200' East of Waldron Street	South	Any Time
Seventh Street	254' East of Waldron Street	Maple Street	South	Any Time
Seventh Street	Dunn Street	Woodlawn Avenue	North	Any Time
Seventh Street	Indiana Street	Woodlawn Avenue	South	Any Time
Seventh Street	Morton Street	College Avenue	South	Any Time
Seventh Street	Union Street	Overhill Drive	Both	Any Time
Sheridan Drive	Woodlawn Avenue	Jordan Avenue	South	Any Time
Sixteenth Street	59' West of Dunn Street	Dunn Street	North	Any Time
Sixteenth Street	Madison Street	Dunn Street	South	Any Time
Sixth Street	1st Alley E. of Walnut	Washington Street	North	Any Time
Sixth Street	2nd Alley East of Oak Street	Elm Street	South	Any Time
Sixth Street	2nd Alley East of Pine Street	Oak Street	South	Any Time
Sixth Street	Adams Street	2nd Alley East of Pine Street	North	Any Time

Sixth Street	Dunn Street	240' West	North	Any Time
Sixth Street	Maple Street	40' west of Maple Street	North	Any Time
Sixth Street	Oak Street	2nd Alley East of Oak Street	North	Any Time
Smith Avenue	Walnut Street	Henderson Street	South	Any Time
Southdowns	High Street	100' W. of High Street	North	Any Time
Southdowns	High Street	185' W. of High Street	South	Any Time
Southdowns Drive	Woodlawn Avenue	Jordan Avenue	North/South	Any Time
Southern Drive	Walnut Street	450' east of Walnut Street	South	Any Time
State Court Street	Highland Avenue	Jordan Avenue	North/South	Any Time
Summit Street	Twelfth Street	Thirteenth Street	West	Any Time
Sunflower Drive	Countryside Lane	End of Cul-de-Sac	West	Any Time
Swain Avenue	Atwater Avenue	Third Street	East	Any Time
Swain Avenue	Second Street	State Court Street	West	Any Time
Tenth Street	College Avenue	Jefferson Street	North/South	Any Time
Tenth Street	Fairview Street	Curve Street	South	Any Time
Tenth Street	Morton Street	College Avenue	North/South	Any Time
Third Street	50' west of Ballantine Road	140' west of Jordan Avenue	South	Any Time

Third Street	105' west of Woodlawn Avenue	95' west of Ballantine Road	South	Any Time
Third Street	120' west of Jordan Avenue	College Mall Road	South	Any Time
Third Street	235' east of Fess Avenue	155' west of Woodlawn Avenue	South	Any Time
Third Street	Adams Street	State Road 37	North/South	Any Time
Third Street	Buckner Street	Euclid Avenue	South	Any Time
Third Street	Euclid Avenue	Maple Street	North	Any Time
Third Street	Maple Street	Jackson Street	South	Any Time
Third Street	Pull-off Spaces East of Fess Avenue		North	Any Time
Third Street	Pull-off Spaces West of Ballantine Road		North	Any Time
Third Street	Pull-off Spaces West of Faculty Avenue		North	Any Time
Third Street	Rogers Street	Walnut Street	North	Any Time
Third Street	Rogers Street	185' east of Fess Avenue	South	Any Time
Third Street	Walker Street	Buckner Street	North	Any Time
Third Street	Washington Street	College Mall Road	North	Any Time
Thirteenth Street	Fess Avenue	60' West of Fess Avenue	North	Any Time

Thirteenth Street	Fess Avenue	171' East of Fess Avenue	North	Any Time
Thirteenth Street	Fess Avenue	Fee Lane	South	Any Time
Thirteenth Street	Forrest Avenue	Fee Lane	North	Any Time
Thirteenth Street	Illinois Street	Summit Street	North	Any Time
Thirteenth Street	Walnut Street	Indiana Avenue	South	Any Time
Thornton Drive	Henderson Street	Troy Court	North/South	Any Time
Twelfth Street	Fairview Street	Jackson Street	North/South	Any Time
Twelfth Street	Indiana Avenue	Woodlawn Avenue	North	Any Time
Twentieth Street	375' West of Dunn Street	Dunn Street	North	Any Time
Union Street	100' S. of Tenth	Tenth Street	West	Any Time
Union Street	432' North of Seventh	452' North of Seventh	West	Any Time
Union Street	Atwater Avenue	Seventh Street	West	Any Time
Union Street	Atwater Avenue	Third Street	East	Any Time
Union Street	Fourth Street	Tenth Street	East	Any Time
Union Street	Seventh Street	175' North of Seventh Street	West	Any Time
Union Street	Third Street	The 1st Alley to the North of Third Street	East	Any Time
University Avenue	Lincoln Street	Woodlawn Avenue	South	Any Time

University Street	East Side Drive	Mitchell Street	North	Any Time
Varsity Lane	Twentieth Street	Dunn Street	East/South	Any Time
Vernon Avenue	Dunn Street	Fritz Drive	South	Any Time
Walker Street	310' N. of First Street	260' N. of First Street	East	Any Time
Walnut Grove	Eleventh Street	Seventeenth Street	East/West	Any Time
Walnut Grove	Tenth Street	Seventeenth Street	East/West	Any Time
Walnut Street	80' North of Sixteenth Street	SR 45/SR 46 Bypass	West	Any Time
Walnut Street	110' North of Fifteenth Street	Sixteenth Street	West	Any Time
Walnut Street	Bypass 45-46	Blue Ridge Entrance	East/West	Any Time
Walnut Street	Country Club Drive	1st Alley N. of 2nd	West	Any Time
Walnut Street	Miller Drive	120' N. of 2nd Street	East	Any Time
Walnut Street	Seventh Street	SR 45 - SR 46	East	Any Time
Walnut Street	Smith Avenue	Fourth Street	East	Any Time
Walnut Street	Walnut Pike	South Drive	East	Any Time
Washington Street	Dixie Street	First Street	West	Any Time
Washington Street	First Street	Fourth Street	East	Any Time
Washington Street	Grimes Lane	Dixie Street	East	Any Time
Washington Street	Grimes Lane	Fourth Street	East	Any Time

Washington Street	Nineteenth Street	Twentieth Street	East	Any Time
Washington Street	Nineteenth Street	100' north of 20 th Street	East	Any Time
Washington Street	Seventeenth Street	Nineteenth Street	East/West	Any Time
Washington Street	Seventh Street	Cottage Grove Ave	East	Any Time
Washington Street	Third Street	1st Alley N. of 3rd	West	Any Time
Watson Street	Bryan Street	1st Alley W. of Bryan Street	North/South	Any Time
Weatherstone Lane	Woodlawn Avenue	800' E. of Woodlawn Avenue	South	Any Time
Willis Drive	Westfield Drive	80' North of Westfield Drive	East	Any Time
Wilson Street	Park Avenue	Woodlawn Avenue	North/South	Any Time
Wilson Street	Rogers Street	Madison Street	North/South	Any Time
Winding Way	Kinser Pike	60' W. of Kinser Pike	South	Any Time
Woodburn Avenue	Fourteenth Street	Sixteenth Street	West	Any Time
Woodburn Drive	Brookdale Drive	Riley Drive	North/South	Any Time
Woodlawn Avenue	1st Alley S. of Atwater	Third Street	East/West	Any Time
Woodlawn Avenue	Fourteenth Street	Seventeenth Street	West	Any Time
Woodlawn Avenue	Grimes Lane	Maxwell Lane	West	Any Time
Woodlawn Avenue	Hillside Drive	Maxwell Lane	East	Any Time
Woodlawn Avenue	Seventh Street	Twelfth Street	East/West	Any Time

Woodlawn Avenue	Thirteenth	Seventeenth	East	Any Time
Woodlawn Avenue	Thirteenth Street	Fourteenth Street	West	Any Time
Woodlawn Avenue	Twelfth Street	Thirteenth Street	East/West	Any Time
Woodlawn Avenue	University Street	Third Street	East	Any Time
Wylie Farm Road	Bayberry Drive	70' East of Bayberry Drive	North	Any Time
Wylie Farm Road	Henderson Street	Bayberry Drive	North	Any Time
Wylie Farm Road	Henderson Street	Sweetbriar Drive	South	Any Time
Wylie Street	1st Alley East of Fairview Street	Rogers Street	South	Any Time
Wylie Street	Curve on Wylie West of Fairview Street	Fairview Street	South	Any Time
Wylie Street	South Dunn Street	South Henderson Street	South	Any Time
Wylie Street	Walnut Street	Washington Street	South	Any Time

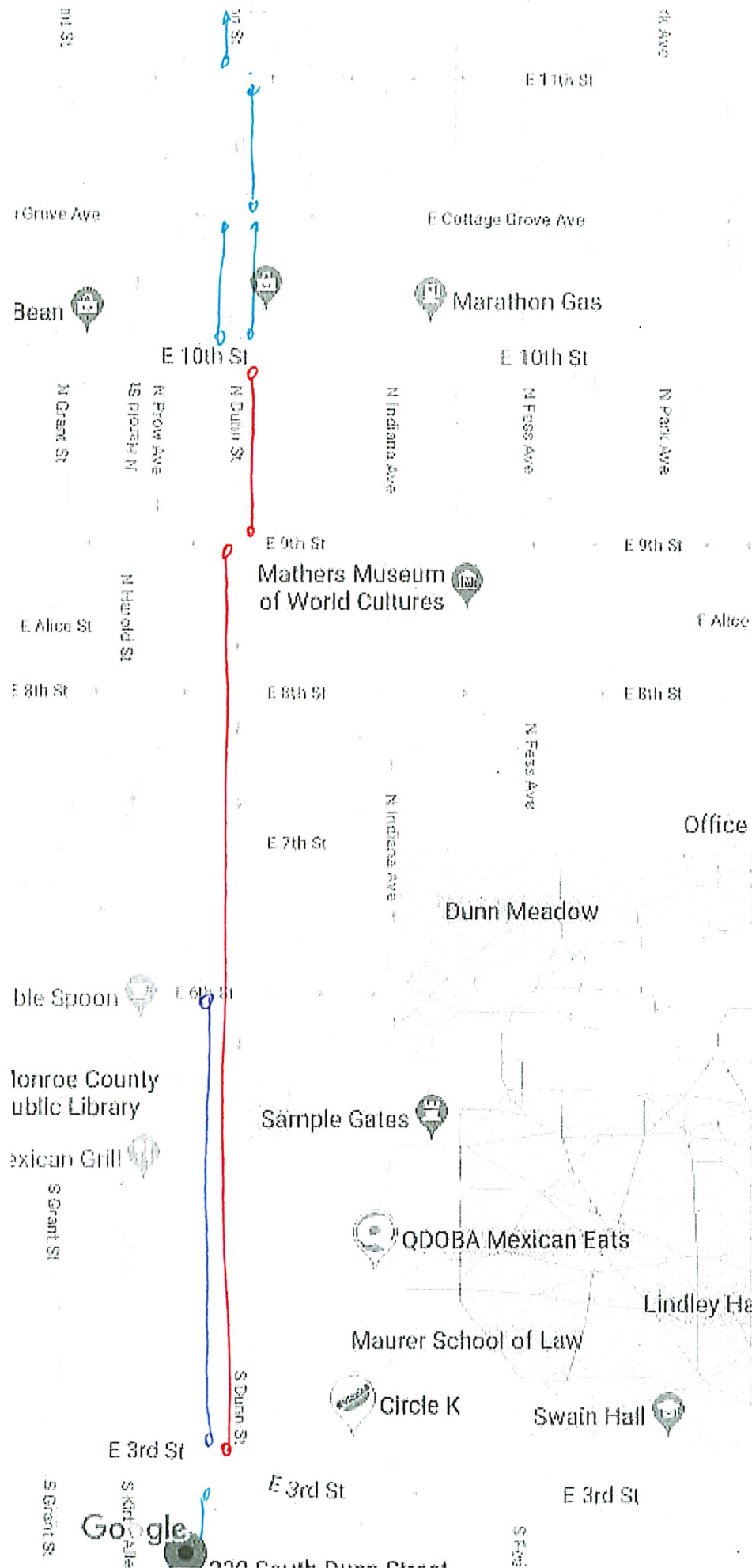
(Ord. 08-19 §§ 7, 8, 2008; Ord. 07-11 § 2, 2007; Ord. 07-07 §§ 5, 6, 2007; Ord. 06-06 §§ 10, 11, 2006; Ord. 04-38 §§ 7, 8, 2004; Ord. 04-14 §§ 1, 2, 2004; Ord. 04-11 § 7, 2004; Ord. 03-28 §§ 7, 8, 2003; Ord. 03-12 §§ 6, 7, 2003; Ord. 02-34 §§ 2, 3; Ord. 02-04 §§ 12, 13; Ord. 01-35 §§ 7, 8, 2001; Ord. 01-09 §§ 14, 15, 16, 2001; Ord. 00-38 § 8, 2000; Ord. 00-15 § 9, 2000; Ord. 99-11 § 6, 1999; Ord. 98-51 §§ 3, 4, 1998; Ord. 98-18 § 10, 1998; Ord. 98-06 § 5, 1998; Ord. 97-51 § 8, 1997; Ord. 97-17 § 5, 1997; Ord. 96-49 §§ 5, 6, 1996; Ord. 96-29 §§ 6, 7, 1996; Ord. 96-11 §§ 1, 2, 1996; Ord. 95-19 §§ 5, 6, 1995; Ord. 94-27 § 3, 1994; Ord. 94-22 § 5, 1994; Ord. 93-47 § 4, 1993; Ord. 93-25 § 1, 1993; Ord. 93-21 § 3, 1993; Ord. 93-09 § 7, 1993; Ord. 92-47 §§ 5—7, 1992; Ord. 92-26 §§ 3, 4, 1992; Ord. 92-7 §§ 5, 6, 1992; Ord. 91-59 §§ 3, 4, 1991; Ord. 91-34 §§ 1, 2, 1991; Ord. 91-26 §§ 4, 5, 1991; Ord. 91-12 §§ 11, 12, 1991; Ord. 91-1 § 1, 1991; Ord. 90-24 § 3, 1990; Ord. 89-46 §§ 8, 9, 1989; Ord. 88-48 § 6, 1988; Ord. 88-21 § 2, 1988; Ord. 88-10 §§ 4, 5, 1988; Ord. 88-3 § 5, 1988; Ord. 87-38 §§ 1, 2, 1987; Ord. 87-14 § 1, 1987; Ord. 87-7 § 5, 1987; Ord. 86-61 § 1, 1986; Ord. 86-50 §§ 4, 5, 1986; Ord. 86-15 § 3 (part), 1986; Ord. 86-3

§§ 2, 3, 1986; Ord. 85-38 § 4, 1985; Ord. 85-27 § 4, 1985; Ord. 85-12 § 2, 1985; Ord. 84-60 § 6(a) 1984; Ord. 84-24 § 1, 1984; Ord. 84-21 §§ 3, 4, 1984; Ord. 84-5 § 2, 1984; Ord. 83-62 § 1, 1983; Ord. 83-61 §§ 4, 5, 1983; Ord. 83-56 §§ 2, 3, 1983; Ord. 83-28 § 4, 1983; Ord. 83-23 § 4, 1983; Ord. 83-21 § 1, 1983; Ord. 83-19 § 1, 1983; Ord. 83-1 §§ 1, 5, 1983; Ord. 82-1 § 1 (part), 1982).

(Ord. No. 09-06, §§ 2, 3, 12-2-2009; Ord. No. 10-15, §§ 8, 9, 11-4-2010; Ord. No. 11-07, § 14, 8-3-2011; Ord. No. 11-19, §§ 2, 3, 11-2-2011; Ord. No. 12-22, § 4, 10-3-2012; Ord. No. 13-10, §§ 10, 11, 7-17-2013; Ord. No. 13-22, § 6, 12-11-2013; Ord. No. 14-22, §§ 7, 8, 10-29-2014; Ord. 15-08, § 3, 4-8-2015; Ord. 15-27, § 9, 12-16-2015; Ord. No. 16-14, §§ 2, 3, 7-12-2016; Ord. No. 17-22, § 7, 5-17-2017; Ord. No. 18-26, §§ 7, 8, 12-19-2018)

Ordinance 19-13
Proposed Changes

- Red: Remove from Schedule M: No Parking
- Purple: Added to Schedule M: No Parking



ORDINANCE 19-14
TO AMEND TITLE 2 (ADMINISTRATION AND PERSONNEL) OF THE
BLOOMINGTON MUNICIPAL CODE
Re: Amending Chapter 2.12.110 (Parking Commission) to Loosen Requirements for
Appointments to the Parking Commission

WHEREAS, the City established a Parking Commission in 2016; and

WHEREAS, some of the seats on the Commission have proven difficult to fill or keep filled;

NOW, THEREFORE, BE IT HEREBY ORDAINED BY THE COMMON COUNCIL OF THE CITY OF BLOOMINGTON, MONROE COUNTY, INDIANA, THAT:

SECTION 1. Section 2.12.110, entitled "Parking Commission" shall be amended as follows:

(c) Qualifications of Voting Membership.

- (1) One member appointed by the mayor and one member appointed by the common council shall be a merchant owning and operating a business located at an address within the City limits;
- (2) One member appointed by the mayor shall be a board member or an employee of a non-profit organization which operates at property that is owned or leased by the non-profit organization within the City limits;
- (3) Four members, two appointed by the mayor and two appointed by the council, shall be residents living within the city limits.;
- (4) One member appointed by the common council shall be from among its membership; and
- (5) One member appointed by the mayor shall be from within the transportation and traffic services division of the planning and transportation department.

SECTION 2. If any section, sentence or provision of this ordinance, or the application thereof to any person or circumstances shall be declared invalid, such invalidity shall not affect any of the other sections, sentences, provisions, or applications of this ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared to be severable.

PASSED by the Common Council of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2019.

DAVE ROLLO, President
Bloomington Common Council

ATTEST:

NICOLE H. BOLDEN
Clerk, City of Bloomington

PRESENTED by me to the Mayor of the City of Bloomington, Monroe County, Indiana, upon this _____ day of _____, 2019.

SIGNED and APPROVED by me upon this _____ day of _____, 2019.

JOHN HAMILTON, Mayor
City of Bloomington

SYNOPSIS

This ordinance is sponsored by CM Volan and amends Title 2 of the Bloomington Municipal Code (Administration and Personnel) to loosen requirements for certain seats on the Parking Commission which have proven difficult to fill or keep filled.

DRAFT

For Ordinance 19-14 Memo, please see
Ordinance 19-13 Memo

2.12.110 - Parking commission.

- (a) *Purpose.* It shall be the primary purpose of the parking commission (commission), in coordination with decision-makers and other entities as is necessary or prudent:
- (1) To develop, implement, maintain, and promote a comprehensive policy on parking that takes into account the entirety of, and furthers the objectives of, the city's comprehensive plan; and
 - (2) To coordinate parking activities, to carry on educational activities in parking matters, to supervise the preparation and publication of parking reports, to receive comments and concerns having to do with parking matters, and to recommend to the common council and to appropriate city officials ways and means for achieving the city's comprehensive plan objectives through the administration of parking policies and the enforcement of parking regulations.
- (b) *Composition—Appointments.* The parking commission shall be composed of nine voting members. These voting members shall be composed of five members appointed by the mayor and four members appointed by the common council.
- (c) *Qualifications of Voting Membership.*
- (1) One member appointed by the mayor and one member appointed by the common council shall be a merchant owning and operating a business located at an address within ~~Schedule U—On-Street Metered Parking~~ the City limits;
 - (2) One member appointed by the mayor shall be a board member or an employee of a non-profit organization which operates at property that is owned or leased by the non-profit organization ~~and located within Schedule U—On-Street Metered Parking~~ within the City limits;
 - (3) Four members, two appointed by the mayor and two appointed by the council, shall be residents living within the city limits. ~~At least one of these four shall be a resident living at an address within Schedule U—On-Street Metered Parking, and at least one other of these four shall be a resident living at an address within a residential neighborhood permit parking zone as described in Section 15.37.020;~~
 - (4) One member appointed by the common council shall be from among its membership; and
 - (5) One member appointed by the mayor shall be from within the transportation and traffic services division of the planning and transportation department.
- (d) *Terms.* The initial terms of three mayoral and two council citizen appointments shall expire on January 31, 2018. The terms of the remaining initial citizen appointments shall expire on January 31, 2019. Thereafter, all terms of citizen appointments shall be for two years and expire on January 31. The terms for the one mayoral appointment made from within the planning and transportation department and the one council appointment made from within the members of the council shall be for one year and expire on January 31.
- (e) *Powers and Duties.* The commission shall meet at least one time each month, unless it votes to cancel the meeting. Its powers and duties shall include, but are not limited to:
- (1) Accessing all data regarding the city's parking inventory, including usage, capital and operating costs, so long as the data is released in a manner consistent with exemptions from disclosure of public records set forth in Indiana Code § 5-14-3-4;
 - (2) Reviewing the performance of all meters, lots, garages, and neighborhood zones in the city's parking inventory, and reviewing the performance of all divisions of city departments devoted specifically to parking management;
 - (3) Making recommendations on parking policy, including but not limited to: pricing, hours of operation, addition or removal of parking spaces, and changes when necessary to city code, enforcement procedures, or any other aspect of parking management policy;
 - (4) Submitting an annual report of its activities and programs to the mayor and council by October of each year;

- (5) Adopting rules and regulations for the conduct of its business; and
 - (6) Applying for appropriations through the mayor, or researching and applying for grants, gifts, or other funds from public or private agencies, for the purpose of carrying out any of the provisions of this section.
- (f) *Staff.* The commission shall be staffed by the transportation and traffic services division of the planning and transportation department.

(Ord. No. 16-22, § 1, 11-2-2016)

In the Council Chambers of the Showers City Hall, Bloomington, Indiana on Wednesday, June 12, 2019 at 6:35pm, Council President Dave Rollo presided over a Regular Session of the Common Council.

COMMON COUNCIL
REGULAR SESSION
June 12, 2019

Members present: Ruff, Piedmont-Smith, Rollo, Volan (left at 9:59pm), Sims, Sandberg

ROLL CALL [6:35pm]

Members absent: Chopra, Granger, Sturbaum

Council President Dave Rollo summarized the agenda.

AGENDA SUMMATION
[6:35pm]

Councilmember Isabel Piedmont-Smith moved and it was seconded to approve minutes from the meetings of April 17, 2019 as corrected, May 15, 2019 as corrected (as a continuation of a meeting originally convened on May 1, 2019), May 15, 2019, May 29, 2019, and June 5, 2019. The motion was approved by voice vote.

APPROVAL OF MINUTES
[6:39 pm]
April 17, 2019
May 15, 2019
May 15, 2019
May 29, 2019
June 5 2019

Councilmember Susan Sandberg commented on serving on the review committee for the Fourth Street Garage project and gave an update on the design process.

REPORTS
• COUNCIL MEMBERS
[6:40pm]

Councilmember Jim Sims said he looked forward to hearing from students in attendance at the meeting.

Councilmember Steve Volan expressed his gratitude for various student-lead projects and proposals affecting city government.

Councilmember Andy Ruff spoke about opposition to Interstate Highway 69 and its impact on the community and the environment.

Rollo read a disclosure of conflict of interest statement on behalf of Dorothy Granger related to Resolution 19-09 and her work for the Shalom Community Center.

Volan moved and it was seconded to accept a Disclosure of Conflict of Interest filed by Councilmember Dorothy Granger. The motion was approved by voice vote.

Andrew Findley, Bloomington Arts Commission Chair, gave an annual report on the activities of the Commission. He spoke about the Commission's 2017-2019 Strategic Plan and the city's Public Art Master Plan. He explained that over the past year, the Commission oversaw the Percentage for the Arts program, had commissioned the Bloomington Bicentennial community song, had partnered with Ivy Tech to promote its annual Community Arts Awards, and had continued the Grants for Arts program.

- The MAYOR AND CITY OFFICES [6:50 pm]

Bloomington Arts
Commission and
Bloomington
Entertainment and Arts
District Annual Report

Sean Starowitz, Assistant Director for the Arts, Economic and Sustainable Development Department, spoke about the Bloomington Entertainment and Arts District (BEAD) rebranding. He explained that the goal of BEAD was to combine the business and creative sector to advance commerce, build the community, and spur sustainable economic development. He discussed the 2019-2021 Strategic Plan for the BEAD, as well as other activities within the district.

Volan commended Mayor Hamilton and the administration for proposing the increase to the Grants for Arts budget that the Council approved.

Council Questions:

Rollo asked if the space needs assessment included display space for artists.

REPORTS (cont'd)

Starowitz answered that it would be included, but explained that the assessment looked at how to utilize spaces and resources efficiently to further leverage arts and cultural assets.

Devta Kidd, Director of Innovation, introduced herself and provided information about her experience and educational background. She spoke about national trends in cities related to employing innovation directors, as well as the role of innovation director in Bloomington. She discussed recommendations implemented in Bloomington from 2016 to 2018. She reviewed project goals and priorities for 2019 and beyond.

Innovation Director Report

Sims asked Kidd if the city's involvement with the MetroLab Network would include relationships with Indiana University (IU) or Ivy Tech to study community issues.

Council Questions:

Kidd said she expected it would include working with IU but was not sure whether Ivy Tech would be included.

Scott Robinson, Assistant Director for Planning and Transportation, introduced Wells and Associates, a consultant the city hired to assist with transportation demand management.

Introduction of Transportation Demand Management Consultant

Justin Schor, Project Manager from Wells and Associates, introduced himself and his company. He explained that his company focused on both supply and demand solutions for transportation. Schor described his company's approach to evaluating community needs and creating a transportation demand management program.

Rollo asked which stakeholders the city would consult during the process.

Council questions:

Robinson said stakeholders had not yet been identified. He expected the city would consult with IU and major employers, along with other groups that would be determined as the process moved forward.

Rollo asked if people facing mobility challenges due to low income would be included in the stakeholder group.

Schor responded that the survey would hopefully reach as many people as possible. Schor said the stakeholder group would be inclusive.

There were no council committee reports.

COUNCIL COMMITTEES

Karen McQueen, Kira DeFelice, Emma Wild, Maddy Waters, Ruth Bartlett, Stefan Bartlett, Nicholas Waters, Marcella Waltsek-Medina, Giuliana Samarotto, and Lilly Lauderma, students from The Project School, gave a group presentation to the Council. The students asked the Council to consider planning and budgeting for a new playground at Waldron, Hill and Buskirk Park that would adhere to Universal Design principles, making the playground more accessible and equitable for community members with disabilities.

PUBLIC [7:58pm]

Volan thanked the class for preparing the presentation. Volan asked if the class had considered whether people living close to the playground would be bothered if the playground included musical instruments.

Council Questions:

Maddy Waters said that the playground would not have any instruments that were louder than the sound of a yelling child, so he did not think anyone would be bothered.

Stefan Bartlett added that the playground might include a cocoon feature, where kids could relax.

Volan noted that all city parks had some accessible features and Switchyard Park would be fully accessible. He added that the Parks and Recreation Department welcomed discussions on adding more accessible features at Waldron, Hill and Buskirk Park.

REPORTS (cont'd)

Sandberg thanked the class for the presentation and the hard work. On the issue. She suggested the class take on the issue of scooter legislation next.

Ruff thanked the students and complimented their hardwork.

Piedmont-Smith asked students to feel free to remind councilmembers about the upcoming park project.

Sims thanked the students for their presentation and asked the councilmembers to rise and repeat the slogan, "Unaccessible is Unacceptable!"

Darrell Boggess spoke in support of young advocacy.

Additional public comment:

Greg Alexander spoke about pedestrian and bicycle infrastructure.

Rollo reconstituted the Council Rules Committee and appointed Piedmont-Smith, Volan, Sims, and City Clerk Nicole Bolden to the Committee.

APPOINTMENTS TO BOARDS
AND COMMISSIONS

Volan moved and it was seconded that Ordinance 19-11 be read by title and synopsis only. The motion was approved by voice vote. Chief Deputy Clerk Stephen Lucas read the legislation by title and synopsis, giving the committee do-pass recommendation of Ayes: 2, Nays: 0, Abstain: 7.

LEGISLATION FOR SECOND
READING AND RESOLUTIONS
[8:15pm]

Volan moved and it was seconded that Ordinance 19-11 be adopted.

Barbara McKinney, Human Rights Director/Attorney, presented the ordinance to the Council. She stated the ordinance was suggested by Mayor Hamilton and Councilmember Granger, and had received unanimous support from the city's Human Rights Commission. She explained that the policy was being clarified to make it clear that the city's prohibition on discrimination and harassment also protected independent contractors doing work for the city.

Ordinance 19-11 To Amend Title 2 of the Bloomington Municipal Code Entitled "Administration and Personnel" – Re: Amending Chapter 2.21 Entitled "Department of Law" to Prohibit Discrimination and Harassment of Any Person Doing Sanctioned Work for the City

Piedmont-Smith moved and it was seconded to adopt Amendment 01 to Ordinance 19-11.

Amendment 01 to Ordinance 19-11

Amendment 01 to Ordinance 19-11 Synopsis: This amendment is sponsored by Councilmember Granger and Piedmont-Smith and replaces the words "she or he" with the words "she, he, or they" in Section 1 of the ordinance and in the synopsis to provide for a non-binary pronoun.

Piedmont-Smith presented the amendment to the Council.

Jada Bee spoke in support of the amendment and suggested future city documents include non-binary language.

Public comment:

Rollo thought it was a good idea to review other city documents to include non-binary language.

Council comment:

The motion to adopt Amendment 1 to Ordinance 19-11 received a roll call vote of Ayes: 6, Nays: 0, Abstain: 0.

Vote on Amendment 1 to Ordinance 19-11 (cont'd)

Volan asked McKinney if she could summarize her responses to questions posed by councilmembers the previous week.

Ordinance 19-11 (cont'd)
Council questions:

McKinney restated a question about creating a new independent commission to deal with certain claims of harassment. She said there were already ways to address such claims, either through existing commissions at the state and local level or through other internal processes.

Piedmont-Smith asked McKinney to address what would happen if the mayor was accused of harassment.

McKinney said, to the extent possible, it would be investigated and handled internally, but the city could also hire outside counsel to assist. She explained there were limitations on what actions could be taken against a mayor.

Piedmont-Smith asked McKinney to clarify what processes were available to independent contractors.

McKinney explained how the city would handle such complaints of harassment. She reiterated that a new independent commission would not be useful because the city had enough resources internally and externally to handle such situations.

Piedmont-Smith asked what the city would do to publicize the passage of the ordinance.

McKinney said that city employees had access to the information in their employee manual. She added that all future contracts would include a provision about the harassment policy.

Sims asked if current contractors would be notified of the new policy change.

McKinney said she was unsure how the city would contact current contractors, but said many contractors often renewed their contracts with the city, so it was possible they could be notified when their contracts were renewed.

Rollo asked for additional information about how the city would handle a complaint of harassment against the mayor and whether the city's Human Rights Commission (HRC) would investigate.

McKinney said the HRC could not investigate in that instance. She explained when the city would need to use outside counsel do to a conflict of interest.

Ryan Shadday, Chair of the HRC, spoke in favor of the ordinance.

Public comment:

Brandon Drake spoke in favor of the ordinance.

Kate Blake spoke in favor of the ordinance.

Jada Bee stated she was worried about loopholes in the policy because she wanted to ensure that everyone was protected.

Kenneth Shafer spoke about his experience as an independent contractor in both government and corporate areas. He gave recommendations for handling cases of harassment when dealing with independent contractors.

The Council discussed whether there were any time limitations on public comment.

Volan asked whether the ordinance should refer to independent contractors or contracted personnel.

McKinney said that the term independent contractor was used intentionally and was the preferred term because it was a well-defined legal term.

Ordinance 19-11 (cont'd)
Additional council questions:

Sandberg said she supported the ordinance and thanked McKinney for her work.

Council comment:

Sims said he supported the legislation, and thought the language of the ordinance was broad enough to include everyone, thought he noted it could always be amended in the future.

Piedmont-Smith said she supported the legislation.

Rollo said he appreciated the amendment and would support the ordinance, though he said there might still be more thought needed to ensure a fair process if the harassor was the mayor.

Volan said he was satisfied by McKinney's answer about why an independent commission was not needed.

The motion to adopt Ordinance 19-11 as amended received a roll call vote of Ayes: 6, Nays: 0, Abstain: 0.

Vote on Ordinance 19-11 as amended (8:54pm)

Volan moved and it was seconded that Resolution 19-11 be introduced and read by title and synopsis only. The motion was approved by voice vote. Lucas read the legislation by title and synopsis.

Resolution 19-11: The Bloomington Youth for Environmental Sustainability (Y.E.S.) Society's Resolution for Environmental Responsibility and Improvement

Volan moved and it was seconded that Resolution 19-11 be adopted.

Rollo read the resolution.

Maddie Clemmer and Jerrett Alexander explained that The Youth for Environmental Sustainability (Y.E.S.) proposed Resolution 19-11. Alexander described the history of the group's development. Clemmer said that various city commissions had written letters of support. Clemmer and Alexander reviewed the various recommendations in the resolution to make Bloomington more environmentally sustainable.

Emma Hickman, Arpan Boaz, and Emma Sire spoke about the importance of addressing climate change and in support of the resolution.

Clemmer and Alexander thanked those that had worked on and supported the resolution.

Council questions:

Volan asked if the students from Bloomington High School North and South represented their schools or were simply part of Y.E.S.

Clemmer answered the students did not represent the schools but were present to support the resolution.

Volan asked if any thought went into bringing the schools closer into the city in order to make bicycle routes more accessible.

Clemmer stated she did not believe moving the schools would be possible. She said she was unable to find a safe bicycle route to some of the schools.

Sims asked when the proposed water refilling stations would be shut off. Resolution 19-11 (cont'd)

Clemmer explained that most outdoor filling stations would shut off when the water fountains shut off. She said the water filling station at Switchyard Park would remain on at all times of the year.

Alexander added that stations would shut off when the fountains shut off because it would be too costly to install new infrastructure.

Piedmont-Smith asked what projects Y.E.S. would be working on in the future.

Alexander explained that Y.E.S. hoped to present the resolution to the Monroe County Council and to eventually approach other cities in Indiana to adopt the resolution.

Clemmer said the group hoped to spread awareness statewide and to include more lofty goals in the resolution over time.

Rollo asked what lessons were learning during the process of writing the resolution.

Clemmer and Alexander spoke about their experiences and plans for the future.

Darryl Boggess, Nidhi Krishnan, Joelle Jackson, Sara Rainier, Angie Shelton, Madeline Hirschland, Wesley Cammenga, Cathleen Boggess, Bella Clemmer, and Darby FitzSimmons spoke in support of the resolution. Public comment:

Alex Crowley, Director of Economic and Sustainable Development, voiced support for the resolution on behalf of the city administration.

Ruff complimented the students for their work. He said climate change was a unique issue that meant policy makers could not proceed with business as usual. He urged young people to keep pressuring politicians to make changes. Council comment:

Volan pointed out that the resolution was not city code, but should be kept in mind when the city thought about the built environment. He said that the resolution should be used as a guide for creating policies concerning transportation and the environment.

Piedmont-Smith thanked the presenters for creating and proposing the resolution. She said some of the provisions of the resolution were easier to accomplish than others, but said hard goals needed to be set to adequately address climate change. She encouraged students to hold public officials to the goals that were being set. She read a quote by activist Greta Thunberg.

Rollo stated he was impressed with all those who had helped create the resolution. He said the environment was changing and everyone needed to reexamine their one habits to help address the issue. He saw the resolution as a directive to the city when making future policies. He said he supported Resolution 19-11.

The motion to adopt Resolution 19-11 received a roll call vote of Ayes: 6, Nays: 0, Abstain: 0.

Vote on Resolution 19-11
[9:57pm]

Volan moved and it was seconded that Resolution 19-12 be introduced and read by title and synopsis only. The motion was approved by voice vote. Lucas read the legislation by title and synopsis.

Volan moved and it was seconded that Resolution 19-12 be adopted.

Doris Sims, the Director of Housing and Neighborhood Development Department, presented the resolution to the Council, and asked that the Council approve an intra-category transfer of funds within the housing trust fund. She said the funds would be used for an affordable housing project.

There was no public comment.

Sandberg said she supported the resolution and the project it would help fund. .

Piedmont-Smith thanked Sims and other city staff for their work. She said the approval process for such transfers was important.

The motion to adopt Resolution 19-12 received a roll call vote of Ayes: 5, Nays: 0, Abstain: 0.

Sandberg moved and it was seconded that Resolution 19-09 be introduced and read by title and synopsis only. The motion was approved by voice vote. Lucas read the legislation by title and synopsis.

Sandberg moved and it was seconded that Resolution 19-09 be adopted.

Piedmont-Smith summarized the process followed by the Council's Jack Hopkins Social Service Funding Committee and described the proposed resolution.

Sandberg voiced her support for the resolution and emphasized the importance of the program to providing support to different non-profit organizations within the community.

Rollo asked how much funding had been requested.

Council Administrator/Attorney Dan Sherma said the total request for funding amounted to \$617,394.

There was no public comment.

The motion to adopt Resolution 19-09 received a roll call vote of Ayes: 5, Nays: 0, Abstain: 0.

Resolution 19-12: To Review and Approve the Intra-Category Transfer and Expenditure of \$100,000 or More within a Covered Fund under Ordinance 18-10 (Additional Fiscal Oversight by the Common Council) – Re: A \$250,000 Transfer from Line 399 (Other Services and Charges) to Line 396 (Grants) within the Housing Trust Fund (#905) and Expenditure of Transferred Funds

Public comment:

Council comment:

Vote on Resolution 19-12
(10:02pm)

Resolution 19-09: Authorizing the Allocation of the Jack Hopkins Social Service Program Funds for the Year 2019 and Related Matters

Council questions:

Public comment:

Vote on Resolution 19-09
[10:10pm]

Sandberg moved and it was seconded that Resolution 19-10 be introduced and read by title and synopsis only. The motion was approved by voice vote. Lucas read the legislation by title and synopsis.

Resolution 19-10: Waiving Current Payments in Lieu of Taxes (PILOT) by the Bloomington Housing Authority to the City

Sandberg moved and it was seconded that Resolution 19-12 be adopted.

Doris Sims, the Director of Housing and Neighborhood Development Department, stated the resolution was an annual action taken by the city to forgive the payment in lieu of taxes (PILOT) by the Bloomington Housing Authority (BHA), which amounted to \$35,365 for the year.

Amber Scoby, Executive Director of the BHA, said BHA had to calculate and request forgiveness from the PILOT each year. She explained that by forgiving the PILOT, the city enabled BHA to use that money to serve residents.

There was no initial council comment.

Council comment:

There was no public comment.

Public comment:

Sandberg said the resolution had been thoroughly discussed at the previous meeting and expressed her support for Resolution 19-10.

Council comment:

The motion to adopt Resolution 19-10 received a roll call vote of Ayes: 5, Nays: 0, Abstain: 0.

Vote on Resolution 19-10
[10:14pm]

Sandberg moved and it was seconded that Resolution 19-08 be introduced and read by title and synopsis only. The motion was approved by voice vote. Lucas read the legislation by title and synopsis.

Resolution 19-08: To Approve a Revised Cooperation Agreement Between the City of Bloomington ("City") and the Bloomington Housing Authority ("BHA") for Provision and Operation of Low Income Housing Units and Associated Payment In Lieu of Taxes by BHA in Exchange for City Services

Sandberg moved and it was seconded that Resolution 19-08 be adopted.

Amber Scoby, Executive Director of BHA, explained the need to update the cooperation agreement between the city and BHA.

Amanda Stephens, an attorney and a representative of BHA, explained the changes made to the agreement.

There was no public comment.

Public Comment:

Sims said he supported the resolution and thanked those who had worked on it. He said public housing was one part of the housing stock that the community needed.

Council Comment:

Piedmont-Smith thanked Scoby and Stephens for making the agreement as clear as possible. She said the agreement would help BHA continue to provide housing for those in need in the community.

Rollo reminded the Council and the public that the resolution had been previously reviewed.

The motion to adopt Resolution 19-08 received a roll call vote of Ayes: 5, Nays: 0, Abstain: 0.

Vote on Resolution 19-08
[10:22pm]

There was no additional public comment.

ADDITIONAL PUBLIC COMMENT

There was no legislation for first reading.

LEGISLATION FOR FIRST
READING

There wer no changes to the council schedule.

COUNCIL SCHEDULE

The meeting was adjourned at 10:24 pm.

ADJOURNMENT

APPROVED by the Common Council of the City of Bloomington, Monroe County, Indiana upon this
____ day of _____, 2019.

APPROVE:

ATTEST:

Dave Rollo, PRESIDENT
Bloomington Common Council

Nicole Bolden, CLERK
City of Bloomington